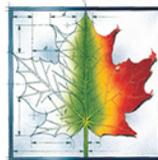
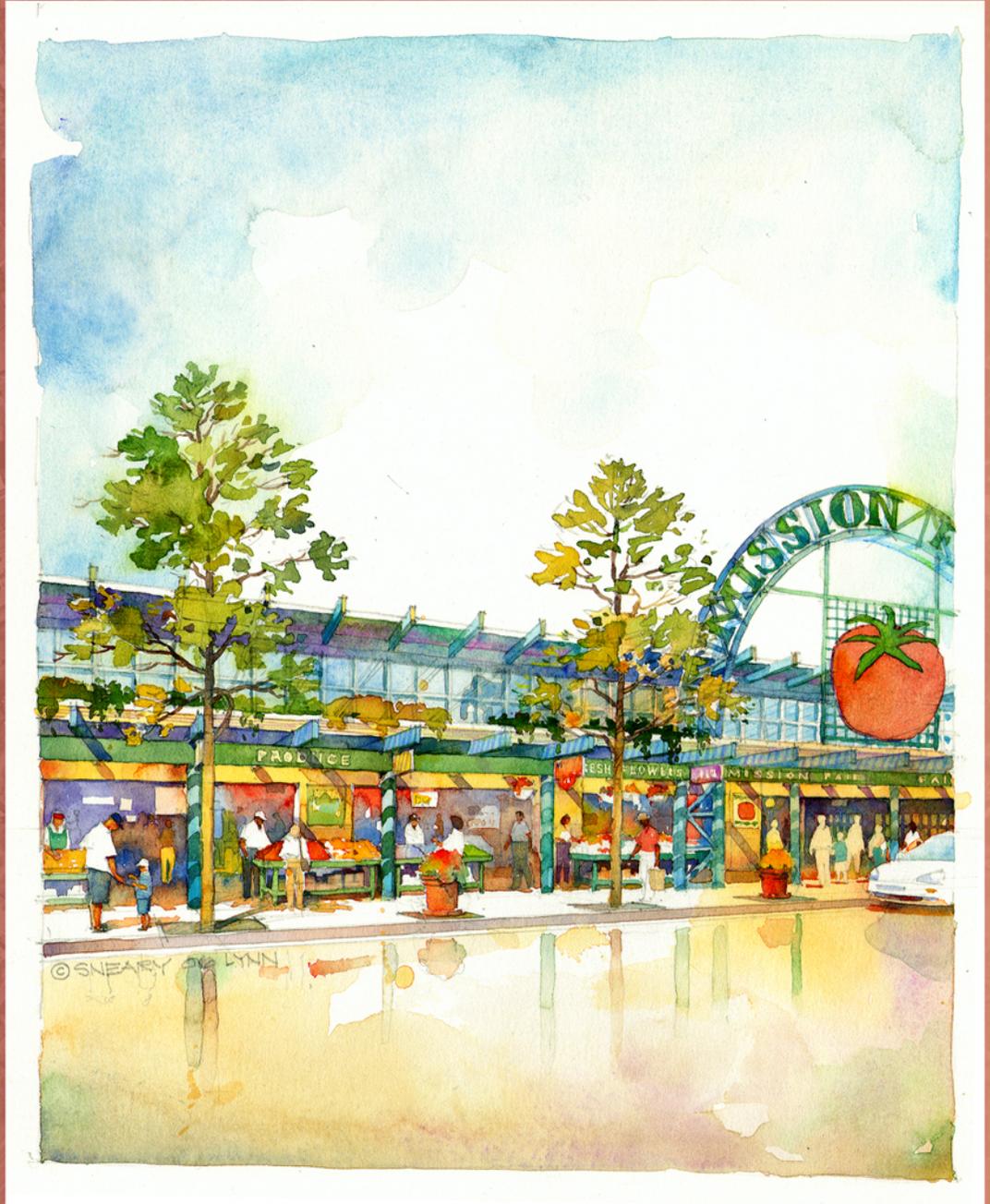


EAST GATEWAY REDEVELOPMENT PLAN



INTEGRATING NATURE
AND ARCHITECTURE

BOWMAN
BOWMAN
NOVICK
INC



Collins Noteis &
Associates, LC

Urban planning, community development and government affairs

The East Gateway Redevelopment Plan represents the unique and strategic alliance of three cities; Mission, Fairway, and Roeland Park, Kansas, to direct the course of redevelopment through a unified planning effort. As such, it addresses a diverse range of issues specific to the tri-city area including the need for more urban housing types; the desire to sustain existing and attract new businesses and residents; the need for increased tax bases; and the desire to attain quality redevelopment while at the same time maintain the character and local flavor that defines this northeast Johnson County community.

Laura McConwell Mayor of Mission
 John St. Clair Mayor of Fairway
 Steve Petrehn Mayor of Roeland Park

East Gateway Committee

Dave Shepard	Committee Chair & Mission City Council
Dale Chaffin	Mission Resident
Tina Fisher	Mission Business Representative
Jack Fox	Fairway Resident
Scott Gregory	Roeland Park City Council
Chris Handzel	Roeland Park Planning Commission
Rex Hoy	Roeland Park Business Representative
Keith Johnston	Fairway Planning Commission
George Lafferty	Fairway Resident
Jim Mitchell	Mission Planning Commission
Beverly O'Donnell	Mission Business Representative
Steve Petrehn	Roeland Park Mayor
Roberta Riley	Mission Resident
Susan Smith	Fairway City Council
Andrew Starr	Mission Resident
Tom Thompson	Mission Resident

Planning Team

Bowman Bowman Novick, Inc.
Collins Noteis & Associates, LC

Lorie Doolittle-Bowman	Principal	BBN
Brent Bowman	Principal	BBN
Paul Novick	Principal	BBN
Vicki Noteis	Principal	CNA
Bob Collins	Principal	CNA
Bill Miller	Architect	BBN
Tony Atchity	Graduate Architect	BBN
Austin Massoth	Graduate Architect	BBN
Tom Swenson	Traffic Engineering Consultant	TransSystems Corp.
Charlie Sievert	Civil Engineering Consultant	Black & Veatch

The East Gateway Redevelopment Plan involved a participatory process that included hundreds of citizens from Mission, Fairway and Roeland Park during the Visual Preference Workshop, Public Meetings, Steering Committee Meetings, and Joint Meetings of the Tri-City's Planning and Zoning Commissions and City Councils.



Executive Summary	1
Density	1
Mixed Use	2
Neighborhood & Small Business Preservation	2
Street Alignments, Traffic Calming, & Parking	2
Martway	3
Gateways	3
Trails and Sidewalks	4
Public Realm Improvements	4
Mission Mall Redevelopment Project	4
Character / Exterior Finish	5
Part One	6
Introduction	7
Background & Planning Process	7
Public Input	7
Visual Preference Survey	8
Key Issues	8
Conclusions	10
Part Two - Recommendations for the Study Area	11
A. Land Use/ Mixed Use/ Density	12
B. Height	13
C. Street Hierarchy	14
D. Trail and Sidewalk System	18
Part Three - Concepts for Individual Project Sites	20
Part 4 – Public Policy and Implementation	28
Zoning Recommendations	29
Amendments to Design Guidelines	29
Support for Redevelopment	29
Recommended Public Improvement Projects	30
Recommended Public Improvement Map	31
Appendices	
A. Summary of Traffic Issues	
B. Traffic Report by TransSystems	
C. Form Based Codes Description	
D. Amended Mission Design Guidelines for the Johnson Drive Corridor	

The East Gateway Redevelopment Plan represents the unique and strategic alliance of three cities; Mission, Fairway and Roeland Park, Kansas, to direct the course of redevelopment through a unified planning effort. As such, it addresses a diverse range of issues specific to the tri-city area including the need for more urban housing types; the desire to sustain existing and attract new local businesses and residents; the need for increased tax bases; and the desire to attain quality redevelopment while at the same time maintain the character and local flavor that defines this northeast Johnson County community.

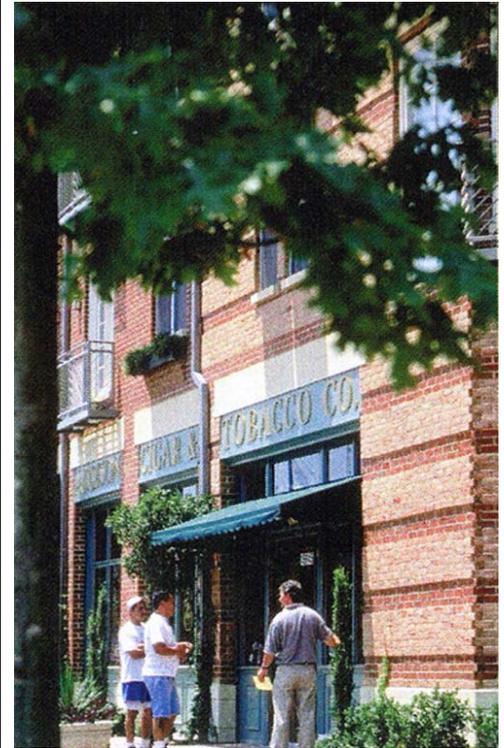
The Plan incorporates elements that will revitalize these first ring suburbs by establishing new zones of higher density mixed-use development while preserving existing single family neighborhoods. The Johnson Drive Corridor will be a thriving district, not only for daily business but a newly defined place for special events. Parks and trails in all three cities will connect with new housing, existing neighborhoods, and the revitalized Mission Mall; christened *The Gateway*.

Public spaces will have landscaping and streetscape amenities to help the area shine as a special identifiable place for both residents and visitors. Mission, Fairway and Roeland Park now have a connected strategy and a complementary vision to enhance their quality of life for the next 20 years.

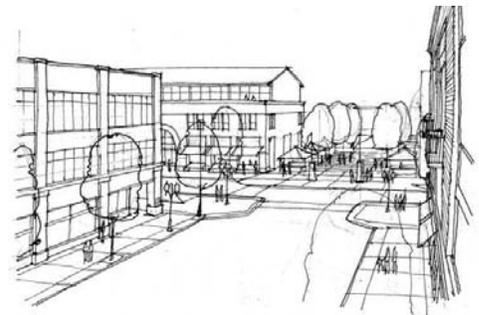
In short, the East Gateway Redevelopment Plan makes a great community even better.

Density

The East Gateway Redevelopment Plan supports higher densities throughout the planning area. Two of the primary advantages are the opportunity for more urban housing options, and more unique and interesting retail stores.



Nearly every site is proposed to be of a higher density with buildings sited at build-to lines.



The table below illustrates the comparison between existing and proposed housing, retail, and commercial space within the East Gateway Redevelopment Area: (Excluding “The Gateway” project at the Mission Mall Site)

EAST GATEWAY	Total of possible and planned redevelopment in the study area	Loss of existing development	Net change
Residential Units	475 Units	0 Units	+ 475 Units
Commercial / Retail Area	355,000 sf	389, 000 sf	- 39,000 sf
Office Area	189,000 sf	78, 000 sf	+ 111,000 sf
Parking Stalls	1,500	Approx. 1000	+ 500
Green Space	4.5 Acres	0 Acres	+ 4.5 Acres



New housing types are incorporated into the plan to complement the higher density and mix of uses, creating a more urban lifestyle environment



These medium to high density residential and mixed use buildings are intended to help retain area residents and attract new residents to the community.



Specific sites in the plan are designed to promote local retail such as the “Mission Fair”, which is depicted above. This component of the plan is envisioned to be a lively market like setting.

Mixed Use

The East Gateway Plan proposes considerably more Mixed Use and Mainstreet land uses than exist today in the study area. This concept responds to the desire for increased densities and a shift to a more urban lifestyle environment. Nearly every site within the area is shown to be of higher density with buildings sited at built-to lines creating a public realm zone along roadways and other strategic areas.

The plan also addresses the need for more parks and open space to accommodate the increased density of residential and commercial uses.

Retention of Local Small Business, Housing Options and Neighborhood Preservation

The Plan incorporates a mix of residential, retail and commercial uses in a manner which preserves the integrity of adjacent single family residential neighborhoods. This was a key objective of the East Gateway Redevelopment Committee. Also an important consideration was the desire for more urban housing options.

The East Gateway Redevelopment Plan divides the area into 17 development / redevelopment sites (see part 3). Sites 1, 2, 3, 4a, 7, 14 and 17 provide the tri-city area with increased urban type housing options, adding nearly 500 living units. In addition, “The Gateway” project will incorporate approximately 400 living units and 150 hotel rooms.

Sites 4b, 5a-d, 8 and 12 incorporate traditional commercial/retail space specifically designed to accommodate small local retail. These sites offer the desired 25 ft. wide storefronts and depths of 70 ft. The concept recommended for Site 5B is intended to attract smaller retail entities in a *market-like* setting. The concept incorporates a building façade treatment with a south-facing covered walkway and operable storefront walls that allow individual shops to open out onto the adjoining sidewalk during temperate weather.

In all of the above redevelopment concepts, the existing rear property lines abutting single family residential properties were retained. This was an important consideration for neighborhood preservation.

Street Alignments, Traffic Calming, and Parking

Several recommendations for roadway realignments and improvements are incorporated in the East Gateway Redevelopment Plan to improve the grid network of primary

and secondary streets for safer, more uniform street intersections and pedestrian crossings.

Two such examples are the extension of Martway through The Gateway project terminating into Johnson Drive, and the reconfiguration of Ash and Roeland Drive north of Johnson Drive.

Traffic calming techniques are recommended for Johnson Drive including the transition from two-lane to one-lane east and west with a center turn lane. This reconfiguration is proposed to occur west of the second entrance to The Gateway from Roe Avenue, and will continue west through the intersection of Johnson Drive/Nall. The current intersection of Johnson Drive/Roe Avenue will not be modified. These recommendations will be further researched by a special task force appointed by Mayor McConwell.

Crosswalk locations include:

- Shawnee Mission Parkway at Roeland Drive, Nall
- Johnson Drive at Roe Avenue, Martway, Roeland Drive, “The Promenade”, Birch and Nall
- Martway at Roeland Drive, “The Promenade”, Birch and the Johnson Drive Mews.

Parking requirements are met in a variety of ways in the East Gateway Redevelopment Plan.

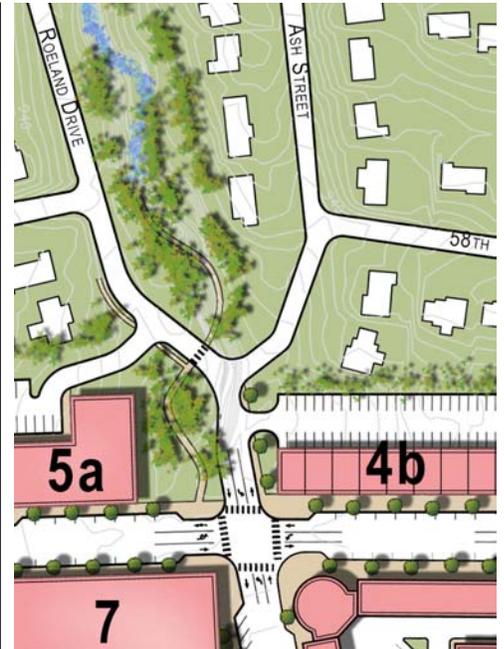
- Surface parking lots behind buildings or in concealed courtyards
- Subsurface and above ground parking structures
- On-street parallel and angled parking where considered feasible by the City’s traffic engineer.

Martway

The Plan sets a vision for Martway as a “parkway”, elevating its importance as a primary vehicular and pedestrian route in the East Gateway District, and improving its appearance. An increased right-of-way will accommodate wider sidewalks (8ft.) with adjacent zones of plantings, turf, and street trees, and on-street parallel parking where feasible. The current traffic configuration of one lane east and west with a center turn lane will be preserved. Other important features include the proposed Martway Park, a future Transit Center, “The Promenade”, and a roundabout with a fountain.

Gateways

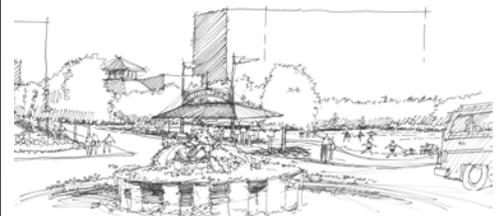
The locations for three major gateways were identified in The Plan. A combination of landscape features, building forms and



Roeland Drive and Ash Street are reconfigured to improve the intersection at Johnson Drive. The additional R.O.W. to the west will allow a more formal gateway to Roeland Park and a trail connection to Cooper Creek Park.

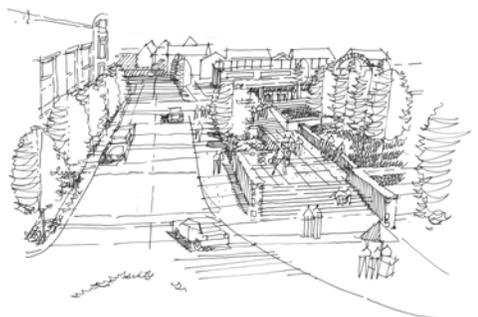


Martway, currently flanked by large surface parking lots on either side (pictured above) is envisioned to become a parkway, with a roundabout and fountain, a new park, a future transit center, streetscape improvements, wider sidewalks, and an extension into “The Gateway” development.





A trail and sidewalk system will have multiple points of connection, and provide a safe and thorough network of paved surfaces to promote pedestrian friendly neighborhoods. The Promenade (depicted below) at the Capitol Federal Bank site will provide Public Plaza space adjacent to the Sidewalk system.



A full complement of public realm improvements will enhance this live, work and play environment.

signage are proposed to denote these public realm improvements.

- *Roeland Drive and Johnson Drive*
- *Johnson Drive and Roe Avenue*
- *Shawnee Mission Parkway and Roeland Drive*

Trails and Sidewalks

The Plan incorporates a safe and thorough network of paved surfaces to facilitate a pedestrian-oriented district. Due to the urban nature of the district, the trail route utilizes some portions of the primary sidewalk system. Trail connections are proposed in several locations to link up with existing and proposed trails of the MetroGreen Plan. In addition, a system of primary and secondary sidewalks is proposed throughout the district, adjacent to roadways and in areas to correct deficiencies in the current sidewalk system.

One of the more significant public improvements recommended in the East Gateway Redevelopment Plan is the acquisition and redevelopment of the existing Capitol Federal Bank site into a pedestrian mall. *The Promenade* concept provides a critical pedestrian link between Johnson Drive and Martway in the center of the district. The concept includes public plazas, landscaping and a fountain.

Public Realm Improvements

A variety of public realm improvements are recommended in the East Gateway Redevelopment Plan. These include:

- Roundabout and fountain on Martway
- *The Promenade*; a pedestrian plaza between Johnson Drive and Martway
- Martway Park; between Roeland Drive and the Mission Bowl.
- A public plaza over a proposed subsurface parking garage at the Mission Bank.
- Large landscaped areas within the right-of-ways surrounding the Roe/Johnson Drive intersection.
- New Gateway monuments throughout the district.
- Streetscape improvements including trees, sidewalk enhancements, architectural lighting, site furnishings, plantings, irrigation, and environmental graphics.
- Trails, trail connections, and a trail head (Fairway Office Park).
- A future Transit Center on Martway.

Mission Mall Redevelopment Project (*The Gateway*)

The planning team coordinated with the Cameron Group and Slaggie Architects on several planning issues to ensure

consistency and sensitive integration of all redevelopment recommendations. These included:

- Incorporation of a regular block/grid pattern of internal streets.
- A combination of mixed-use buildings oriented to external and internal streets and pedestrian ways.
- Larger retail buildings are oriented to busier streets. Buildings will not turn their backs on public ways.
- Bulk of parking is concealed.
- A sidewalk system that links to the proposed trail/sidewalk system.
- Increased height allowances to make the development both attractive and feasible.
- Cohesive but not unilateral use of building materials.
- Varied and sensitive architectural treatments that express the character of the study area.
- Public realm improvements that enhance a pedestrian-friendly environment and establish an identity for the district.

Character/Exterior Finish

The public input received throughout the planning process stressed the importance of maintaining the eclectic and historic character of the area, particularly on Johnson Drive. This was a major objective in the design of concepts for each site. In response to this, several project sites were designed to retain existing small businesses whose storefronts may vary between 25 and 50 feet in width.

Several modifications to the Mission, Kansas Design Guidelines for the Johnson Drive Corridor are incorporated into the final recommendations of the East Gateway Redevelopment Plan in order to address these concerns. Amendments to the Design Guidelines are included in Appendix D of this report.



“The Gateway” development has been proposed to include many pedestrian friendly features, including pedestrian only “garden walks” (depicted above)



Martway will continue through “The Gateway” and buildings will be oriented to external and internal streets. Images Courtesy of Slaggie Architects



Maintaining the local flavor of the Johnson Drive Corridor was a guiding principle in developing the East Gateway plan

PART ONE INTRODUCTION



Introduction

In August, 2005, the three northeast Johnson County, Kansas cities of Mission, Fairway and Roeland Park joined together to sponsor a redevelopment study of the area surrounding the Mission Mall. The boundaries of the area (fold out map on last page of this booklet) included the Fairway Office Park at 55th and Shawnee Mission Parkway southwest along Shawnee Mission Parkway to Nall Avenue, and from Johnson Drive to Martway. Triggered by the recent sale of the Mission Shopping Center to the Cameron Group of New York, the three cities agreed to proactively work with elected officials, planning commissions and residents to determine future development guidelines for the area, in anticipation of additional development interest and pressure precipitated by the new concept for the Mission Mall. All three cities expressed a need to be prepared for development opportunities that will arise and a desire to have future projects be compatible and mutually supportive.

The East Gateway Redevelopment Study Team, comprised of elected and appointed officials and citizens of the three cities, was formed to guide the planning process and make recommendations to their respective City Councils.

Background and Planning Process

Building upon other recent planning efforts in the area, the planning team of Bowman Bowman Novick Inc., in association with Collins Noteis & Associates, met with the East Gateway Committee to clarify the mission and goals for the project. The Committee strongly supported public input about the future redevelopment of the entire study area, as well as the proposed Mission Mall Redevelopment project, and stressed the importance of coordinating future plans for the entire Johnson Drive Corridor. The Planning Team reviewed recently completed plans for the area, including The Mission, Kansas Design Guidelines for the Johnson Drive Corridor; The Mission/Rock Creek Redevelopment Masterplan (for Mission's Downtown District), the simultaneous planning effort for the Mission West Gateway Area (from Lamar to Metcalf); Mid-America Regional Council's "Idea Book for First Ring Suburbs"; and all relevant comprehensive plans and zoning ordinances for the three cities.

1) Public Input

The public participation component of the planning process for the East Gateway Redevelopment Study consisted of an initial interactive Visual Preference Workshop, attended by over 150 citizens; and three additional open public meetings to share the results of the Visual Preference Survey, review the initial

Mission Statement of the East Gateway Redevelopment Committee

*To develop **plans and guidelines** that ensure successful and appropriate redevelopment of areas within the City of Mission, Kansas, and adjoining areas within the Cities of Roeland Park and Fairway, Kansas. In developing these guidelines, we will work with and consider input from citizens, City Councils, business owners, property owners, real estate developers and other entities determined by the committee.*

*We are **committed** to helping our communities achieve the following goals:*

- ***Diversify, expand and improve** the communities' tax base and property values by encouraging appropriate development patterns which do not conflict with the needs or integrity of existing residential/small business neighborhoods.*
- ***Redevelop** aesthetically pleasing, economically successful, and ecologically sustainable neighborhoods that present and future generations will enjoy.*
- ***Establish** a pedestrian-friendly environment encouraging healthy lifestyles.*
- ***Promote** a mixed range of housing opportunities by establishing zoning regulations that support a Sustainable Lifestyle Community.*
- ***Reduce** flood potential while improving water quality in the redevelopment study area and downstream.*



The impetus for the East Gateway Redevelopment study was the announcement of the sale and planned redevelopment of the Mission Center mall.



The study area included portions of Mission,



Roeland Park...



And Fairway.

proposed recommendations for the redevelopment area and comment on a joint presentation by the Cameron Group and Slaggie Architects on the proposed new Mission Mall redevelopment concept. A final public meeting focused on the key issues and proposed recommendations for the final draft of the Concept Book. In addition, two unique joint meetings of the three cities' Planning Commissions and City Councils were held to coordinate issues between cities. The East Gateway Redevelopment Committee met nine times in open meetings to lead and coordinate key issues during the development of the plan.

2) Visual Preference Survey

The Visual Preference Workshop was held for the public on October 13, 2005, with over 150 citizens in attendance. The workshop provided dialogue stations on the topics of:

- Housing
- Height
- Mixed Use Zoning
- Density
- Street Hierarchy
- Public Realm
- Traffic
- Character and Exterior Finish of Buildings
- Parking
- Land Use

Each "Issue Station" was staffed by the Planning Team, and citizens could move between them as their interests dictated. A three-dimensional model was used to portray new development possibilities into existing neighborhoods.

Public input was creative and enthusiastic. Although many questions were raised regarding the Mission Mall redevelopment site specifically, feedback was elicited for the entire corridor. Residents expressed enthusiasm for new redevelopment, and higher densities along the corridor, but also expressed concerns about keeping the character and image of the area intact, and not allowing national franchise stores to negatively impact local businesses.

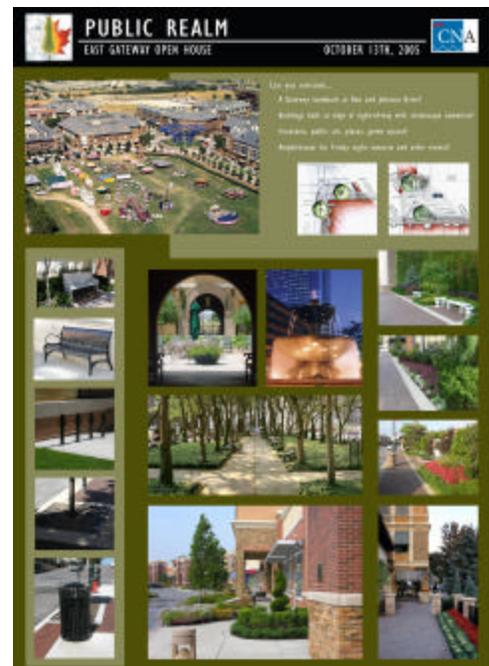
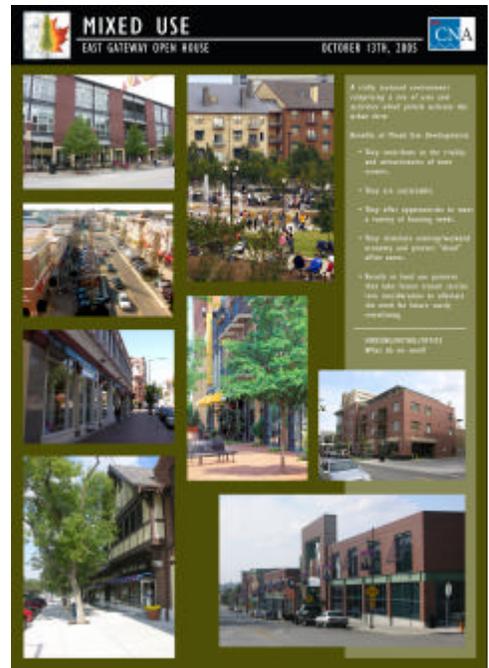
3) Key Issues

As the Planning Team worked through the issues raised at the Visual Preference Survey, eleven "Key Issues" became the framework for the planning effort in the corridor and helped to guide discussions with the Cameron Group on the design of Mission Mall site.

The key issues and questions were as follows:

1. There is a general consensus that the strip of single family houses converted to commercial uses on the north side of Johnson Drive in Roeland Park should be redeveloped to complement the new Mission Mall site and provide a gateway into Mission and Roeland Park. However, these properties will be a challenge for redevelopment if the property remains only one-half block deep. The narrow width affects the realistic size of marketable new retail space and reduces the amount of parking available for taller buildings. Is there a possibility that the adjacent neighborhood would permit deeper encroachment allowing a 3-story mixed use complex combining retail or office space on the first floor and a more urban loft type housing unit above?
2. The layout of the new mall redevelopment project should be externally oriented, with buildings facing the streets. A regular block or grid pattern is preferred.
3. Consider developing Martway into a parkway and possibly realigning it to give more space to the south side.
4. Consider extending Martway through the redeveloped Mission Mall site and onto Johnson Drive. A public plaza, green space, amphitheater, etc. should be incorporated into this space.
5. Reevaluate the Fairway Office Park. Is multi-family housing along the west edge a future consideration to create more of a neighborhood "edge" for surrounding single-family housing? Are there other considerations we should be aware of?
6. Consider a stronger landmark or gateway element at the intersection of Roeland Drive and Johnson Drive, and at the old cloverleaf site where Johnson Drive intersects Shawnee Mission Parkway. One idea from the public workshop suggested commemorating the original Roe Farmstead at this site.
7. Incorporate a transit center into the East Gateway study area. The Planning Team will be exploring alternative sites with the Johnson County transit staff and Mid-America Regional Council, who is staffing the Smart Moves Project.
8. Incorporate into the development plan a concept for relocation sites for local businesses that are being displaced by the Rock Creek Redevelopment Plan with designated areas for future small retail businesses. National chains and big-box retail would be prohibited in this area. Explore this concept on the north side of Johnson Drive west of and including the BP Station.
9. Consider affordable housing on the south side of Martway, as opposed to the Mall redevelopment site.

10 visual preference boards were created that focused on the key issues and provided a reference tool for the public, two of those boards are shown below.





Retaining the character of Johnson Drive was identified as a goal of the planning team

10. Explore with Roeland Park trail and park connections along Cooper Creek north of Johnson Drive on Roeland Drive.
11. Explore opportunities for more dense commercial development at the northeast corner of Nall and Shawnee Mission Parkway

CONCLUSIONS

The Planning Team's discussions of the issues outlined above in the Visual Preference Survey Workshop and subsequent public meetings and committee meetings were summarized for the East Gateway Committee on October 25, 2005. Most of the attendees were enthusiastic about the upcoming development activity. Most residents supported a mixed use model for the corridor, supporting higher density development. Significantly higher structures, (known at the time as a real possibility on the Mission Mall redevelopment site) were not opposed, but some wariness was expressed regarding any effect on surrounding neighborhoods. Taller buildings of 10 or 12 stories could be supported if they were placed on the Shawnee Mission Parkway side of the mall site. Retaining the area's eclectic and historic character, even within the new mall redevelopment site was an important concern and widely expressed. Retaining on-street parking and adding quality off-street parking was supported by attendees. Exploring the opportunity to narrow Johnson Drive in order to achieve a more pedestrian-oriented district was generally supported.



Public Realm improvements, such as parks, trails, streetscapes and plazas, are an extremely desired component of future redevelopment in the District.

PART TWO

RECOMMENDATIONS FOR THE STUDY AREA

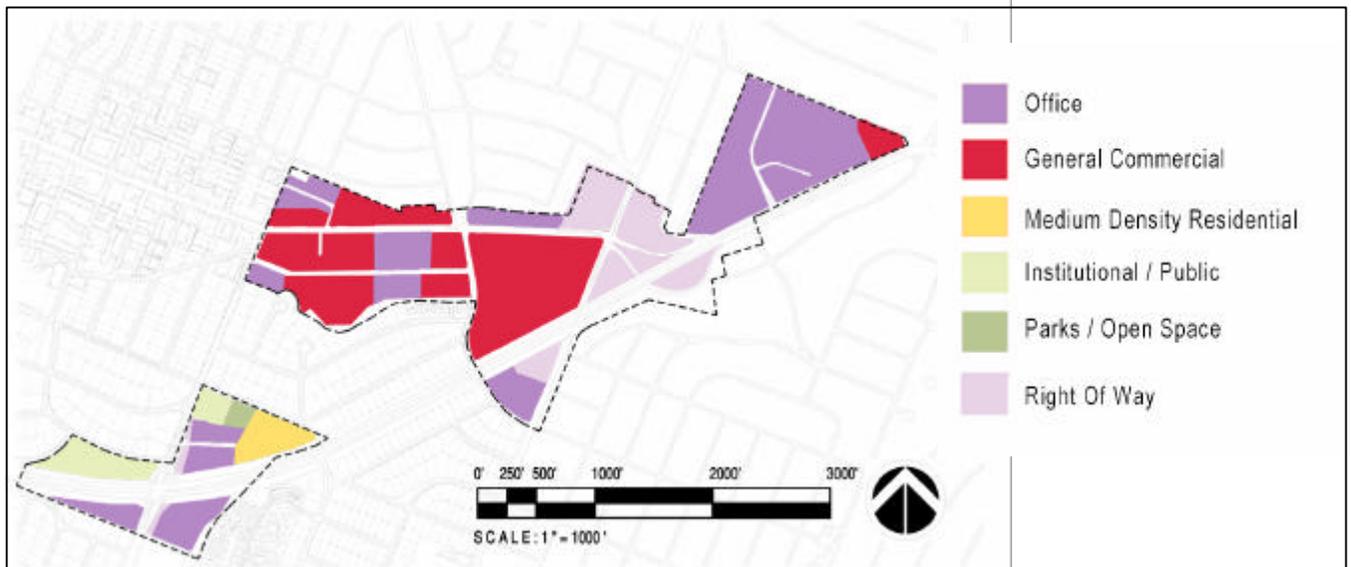


June, 2006
BOWMAN BOWMAN NOVICK
COLLINS NOTEIS & ASSOCIATES

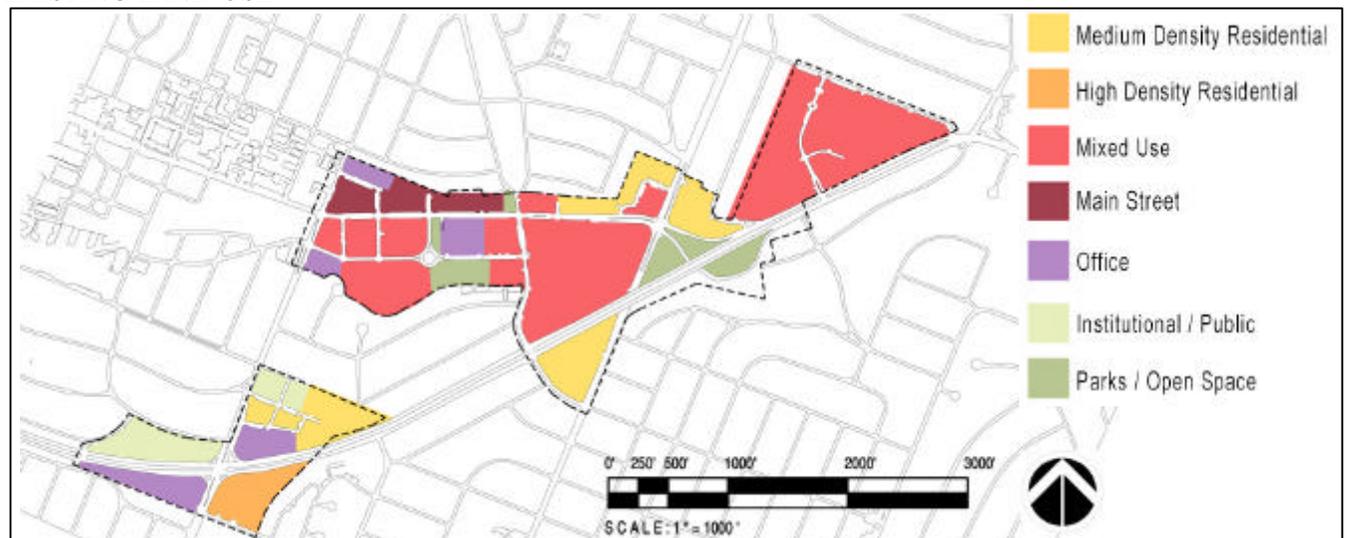
RECOMMENDATIONS FOR STUDY AREA

A. Land Use/Mixed Use/Density

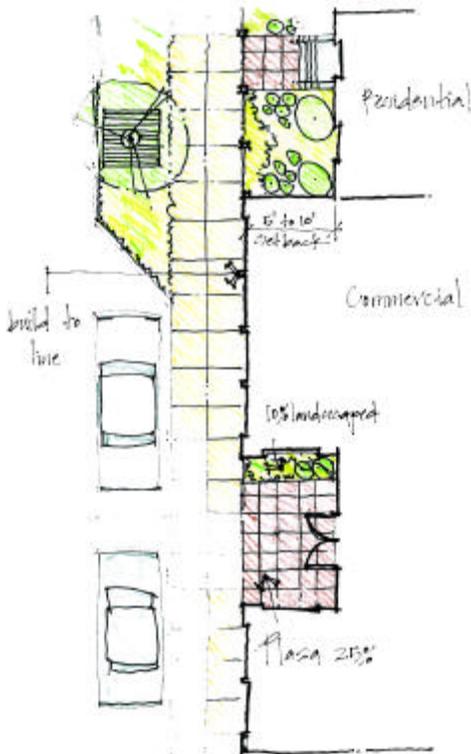
The Land Use Plan for the East Gateway Redevelopment Area was guided by the desire of the citizens of Mission, Fairway, and Roeland Park, Kansas to share in the creation of a Mixed-Use walkable community. Such a community will support a higher density level of residential, commercial and retail uses that in turn support and enhance a sustainable live, work and play environment. While small commercial and multi-family residential areas are also defined in the East Gateway, they are done so to complement Mixed-Use redevelopment and to provide a sensitive and necessary transition to existing single-family neighborhoods bordering the study area.



EXISTING LAND USE



PROPOSED FUTURE LAND USE



This diagram shows the street to sidewalk to storefront relationship that can be created by establishing a consistent build-to line.



Building heights taller than those currently existing in the district are encouraged and acceptable at several specific locations (including two planned towers in “The Gateway” development; rendered above) in order to mark the area as a destination and make redevelopment financially feasible.

Image courtesy of Slagge Architects

Nearly every site within the East Gateway Redevelopment Plan is shown to be of higher density, with the majority of new construction occurring at build-to lines that create and define a public realm zone along roadways and other strategic areas. Small Commercial/Retail zones are maintained in specific areas to not only help retain local businesses but to attract other small business enterprises to the community. These areas for Small Commercial/Retail use are planned primarily for the north side of Johnson Drive between Roe Avenue and Nall, where property depth is constrained by the existing residential neighborhoods immediately north in both Roeland Park and Mission. Because of the physical constraints of the properties here, redevelopment of these Small Commercial/Retail zones are planned to be a maximum of two stories, with adjacent on-street parking and parking lots located in the rear.

The area north of Johnson Drive between Roe Avenue and Nall also incorporates zones of Multi-Family Residential that provide each of the three cities with a wide range of housing options that are currently in short order. These new housing options are designed to complement the zones of Mixed Use redevelopment, as well as to retain area residents and respond to the demand for more urban housing stock.

The varied uses included in the East Gateway Redevelopment Plan will benefit from a comprehensive trail system and pedestrian-friendly streetscapes and plazas that connect each use and provide linkages to the trail system proposed throughout the greater Kansas City Metropolitan Area. Similarly, the Plan incorporates the infrastructure needed to implement the Smart Moves Transportation Plan, with a site reserved for a future Transit Center on Martway, centrally located within the East Gateway Redevelopment Area.

B. Height

The following building heights are allowable within the East Gateway District:

- North Side of Johnson Drive between Roe and Nall, including the cloverleaf at the northeast corner of Roe and Johnson Drive (Site Two): **3 story maximum with the exception of the part of Site Three planned for mixed use**
- The area bounded by Martway, Johnson Drive, Roeland Drive, and Nall: **3 story minimum, 8 story maximum.**
- Properties included in Sites 15, 16, 17, and properties adjacent to the intersection of Nall and Shawnee Mission Parkway: **5 story Maximum**
- Properties bounded by Roeland Drive, Shawnee Mission Parkway, and Roe (Site Fourteen): **4 story maximum**

June 2006

BOWMAN BOWMAN NOVICK
COLLINS NOTEIS & ASSOCIATES

- Residential Redevelopment with the Fairway Office Park (Site One): **3 story Maximum**

C. Street Hierarchy

The East Gateway Redevelopment Plan includes several recommendations for future roadway realignments and improvements. These recommendations were designed to improve on the existing grid network of primary and secondary streets for safer and more uniform street intersections, and pedestrian crossings.

Reconfiguration of some side streets, such as Ash and Roeland Drive on the north side of Johnson Drive, are reconfigured to consolidate the quantity of intersecting roads onto Johnson Drive and to provide a more perpendicular (orthogonal) arrangement. Other side streets, such as Birch north of Johnson Drive, are reconfigured to align with its counterpart on the south side of Johnson Drive. These subtle changes to the street network will benefit the East Gateway community by providing clarity and increased safety for vehicular traffic and pedestrians.

All of the streets within the East Gateway Redevelopment Area are contained by regular and uniform building lines. Traffic counts, parking strategies, and frontage types have all been considered in the final recommendations described as follows:



The reconfiguration of Roeland Dr. (above, existing and proposed road configurations) will allow for a more coherent intersection.

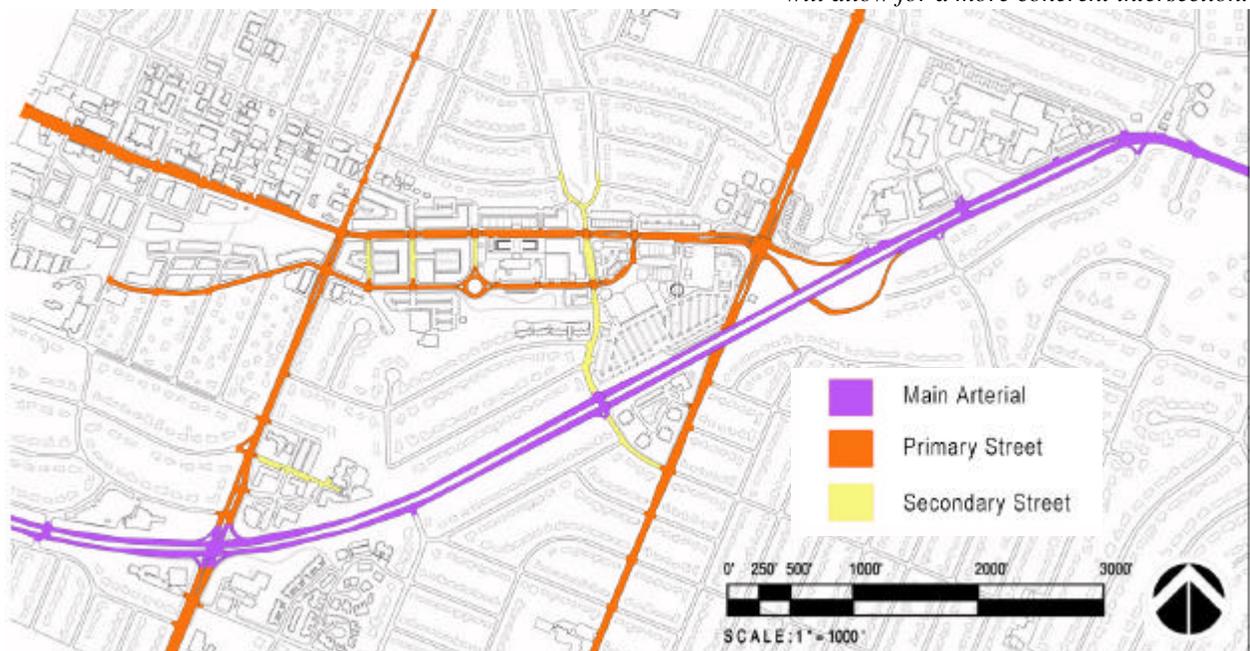


Diagram of Street Hierarchy within the District



View of Shawnee Mission Parkway



Proposed pedestrian crossing at Shawnee Mission Parkway and Roeland Drive.



View of Johnson Drive as it exists today. Numerous improvements are proposed to make Johnson Drive more pedestrian friendly and attractive.

Shawnee Mission Parkway

Considered a “main arterial” roadway and falling under the auspices of the Kansas Department of Transportation, Shawnee Mission Parkway provides the means for the most efficient through-traffic for motorists traveling between the Country Club Plaza to the east, and the cities of Merriam and Shawnee to the west. It also provides the most direct route to the interstate highway system.

The Gateway redevelopment project (former Mission Mall) proposes the closure of the west bound access ramp from Roe Avenue in order to allow for right-in and right-out access from Shawnee Mission Parkway. This is designed for more direct access from Shawnee Mission Parkway to the retail destination and to balance the traffic load for other entrances onto The Gateway property.

The East Gateway Redevelopment Plan recommends the addition of a crosswalk at the intersection of Roeland Drive and Shawnee Mission Parkway to provide a link to the surrounding neighborhoods on the south. This crosswalk will require substantial coordination with KDOT in order to ensure a safe walking zone for pedestrians.

Representatives of the single family residential neighborhood adjacent to Shawnee Mission Parkway voiced concern about the potential increase of unwanted traffic noise. An increase in traffic due to traffic switching over from Johnson Drive, the success of The Gateway redevelopment, and overall increased population in the area is the cause of their concern. Thus it is recommended that the City of Mission work with KDOT to obtain base information of existing noise levels in the area so they can be monitored over the coming 3 years. The results of this testing will help ascertain whether noise abatement measures are warranted.

Johnson Drive

Johnson Drive is a primary east/west street in the East Gateway Study Area, and is recommended to receive substantial improvements to calm traffic, beautify the area, and provide a more pedestrian friendly environment. These changes occur between the second entrance to the East Gateway site west of Roe Avenue and continue through Nall Avenue. Here the roadway is proposed to be narrowed to one lane east and one lane west with a center turn lane (three lanes total). The recent intersection improvements at Roe Avenue and Johnson Drive will not be modified.

June 2006

BOWMAN BOWMAN NOVICK
COLLINS NOTEIS & ASSOCIATES

The narrowing of Johnson Drive through the East Gateway Study Area permits increased right-of-way for the proposed on-street parking and pedestrian oriented streetscape on both sides of Johnson Drive. This is particularly beneficial to the properties on the north side since the depth of the sites here are very shallow and make redevelopment somewhat of a challenge.

Other recommended streetscape improvements to Johnson Drive include bulb-outs that define on-street parking zones, wider pedestrian sidewalks, well-defined crosswalks at key locations, a regular spacing of street tree plantings and other planting beds offering seasonal color and texture. Lighting, environmental graphics, site furnishings and other amenities are also recommended.

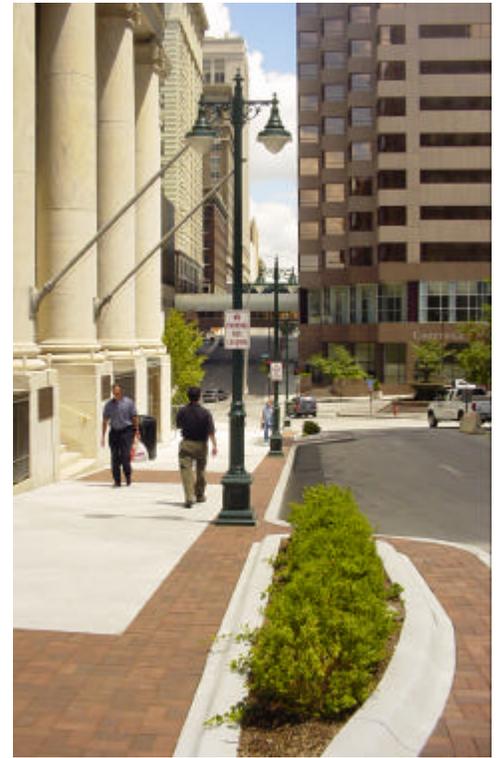
Martway

As another primary east/west street in the East Gateway Study Area, Martway runs somewhat parallel to Johnson Drive as it meanders in and out of the single-family and multi-family residential neighborhoods south of Johnson Drive. Since it also connects the east and west commercial areas of the Johnson Drive Corridor, Martway often serves as a bypass route for motorists.

Early in the development of the East Gateway Redevelopment Plan, an opportunity was recognized to significantly improve Martway “as a Parkway”. Of key importance here is the extension of Martway through The Gateway redevelopment project, thus moving its terminus (formerly at the underground parking structure for the Mission Mall) to Johnson Drive. Improving the attractiveness of Martway will enhance the vehicular and pedestrian system in the East Gateway Study Area and in turn improve the attractiveness of the surrounding uses including the proposed Transit Center, Martway Park, The Mission Promenade, and all the Mixed Use redevelopment of the parcels to the north between Roeland Drive and Nall.

Parkway improvements include a roundabout with a fountain at the intersection of *Promenade Street* (the alley between the existing Capitol Federal Bank and the Mission Mart) as an important focal point. A roundabout in this location will also provide the bus turn-around spot necessary for the future transit system.

Martway Parkway will remain one lane east and one lane west with a center left turn lane. On-street parallel parking will be provided where feasible, as well as a streetscape scheme that includes sidewalks, street trees, lighting, and feature plantings.



Streetscape improvements are recommended for Johnson Drive, Martway, and the north/south streets connecting the two to make the heart of the East Gateway District more of a pedestrian friendly environment.



A transit center is proposed on Martway adjacent to “The Gateway”. The site selected allows for a linear transit center, is central to the district, and allows for connections between local and rapid transit lines. For more information on the selected transit center site, see appendix A



No changes are planned for the intersection of Roe and Johnson Drive, except for an improved pedestrian crossing and the addition of landscape features.



This aerial image shows the layout of Birch, Johnson Drive and Nall as they exist today.



The Plan calls for realigning Birch Street to improve the intersection and create a more clearly defined street grid.

Roe Avenue

Roe Avenue is a primary north/south street in the East Gateway Study Area. The recent improvements to the intersection at Roe and Johnson Drive have been widely accepted by area residents so only a few changes are proposed here. The incorporation of a well-defined crosswalk on the north side of Johnson Drive and a more well-defined sidewalk from Johnson Drive to Roeland Drive are recommended.

Roeland Drive and Ash

The segment of Roeland Drive beginning at Roe Avenue north to Cooper Creek in Roeland Park is considered a secondary street. Its width and characteristics are varied in this segment, and it is recommended that the portion between Roe and Shawnee Mission Parkway (near Site 14) be widened to allow one lane north and one lane south with a center turn lane. The proposed redevelopment scheme for Site 14 on the east side of Roeland Drive at Roe will eliminate several curb-cuts that presently exist and make this segment of Roeland Drive a safer and more efficient link to the East Gateway area.

It is also recommended that Ash Street be reconfigured to terminate into Roeland Drive north of Johnson Drive to provide a more legible and safe intersection. This results in a more substantial land area west of Roeland Drive for an enhanced “gateway” and feature landscape for the City of Roeland Park. It would also provide a more prominent trail connection to Cooper Creek to the north.

W. 58th Terrace and Birch Street

West 58th Terrace and Birch Street are secondary streets in the East Gateway Study Area. It is recommended that the segment east of Nall be reconfigured to terminate into a realigned segment of Birch Street. This scheme aligns the north/south segments of Birch where it intersects Johnson Drive, and will better allow for pedestrian crossings. The realignment also provides a more regular and uniform building line for the proposed redevelopment parcels on either side of Birch north of Johnson Drive.

The segment of Birch south of Johnson Drive terminates at Martway and provides the access for future parking structures insulated within the Mixed Use redevelopment parcels to the east and west.

Johnson Drive Mews

The Plan recommends that the north/south alley behind the existing Kinko’s retail store between Johnson Drive and Martway become a public street offering access to a future

parking structure and service area at the adjacent redevelopment sites. It is proposed that this street be constructed as a *mews* with ample pedestrian and vehicular traffic zones and amenities such as plantings, lighting, and site furnishings.

Nall and West 61st Terrace

Nall Avenue defines the west boundary of the Study Area and is considered a primary north/south street. Nall provides access to several redevelopment sites including the Countryside Church, Mission Springs Assisted Living, Harrington Bank, Walz Tetrick building, Lutheran Church, and several small commercial office structures. The recommendations for this area include reconfiguration of the streets internal to the existing development at the northeast corner of Nall and Shawnee Mission Parkway. Specifically, it introduces an extension of West 61st Terrace onto the site for a more uniform street grid system and improved access to the Mission Springs Assisted Living facility.

D. Trail and Sidewalk System

The proposed trail and sidewalk system in the East Gateway Development Plan provides multiple points of access and connects the redevelopment area to its surrounding community. Providing a safe and thorough network of paved surfaces to facilitate a pedestrian-oriented district is the key objective here. Due to the more urban nature of the district, the trail route utilizes some portions of the primary sidewalk system, such as the curving segment through The Gateway.

At the east end of the study area, the trail commences at Buena Vista Street and suggests a future extension north and then east to the Shawnee Indian Mission (Mission Road and W. 53rd Street), and further east to the Fairway Shops at Belinder and Shawnee Mission Parkway.

The trail is routed along the north side of Shawnee Mission Parkway through the Fairway Office Park, where open space surrounding the storm water detention area could serve as a future trail head/rest stop. The trail follows the southern edge of the proposed housing developments east and west of Roe Avenue in Roeland Park. A defined crosswalk is proposed at Roe to facilitate this connection.

Midway between Roe and Roeland Drive, the trail turns south and crosses Johnson Drive (another crosswalk is proposed) to pick up its connection to Martway and to provide access to The Gateway. The trail crosses Roeland Drive at Martway where it follows the line of Rock Creek, before bending back to Martway at the site of Mission Bowl. Another bend to the north and a crosswalk at Nall Avenue and Johnson Drive link the trail to its counterpart through the Downtown District of Mission.



An example of a mews in Addison, Texas.



61st Terrace is proposed to be realigned to create a more uniform street grid.



The trail will extend along the north side of Shawnee Mission Parkway past Roe, providing an opportunity for future extensions to the Shawnee Indian Mission and the Fairway shops, as well as other nearby trail systems.

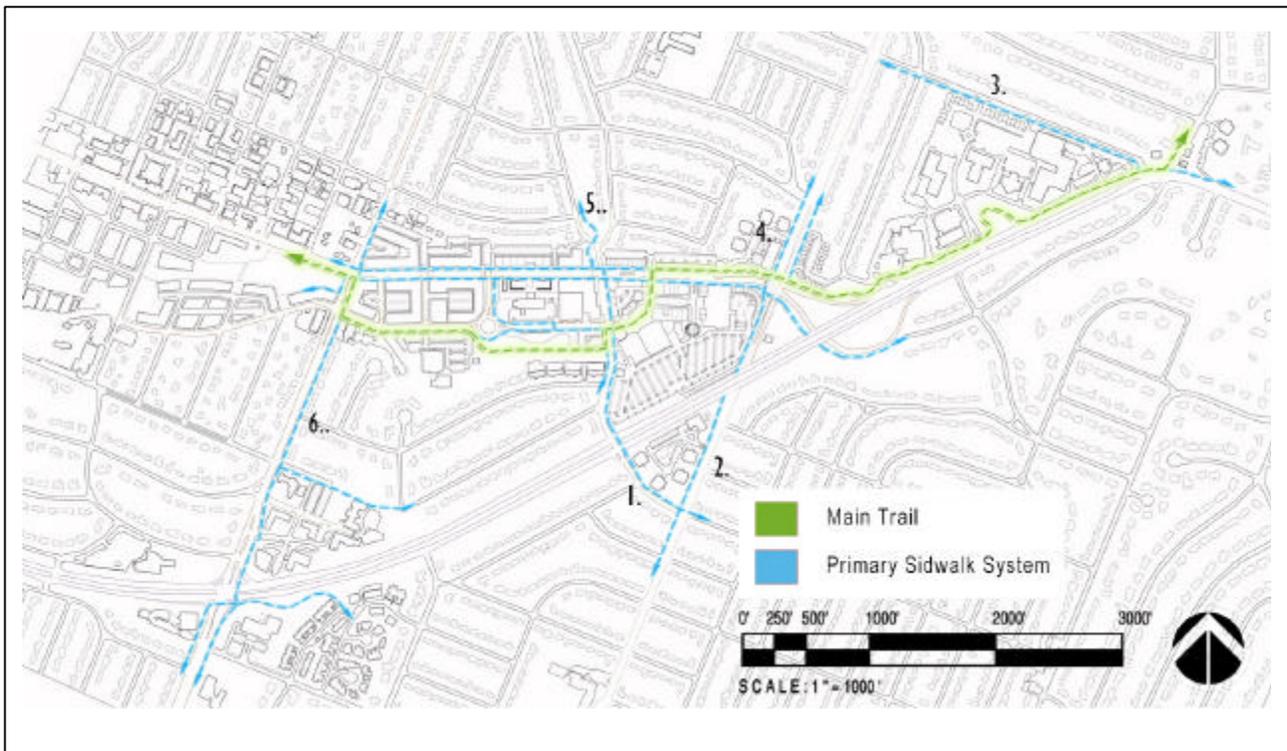


The sidewalk system in the East Gateway District currently has several areas that either have unsafe and narrow sidewalks or lack sidewalks completely.

The primary sidewalk system proposed in the East Gateway Redevelopment Plan completes the network for a pedestrian-oriented district. Aside from the obvious locations adjacent to primary and secondary roadways in the study area, the sidewalk system is also proposed to correct deficiencies in several key locations:

1. Link between The Gateway and proposed medium density housing south of Shawnee Mission Parkway.
2. Link between the Roe/Johnson Drive intersection and the Roeland Drive/Roe intersection.
3. Lack of sidewalks on W. 55th Street adjacent to proposed medium density housing in Fairway Office Park.
4. Lack of sidewalks on both sides of Roe, north of Johnson Drive.
5. Link between Roeland Drive/Johnson Drive north to Cooper Creek.
6. Lack of sidewalks in the vicinity of Nall Avenue and Shawnee Mission Parkway, and along Nall to Johnson Drive.

Trails are recommended to be a minimum width of 8 feet and be paved with either asphalt or concrete. Wayfinding and informational signage consistent with other signage within the district, is also proposed. Sidewalk standards are described in the Mission Johnson Drive Corridor Design Guidelines for both primary and secondary locations.



PART THREE CONCEPTS FOR INDIVIDUAL PROJECT SITES



Concepts for Individual Project Sites

The final recommendations contained in the East Gateway Redevelopment Plan encompass a variety of concepts that respond to the “Key Issues” identified at public and committee meetings, and represent the vision for future public and private development within the tri-city study area. The following is a summary of the recommendations and corresponding site specific implementation actions (refer to the last page of this booklet for a fold out map of the entire redevelopment plan)

Site 1 Fairway Office Park

Redevelopment considerations for the Fairway Office Park take advantage of its Mixed-Use zoning classification. The concept for Site 1 includes the removal of excess surface parking and the incorporation of more urban housing options for the area. The concept adds 5 town homes and 15 row houses at the northwest corner of the site, adjacent to existing single family residential. Rear access garages and additional landscape buffers enhance this mixed use environment.

Proposed Implementation Actions:

- City: **Fairway**
- Seek agreement with the current property owner concerning the development of a residential project in the parking area.
- The site’s current BP-3 Zoning District designation allows residential uses with modification of the development plan.

Site 2 NE Corner of Roe and Shawnee Mission Parkway

The redevelopment concept for Site 2 was developed through a separate planning study conducted by the City of Roeland Park and their consultants; Gould Evans Architects. It includes 16 town homes and 9 patio homes with a surface parking concealed from surrounding street frontages.

Proposed Implementation Actions:

- City: **Roeland Park**
- Prepare a Request for Proposal for development which would provide for a medium density residential project which fits the character of the East Gateway Redevelopment Plan.
- Identify city limits and enter into agreements with the City of Fairway on property within the City of Fairway north of Johnson Drive.



*Site 1:
Fairway Office Park; 20 Townhomes*



*Site 2:
East Cloverleaf; 25 Patio & Townhomes*





Site 3:
West cloverleaf; condominium buildings & a mixed use tower. Total of 122 Units.

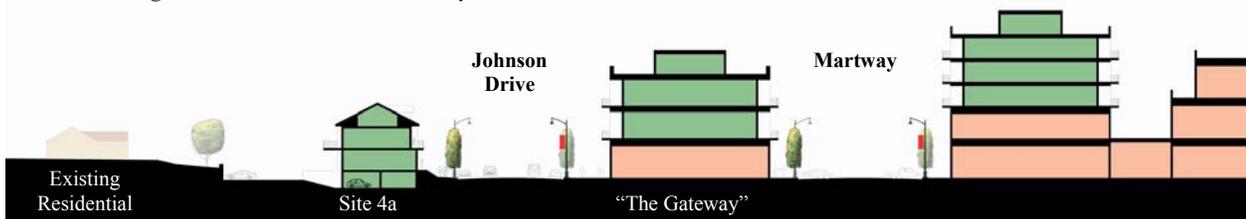


Site 4a:
10 Rowhouses



Site 4b:
9 2-story Mixed Use Units

Below:
Section Through sites 4a and “The Gateway”



Site 3 NW Corner of Roe and Shawnee Mission Parkway

A high density residential tower development is proposed at the northwest corner of Roe and Shawnee Mission Parkway as a complement to the residential tower of The Gateway development across the street, and to provide increased density and more urban housing options for Roeland Park. The 10-story tower offers 90 living units of 1150 square feet each with a subsurface parking garage and an outdoor pool/patio area. Four additional 2-story condominium buildings adjacent to existing single family residential complete the complex, adding 32 living units to the development.

Proposed Implementation Actions:

- City: **Roeland Park**
- Assemble or assist in gaining control of non-Roeland Park ownership on the site.
- Consider adopting an overlay district to allow mixed use.
- Seek Development Proposals for a mixed use development allowing commercial development on first floor.
- Establish Design controls and approve plan for site.

Site 4 North side of Johnson Drive

This redevelopment concept responds to the desire for more urban housing options and the desire for Neighborhood preservation. Site 4A incorporates ten 3-story row houses with rear access garages on the first floor and surface parking behind. The scheme complements the housing types included in The Gateway development across Johnson Drive.

As the properties increase in depth moving west toward Nall, the land use classifications change to “Mainstreet”. This is intended to incorporate a mix of small retail/commercial with medium density housing where parking can be supported on site. Site 4B incorporates a two-story building with ten residential loft units over ten retail spaces. This recommendation also responds to the desire to retain existing and encourage new local retail in the area.

Proposed Implementation Actions:

- City: **Roeland Park**
- Adopt code that allows zero lot/town home development as part of overlay district.
- Assist in the alignment of Johnson Drive right of way and reconstruction of Johnson Drive

Site 5 North side of Johnson Drive

Concepts recommended for Sites 5A, 5C and 5D allow for more traditional retail, commercial and restaurant businesses with storefronts in 25 ft. increments and a building depth of 70 ft.

The concept recommended for Site 5B is intended to attract smaller retail entities in a *market-like* setting with storefronts of 25 ft. increments and a building depth of 70 ft. The concept incorporates a building façade treatment with a south-facing covered walkway and operable storefront walls that allow individual shops to open out onto the adjoining sidewalk during temperate weather.

Sites 4 and 5 redevelopment concepts retain the existing rear property lines abutting single family residential properties. This was an important consideration for neighborhood preservation.

Proposed Implementation Actions:

- Cities: **Mission & Roeland Park**
- **Mission** to adopt Form Based Codes including site design parameters.
- **Mission** to identify future right of way and street alignment for Johnson Drive.
- **Mission** assists in site assembly.
- **Mission** to redesign Birch Street & 58th Terrace.
- **Mission** to amend Land use Plan.
- **Roeland Park** to manage the realignment of Roeland Drive and Ash Street and incorporate trail and bikeway extensions.
- **Roeland Park** to design and implement Roeland Park Gateway entrance feature north of Johnson Drive.



*Site 5a:
1 story Retail/Restaurant*



*Site 5b:
"Mission Fair" Site, 1-story Retail*



*Site 5c & 5d:
1-story retail*



Rendering of the "Mission Fair" concept



Site 6:
Renovated Mission Bank



Site 7:
"Grocery Store Site". 3 story Mixed Use Building with a grocery store envisioned as the street level tenant.



The renovated Mission Bank Site is proposed to be similar to the Procter and Gamble Headquarters (Pictured above).

Site 6 Mission Bank Building

This prominent structure within the Johnson Drive Corridor is one of the few areas where the East Gateway Redevelopment Plan illustrates a building remote from the build-to line. Because of its viability as a commercial office building, the Plan proposes several improvements rather than complete removal of the existing development. These include incorporation of a subsurface parking garage in lieu of the surface parking lot, the addition of a more visually pleasing and prominent entry lobby for the building, and construction of a public plaza over the subsurface garage. The plaza concept is reminiscent of the Proctor and Gamble Headquarters in Cincinnati, Ohio with pergolas, walkways, fountains, turf, planted areas and site furnishings that offer pedestrians a place of respite in the center of the District.

Proposed Implementation Actions:

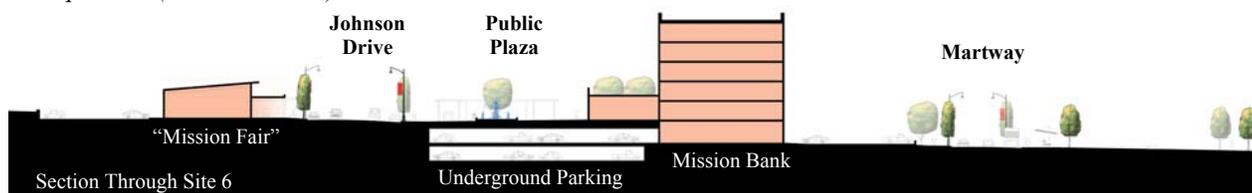
- City: **Mission**
- Begin discussion with owner concerning long term plans
- Consider a Public/Private partnership to develop the public improvements (garage and plaza).
- Design and construct Public Parking/ Public Plaza

Site 7 SW corner of Roeland Drive and Johnson Drive

The Plan proposes a mixed-use redevelopment concept for the corner site immediately west of The Gateway development. A 3-story building abuts the right-of-way along Roeland Drive and Johnson Drive with first floor retail (grocery store) and two floors of residential condominiums. A two-story parking garage is proposed behind the building, accessible from Martway, with a rooftop pool and landscaped area for the residential units. This concept adds 24 living units to the area, and 35,000 square feet of retail space.

Proposed Implementation Actions:

- City: **Mission**
- Adopt Form Based Codes including site design parameters.
- Develop redevelopment plan for site.
- Amend TIF Plan if necessary.



June 2006

BOWMAN BOWMAN NOVICK
COLLINS NOTEIS & ASSOCIATES

Site 8 Wendy’s site

Pending the final design of the Rock Creek storm water drainage improvements, the concept for the existing Wendy’s site includes a 10,000 square foot retail/restaurant development with outdoor dining overlooking the Martway Park. It is possible that this site will be considered unfeasible for building due to the Rock Creek Improvements. Continued coordination with the City of Mission will be required over the coming months.

Proposed Implementation Actions:

- City: **Mission**
- Identify location of Rock Creek storm drainage alignment and evaluate feasibility of site for construction.

Site 9 South side of Martway

The Plan proposes the City’s acquisition of property on the south side of Martway between Wendy’s and the Mission Bowl for a new public park. The 3-acre area will complement the adjacent residential development proposed in the Rock Creek Plan and the East Gateway development.

The park concept includes a possible future Transit Center with a linear transit stop. It includes a pull-off lane for buses to load and unload, ample sidewalks, and a linear pavilion that doubles for bus passenger and park users. Other possible locations for the Transit Center are the intersection of Shawnee Mission Parkway and Roeland Drive.

Proposed Implementation Actions:

- City: **Mission**
- Acquire Property
- Coordinate planning, design and construction with Johnson County Transit and MARC on transit center.
- Incorporate bike and pedestrian plan with design
- Design and construct new Martway Park and open space.

Sites 10, 11 East sections of Mission Mart

Mixed use retail and commercial office buildings are proposed at the Mission Mart sites. The redevelopment concept brings building facades to the build-to line, and conceals parking in an internal parking structure. Access to parking garages is from Birch Street, and the *Johnson Drive Mews* between Sites 11 and 12.

Though the physical size of Sites 10 and 11 make them suitable for larger retail uses; it is important to note that the intent is to provide storefronts on Johnson Drive, Martway and side streets.

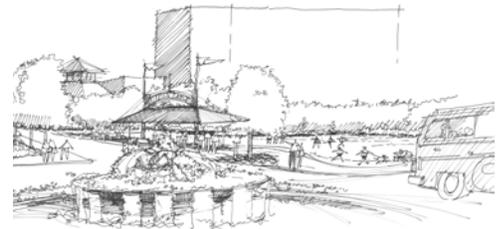
June 2006
 BOWMAN BOWMAN NOVICK
 COLLINS NOTEIS & ASSOCIATES



Site 8:
 1-story retail/restaurant



Site 9:
 Martway Park and Transit Center.



Rendering of Martway after improvements



Sites 10 & 11:
 3 story Mixed Use Buildings



Site 12:
1 story retail/restaurant building



Site 13:
Mission Bowl Site Improvements



The Mission Bowl is retained in the redevelopment plan, with improvements recommended to its parking lot.

Proposed Implementation Actions:

- City: **Mission**
- Begin discussion with owner concerning long term plans
- Create site guidelines including parking, height, set backs, use, landscaping, screening and other design considerations

Site 12 Westernmost section of Mission Mart (Kinko's)

The site at the SE corner of Nall and Johnson Drive sits adjacent to the proposed Martway and Rock Creek Parks of the East Gateway and Downtown Districts. A one-story retail/restaurant building is proposed in this location with rear surface parking.

Proposed Implementation Actions:

- City: **Mission**
- Begin discussion with owner concerning long term plans
- Create site guidelines including parking, height, set backs, use, landscaping, screening and other design considerations

Site 13 Mission Bowl

Flanking the west end of the proposed Martway Park, is the existing Mission Bowl development combining indoor and outdoor recreation. Several improvements to this development are recommended including landscape treatments to provide visual relief and screening for the surface parking lot and re-organization of the lot to create better defined internal circulation and entry/exit. The recommended increased right-of-way along Martway will allow a more defined sidewalk in front of the Mission Bowl and thereby enhance a pedestrian-friendly environment.

Proposed Implementation Actions:

- City: **Mission**
- Assist owner in parking lot, lighting and landscape plans for the Mission Bowl and coordinate with future design and extension of pedestrian and bike trail and general exterior improvements.

Site 14 Triangular site at Roeland Drive, Shawnee Mission Parkway and Roe

The concept for Site 14 at Shawnee Mission Parkway and Roeland Drive, changes the land use from Office and Right-of-Way to Medium Density Residential. Six multi-story condominium buildings add 96 living units to the area and are supported by a clubhouse and pool complex.

Proposed Implementation Actions:

- City: **Mission**
- Encourage consolidation of property into one parcel and encourage medium density residential

Sites 15, 16 Northeast corner of Shawnee Mission Parkway and Nall

Several changes to the existing developments at this intersection include reconfiguration of internal roadways, expansion of the Mission Springs Assisted Living facility, and the incorporation of a new 4-story office building and subsurface parking garage at the corner.

Proposed Implementation Actions:

- Amend land use plan to allow senior housing at site 15.
- Require extension of W. 61st Terrace through the site as part of redevelopment package.
- Amend Land use plan to encourage higher density at site 16
- Encourage development of property at site 16 by allowing office and mixed use in buildings up to 5 stories

Site 17 Southeast corner of Nall and Shawnee Mission Parkway

The concept for Site 17 at Shawnee Mission Parkway and Nall changes from Office to High Density Residential. A five story condominium building is proposed here adding 50 living units to the area. Parking is provided in a subsurface garage, and surface stalls for visitors.

Proposed Implementation Actions:

- City: **Mission**
- Encourage high density residential with building up to 5 stories. Condominium development recommended.



Site 14:
6 4-story condominium buildings (96 units) and a clubhouse.



Site 15:
Expansion of assisted living facility



Site 17:
5 story condominium building (50 units)

PART FOUR PUBLIC POLICY



PUBLIC POLICY AND IMPLEMENTATION**A. Zoning Recommendations**

- Formalize the review and enforcement of each city's plan review process within the study area. Assure that plans submitted for building permits are consistent with the adopted Mission, Kansas Johnson Drive Corridor Design Guidelines. Each city should identify steps in the review process and require consistency with adopted plans.
- Amend each city's comprehensive plan to include the provisions within the East Gateway Redevelopment Plan.
- Fairway, Roeland Park and Mission should adopt provisions which require adherence to the Plan including zoning, design guidelines and streetscape design.
- Roeland Park should consider modifications to its code which would include an overlay district for the four sites within the study area. The overlay district should allow office, commercial and zero lot line residential. An example is the 47th Street Overlay District recently adopted.
- Develop Form Based Codes as an alternative to the use of existing zoning classifications for the development area made up of sites 4b, 5 (a,b,c,d) and 7. These sites make up an area, partly in Roeland Park and Mission, in which the character of the redevelopment is extremely important to the East Gateway District. The use of Form Based Codes will allow the detailed description of the ultimate form of the proposed development. This includes sites in both Roeland Park and Mission. (Refer to Appendix C – Form Based Codes)

B. Amendments to Design Guidelines

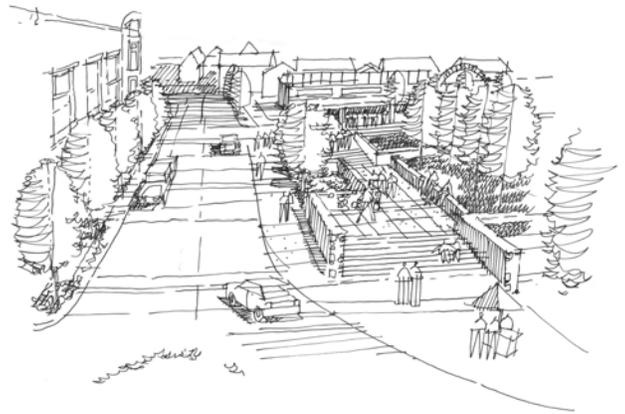
- Protect Mission, Roeland Park and Fairway's character through good design: Prepare and adopt or amend existing design guidelines that are applicable to each city's code.
- Formalize the ongoing oversight of the East Gateway Redevelopment Area by extending the appointment of the East Gateway Redevelopment Committee to assure attention to and coordination of key planning and development issues, long term commitment and public confidence and support.
- Develop and implement an East Gateway District Environmental Graphics Plan that is consistent and informative. Plan should identify parking, shopping, transit, public spaces pedestrian and bike paths, and other points of interest.
- Incorporate transit-oriented development standards into all project areas and provide for transit-friendly design, setbacks and accessibility.
- Incorporate standards that reduce flood potential while improving water quality in the redevelopment study area and downstream.
- Amend Mission Design Guidelines to add specific recommendations for the East Gateway area. These are described in Appendix D

C. Support for Redevelopment

- Foster a strong diversified economy through the creation, attraction and retention of businesses which provide knowledge-based, high quality employment opportunities for a successful workforce. Along with the redevelopment in the area, attention should be paid to a strategy to attract business to the tri-city area and retain existing business.
- Encourage development through the use of quality design, expedited review and approval, coordinated public improvements, state and local incentives and flexible zoning.
- Target redevelopment tools such as TIF, Neighborhood Improvement Districts, Transportation Development Districts, land acquisition and public improvements to reinforce the Design Guidelines.
- Develop specific guidelines for use of land acquisition tools in the East Gateway area which are consistent between the three cities.

E. Recommended Public Improvement Projects (Refer to Public Improvement Plan on Next Page)

- A. Realignment of Roeland Dr. and Ash north of Johnson Drive / Creek Relocation. Realign Roeland Drive to be the priority street north of Johnson Drive. Improve the park area, pedestrian trail and bike path, and re-route creek.
- B. Reconstruction of Johnson Drive from Roe to Roeland Drive. Establish lanes, parking, right of way, curbs, curb cuts, access to new mall. Allocate sufficient right of way to the development of the residential development north of Johnson Drive. Construct curbs, streetscape and curb cuts.
- C. Roeland Drive/ Johnson Drive intersection. Improve the intersection including pedestrian crosswalks, lighting, streetscape etc. Incorporate gateway concept.
- D. Martway and property acquisition for parkland. Acquire property south of Martway for the construction of transit center, parkland and pedestrian/bike way.
- E. Transit Center
- F. Martway, Martway Circle and Mission Promenade, property acquisition and design. Prepare a street design and streetscape plan for Martway from Nall to Johnson Drive.
- G. Design and implement screening and landscape plantings that improve the appearance (conceal) the water treatment facility on Roe Avenue.
- H. Johnson Drive-Roeland Drive to Nall improvements. Reduction to three lane right of way with parking. Design streetscape and pedestrian crossing.
- I. Parking/ Public space. Acquire or encourage existing property owner to dedicate for public use the property north of the Mission Bank. Parking and open space will be designed.
- J. Design and implement the gateway feature and crosswalks at Shawnee Mission Parkway and Roeland Drive.



View of "The Promenade" – Item F

The following items are general in nature and are not located on the map on page 31

- K. Conduct a traffic analysis of the redesign of Johnson Drive to three lanes throughout the entire corridor from Roe to Metcalf. Assess the impact and applicability of traffic calming recommendations and develop preliminary design and streetscape plans for implementation in each section of the corridor.
- L. Coordinate public infrastructure improvements with future development: Develop a five year plan for public infrastructure improvements in the corridor. Prepare in advance the anticipated street, curb, sidewalk and streetscape, utilities preliminary plans and cost estimates.
- M. Proceed with design of the Public Realm amenities in the east Gateway corridor such as Martway, Johnson Drive/SM Parkway, etc: Prepare preliminary designs for key development generating public improvements such as the Martway roundabout, the *Mission Promenade*, the Mission Bank Public Gardens and the Martway Park site.
- N. Mission should select a master developer for the sites.
- O. Proceed with development of Form Based Codes for specified sites (see part 3)
- P. Pedestrian – Bikeway design. Design bikeway for all areas through the East Gateway plan area.
- Q. Design and implementation of an East Gateway Signage Plan to include parking, public access, transit, bike and pedestrian routes, parks and special events.

ACTION ITEM	Mission	Roeland Park	Fairway
Formalize review & enforcement of city’s plan review process inside study area. Identify steps to require consistency with adopted plans. Assure submitted plans for building permits are consistent with Corridor Design Guidelines.			
Amend city’s Comprehensive Plan to include provisions of East Gateway Redevelopment Plan			
Adopt changes in zoning ordinance, design guidelines and streetscape guidelines that are required in East Gateway Redevelopment Plan.			
Adopt changes to zoning ordinance.			
Adopt recommended Design Guidelines and Streetscape Guidelines for the specific parts of the Redevelopment Corridor that are inside city limits.			
Consider modification to zoning ordinance for overlay district for 4 sites within study area. District should allow for mix of office, retail & zero lot line residential. (Example is 47 th St. Overlay District).			
Develop form-based codes for sites 4b,5a,b,c,d, and 7.			
Formalize the ongoing oversight of the East Gateway District by extending the appointment of the East Gateway Redevelopment Committee.			
Develop and implement District Signage Plan that is consistent and informative. Signage should identify parking, shopping, transit, trails, public spaces, pedestrian and bike paths. Three cities should coordinate design and implementation			
Incorporate transit-oriented development standards into all project areas and subsequent new projects. That recognize transit friendly design, accessibility and setbacks.			
Amend Mission Design Guidelines to include specific recommendations for the East Gateway Plan, as described in Appendix D			
Develop a coordinated strategy to retain and attract knowledge-based, high-quality employment opportunities for a successful workforce			
Encourage development through use of quality design, expedited review & approval, coordinated public improvements, state & local incentives & form-based codes.			

ACTION ITEM	Mission	Roeland Park	Fairway
Target redevelopment tools such as TIF, Neighborhood Improvement Districts, land acquisition and public improvements to reinforce design guidelines.			
Develop specific guidelines for use of land acquisition tools in East Gateway Corridor which are consistent between three cities.			
City of Fairway contact current property owner of the Fairway Office Park to seek agreement on Residential units on northwest corner of site. Seek residential developer. Zoning already in place.			
City of Roeland Park prepare Request For Proposal for medium density residential development at the Northeast Corner of Roe and Shawnee Mission Parkway as per recommendations for Site 2.			
Identify city limits and enter agreement with City of Fairway on property within Fairway north of Johnson Drive.			
Roeland Park assemble or assist in gaining control of private ownership on Site 3 at the northwest corner of Roe and Johnson Drive.			
Adopt an overlay zoning district or other method to allow for mixed-use development.			
Prepare a Request for Proposal for development of a mixed-use mid to high-rise building for residential condominium units with commercial use on the first floor and four 2-story condominium units, as per Site 3. Design should complement the Gateway Mall Development on south side of Johnson Drive, so that a true gateway landmark is introduced to the area.			
Roeland Park amend zoning and/or subdivision regulations (or adopt a zoning overlay) to allow zero lot line/town home development on the north side of Johnson Drive between Roe and Ash, as per recommendations in Site 4a and 4b.			
Roeland Park designate a crosswalk at Johnson Dr. and Roe Blvd			
Coordinate with Mission and assist (possibly with developer) in the realignment and reconstruction of Johnson Drive.			
Roeland Park coordinate with developer of Sites 4a and 4b new sidewalk on the north side of Johnson Drive, as per standards in this plan.			
Roeland Park initiate coordination and engineering of the reconfigured intersection at Ash and Johnson Drive that realigns Ash to terminate into Roeland Drive NORTH of Johnson Dr. for a safer intersection that aligns with Roeland Dr. south of Johnson Dr.			
Roeland Park investigate the possibility of doing Cooper Creek flood control improvements at the same time as intersection improvement.			
Roeland Park design and install a new gateway sign (as per design standards) for new intersection at Roeland Drive and Johnson Drive.			
Roeland Park design and construct pedestrian/bike trail connection at new intersection of Roeland Dr. and Johnson Drive.			

ACTION ITEM	Mission	Roeland Park	Fairway
Mission adopt form-based codes to implement Sites 5a and 5b			
Mission amend Land Use Plan.			
Identify future right of way and street alignment for Johnson Dr., so that sidewalk, parking and lot line can be fixed.			
Mission assist in site assembly for sites 5a and 5b.			
Mission redesign Birch St. at 58 th Terr., as per recommendations for Sites 5a and 5b			
Mission seek developer for Site 5a and the Mission Fair at Site 5b.			
Mission initiate discussion with Mission Bank about long-term plans			
Develop public/private partnership to fund improvements (garage and front plaza). Design and construct garage and plaza.			
Mission continue discussions with Capitol Federal about their potential plans to move and stay in the district. Assist in identification of new location. Acquire property at existing site to design and construct "Promenade " that connects Johnson Dr. to Martway and new Park.			
Designate crosswalk at Johnson Dr. and the Promenade.			
Mission initiate discussion regarding long-term plans for existing grocery store at Johnson Dr. and Roeland Dr. Explore development for new building with first floor grocery store and two floors of residential units above. The new building would be constructed at the right of way line, with new parking structure behind			
Mission prepare redevelopment plan for Site 7, as per recommendations in this plan.			
Mission adopt form-based code, including site design parameters for Site 7.			
Mission amend TIF Plan for Site 7, when appropriate.			
Mission designate crosswalk at corners of Johnson Dr. and Roeland Dr. and at Martway and Roeland Dr. where Martway intersects with new Gateway Mall entrance.			
Mission identify location of Rock Creek storm drainage alignment and structural stability to evaluate feasibility of site for construction of 10,000 square foot retail/restaurant development, as per recommendations for Site 8. If site is considered sound for development, prepare development plan.			
Mission acquire property on south side of Martway between Roeland Dr. and Mission Bowl property for construction of 3 acre city park.			
Mission incorporate bike and pedestrian trail with park.			

ACTION ITEM	Mission	Roeland Park	Fairway
Mission design and construct new Martway Park.			
Mission initiate discussion with current property owner of Mission Mart regarding long-term plans.			
Mission create site guidelines for parking, height, setbacks, use, landscaping, screening and storefront façade design, as per recommendations for Sites 10 and 11 and the Design Guidelines.			
Mission initiate discussion with current property owner at southeast corner of Nall and Johnson Dr. (Site 12) concerning long-term plans. Mission create site guidelines for parking, height, setbacks, use landscaping, screening and storefront façade design, as per Design guidelines and recommendations for Site 12.			
Mission assist property owners of Mission Bowl in designing improvements for parking lot, lighting and the future extension of pedestrian and bike trail through the property, and general exterior improvements.			
Mission encourage consolidation of triangular property on Site 14 at Shawnee Mission Parkway, Roe Blvd., and Roeland Dr. into one parcel.			
Mission encourage redevelopment plan for medium-density residential, as per recommendations for Site 14.			
Mission amend land use plan to allow for senior housing at Site 15 (northeast corner of Shawnee Mission Parkway and Nall. Require extension of W. 61 st Terrace through the site as part of redevelopment package.			
Mission amend land use plan to encourage higher density at Site 16. Encourage development of property by allowing office and mixed-use in buildings up to 5 stories			
Mission encourage change from office use to high-density residential, with subsurface parking garage at southeast corner of Nall and Shawnee Mission Parkway (Site 17). Condominium development is recommended.			
Mission designate crosswalk at Shawnee Mission Parkway and Nall.			
Mission continue to monitor and coordinate with the “Smart Moves” transit initiative from MARC for a location for a new rapid rider transit stop in the district close to Shawnee Mission Parkway. The recommended location in this plan is a linear stop, with a pull-out lane, pavilion and screening along Martway on the north side of the new Martway Park. Other possibilities include the intersection of Shawnee Mission Parkway and Roeland Dr., either incorporated into Site 14, with significant pedestrian safety measures; a central median concept along Shawnee Mission Parkway or the northwest corner of the intersection, as part of the mall development.			
Mission coordinate capital improvement funding and developer responsibilities for all necessary sidewalk and trail connections and public realm amenities for landscaping, streetscape and signage.			
Mission design and reconfigure Birch Street at W. 58 th Terrace.			

SUMMARY OF TRANSIT ISSUES

On November 17, 2005, the Planning Team met with area transit planners to coordinate transit and development issues in the corridor. The meeting included representatives from Bowman Bowman Novick, Collins Noteis & Associates, Mission City staff, Mid-America Regional Council’s Smart Moves staff and consultants and staff from the Johnson County Transit office.

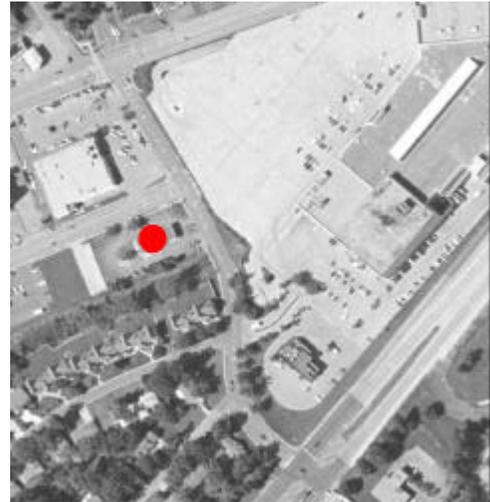
MARC’s Smart Moves Project includes reviewing existing local transit routes and ridership, as well as new Rapid Rider routes that are proposed for major transportation corridors (see image on next page). This new rapid transit route is currently planned for Shawnee Mission Parkway at the Mission Mall site. The new bus rapid transit (BRT) routes will differ from local routes by distance between stops, frequency of stops, headway times and possibly vehicle design. They did clarify that any “park and ride” facilities would be located much farther out on the selected routes.

The Smart Moves staff and consultants summarized their work to date (the project is not yet completed). They stated that the Shawnee Mission Parkway route was definitely fixed, but that they expected local bus traffic to remain on Johnson Drive or possibly even move to Martway. They supported narrowing Johnson Drive to a more pedestrian-friendly width, which is one of the primary tenants of transit-friendly design. They reviewed the results of a workshop they had conducted with local residents, business owners and elected officials in June of 2005.

As a result of this workshop and their planning efforts, the Smart Moves staff and consultants recommended that their first choice for a transit stop would be the current Wendy’s site, because it was close to the new mall development and within easy walking distance to other destinations in a mixed use neighborhood. The other potential site listed in their report is the Mission Mall site, on the Roe Avenue side, with underground access for residents in Fairway on the east side of Roe. The Smart Moves staff was critical of the current Lamar site due to a lack of connected sidewalks and waiting space, and pedestrian safety issues.

Meeting attendees discussed the current status of the proposed Mission Mall Redevelopment Plan and the Mission Redevelopment Corridor and the pros and cons and realities of the potential sites.

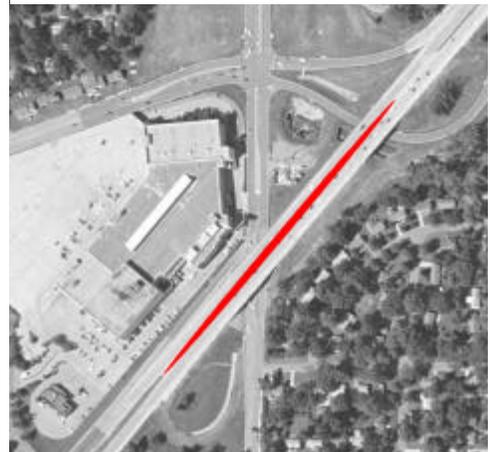
After continued review of site requirements, the Rock Creek Relocation Project and the specific design of the new mall site, it became apparent that a compromise would be required. The Planning Team supports the future transit plans and strongly recommends that the future transit ideas be incorporated into the Redevelopment Plan now. However, the Mission Mall site developer is not interested in incorporating the transit mall into his site. The Shawnee Mission Parkway site is difficult because either a bridge over Shawnee Mission Parkway or a tunnel under it would require approval from the Kansas Department of Transportation and would be a very expensive option. It may be possible, if the BRT



Before selecting the Martway park site for the transit center, numerous potential sites were examined and found unfavorable; including the current Wendy’s Site...

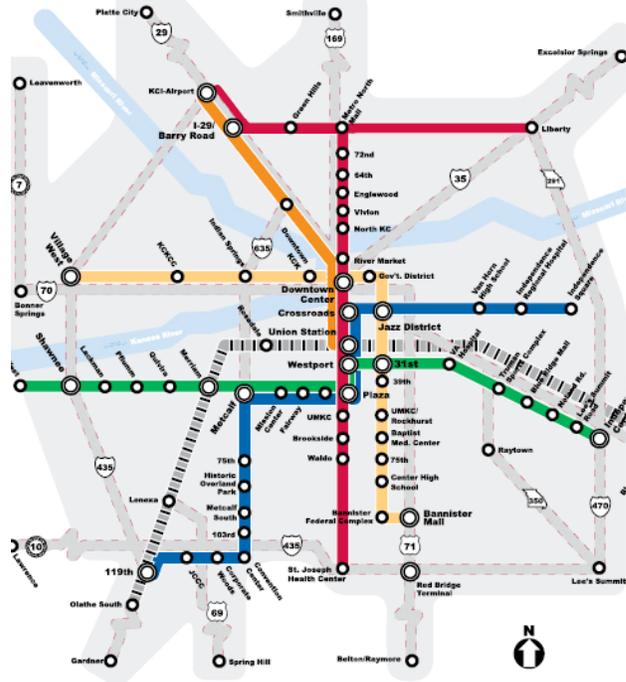


The east side of the Mission Mall site, and...

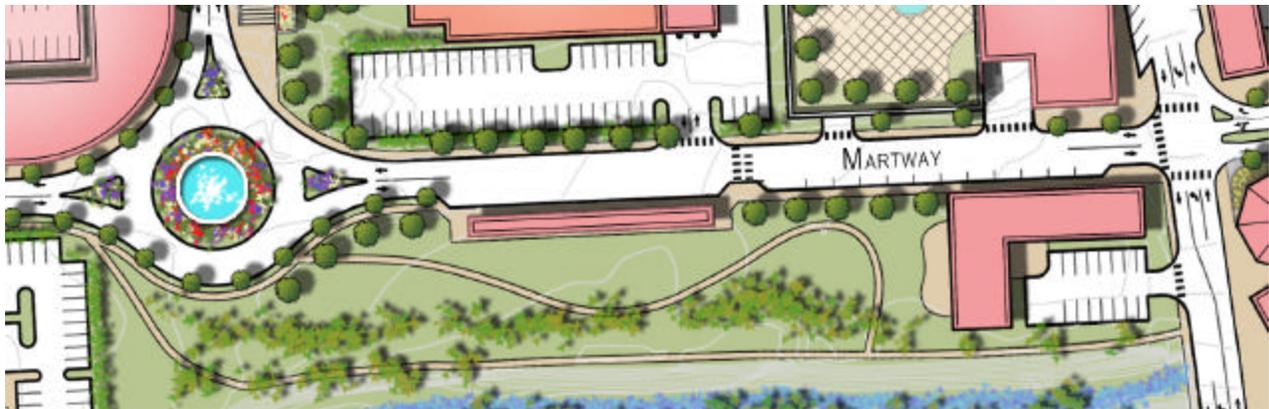


A transit center in the median of Shawnee Mission Parkway

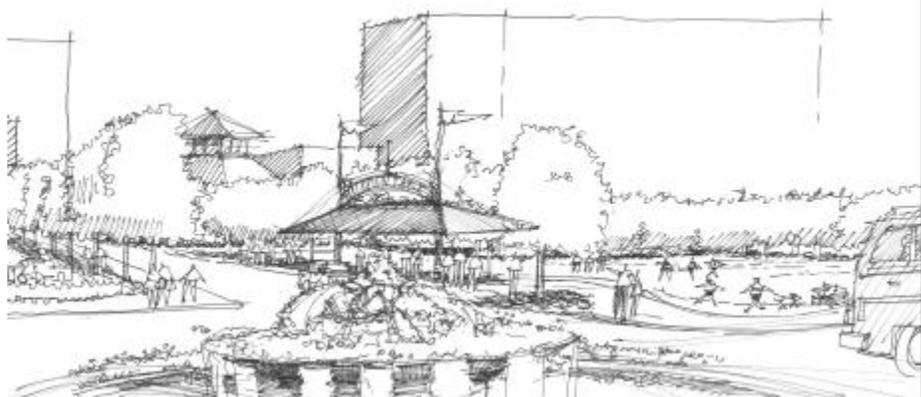
System is coordinated with KDOT along this route, that a median option could work, but would affect the entire line. The Public Works staff of Mission and their consultants on the Rock Creek project maintain that the Wendy’s site may not be feasible for construction, and after reviewing the design requirements, the Planning Team found the transit Center will not fit there. The recommended site is a linear transit stop along Martway, adjacent to the proposed park, with significant screening and design features to make it visible and safe. This option can be added easily when the bus rapid transit project is ready to operate, is within easy walking distance to everything in the corridor, and is a short distance off Shawnee Mission Parkway for transit vehicles to turn on Roeland Drive, Martway and turn around at the traffic circle, without creating a back-up on the narrowed Johnson Drive. After a nearby resident expressed concern about the noise and odor, the team again examined the options available, including a stop at the intersection of Johnson Drive and Shawnee Mission Parkway. Any stop on Shawnee Mission Parkway itself presents numerous pedestrian safety issues, problems with enough room for multiple transit vehicles, safe roadway conditions and limited turning movements. If and when the rapid transit system is constructed, Mission should be prepared with an easily assembled and designed site in an appropriate place. The Planning Team believes the best site is the linear Martway site.



This diagram shows the proposed BRT routes, the green and blue routes will pass through Mission and utilize the proposed transit center.



The proposed transit center in the East Gateway Redevelopment plan is located near the “The Gateway” redevelopment. It is also advantageously located between Johnson Drive, Shawnee Mission Parkway, Roe, and Nall – lending itself to transfers between local and rapid bus lines. The planned traffic circle will simplify bus circulation, and the adjacent park provides an opportunity to screen the transit center from nearby residential neighborhoods.



Traffic Engineering Report for the East Gateway Redevelopment Plan

Prepared by: Tom Swensen, P.E. TransSystems Corporation

The street and highway system in the East Gateway study area combines the characteristics of the old urban grid network with suburban thoroughfares. The basic infrastructure has not changed dramatically in the past 50 years but the function of some streets within the immediate area and the region has changed over time.

The major streets in the redevelopment area include Shawnee Mission Parkway and Roe Avenue. Shawnee Mission Parkway, first developed as a state highway, is a significant east/west route in northeast Johnson County as it links suburban communities with I-35 and the Country Club Plaza and points beyond in Kansas City, Missouri. It carries the highest traffic volumes of any street in the area - about 30,000 vehicles per day. With its long and storied history, traffic volumes have tended to increase and decrease as development patterns shift and street/highway improvements were implemented. The current traffic volumes on Shawnee Mission Parkway are 10 to 20 percent less than the volumes counted in 1990.



Shawnee Mission Parkway through Fairway

Roe Avenue, a former state highway, links communities in this area with I-35 and points beyond in Kansas City, Kansas. Forty years ago this was the principal connection between Johnson and Wyandotte Counties as the Roe Avenue extension across I-35 became the 18th Street Expressway, a part of the original Kansas Turnpike. Roe Avenue still serves as an important arterial street and carries about 20,000 vehicles per day.

Johnson Drive is the primary east/west street through the commercial areas of Mission. Beginning in the far western reaches of Johnson County, Johnson Drive effectively terminates immediately east of Roe Avenue where it connects with Shawnee Mission Parkway. In fact, Johnson Drive was also developed as a state highway originally, preceding the construction of Shawnee Mission Parkway. Recent studies have shown that most drivers using Johnson Drive either begin or end their trips in or near Mission; there is very little traffic using this street to merely pass through.

Other important streets in the study area include Nall Avenue, Roeland Drive, Martway and 55th Street. These streets tend to serve as links to and between other streets carrying traffic into and out of, or circulating within, the study area.

Redevelopment rarely brings about the need for wholesale changes in the transportation infrastructure. The East Gateway Redevelopment Area does have the advantage of significant infrastructure in place and the evaluation of existing traffic conditions indicates that these streets currently offer residents, workers and shoppers very high levels of traffic service. Further, the mixed-use nature of the proposed redevelopment tends to lessen the impact of new uses to an area as people are afforded opportunities that do not necessitate separate vehicle trips. For example, residents will be within walking distance of retail and employment facilities and workers can walk to lunch or even have transit options for commuting.

The best transportation strategy for the East Gateway redevelopment includes the preservation of connections to surrounding major streets like Shawnee Mission Parkway and Roe Avenue and to enhance the ability to circulate in and around the area on streets like Johnson Drive and Martway.

The most notable change to the street system is the conversion of Johnson Drive from a four-lane street to a three-lane street. Studies of existing conditions indicate that Johnson Drive would continue to function at high levels of service if the street was striped for three lanes today.



Martway as it exists today

In the urban venue contemplated with the redevelopment plan, a three-lane street is often superior to a four-lane street in terms of vehicular capacity and safety. The three-lane cross section is also easier and safer for pedestrians. Martway, also recommended to remain a three-lane street, provides significant untapped capacity for east/west travel within the City of Mission. With more and better connections between Johnson Drive and Martway, drivers will be afforded more travel path choices. This network helps avoid high concentrations of traffic at any given intersection and lessens the potential for more traffic signals.

The largest redevelopment component in this area is The Gateway, a proposed mixed-use development to replace the Mission Mall. Traffic studies for that project indicate that improvement needs are concentrated at the Shawnee Mission Parkway intersection with Roeland Drive, one of those critical connections to the major street system. Johnson Drive, Martway and Roeland Drive are all expected to perform well even with the additional traffic this project will generate.

Perhaps the only significant long-range street improvement that should be considered for this area is the expansion of Shawnee Mission Parkway from four lanes to six lanes. This street is an important connection between I-35 and Kansas City, Missouri (and points in between). The strain of this heavy commuter traffic is witnessed at intersections like Lamar, Nall and Roeland Drive. Now that Merriam has improved its section from I-35 eastward through the intersection with Antioch Road, the value to continuing that six-lane cross section to the east makes sense. It is also likely that Shawnee Mission Parkway will never be widened east of Roe Avenue. The proximity of houses and buildings to the street makes such a proposition impractical.

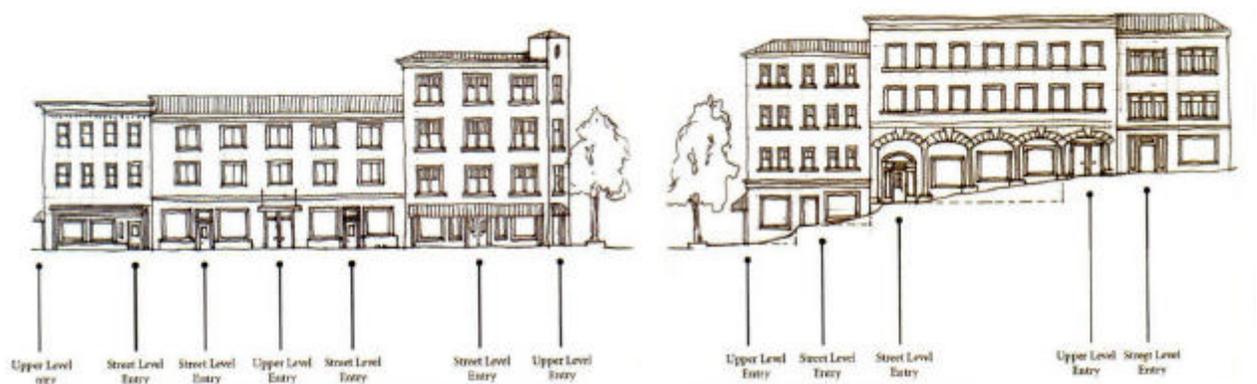


The nature of the redevelopment will bring about considerably more pedestrian activity. It is therefore important to plan for and construct sidewalk along both sides of all streets unless circumstances dictate otherwise. Crosswalks need to be carefully planned for good visibility between pedestrians and motorists as well as clear assignment of responsibility.

In summary, the East Gateway Redevelopment Area is fortunate to have significant infrastructure in place that can accommodate the vision identified in this study. The backbone system is in place; future transportation considerations include the proper positioning of driveways, modest intersection improvements to maintain a high level of service and circulation, and pedestrian facilities throughout the area.

FORM-BASED CODES

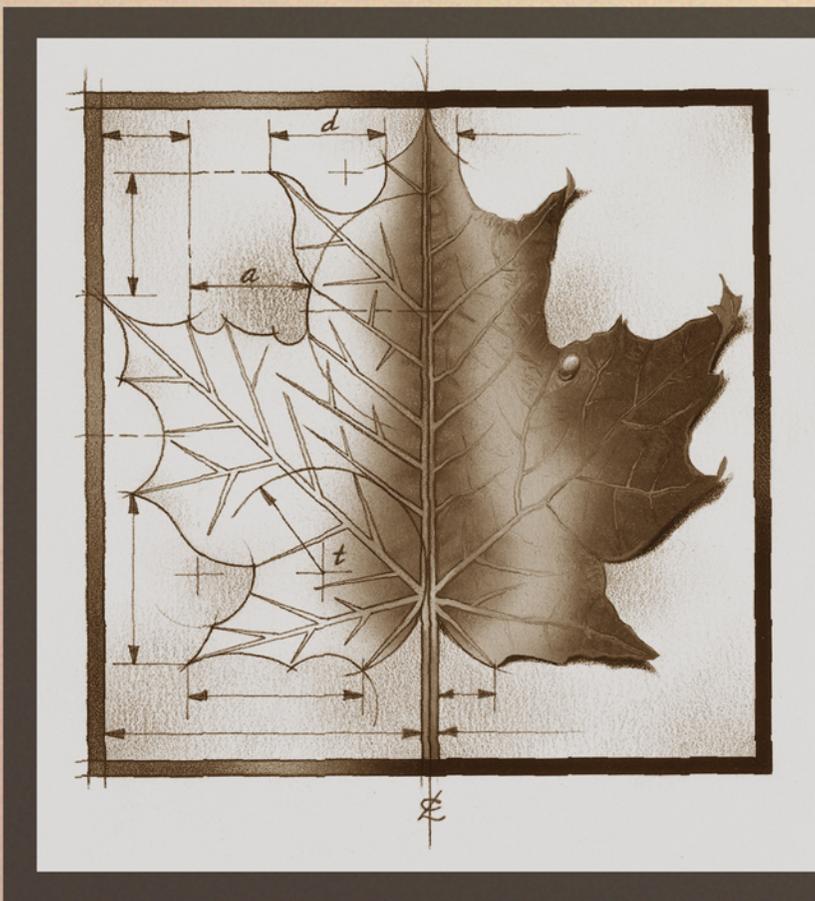
- Form-based codes are a regulatory approach designed to shape the physical form of development while setting only broad parameters for use.
- Rather than regulating use by zoning districts, form-based codes regulate development by building type, street type, location (character area), and façade features. They stress the appearance of the streetscape, or public realm.
- They support mixed-use neighborhoods with a range of housing types by focusing more on the size, form, and placement of buildings and parking, and less on land use (residential vs. commercial) and density (housing units per acre).
- Regulations are established by building type rather than zoning district in form-based codes. This provides the freedom to create one set of rules for one building type and another set for a different type.
- There are still regulations, such as minimum and maximum heights of buildings, but overall, there is more flexibility for a land owner, developer, or building owner to meet changing real estate markets by having the flexibility to build single-family homes, apartments, offices or retail based on market demand, as long as the building form conforms to the community's vision as expressed in the form-based codes.



This image shows how form based codes use diagrams to show requirements on different building elements, in this case locations of entries. Image courtesy of APA parks and planner volume 3 #3 2005

APPENDIX D TO THE
EAST GATEWAY REDEVELOPMENT PLAN

AMENDED
MISSION, KANSAS
DESIGN GUIDELINES
FOR THE
JOHNSON DRIVE CORRIDOR



Prepared for the City of
Mission, Kansas by
Bowman Bowman Novick,
Inc.

AMENDED JUNE, 2006

TABLE OF CONTENTS

AMENDMENTS AND ADDITIONS TO THE DESIGN GUIDELINES

glossary of terms	0 - 1
building orientation & siting	3 - 1
parking	3 - 2
site access	3 - 3
screening	3 - 4
landscaping	3 - 5
walls & fences	3 - 6
facades	4 - 1
proportion & scale	4 - 3
materials	4 - 5
roofs	4 - 7
display windows	4 - 8
entrances	4 - 10
awnings and canopies	4 - 11
signs	4 - 13
lighting	4 - 15
parking structures	4 - 16
Residential Design Guidelines	6 - 1

Brick

a solid or hollow masonry unit of clay or shale, molded into a rectangular shape while plastic, and then fired in a kiln.

Build-to line

A line established on a building site that defines where the street façade, or a mandated percentage of the street façade must rest.

Colored & Textured Concrete Masonry Units

Various styles of concrete masonry units that utilize different finishes (e.g. split face, ground face, glazed) to provide a more aesthetically pleasing

Condominium

A real estate ownership in which there is individual ownership of a single unit in a multiple unit building and an interest in common property jointly owned by all unit owners. In most cases, an association operates the complete and common property.

EIFS

Exterior Insulation and Finish System, exterior wall cladding system consisting primarily of polystyrene foam board with a textured acrylic finish that resembles plaster or stucco.

Facade Area

The entire area of a particular facade excluding the area of window and door openings.

Fenestration

The arrangement and design of windows in a building.

Patio Home

A detached single family home built on a very small lot.

Predominant Exterior Materials

The major type of exterior wall construction material used in a building.

Smooth Face Concrete Masonry Unit

Also called grey block, this type of material is typically used in industrial applications, or as a structural substrate for brick, stone, or other more aesthetically pleasing materials.

Stone cultured

A man-made stone look-alike product that is adhered to the facade of a building.

Stone natural

Rock that has been selected or processed by shaping, cutting, or sizing and which is laid up in units and anchored to the building foundation.

Stucco Plaster

A cementitious exterior finish material, usually textured, composed of portland cement, lime, sand and water and applied over a masonry or wire mesh substrate. Often confused with EIFS.

Townhouse

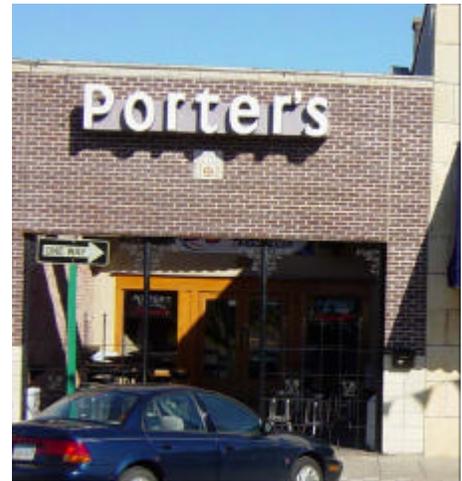
One row of houses connected by common side-walls. Rowhouses also fit in this category

Zero lot line

The location of a building on a lot in such a manner that one or more of the building's sides rests directly on a lot line.

Appropriately sited buildings can greatly enhance the formation of the public streetscape. To the greatest extent possible, buildings should be sited to provide high quality, functional and livable outdoor public spaces that enhance the use of the building, the street frontage, and the surrounding neighborhoods.

- All buildings must be oriented parallel or perpendicular to public streets within the entire Johnson Drive Corridor.
- New construction and infill buildings shall be oriented to primary streets and must be built to the “build-to” line established in the new redevelopment plans for the Downtown, West Gateway and East Gateway Districts. Buildings must extend the entire width of the property along the primary street, and abut any existing adjacent building on either side. A uniform alignment of facades along the sidewalk edge is the key objective. Setbacks over a portion of the build-to line are acceptable if the resultant area contributes positively to the pedestrian environment in the way of public plazas, areas for outdoor dining, well-defined display areas for retail merchandise or public art. Grade level retail or commercial uses shall have a minimum 75% of the street frontage built to the build-to line. Where interior courtyards abut the street, they shall be defined by an edge treatment of landscaping or fencing (or a combination thereof) that is consistent with the streetscape theme and in accordance with these Design Guidelines. Setbacks at residential developments may have an additional 5-10 feet for front stoops and stairways that allow the living units to be elevated above grade for privacy.
- Where buildings in the West Gateway District or existing buildings in other Districts, do not extend to property lines, property edges shall be defined by the use of appropriate walls, plazas, or landscaping.
- Building configurations that create awkward relationships with adjacent properties, buildings, and streetscape shall be avoided.



A business can build to the property line and still have a front court, as shown by the Porter's building in this photo.



This building illustrates a consistent façade treatment on all sides of the building.

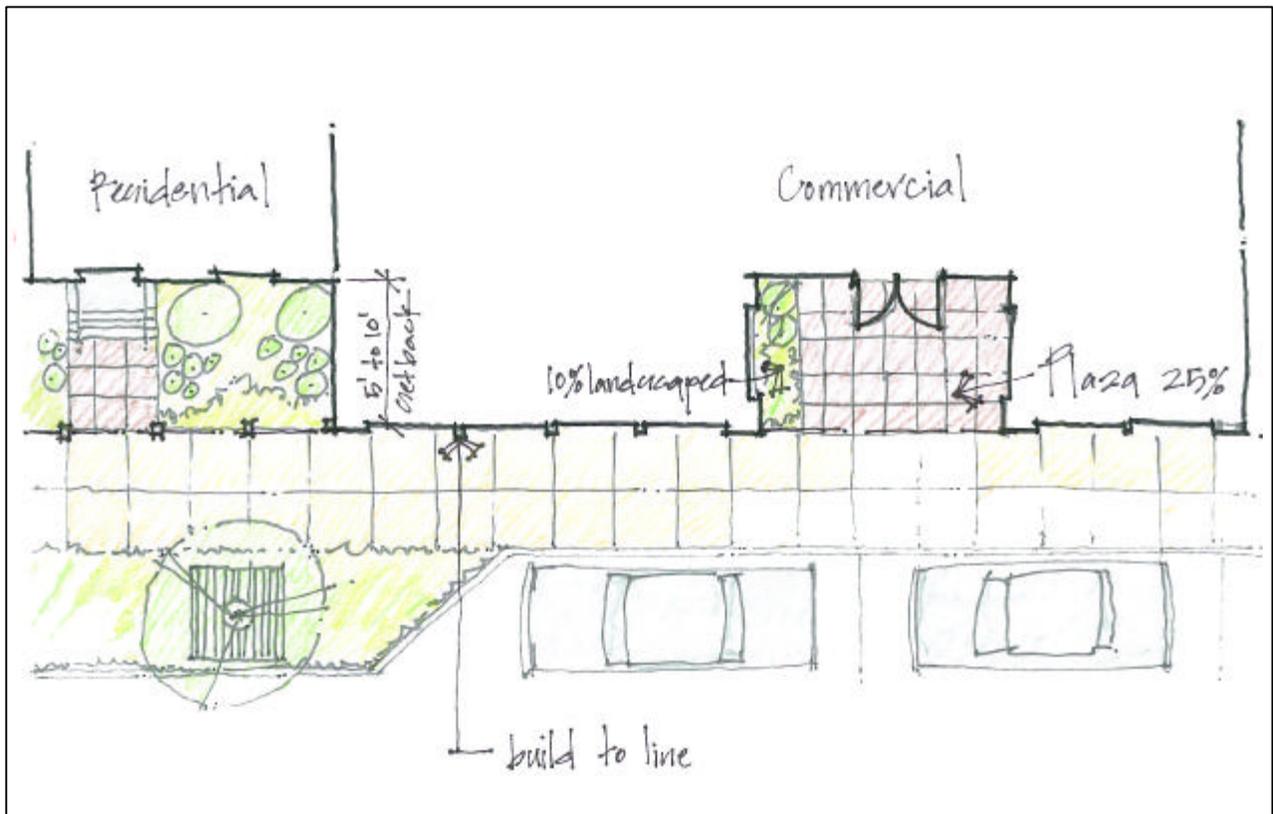


This building effectively addresses both streets of the intersection.

- Buildings fronting multiple streets shall have consistent façade treatments with respect to materials, scale, proportion and detailing. (Refer to Guidelines for Buildings – Section 4).
- Buildings on corner lots shall incorporate a form that addresses both streets of the intersection. (Refer to Guidelines for Buildings – Building Facades, Entrances, for additional requirements.)



The consistent sidewalk to building relationship that results from a build-to line promotes pedestrian activity and will be beneficial to businesses in the corridor

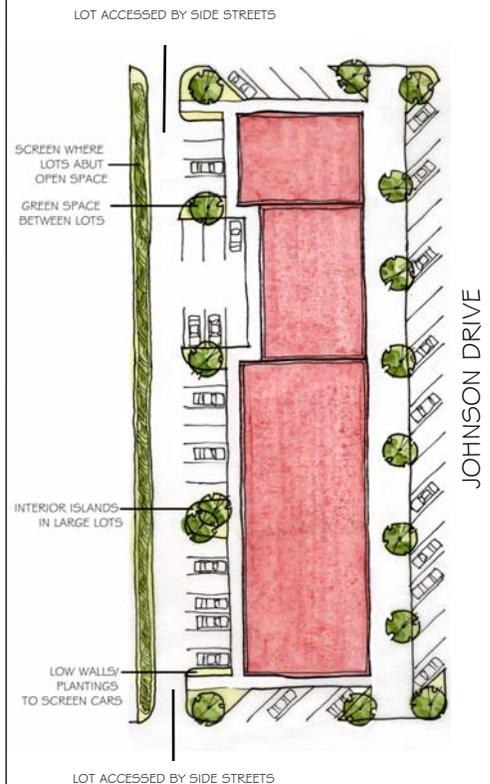


This diagram shows how a site design that maintains a build to line for both residential and commercial properties can provide a consistent edge for pedestrian circulation, while allowing for entry plazas and courtyards.

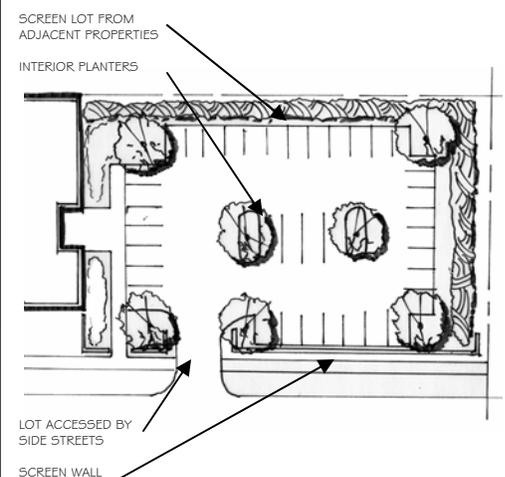
PARKING

Parking quantity should not be the only issue considered in parking area design. Physical organization, distribution, pedestrian links, and screening are issues that must be addressed in parking lots and on-street parking.

- Parking lot islands and “green space” shall be incorporated within all parking lots to provide shade and visual relief from large expanses of pavement. Parking lots shall be provided with a minimum of 6% green space. On-street parking shall have 100 square feet of green space for every 10 parking spaces.
- Existing surface parking lots on Martway and side streets between Martway and Johnson Drive shall be effectively screened from the surrounding street network. Where a parking lot abuts a street, a minimum 9’ wide buffer zone (including sidewalks) shall be required to effectively screen views to parked vehicles. Where a parking lot abuts an interior property line (not a street) a minimum of four feet shall be maintained as a green space. Screening shall be a minimum height of three feet and be of either softscape (vegetation), hardscape (walls or fencing), or a combination of both. (Refer to Guidelines for Site Development – Screening).
- Parking lots and on-street parking shall be designed in such a way to provide a clear, direct path between the parking areas and the business destination. 90 degree stalls are not allowed at on-street parking areas on Johnson Drive.
- Parking lots in the Downtown District shall not front on Johnson Drive, but be located behind the building with access from side streets.
- On-street parallel or angled parking shall be provided on Johnson Drive and Martway where considered feasible by the City’s traffic engineer.



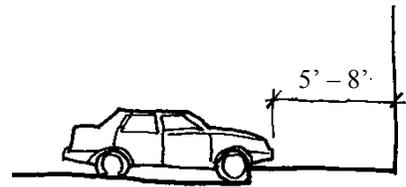
Parking quantity is only one of many issues that should be considered when designing parking lots. Landscaping, screening and access are issues that should be well designed both in small lots (above) and large lots (below).



SITE ACCESS

Convenient, safe, and direct ingress and egress to individual properties is critical to the commercial success of the Johnson Drive Corridor. Site access shall provide for the safe movement of both pedestrian and vehicular traffic.

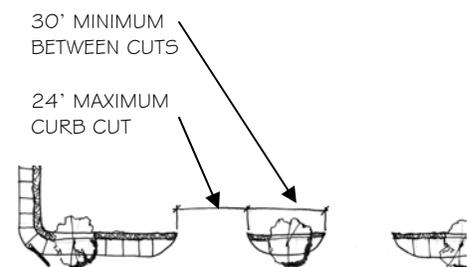
- Primary sidewalks along Johnson Drive shall provide a minimum walking surface of 8 feet in width. Primary sidewalks along Martway shall provide a minimum walking surface of 6 feet in width. Secondary sidewalks; those along the side-streets between Johnson Drive and Martway, shall provide a minimum walking surface of 5 feet in width. When adjacent to on-street parking, primary and secondary walkways shall be increased in width to prevent vehicle overhangs from encroaching into the sidewalk width.
- Walkways must be ADA compliant.
- The incorporation of amenities such as seasonal planters and urns, benches, bike racks, and trash receptacles are encouraged within the walkway system adjacent to individual properties. These amenities shall be coordinated with the City of Mission to ensure compatibility with public streetscape improvements. Amenities shall be placed to provide a continuous clear zone for pedestrians, and be placed to avoid conflict with vehicular sight lines at ingress/egress locations.
- Newspaper boxes shall be grouped together and placed within a screening element to minimize the visual impact along the corridor.
- Curb cuts shall be as established in the redevelopment plans for the Downtown, West Gateway, and East Gateway Districts.
- Existing curb cuts within all Districts shall be minimized to provide a maximum ingress/egress opening width of 24 feet. The minimum distance between curb cuts shall be 30 feet.
- Service and delivery access areas shall be separated from on-street parking areas and sidewalks. Service and delivery access areas shall not be located along Johnson Drive. Service and delivery access areas along Martway and north/south streets between Johnson Drive and Martway shall be effectively screened from the adjacent roadway by the use of landscaping, fencing, or a combination of both.



Vehicle overhangs cannot encroach on minimum sidewalk widths



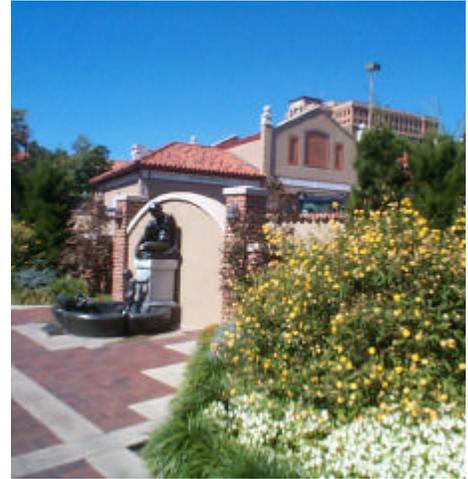
Benches and other amenities are encouraged within the walkway system



SCREENING

Minimizing or eliminating the views to undesirable areas will improve the overall visual quality of the Johnson Drive Corridor. Appropriate areas to be screened include parking lots, delivery areas, loading docks, dumpsters, ground mounted mechanical equipment, utility service connections, ice and vending machines, freezers and coolers, and transformers.

- Where service areas and utility banks are located adjacent to residential property, a minimum eight feet of green space with appropriate screening shall be maintained.
- Screening shall be required between existing Neighborhood Preservation Zones and the adjacent Mixed Use and Commercial properties within the East Gateway District. Screening shall consist of a 4ft. wide zone of landscaping, fencing or walls, or a combination of both that effectively buffers a zone of at least 6ft. in height. (Refer to Landscaping, Walls and Fences for additional requirements.)
- Screening can be accomplished by the incorporation of soft-scape (plant materials) or hard-scape (fences and walls).
- Plant materials are encouraged for screening uses. The City has an approved Plant Materials Pallet available for property owners within the Johnson Drive Corridor. (Refer to Appendix)
- Architectural elements such as fences and walls shall be of solid construction to prevent visibility of the area to be screened. Fences and walls shall be located and sized to adequately conceal the area in question, and shall be no less than six feet in height.



This wall surrounding a parking lot shows how a combination of softscape and hardscape can attractively screen undesirable elements.



Mechanical and utility service areas such as this shall be screened from public view.

SCREENING

- Fences and walls shall be designed to reflect and/or complement the architectural style of the adjacent building and shall incorporate similar materials to that of the adjacent building. (Refer to Guidelines for Buildings Section 4 Building Materials and Building Facades.)
- Screening elements shall be maintained in good repair. Painted surfaces shall be free of peeling and faded paint. Landscape screening shall be trimmed as required to provide a quality appearance. Regular maintenance of plant materials such as fertilization and watering is also required in order to maintain an effective level of screening.
- Refer to Guidelines for Site Development – Landscaping, and Walls and Fences for additional requirements.



A combination of berms, shrubs, and trees can effectively screen undesirable views or differing land-uses.



A surface parking lot screened from view

LANDSCAPING

Effective and attractive landscaping can greatly enhance property values and contribute to the pedestrian experience throughout the Johnson Drive Corridor. Landscaping is also a recommended material for use as a screening element as described in the Guidelines for Site Development – Screening.

- Similar to the amenities discussed in Site Access, property owners are encouraged to provide landscaping on private property that is compatible with that on public property to ensure a consistent appearance along the corridor. A plant material pallet has been developed by the City to provide guidance on plant material selection. (Refer to Appendix)
- Shade along the corridor is a priority. The redevelopment plans for the Downtown, West Gateway and East Gateway include streetscape improvements designed to enhance the pedestrian environment. Street tree plantings and planters located within the right-of-way and in roadway medians are planned to incorporate plant materials providing year round color, texture, and interest.

Other tree, shrub, groundcover and flower beds are encouraged on private property to further enhance the Johnson Drive Corridor. Courtyard and patio spaces on private property shall be planted with a mix of shade/ornamental trees and shrub plantings equal to a minimum of 10% of the area. Plant materials shall conform to the Design Guidelines recommendations (Refer to Mission Plant Palette).

- Automatic irrigation systems are recommended for landscaped areas on private property, and within the right-of-way for tree wells and planters within the public streetscape. When an area is too small to warrant an automatic irrigation system, the incorporation of “quick couplers” are encouraged to provide the water service necessary for manual watering.
- Refer to Guidelines for Site Development – Parking, and Screening, for additional landscape requirements.



Property owners are encouraged to provide landscaping elements.



Property owners should landscape with materials compatible to that of public property.



Landscaping can create pleasant courtyards in an urban setting.

WALLS AND FENCES

Material and design details for walls and fences within the corridor should be designed to complement the architectural character of the Johnson Drive Corridor. Both quality design and materials are paramount to this initiative. Walls and fences are also approved methods for screening (refer to Guidelines for Site Development – Screening).

- Similar to screen walls and fences, the materials and style of non-screening walls and fences shall match or complement those of adjacent buildings.
- Chain link, wood, stockade, or corrugated metal fencing is not appropriate in the Corridor and is prohibited.
- Walls should be designed to provide architectural interest. This can be accomplished by incorporating slight changes in elevation of fence or wall panels, or by varying setbacks to create undulating surfaces that break up long stretches of the wall or fence. Variation of panel design can also serve to create visual interest in long stretches of walls or fences.
- Walls exceeding 25 feet in length shall incorporate plant materials for added visual interest.
- Convenient pedestrian openings shall be coordinated with the surrounding sidewalk system and parking lot and building entrances. Effective sight lines shall be maintained in pedestrian openings of walls and fences to avoid safety hazards.



Walls should compliment the architecture of the building and be built of solid materials.



Convenient pedestrian openings should be incorporated into walls.



Variation of panel design can create visual interest in long stretches of walls.

BUILDING FACADES

Façade Treatments

Forms and elevations of new buildings should be detailed and articulated to create interesting facades. All elevations of the building will be evaluated in the Design Review.

- Provide consistent, sympathetic treatment of all exterior facades within the District in regards to color, materials, architectural form, and detailing. A healthy mix of complementary building materials is encouraged, but not to be overdone. Individual façade elements should respect the scale of immediately adjacent building elements. Construction infill should incorporate some of the detailing present in the surrounding existing buildings. Design control devices may include, but are not limited to, façade materials, vertical and horizontal datum lines, and window size and shapes.
- Vary setbacks on wall surfaces to form entrances, express structural elements, or to create special exterior areas such as planters, seating, etc.
- Building elements that create strong patterns of shade and shadow are encouraged.
- Visible rear and side facades should provide consistent visual interest by incorporating characteristics similar to the front façade.
- Stylized, nostalgic, or thematic architecture which is characteristic of a particular trend, historic period, corporate or franchise style is prohibited in the Downtown and East Gateway Districts, particularly when the intent is to use the building as advertising.
- A concentration of building details at ground or sidewalk level is an appropriate way to create visual interest and enhance the pedestrian environment. It also helps to promote walk-in commercial activity. This is particularly relevant within the Downtown and East Gateway Districts.



Mission benefits from a variety of architectural styles. Southwestern architecture,



Art deco architecture,



And modern architecture all exist in the area.

BUILDING FACADES

- Diversity of architectural design shall be encouraged within the Corridor, especially that which includes local character and materials.
- Building facades should be well maintained. When restoring an existing façade, avoid sandblasting or using harsh chemicals to clean masonry.
- Covering, altering, or damaging historically significant architectural details and façade elements is prohibited.
- Building elements and materials that contribute to the original character of the building should not be removed.
- Historic unpainted masonry should not be painted. Painting a portion of a multi – tenant building is prohibited.



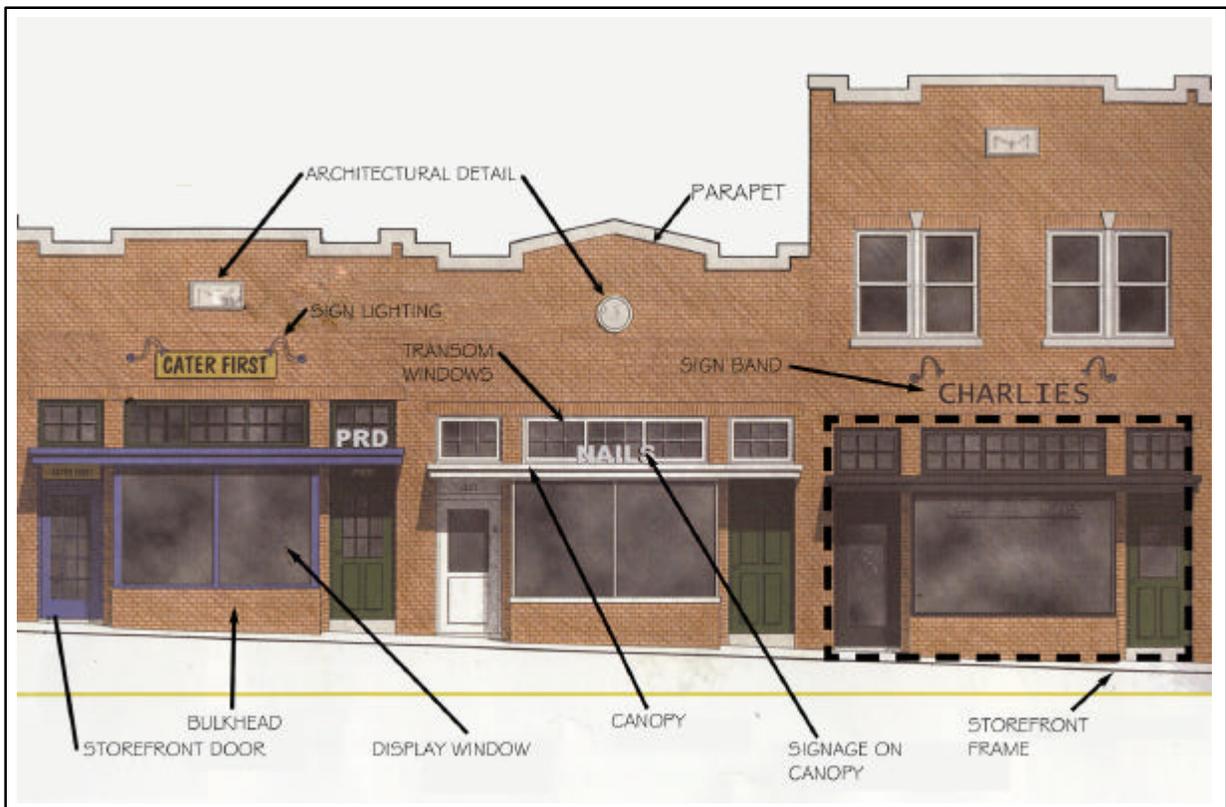
Historic photo of the Johnson County Courthouse building. Note the Mission style architectural detailing.



The same building today with signage and awnings covering all original detailing.



Individual storefronts on a multiple tenant building shall not be treated as a separate building, finish and materials shall be consistent.



The anatomy of a storefront facade

BUILDING PROPORTION AND SCALE

The scale of the shopping District in Downtown Mission, Kansas is similar in scale to others in nearby communities which serve as good models of appropriate scale and proportion. The shops in Prairie Village, Fairway, Brookside, and the Plaza have quality building stock with complementary massing, proportion, and human scale elements.

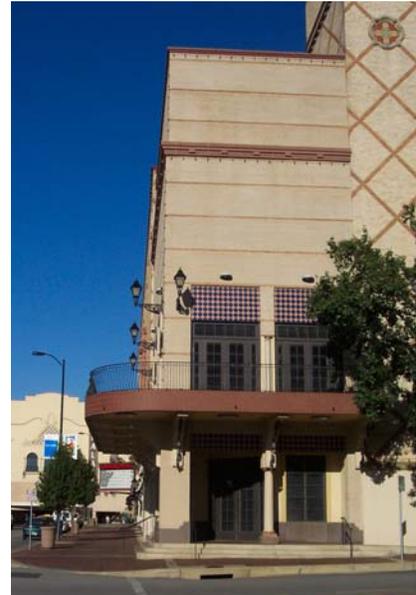
- Buildings should be designed to be compatible in scale and proportion to buildings within their immediate context on Johnson Drive. Designs should incorporate architectural elements that relate to the human scale.
- “The upper stories of buildings in the Downtown District beyond the 2nd story shall incorporate a minimum 8’ step back from the front façade of lower stories.” Stories beyond the second story in the East Gateway shall be articulated by the use of setbacks (18” min), a change of materials, or other similar treatments that enhance the proportion and scale of the overall façade. See Zoning Ordinance for zoning regulations.
- Lower levels of buildings should be differentiated architecturally from upper levels.
- “Reiterating masonry coursing of nearby buildings is one method of achieving consistency in façade proportions.”
- Incorporate elements that give the building perceptible scale. Large buildings in particular should be designed to reduce their perceived height by dividing the building mass into smaller scale components.
- Rhythm and repetition of building elements is strongly encouraged. This is accomplished through incorporation

What is proportion?

Proportion is defined as the relation between respective parts or between parts and the whole, in a building or any work of art, whether considered purely visual or numerically.

What is scale?

When defining the concept of relative size, the term scale is used. Scale relates to the relationship of one element to another or of one element to the whole; also, the proportional relation that a representation of an object or area bears to the object or area itself.



Elements such as this second floor patio humanize the building scale for pedestrians.

BUILDING PROPORTION AND SCALE

of regular or patterned units that are organized to provide a continuance, flow or sense of movement.

- Organize building elements and the spaces between them in a logical sequential manner.
- Spaces between building masses (i.e. alleys, recessed entries, courtyards) should be scaled and proportioned to maximize the comfort of users. These public areas should not be dark or secluded spaces. They should incorporate detailing that adds interest, orientation, and spatial definition.
- Use columns, fenestration, doorways, roof elements, wall patterns, light fixtures, signs, paving patterns and landscape to create rhythm.



Awnings, landscaping and paving patterns create a pleasing rhythm in this retail courtyard.



Spaces between buildings, such as this alley, should be scaled, proportioned and detailed to maximize user comfort.



Buildings that incorporate a sense of rhythm in the organization of materials and details help to make the streetscape an enjoyable place for pedestrians.

BUILDING MATERIALS

Designers, property owners, and developers are encouraged to creatively blend new construction with existing in ways consistent with a singular district. The selection of quality building materials is paramount to achieving this goal.

- Buildings should be built of high quality, sustainable long-term components. Non-durable materials such as thin layer synthetic stucco (EIFS) are generally discouraged and shall not be used within 8 feet of ground level unless specially reinforced and located away from pedestrian accessible areas. EIFS and Stucco plaster shall not be used for more than 25% of the façade area of any one story.
- Buildings should be constructed to be as maintenance free as possible. Exterior materials should not be considered temporary surfaces to be replaced during the life of the building.
- Materials should not artificially simulate other materials. If brick is proposed, it shall be real brick – not Z brick or other such imitation material.
- Predominant exterior building materials should include hard surfaced exterior wall materials such as:
 - stone,
 - stucco plaster – shall be limited to areas 4 feet or more above ground level.
 - brick,
 - ceramic tile,
 - colored and textured concrete masonry units
- Predominant exterior building materials shall not include the following:



Bulkheads should be built out of sturdy, long lasting materials.



Quality materials such as brick and masonry not only create a long lasting structure, but allow for attractive detailing.

BUILDING MATERIALS

- Smooth face concrete block
 - Tilt-up and pre-cast concrete panels
 - T-1-11 and other wood shingle, and composite sheet sidings
 - EIFS
- Clear glass is preferred; glass that is highly tinted or tinted in unnatural colors or with a reflective finish is not permitted.
 - Color and texture for architectural finishes should be selected to provide visual unity. Colors that offer low reflectance in subtle neutral or natural tones are preferred over the use of high intensity, metallic, fluorescent, or black. Brighter colors may be featured at trim and accent areas. Material and/or color changes should occur at a change of plane in building elements. However, material or color changes at outside corners of a building that give an impression of an artificial façade are discouraged.
 - Predominant colors for building facades in Mission should match or complement the natural yellow, pale tan, brick, beige, and brown and terra cotta tones existing throughout the Corridor.
 - Designers should choose accent colors that complement the predominant color of the building façade, but the combination of body coat and accent colors should never exceed a total of four colors on the building façade.



Clear glass, masonry, and metal trim are some of the recommended materials for the Corridor



The body coat of a building should be chosen to match or complement the existing earth tones predominant in the Corridor (yellow, pale tan, beige and brick tones).

BUILDING ROOFS

Roofs should not only be designed to protect a business from the elements, but also to reinforce the lines, scale, and style of its neighboring buildings.

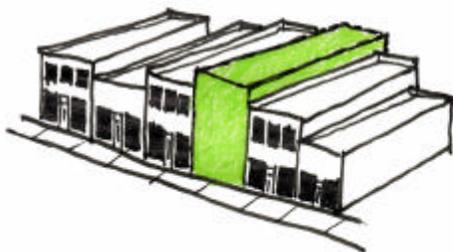
- Rooflines in the Downtown District should reinforce the horizontal or linear aspect of the Downtown streetscape. Roofs should be simple geometric forms consistent with the area.
- Flat or low slope roofs (less than a 4:12 slope) shall be hidden by a parapet on all facades facing major streets.
- There shall be no exposed scuppers, gutters or downspouts on the facades facing major streets. Roof drainage shall be accomplished by the use of internal roof drains or by sloping roofs to the rear of the building.
- Natural roof materials such as wood, slate, and clay tiles shall be maintained in their original finish.
- False roofs, including mansard roofs, are prohibited.
- Exposed gable or hip roofs are prohibited.
- Roof mounted mechanical equipment shall be hidden from pedestrian view by roof parapet walls.
- Roofing of infill development should not introduce a new roof form to the area.

“The Universal, Eternal, Inviolable 11th Commandment: Thou shalt not allow one more mansard roof in San Jose.”

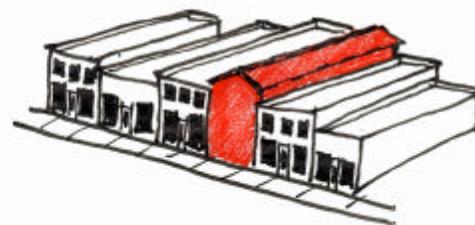
An excerpt from *16 Rules for Developers*, in *Rebuilding Downtown San Jose: A Redevelopment Success Story* by Jay Claiborne



A false roof in Mission.



Roofing of infill development should fit in with the context of downtown



And it should not introduce a new roof form to the area

DISPLAY WINDOWS

The goal of every competitive business is to attract customers and generate increased sales. Storefront display windows in the Downtown and East Gateway District offer the greatest marketing opportunity to attract the attention of passers-by and to generate new sales.

- First floor buildings fronts should incorporate a large expanse of glass; at least 75% of the facade. This is required in the East Gateway on Johnson Drive and on Martway and is encouraged on side streets between Roe and Nall.
- Storefront display systems should retain the simplicity of the design characteristics of the District. Historic features such as decorative moldings should be preserved, and new windows should match the originals.
- When restoring an existing building, tinted or textured glass should be replaced with transparent glass to increase visibility. Consider “low E” glazing to reduce negative effects of ultra violet rays. New storefront display windows should be of transparent glass. Highly tinted, colored or reflective glass is prohibited.
- Glass block shall not be substituted for storefront display windows.
- Simple vertical framing is encouraged. Avoid horizontal window division except to divide storefront display windows from transom windows.
- Elevate display windows above sidewalks approximately 18-24 inches. Bulkheads should be constructed out of sturdy, easy to maintain materials such as stone, brick, tile and decorative block.
- Businesses are encouraged to provide tasteful and frequently updated window displays that can market a variety of goods and service a business offers, and give the Corridor character and interest.
- Use aluminum or wood frame construction that provides adequate insulation and prevents condensation.



Storefronts should have interesting and regularly updated displays.



Storefronts should provide a clear view of the shop's merchandise.



Elevate display windows above sidewalks 18 to 24 inches.

BUILDING ENTRANCES

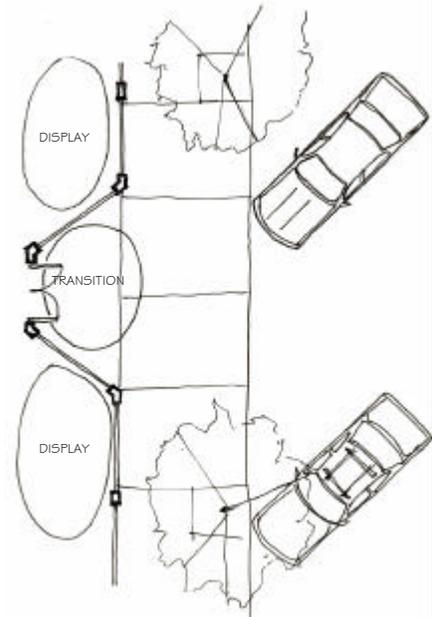
Entrances

Entrances should be designed to allow individual businesses to present a clear defining image without compromising the sense of unity of the whole building block or façade.

- Entrances should be accentuated and oriented in a way that engages the primary public street.
- Entrances should be easily identifiable.
- Entrances to individual stores or users should be articulated.
- Large buildings that front multiple streets should provide multiple entrances. Likewise, buildings that front on-site parking areas should provide entrances convenient to them.
- An appropriate use of civic art and artistic crafting of building materials can help distinguish building entrances.
- A recessed entry provides a pleasant transition from the sidewalk to the interior of a commercial building. It helps ease the change from the public street to the more private interior. Recessed entrances also provide a safe place for entry doors to open without extending into the public right-of-way.
- Buildings with recessed entrances should have the door positioned parallel to the street (except at corner entries). The sidewalls of the recessed entry should continue the storefront display.
- Commercial doors act as part of the storefront display area, and should continue the same general principles of the display window and bulkheads.
- Front doors are typically tall and stately, and incorporate large areas of glass that are elevated above sidewalk level.



Corner entrances are an effective way of engaging multiple streets.



Recessed entries provide a safe, convenient place for entering and exiting a business.



Sidewalls of a recessed entry should continue the storefront display

BUILDING ENTRANCES

- Solid doors with no glass are prohibited. Similarly, residential style doors are prohibited on all commercial applications.
- Heavily ornamented doors (leaded glass, fancy window shape, etc.) should be avoided. Rather, subtle decorations such as metal kickplates, decorative hardware, or professionally painted signs are encouraged to make the front door special.
- Secondary doors (such as those leading to 2nd floor spaces) should have smaller windows, and be placed less prominently to the main commercial entrance.
- Buildings with rear parking lots should incorporate rear doors with the same design principles as front entrance doors.
- Sealing or closing-off existing entry doors, blocking them with merchandise or any obstruction, or covering glass door panels with an opaque material is prohibited.
- The addition of new doors that are not required by code and which are inconsistent with the existing storefront are prohibited.



Subtle decorations such as metal kickplates, and decorative hardware are encouraged.



Secondary entrances and signage should not be as prominent as primary entrances.

Residential style doors with intricate patterns are prohibited

Storefront doors should be tall and stately, and incorporate large areas of glass

BUILDING AWNINGS AND CANOPIES

Metal canopies and fabric awnings serve to provide a pleasant sidewalk space for shoppers, add character and interest, and may serve as backdrops for signage and graphics. Awnings are a desirable feature in the Corridor (especially on south facing facades), but inappropriately designed, they can significantly reduce the appeal and diversity of the Corridor streetscape.

- Awnings and canopies should fit the opening they are intended to cover.
- Awnings and canopies shall not extend across multiple openings or buildings.
- Awnings and canopies should be designed to create visual interest and diversity for individual businesses.
- Awnings and canopies for a single business should be consistent in color, pattern, and mounting.
- Canopies across the front of one building with multiple businesses should be aligned horizontally and be of a common profile.
- Fabric and canvas awnings are encouraged in all districts. Solution dyed acrylic is the preferred material due to its good mildew and abrasion resistance and dimensional stability. A diverse range of colors and patterns are available, and should be selected to enhance building features and complement building colors.
- Back-lit or internally lit awnings are prohibited in the Downtown and East Gateway Districts
- Elaborate facings or extravagant designs on awnings or canopies should be avoided. Flat canopies should be simple and unadorned.



Awnings can provide shade, pedestrian scale, and color to a streetscape...



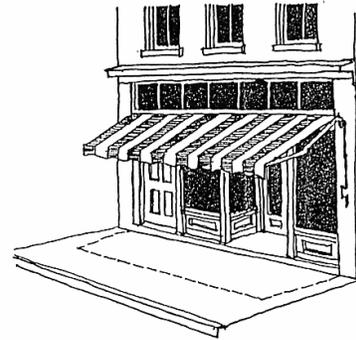
and can be used to create visual interest.



But they can be oversized and over-extravagant.

BUILDING AWNINGS AND CANOPIES

- Graphics and signs on awnings and canopies should be appropriately scaled and located.
- Awnings and canopies must provide a minimum vertical clearance of 8 feet from pavement level.
- Permanent canopies and marquees are encouraged if they fit the architectural style and aesthetic of the building.
- Awnings and canopies should not conceal architectural details of the building.



A roll up awning is a great choice for owners that want shade in the summer months and sunlight in the winter.



Awnings and canopies can be creatively conceived using canvas or metal (above), or can be tastefully done in a more traditional form. (Right)



BUILDING SIGNS

Building signs convey more about an individual store (and business district) than any other storefront element. Signs can add visual interest and detail to a building, as well as communicate the quality of business within. In order for signs to be effective, they must be harmonious in scale and color with the building and neighboring buildings. The desired intent is for signage to be integrated such that it becomes a natural part of the building façade.

- Signs must be professionally crafted.
- Signs should fit the overall proportions of the building and be integrated into the building design.
- Signs should be mounted no higher than the bottom of second floor windows.
- Signs should not call attention to themselves at the expense of neighboring businesses by virtue of their color, scale, lighting, materials, or other obtrusive features. Signs should not dominate the building façade.
- Signs that impact surrounding residential areas with light pollution or obstruction of sight lines are not allowed.
- Secondary and/or rear entrance signs are encouraged when parking is available behind or on sides of the building, or where deliveries are made.
- Advertising placards or permanent billboards are prohibited in storefront windows of the Downtown and East Gateway Districts.
- Signs should not interfere with operable doors, windows or architectural detailing.
- “Shingles” or projecting signs that are more pedestrian oriented are encouraged within the Downtown and East Gateway Districts, and should be designed to be proportional to the building façade. Business name signs painted on plate glass are also encouraged.
- The address and hours of operation should be displayed.
- Directory signs for buildings containing multiple businesses are encouraged.
- Creative signs (within the context of the building) and signs that incorporate bold and vibrant colors (while exemplifying good graphic design) are encouraged. The incorporation of logos or icons are also encouraged.



Creative and careful selection of specific sign elements can help to promote the personality of a business.



Oversized and ill- designed signage can leave a negative impression.



Cluttering windows with too many signs can block views of merchandise and present a confusing and overwhelming image. Advertising placards such as this are prohibited in the Downtown and East Gateway Districts.

BUILDING SIGNS

- Quality historical signs should be preserved.
- Individual cut and mounted aluminum or bronze letters are acceptable.
- Preferred sign materials are sign foams, redwood and cedar for sand blasted signs, glass, molded plastic, flat plastic with a return, stainless steel, brass, aluminum, bronze and rolled steel. Signage materials should be consistent and complimentary with architectural materials. Neon signs may be used to indicate a business name or trade, but not to advertise products or sales or “business open” signs. Box signs are not allowed.
- Signs should be installed on the frieze or sign band of a building, or on windows, doors, or awnings. Establish a sign band on buildings that may not have a clearly demarcated area for signs.
- Refer to City Sign Ordinance for additional requirements.
- Signage shall be ground lit or otherwise washed from a light source which is concealed
- Pole-mounted signs are prohibited



Signs should fit in their sign band, as in this example.



Exterior menu displays and area directories are a great way to attract customers



Gold leaf on glass



Layered and hand-painted sign



Pedestrian orientated shingle or projecting sign



Pin mounted, individually cut letters



Metal sign with dimensional letters



Creative metal sign

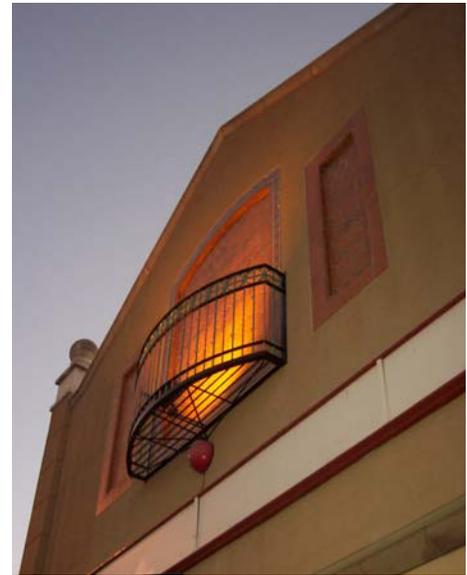
LIGHTING

Effective façade lighting can enhance the pedestrian environment and create a dramatic visual effect that encourages nighttime activity.

- Lighting should be architecturally integrated with the building style, material, and color.
- Control lighting intensity to assure light pollution and glare are not directed toward neighboring areas and motorists. Fixtures should not reveal the light source. Use fixtures with diffusing or other lenses to control adverse lighting effects.
- Use down-lighting, bollards, and wall-mounted sconces to reinforce circulation corridors.
- Area lighting should result in a minimum 1/2 foot candle at all pedestrian areas.
- Illumination of portions of buildings, direct or indirect, may be used where the utility or aesthetic results can be demonstrated. Use lighting to illuminate signage or special architectural details, and to illuminate merchandise in display windows.
- Wall mounted flood lights, flashing, pulsating or moving lights, non-commercial lighting fixtures, or the use of neon tubing to border windows and doors is prohibited.
- The use of color correct fixtures, such as metal halide, is preferred.
- In parking lots, no cobra head lights or building mounted flood lights are allowed. Lights shall have a minimum height of 12 feet and a maximum height of 20 feet. Light sources are to be directed away from residential neighborhoods with hoods and shades.
- To accent landscaping, landscape light fixture shall be utilized. They should be directed away from the property line and should have extension shields to minimize glare and light source visibility.



Illuminating a storefront at night provides advertising, discourages criminals, and promotes nighttime activity.



Carefully designed lighting of architectural details can make the ordinary seem dramatic.



Pedestrian areas should be well lit.

PARKING STRUCTURES

Parking Structures are an effective way of creating a large amount of parking in a limited area. Parking structures should incorporate design features that effectively mask the building as a parking structure. Creativity, attention to detail, and an understanding of the site should be utilized when designing these structures.

- Parking structures shall not be allowed on Johnson Drive unless they have commercial or retail uses on the ground floor fronting Johnson Drive. Vehicle access to parking structures is not permitted on Johnson Drive.
- The inclusion of retail, commercial or office uses is encouraged at the ground floor of parking structures.
- Unless otherwise specified, parking structures in the Downtown and East Gateway Districts should be constructed to zero-lot lines.
- Primary façades of the parking structure shall be designed to be compatible with neighboring buildings. Parking structures shall respect adjacent property by providing appropriate transitional elements such as pedestrian alleys, green spaces or height reductions.
- Facades of parking structures shall adhere to the same building quality and aesthetic requirements outlined elsewhere in the Building Guidelines.
- Parking structures shall provide clearly defined pedestrian entrances and circulation.
- Parking structures shall be screened so that views of cars within the structures are obscured from the street. They should incorporate the same level of architectural detail as commercial buildings.



The inclusion of retail, commercial and office use is encouraged at the street level of parking structures.



Parking structures should incorporate the same level of architectural detail as other buildings.



Required fenestration for parking garages can be accomplished with screen elements, giving the structure a more solid look.

RESIDENTIAL BUILDING DESIGN GUIDELINES

Medium Density Residential Building Design Guidelines

Medium Density Residential Buildings that are developed in the East Gateway District shall adhere to all other sections of the Design Guidelines, with the following exceptions, additions and modifications:

Facades

- The design for medium density residential units shall incorporate more massive architectural elements at the building base. Elements above the base shall appear visually lighter through the use of different materials or components such as windows, wall openings and balconies.
- The design for residential buildings shall consist of a variation of massing configurations. A use of a variety of wall and roof planes is recommended to provide complexity and visual interest.
- The townhome facades of medium density residential buildings should be articulated in such a manner as to visually differentiate the individual units.
- Every side of a medium density residential units shall incorporate a consistent level of architectural interest and quality

Proportion and Scale

- Residential building entries next to a public street shall be pedestrian-scaled.
- Building facades should have visual interest as articulated by the use of entrance stoops, porches, balconies, insets, bays and windows.

Materials

- Roof materials shall be durable, high quality materials such as:
 - Wood shake shingles
 - Composition shingles
 - Asphalt shingles
 - Clay or concrete tiles
 - Copper or other metals



Elements of the base shall appear visually lighter.



A variety of massing configurations add visual interest.



Individual Units Shall be articulated.



This two-story townhome development is appropriately scaled within its context.

RESIDENTIAL BUILDING DESIGN GUIDELINES

Roofs

- Pitched roofs of medium density residential buildings shall have a minimum slope of 4:12.
- Residential buildings with sloping roof types shall have a variety of roof forms. Large roof areas shall be articulated by complimentary sheds, dormers or other elements.
- Building functions and elements such as entrances shall be denoted by well designed and corresponding roof forms.



Large roof areas shall be articulated by dormers or other elements.

Entrances

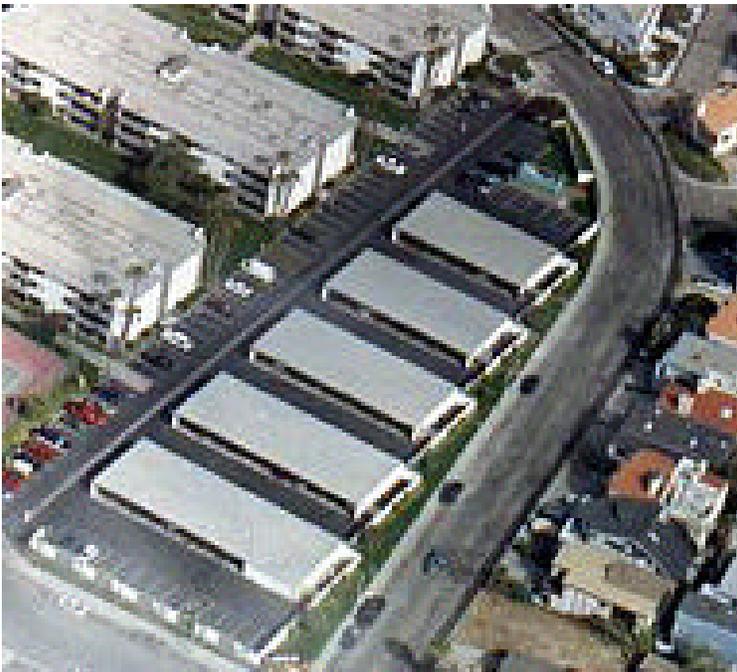
- Residential style doors with intricate patterns are recommended.



Residential style doors, which are prohibited on Commercial and Mixed Use buildings, are recommended for Medium Density Residential buildings.

Parking

- Parking structures, parking lots, carports and garage entries shall be sited internal to building groupings and shall not front on primary streets.
- As much as possible detached carports or garages shall be sited perpendicular to the internal streets of the development to reduce the visual impact on the common areas.



Parking structures and garage entries shall be sited internal to building groupings (above). Detached carports shall be sited perpendicular to internal streets in a development (right)