

CITY OF MISSION, KANSAS
COMMUNITY DEVELOPMENT COMMITTEE

WEDNESDAY, JULY 10, 2019

6:30 P.M.

Mission City Hall

PUBLIC COMMENTS

PUBLIC PRESENTATIONS / INFORMATIONAL ONLY

1. Amended Preliminary Development Plan / Final Development Plan, Tidal Wave Auto Spa 6501 Johnson Drive - Brian Scott ([page 4](#))

The owner of the proposed Tidal Wave Auto Spa at 6501 Johnson Drive is requesting an amendment to the approved preliminary and final development plan to replace the proposed two-story building with a one-story structure. This item was recommended for approval (8-0 - 1 Absent) by the Planning Commission at their June 24, 2019 meeting.

ACTION ITEMS

2. Acceptance of the June 5, 2019 Community Development Committee Minutes - Martha Sumrall ([page 148](#))

Draft minutes of the June 5, 2019 Community Development Committee meeting are included for review and acceptance.

3. Contract Award for Sinkhole Repairs - Brent Morton ([page 155](#))

In April, Council authorized a task order with GBA to provide survey, design and bid phase services for infrastructure failures at 5501 Foxridge and 5939 Woodson. Sinkholes had formed adjacent to these failures causing unsafe conditions for the public. Due to the more immediate concerns and issues raised by these failures, this action item has been included in the packet prior to the bid opening. This would allow for repairs begin without losing another month. If the bids are not acceptable to staff/Council, the item will be continued to the August 7 CDC Committee meeting.

4. 50th & Dearborn Storm Sewer Repairs - Brent Morton ([page 158](#))

In January, the City Council approved a task order with GBA for the design, and bid phase services for a storm sewer failure at 50th and Dearborn Street. The engineer's estimate to repair the failed storm sewer and additional infrastructure is \$166,505, and the bid alternate to slip line the creek channel is an additional \$18,470, for an estimated project total of \$184,975. Funds are available in the stormwater utility fund to move forward with the project in 2019, and staff is seeking formal authorization to solicit construction bids. If approved, a final contract would be brought back for final Council approval.

5. BHC Rhodes Street Asset Inventory - Brent Morton ([page 166](#))

The City has expended resources over the last several years to collect and update data relative to the condition of the residential street network. This has included an updated pavement condition rating and geotechnical analysis (core sampling). Earlier this year, staff began working with BHC Rhodes to collect, inventory and start compiling all existing infrastructure asset information to assist in the development of a formalized asset management system. Initial efforts were focused on stormwater infrastructure, and staff is now seeking authority to begin more detailed work in developing information that will support the creation of a new residential street maintenance program and associated budgeting tools. This action item would approve a task order with BHC Rhodes in an amount not to exceed \$9,000 with funding from the Capital Improvement Fund.

6. Task Order with GBA to Proceed with Foxridge Survey - Brent Morton ([page 173](#))

The Foxridge project (Lamar to 51st Street) is included in the City's Capital Improvement Program (CIP) for construction in 2021 at an estimated cost of \$5.0 million. The project includes the addition of sidewalks and stormwater improvements which may trigger the need for additional right-of-way. In order to move this process forward as efficiently as possible, both for timing and cost estimating purposes, staff is recommending to move the survey work into 2019, ahead of the remainder of design which is budgeted in 2020. A task order with GBA that covers surveying, property research, conceptual hardshell design, estimates for right-of-way needs and project meetings is presented for Council consideration in an amount not to exceed \$49,831.80 from the Capital Improvement Fund.

DISCUSSION ITEMS

7. Facility Conservation Improvement Program - Emily Randel ([page 177](#))

The Kansas Corporation Commission (KCC) offers the Facility Conservation Improvement Program (FCIP) to achieve energy conservation through energy performance contracts. Staff will share a summary of the program and how it may support energy audit and conservation improvements at City facilities.

8. Update on Speed Limit and Traffic Signage - 51st Street, east of Lamar - Brent Morton ([page 179](#))

At the March Community Development Committee meeting, a resident voiced concern about the speed limit (25 mph) on 51st Street, east of Lamar Ave and requested a speed limit increase. During the discussion, the Council also expressed traffic safety concerns for this roadway section, and GBA was tasked with an intersection sight distance evaluation and speed limit recommendation. GBA has completed an assessment of this corridor and does not recommend an increase in speed. They did recommend additional signs and changing the placement of existing signs. Staff will provide an update on the GBA recommendations, and the signage installed by Public Works.

OTHER

9. Department Updates - Laura Smith

Hillary Thomas, Chairperson
Ken Davis, Vice-Chairperson
Mission City Hall, 6090 Woodson St
913-676-8350

City of Mission	Item Number:	1
INFORMATIONAL ITEM	Date:	June 28, 2019
COMMUNITY DEVELOPMENT	From:	Brian Scott

Informational items are intended to provide updates on items where limited or no discussion is anticipated by the Committee.

RE: Amendment of Preliminary Development Plan for Tidal Wave Auto Spa - 6501 Johnson Drive

DETAILS: In 2017, Tidal Wave Auto Spa submitted an application for a preliminary development plan to construct an automobile wash facility at 6501 Johnson Drive, the site of the former Valero gas and convenience store.

The site is zoned “CP-2B” - Planned Retail and Service District. A car wash is a permitted use within this zoning district. The site is also located within the Form Based Code (FBC) overlay district. The FBC is not concerned about the use of the site, but rather the form of the structure(s) that will sit on the site. For this particular site, the FBC requires a building with a minimum of two-stories that sits to the front of the property at the sidewalk. The intent of the FBC is to create a public, or pedestrian, oriented environment along Johnson Drive defined by the built environment.

During the original review and approval process, the applicant attempted to design a car wash facility that would meet the intent of the FBC by proposing a two-story building at the front of the site and the car wash tunnel at the back. The two-story building would have stalls on the lower level that one could drive their vehicle into after being washed to dry and vacuum the vehicle. The second level would be the regional offices for the Tidal Wave company.

The proposed design, however, did not receive the required score to allow it to proceed on a “fast track” (review and approval by the Planning Commission only) for consideration, which is allowed under the FBC. Thus, it was decided that the applicant would have to file for a preliminary development plan that would be considered by the Planning Commission and City Council, and then a final development plan that would be considered by the Planning Commission. This is the more traditional review process for new development projects.

The Planning Commission considered the preliminary development plan in March of 2018 and recommended it for approval by the City Council. The City Council approved the preliminary development in April, and the final development plan was subsequently approved by the Planning Commission in May.

The applicant has since refined construction costs and evaluated their financing for this particular project. In doing so, they came to the conclusion that the two-story building was not financially feasible, and they made a request to amend the preliminary and final

Related Statute/City Ordinance:	
Line Item Code/Description:	NA
Available Budget:	NA

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development plans for this project to provide only a one-story building, or structure, on the site. All other aspects of the final development plan would still be applicable including design, material, streetscape, and overall site plans.

Application of the FBC has proven to be challenging. While the intent of the code is worthy, it does not necessarily match with what a proposed development is able to provide. This project is a good case in point. This particular site is relatively small, so to require a multi-story building with the appropriate amount of parking in the back, is challenging. Staff believes that while the proposed project does not meet the true intent of the FBC, it does incorporate many elements that are sought in the FBC, primarily the structure will be sited at the front of the building with a zero setback. In addition, the development will incorporate trees and streetscape elements that are sought in the FBC and the Johnson Drive Design Guidelines. Generally, it will be an improvement over the current building and use, which has been there for many years.

The Planning Commission considered the request to amend the preliminary and final development plan for the proposed Tidal Wave Auto Spa at 6501 Johnson Drive at their regular meeting on June 24, 2019. The Commission approved recommending the amended preliminary development plan to the City Council for their consideration. The item was recommended by the Planning Commission with an 8-0 vote. One Commissioner was absent.

The minutes of the planning commission are included in the packet for review as well as the staff report and original planning commission minute from March of 2018.

CFAA CONSIDERATIONS/IMPACTS: NA

Related Statute/City Ordinance:	
Line Item Code/Description:	NA
Available Budget:	NA

MINUTES OF THE PLANNING COMMISSION MEETING

June 24, 2019

DRAFT

The regular meeting of the Mission Planning Commission was called to order by Chairman Mike Lee at 7:00 PM Monday, June 24, 2019. Members also present: Pete Christiansen, Jami Casper, Robin Dukelow, Stuart Braden, Brad Davidson, Charlie Troppito and Frank Bruce. Burton Taylor was absent. Also, in attendance: Brian Scott, Assistant City Administrator and Audrey McClanahan, Secretary to the Planning Commission.

Approval of Minutes from the May 20, 2019 Meeting

Comm. Dukelow moved and Comm. Troppito seconded a motion to approve the minutes of the May 20, 2019 Planning Commission meeting.

The vote was taken (8-0-1). The **motion carried**. Comm. Christiansen abstained from the vote.

New Business

Public Hearing - Application # 19-03: Amendment of Preliminary and Final Site Development Plan Review – Tidal Wave Auto Wash (Case # 17 -11)

Mr. Scott: This is preliminary and final site development plan review for Tidal Wave Auto Wash. This was originally Case #17-11, presented to you about a year ago. The property is located at 6501 Johnson Drive, which is the former site of Valero gas and convenience station. The property is within the Form Based Code. Currently, the property is zoned CP-2B, which is Planned Retail and Service District, and the proposed carwash is allowable in the zoning district. The property is also located within the Form Based Code district, specially Block 1 of the FBC. When first presented last year, in March of 2017, there was rather extensive discussion with staff and the Commission regarding the application of the Form Based Code. Staff has been working with Mr. Hardin, the applicant, for the better part of a year and a half to try to apply as much of the Form Based Code [coughing] project. Of course, we went through all of that. The Form Based Code is not so much about the use, but more about the design of the building, and how that design plays in with the corridor. Trying to bring buildings up to the sidewalk with a zero-setback line, try to create a more divergent feel along the Johnson Drive corridor from Lamar, west to Metcalf.

Doing the Form Based Code requires a two-story building in that particular block. I will compliment Mr. Hardin, who has attempted to make a two-story building into a car wash. He proposed having the car wash bays themselves be a tunnel at the back of the property. The front of the property would be the building, and the lower level of the building would be the final detailing of the cars, vacuuming, etc. The second level would be the regional offices for his company. There would be staff there, provide training, so forth.

So, that's what was agreed upon. He decided because he did not score the necessary score for a pass/review of the Planning Commission, that we treat it as preliminary and final development plan, a more traditional two-step property. So, we approved the

MINUTES OF THE PLANNING COMMISSION MEETING

June 24, 2019

DRAFT

preliminary development plan. That was recommended to the City Council and they approved it on April of last year. The applicant came back the following day with a final development plan. We reviewed that, and we presented it to you all, and it was approved by you.

The conditions that were asked for in the preliminary development plan were met in terms of materials, landscaping, site layout, etc. Mr. Hardin can speak more to this in a second, but he's had difficulty making the numbers on this project work, building a two-story building. So, he is requesting this amendment to essentially remove the two-story building and replace it with a one-story building, which would be the bays where vehicles are vacuumed and detailed. Other than two-story versus one-story, everything else is the same. The same material, landscaping, same street scape would be called for. Johnson Drive design guidelines as well as the Form Based Code. The building essentially looks the same other than it's one story. I'm going to stop there, and I will give the floor to Mr. Hardin, and he can present the case.

Petty Hardin, Applicant, appeared before the Planning Commission and made the following comments:

Mr. Hardin: Thank you all for hearing me again. I know it's been about a year since we saw each other last. I know it looks like nothing has been happening with the property, but we have been working diligently behind the scenes to make it work. I know you know we've been working on it at least, I've owned it at least two and a half years, and dealing with the City and staff for at least a year and a half, like Mr. Scott mentioned. I don't want to go back the whole two and a half years and rehash it all, but about nine months ago, I guess, when we got approval for the project, at that point, we released contractor to remove the underground storage tanks from the gas station. So, that contractor did that. There were some concerns, and I heard grumblings about contamination. So, I waited and didn't do anything until we got the final results from that. We do have a "No Further Action" letter from the State of Kansas with no contaminations. That's the first thing. That could have killed the project at that point. So, once we got that clearance, then we went ahead and turned the architects and engineers and everybody loose to compile the full set of drawings that we would do two things with. Number one, submit to the City for a building permit, and at the same time, let out for bids for contractors to bid on the main parts of the project.

When we did get our bids back, we were absolutely shocked at the construction cost. Not the land cost, not the building of the car wash facility itself. It is a little more expensive with a retaining wall in the back, a large retaining wall we have to incorporate into our new car wash building, but it was really the building up front that caused us a lot of difficulty. If you'll remember about a year ago, I've always said in order to be in Mission, I realize it's going to cost a lot of money, and from the beginning I allowed an extra million dollars in the budget to hopefully compensate and offset some of the expenses that the Form Based Code would inform. I was shocked to find that my million didn't go very far, and we exceeded that million by another \$700,000 to \$800,000.

MINUTES OF THE PLANNING COMMISSION MEETING

June 24, 2019

DRAFT

So, with that, I started scratching my head, started talking to our contractors, my partners, looking at other projects, talking with other car wash operators. One of my first calls was actually to Brian at the City. We're very transparent on what our costs are, and what's all involved in the project. So, he and I sat down, and I asked his opinion of what can we do to cut some cost, or find a way to make this work? And he had some good ideas. There's a lot of stacked stone on the building, and that's very expensive. The windows, the insulation, the sprinkler system, stairwells for the second story, HVAC systems. We looked at actually building from the outside that same building but leaving it unfinished up top and have the shell, have the complete look of it. But the more we stripped it out, the more we sacrificed functionality of not being able to use the upstairs, as well as the aesthetics were starting to suffer, too, when you start pulling out features that, the things that cost money look the best. Especially the stacked stone and the things we just mentioned.

So, we've never been over budget on a project to this magnitude. We've always been able to make the numbers for the actual construction work. There's been plenty of situations where we couldn't afford land cost, things of that nature, that killed deals. But we do have six other locations that are under various stages of development in the metro area. This is just a different animal. We've got over 40 projects in other parts of the country that we made work. In the last six months we've really spent a lot of time trying to do everything we can. I can't emphasize that enough. But we've run into these problems, and it's definitely a direct result of the Form Based Code. I'm not here to bash it or anything like that. It is what it is. But it's caused us a lot of problems. And I look in the mirror and say I don't want to be part of the problem; I want to be part of the solution. I know ways that we can make this work if we can redesign this building. That's what we've done in the last three months, is really focused on a complete overhaul of that building, and that's what we've come up with to be the only option that would make this project stay alive. Because it's kind of been derailed, so to speak. And I'm glad Mr. Scott called me a couple months ago and asked me about the status of it. That's when we were able to sit down and start talking about it.

So, that leads us to where we are tonight. One thing I'd like you all to please keep in mind is that this building, like Mr. Scott said, is not an income-producing building. The car wash in the back is the tunnel that generates the revenue. So, as the customers exit the car wash, included in their price, they can vacuum if they choose in the middle of a parking lot in an uncovered area, or up under this structure in the shade, if there's inclement weather, or if it's too hot, or whatever. The upstairs wasn't going to generate any money for us. The downstairs sure doesn't. We're used to spending, on a regular car wash, there are 11 vacuum stalls in this current design. We're used to spending maybe \$150,000 on a canopy, like a gas station, rectangular canopy. And plugging in \$1.7 to \$1.8 million for this building, the only option we had was to redesign it, and that's what I'm here for tonight, is to propose that to you. I think you may have seen some electronic versions. We've taken the original building, like Mr. Scott said, and left all the key features in it. Made it as nice as we could possibly make it. And that's the final product that we came up with. We can make that work and go fast on new drawings. In the big picture, with this redesigned

MINUTES OF THE PLANNING COMMISSION MEETING

June 24, 2019

DRAFT

building, it's my belief that we can achieve everything that the building needs to achieve. There are three things in particular. Functionality – It will serve its purpose; the customers will still be able to vacuum like they need to. Aesthetically, it's going to look as good, in my opinion, as the other one, if not better. It's going to fit in just fine, and most importantly for me, it's the difference between a deal or not a deal – the economics we can make work with this. I think everybody involved with this project, if this is approved and we go forward, I think everyone wins. And what I mean by that is, I think that Tidal Wave builds another parking spot, we win. I believe the City will be very pleased with the final product of what we build. We've got several other locations that are going now. We've got one in Raytown you can look at, but that really wouldn't do a lot of good because this is such a different animal. But I've always tried to stress how we operate, in a Chik-fil-a type manner, customer service, aesthetics, curb appeal – all those things. So, I think the community will appreciate it, I think the customers will rave about it, and our employees will have a great place to work. So, I think it accomplishes all those goals. With that, it's pretty simple. I'm asking for your recommendation for approval to move forward with this redesigned building. That's really all I have tonight. Again, I appreciate your time, and I respect that. I'll be glad to answer any questions.

Chair Lee: Any questions? [None.] Thank you.

The Chairman opened the public hearing. There being no one to be heard, he closed the public hearing.

Comm. Bruce: Mr. Chairman, I am not a great proponent of Form Based Code. I'll put that right at the top. I have reviewed the Form Based Code. I don't see where there's any mechanism to address a business like this in the Form Based Code. That being said, if I lived across the street, I wouldn't want even a two, let alone a five-story building, across Johnson Drive from my residence. So, I don't really have a lot heartburn with a one-story building that replaces a blanket building that has been there for an extremely long time, not generating revenue to the City of Mission.

Also, if you go back to the minutes from the March 26, 2018 meeting, on pages 36 and 37, Mr. Heaven stated that the Form Based Code is a guideline, and he charged the Planning Commission with responsibility to make decisions for the best interests of the City/businesses in the City. And it's obvious to me that the people have gone way above and beyond a normal car wash motif to get something that would come close to the Form Based Code. And moving it up to Johnson Drive, having a ghost building face there, to start the car wash operation. Which otherwise I'm assuming wouldn't be visible to the public. Anyway, I really don't see a problem moving forward with the adjusted plan.

Comm. Troppito: As the other Ward 3 Planning Commissioner, we're pretty much in the same idea. I view this as an improvement over the previous plan that we reviewed. I also want to note that among those improvements, the key one of those isn't what you see, it's what occurred in terms of below ground, with the environmental remediation, and the expense of that. And it benefits the rest of the city, besides the revenues that were just mentioned. I believe, the way I interpret the Form Based Code, this is a fit, and an improvement.

MINUTES OF THE PLANNING COMMISSION MEETING

June 24, 2019

DRAFT

Comm. Davidson: Brian, do you happen to have a drawing of the elevation of the original building? If not, that's okay. I agree with Charlie and Frank. I think that is a type of rendering, of taking off the top floor. I have no problem with it, as well. I have a question of the applicant. On most facilities that you have, the vacuum area, which is, in the case of this building, on Johnson Drive, it's basically just an open canopy in most cases?

Mr. Hardin: This is the first out of 46 locations that we've ever enclosed like this on three sides.

Comm. Davidson: Okay. So, there's a lot of added expense. I can see that with this structure, trying to meet the best you can with the Form Based Code. I have no issues with it either. I like the jut-out of the building material, the turret in the middle, the dormers that breaks up the rooflines. And is that a standing seam roof on that building?

Mr. Hardin: Yes.

Comm. Davidson: I think it looks absolutely great for what you had to work with, to bring it down to one level. The plate lines on the first, are they the exact same elevation height-wise?

Mr. Hardin: [inaudible-off microphone]

Comm. Davidson: But you're not raising the plate line of the first floor, like it was, let's just say on...

Mr. Hardin: I don't know offhand what that is. I can tell you that the height of the proposed... The highest point on top of this cupola here, that would be 28 feet, 7 ½ inches, all the way from the ground to the very top. Showing from the ground to the top, the main roof line shows to be 20 feet 10 ½ inches to the main roofline up here. Then from the ground to the gutter line or the eve would be 12 feet 8 ½ inches.

Comm. Davidson: Okay. So, I'm just looking at the original drawing of the first floor, you know, the elevation of the first-floor wall height is probably around [inaudible]. I think it looks great and I have no problem, Mr. Chairman.

Comm. Braden: Personally, I don't have an issue with a one-story car wash. I do wonder how, in future cases where we have a Form Based Code, for two stories, how that affects that. If that diminishes the regulations that we set forth. I'm struggling with the question. [inaudible] struggling with the question also of how that affects regulating the Form Based Code.

Comm. Troppito: Mr. Chairman, again, Mr. Heaven answered that question. I believe it was Robin asked that, if we would be setting a precedent. And the answer was no. We're professionals in our discipline here. And while I have the floor, I might also mention that having been a member of the sustainability committee, I was very pleased with the report that they gave, and with what Tidal Wave does in trying to protect the environment. It seems like that was very strong suit on their part and they seem to be a regular corporate citizen to boot.

Comm. Dukelow: I struggle with it because of the two-story requirement. I mean as it stated we came to the conclusion last time that while it didn't meet the Form Based Code,

MINUTES OF THE PLANNING COMMISSION MEETING

June 24, 2019

DRAFT

it met the intent because the building fronting Johnson Drive was two stories. And I might add at this point, I don't see a building. It's a carport. A building would imply enclosed conditions. In my personal opinion, it's not a building, it's a carport. That I struggle with. There is also a provision in the Form Based Code for low-rise building types and that would be in the case where the site was no larger than half an acre. And honestly, I can't support the idea of it extending more anyway, so, like I said, I'm have a hard time... I'm having a hard time recommending or not recommending this because I feel it's not really the intent of what we want to see along the Johnson Drive corridor. And I'm at one story, although yes, it's a lovely car wash. I know it's an ugly site now and it's been an ugly site for several years. I mean, I don't mean an ugly site. It's not an ugly project. Don't get me wrong. My only concern is that we've got a lot of examples of developments that have occurred in the Gateway where we have required two stories or at least 60 or 40 percent. I mean, what is the square footage of the building, because the building is just [inaudible]. Technically. I struggle with that. How do you even say what the square footage of the building is when three-fourths of your construction is not technically a building? [inaudible]. Maybe the cupola would make up the 40 or 60 percent. I'll have to come up with something more intelligible by the time we call the roll, because, like I said, I'm struggling with it. Thank you.

Comm. Bruce: Mr. Chairman, Form Based Code is like telling everybody to wear an 8 ½ shoe. Some people are very happy with that; some aren't. I happen to know of people that have intended to create a business in Mission, in the West Gateway, that would have been very beneficial to Mission because it was on empty property that is currently over there. Revenue generate for the city. But, because of the Form Based Code, they choose to go to [inaudible] ridge to building their restaurant because restaurants don't lend themselves to Form Based Code, as well as this car wash. And it's nice to say that the Form Based Code is a wonderful thing, but it's also to say that the City of Mission is looking for corporate partners that are willing to come in here and invest in our cities, upgrade existing property, or vacant property that will revenue-generate for the city going forward.

Now, I'm personally not in favor of approving anything that is even marginal, but something that's certainly attractive to our business community is not a high-risk thing where I'm concerned.

Comm. Dukelow: That is an interesting perspective that Frank just shared. And I'm going to go off of what I said previously, and then what he said, because what he made me realize – and which I alluded to previously – is that our success with the Form Based Code has been more applicable to larger development parcels. The case being over there where we've got the chicken place, Mission Commons, and where we've got the Natural Grocers. Because those sites were big enough to give the developer more flexibility and the ability to go ahead and provide the restaurant, the nail salon, and those other places, along with their square footage, in a building type where they can generate revenue with that additional square footage above the first floor.

Comm. Casper: I'd like to say that I appreciate the effort that's been put forth so far, and also the effort in coming up with a solution that will work for you, and possibly the city. I

MINUTES OF THE PLANNING COMMISSION MEETING

June 24, 2019

DRAFT

don't have a problem with a one-story building. I think the location and placement of it on the project is what we're looking for and it will be one of the nicest-looking carwashes around, hopefully. I don't have an issue with the one-story. Thank you.

Comm. Christiansen: I want to thank you for having faith in our city and coming back and being willing to put in hours of effort to build in our city. I too don't have an issue with the single story, if we're calling it that, being a carport that's not occupiable. I don't know if I would consider it a building. I was in a couple areas where Form Based Code had been applied. I haven't seen much success in it either. Robin, you mentioned the Five Guys. I don't know if the space has ever been occupied above them.

Mr. Scott: Just within the last six months.

Comm. Christiansen: So, just recently. So, I see no issue with this. I think it is important to, even though the Form Based Code is a guideline, I do think we should set the correct precedent for other developers or business owners that come before us, to show them what we're envisioning and what the City is envisioning. [inaudible] line of work with this code. The Form Based Code might not have been thought about for a car wash, obviously, or having a two-story building on the front side of it, though.

Chair Lee: Well, I also have mixed feelings. I understand both parts of it. I will say that when I mentioned to others that we were going to have a two-story car wash, everybody seemed quite shocked and surprised. And if that's what you have, I think everybody in Kansas City would recognize it as that – a two-story car wash. I agree with Robin that the larger the projects, the easier the Form Based Code is to work with. We have one other bank that's going to go in on Johnson Drive [inaudible] down the street from the park. I have mixed feelings. I mean, I believe the Form Based Code could work. Certainly, it's easier to work with for a bigger project. So, I have mixed feelings. [inaudible].

Comm. Troppito: Well, if there's no further discussion, Mr. Chairman, I move that the Planning Commission recommend to City Council approval of Case #19-03: Amendment of the Preliminary and Final Site Development Plan for Tidal Wave Auto Spa at 6501 Johnson Drive, which entails removing the second floor of the building at the front of the property, with all conditions from the original preliminary and final development plan still required.

Comm. Bruce: I'll second that.

Unidentified: Do we need to address the fact that it is a canopy [inaudible]?

Mr. Scott: How do you define a building?

Comm. Dukelow: Condition of occupiable space.

Mr. Scott: It's an occupiable space.

Comm. Dukelow: Not my definition of it [inaudible].

Comm. Troppito: [inaudible] motion...

[crosstalk]

MINUTES OF THE PLANNING COMMISSION MEETING

June 24, 2019

DRAFT

Mr. Scott: Occupiable space?

Unidentified: Yeah. A structure, not so much.

Comm. Dukelow: [inaudible]

Comm. Troppito: I agree with substituting the word "structure."

The vote was taken (8-0). The **motion carried**.

Case #19-04 Non-Conforming Situation Permit, 5966 Barkley

Mr. Scott: This particular item is a non-conforming situation permit at 5966 Barkley. The applicant is Richard Jacobsen, president of Valvoline Instant Oil Change. The property is and has been since it was constructed back in the 1990's, an oil change/lube operation. I believe at one time it was a Valvoline then it was sold. Most recently it operated as a JC Speedy oil change. That particular business closed this winter. Mr. Jacobsen was interested in purchasing the property and reestablishing a Valvoline. He wants to make some changes to the building, minor changes. Replace the wood parapet around the roof line with an EFIS. One portion of that roof line would actually pop over just a bit to allow room for the Valvoline sign. That is within the amount of EFIS in the code. They want to paint the brick; I said no, we like the color of that brick, so they agreed to that. I asked for more landscaping on the site which they agreed to.

The parking lot needs a little help, so they're going to go in and patch it, seal coat it and stripe it. They are providing some parking spaces on site. There is also a drainage issue. I'm not sure where the water is coming from but it's going to drain across the front lawn on the south side and across the sidewalk. There's some moss on the sidewalk. They are aware of that and they'll make efforts to address that previous to the building. That is the case. The applicant is here tonight if you have any questions.

Chair Lee: [inaudible]

Mark Abel, Architect, appeared before the Planning Commission and made the following comments:

Mr. Abel: I'm an architect, working on this project. I'm really just here to answer any questions. I think you mentioned that even a half acre is not regulated by the Form Based Code...

Comm. Dukelow: I think there's [inaudible] to provide...

Mr. Abel: We're even under that, so I guess we're non-conforming, but we're still conforming to the code. If you have any questions, I'd be happy to answer.

Unidentified: [inaudible]

Mr. Abel: I don't know what the percentage is exactly. I can figure it up pretty easy. Can't hold me to it, but it's probably 20 percent, somewhere in that area.

Unidentified: So that's below the threshold.

Mr. Scott: Ballpark, right about 25 percent.

MINUTES OF THE PLANNING COMMISSION MEETING

June 24, 2019

DRAFT

Unidentified: Really, this is the same situation we just had last month.

Comm. Dukelow: In this case, this light area, that's half an acre?

Mr. Abel: Yes, 20,900. One hundred and ninety by 110.

Comm. Dukelow: I guess my biggest question is, what are the improvements you're going to make to the building?

Mr. Abel: We're going to put EFIS on the top band where there's certainly a wood band. That will help that quite a bit, I think. We're going to add new overhead doors, all glass. We're going to take down the wood deck. And new signage, obviously. We're going to have new landscaping. We worked it out with the City, you know, some groupings of plants and what-not. There's a pretty serious drainage issue at the front of the drive, south side, on the east side of the south side. The east two bays have a pretty serious water problem and the water problem is from a sump. That's going to be taken care of. We're going to have all new pavement, which is pretty expensive. Basically, what's there, but it's going to have to be taken apart and patched and coated. That's it.

Richard Jacobsen, Applicant, appeared before the Planning Commission and made the following comments:

Mr. Jacobsen: I own Westco Lube, which is a [inaudible] franchisee. One of the improvements that's important to us, and I believe would be important to everyone sitting here, is we'll replace all the oil storage tanks in the facility, both new and used. Some other things on the main level which doesn't quite meet code because you're supposed to have secondary containment, which they did not, so, all of ours will be in the basement. There's going to be no floor drains, so the basement acts as secondary containment. But we go a second step past that, in that all oil tanks that we purchase either for the storage of fresh oil or used oil, they are all double-wall tanks. In essence, we've got three layers of protection, and we've got two layers of double wall on the tank, and the containment of the basement with no floor drains. We do that because we want to be safe, too.

Comm. Braden: Do you have sensors for the tank to check for leaks?

Mr. Jacobsen: No, we do not. I'm not aware of any manufacturers in this field that have that. They do on gas, I know that, but on the oil, I don't. That's what we're doing that, and that's a substantial cost, but we do that in all of our facilities.

Comm. Bruce: Are there any floor drains in the basement?

Mr. Jacobsen: I don't believe there's any there now. I mean, I've been in that basement twice and I don't think there's any, but if we find any down there, you know, which we've had in previous circumstances, we go in and cement them over. They will definitely be contained because we don't want that either.

Mr. Abel: There's probably not in that there's water in the basement now.

Comm. Bruce: So, the water is being pumped out through the...

Mr. Abel: [inaudible].

MINUTES OF THE PLANNING COMMISSION MEETING

June 24, 2019

DRAFT

Comm. Bruce: And it's surface water?

Mr. Abeln: Yeah. From the foundation drain, I believe. [inaudible].

Comm. Dukelow: And this is a continued use?

Mr. Jacobsen: Correct.

Unidentified: On the green space on the north side, I guess, of the entrance, the north entrance, that green space right there, is that your property? Because the drive-through on the back side, that belongs to the barbecue, correct?

[inaudible, shuffling papers, crosstalk.]

Mr. Abeln: Yeah, the green space on the north side would be ours.

Unidentified: Okay.

[inaudible]

Mr. Abeln: Right. Since I don't have a site plan. But that green space to the north is ours.

Mr. Jacobsen: One of the things that we do on all our properties, we'll go in and put in an irrigation system in all the landscaped areas. We definitely will.

Comm. Davidson: And then, basically, the footprint of the parked surface, you were going to add some parking spaces some place in the project. That's mainly going to be fixing old curbs, and new curbing area to create a few more parking spaces, and then a mill and overlay, or something like that, or asphalt?

Mr. Abeln: It depends on what we find. If it's too far, we're going to put in a new surface all the way across, if it comes to that. We've got another project going right now and that's what we're doing there, too.

Comm. Davidson: My other question on that is, as far as drainage, like, not talking about the gravel and water that was coming over the sidewalk [inaudible]. As far as any of the sheeting of rainwater, is it all flowing in the right direction as far as stormwater, you know, and all that. Because I'm not sure when this structure was originally built, but there could have been some mistakes as far as rainwater direction...

[inaudible] [crosstalk]

Comm. Davidson: Well, I've just driven by that property for many, many years, so I'd like to bring it up.

Mr. Abeln: Yeah, and the reality is, there are mistakes, and it does kind of go into the building, which is why they have that foundation drain. That is, you know, you're observing it enough to have seen it, and it is a problem, and we intend to address that. [inaudible] So, I mean, we need to assess it, and we don't have our topo survey back yet, but we hope to address that in the redoing of the pavement. Anything we can do at that time to get any water away. We don't want to have any water in the building, obviously, or going towards the building. And we're going to pick up that sump pump in the interim also, and pump that straight into the sewer system.

MINUTES OF THE PLANNING COMMISSION MEETING

June 24, 2019

DRAFT

Comm. Davidson: What do you mean...?

Mr. Abel: If the sump pump is dumping all that water out on the sidewalk...

Comm. Davidson: Oh, that's what it's from. It's from the basement....

[crosstalk]

Mr. Abel: Well, it's an outside sump pump. An exterior sump pump going to the foundation drain. It must have had water infiltrating into...

[crosstalk]

Comm. Davidson: ... later point in time.

Mr. Abel: I don't know. But I believe [inaudible] problems, clearly, so they're pumping water [inaudible] outside on the foundation.

Comm. Davidson: That's all my questions.

Comm. Troppito: Do your facilities also have oil spill kits at the ready?

Mr. Jacobsen: Yes, we do. We have them under all of our storage. And not to get into the details, but under each of the pit openings there is a catwalk, you know, the individual is servicing the vehicle from underneath, and below that is a catch pan. But if any of the oil, if the catch pan doesn't catch it and they make a mistake, instead of it going down and going all over the floor, it hits the catch pan underneath this and runs to a containment bucket, if you will. It just makes a safer environment for our people working, too.

Comm. Braden: First of all, I'm understanding or just confirming that you're not adding any more pavement, just repairing.

Mr. Abel: Correct.

Comm. Braden: Okay. And then, for staff, I saw multiple signs. I don't look [inaudible] calculations but I assume we're in conformance with the code as far as area.

Mr. Scott: They actually submitted a sign package. I think they may be a little over in the front, so let's talk about shrinking that down a little bit.

Comm. Braden: Okay.

Mr. Scott: I was kind of kind of looking [inaudible].

Comm. Dukelow: One comment or question. So, you mentioned irrigation. Will those be smart controllers so they're not running when it's raining?

Mr. Jacobsen: I believe they're the smartest they've got. But yes, that's what we do. But I will say that we've had trouble with those in the past. Sometimes they're not performing like they should, but we do. That's part of our package.

Comm. Dukelow: A combination of planting appropriate plant material and smart controllers can make a big difference. My next question, I hate to ask this but, why is this non-conforming?

MINUTES OF THE PLANNING COMMISSION MEETING

June 24, 2019

DRAFT

Mr. Scott: It's within the Form Based Code district, and because of value of improvements greater than 10 percent. I think a \$1,000 is [inaudible]. Obviously, the improvements have been greater than \$1,000...

Comm. Dukelow: [inaudible] Thank you. That's all I have.

Chair Lee: Anything else? I'll entertain a motion.

Comm. Braden: Mr. Chair, I would move that the Planning Commission adopt the findings of fact contained in the staff report and grant a non-conforming situation permit for Case #19-04, 5966 Barkley; Permit for Non-Conforming Situation, with the following conditions: The surface of the lot will need to be patched, seal coated and re-stripped. Draining issues on the south side of the building where water runs across the existing sidewalk will need to be addressed.

Comm. Dukelow: Second.

The vote was taken (8-0). The motion carried.

Old Business

Follow-up on Case #19-02, Non-Conforming Situation Permit – 6591 Johnson Drive

Mr. Scott: This is from the application that we had last month for the former Taco Bell, which is going to be a Slim Chickens. Commissioner Dukelow suggested redoing the parking on the far west of the lot. The original suggestion was to have angled parking. The applicant did submit some drawings for angled parking. We discussed those. Their concern was that, that parking is not only for the proposed Slim Chickens, but also for some other uses in the shopping center, and folks might have difficulty coming into the shopping center from the north, and then swinging around to that angled parking. They might be forced to either drive through the shopping center to get to the angled parking or just skip it altogether. Try to reduce the amount of traffic going through the shopping center; it may be best to have straight perpendicular parking where somebody can pull right into that. They did provide a four-foot grass area or strip between the two parking rows. So, we agreed with that and thought that was a nice comprise to the project.

Comm. Braden: I guess I have one question, Mr. Chair. Is there a gap in there or some way for a person to walk across and back? Is there a sidewalk? I was wondering how to get from one part of that strip to the other without walking on the grass. [inaudible]

[No audible response]

Comm. Dukelow: I was going to ask about landscaping.

Mr. Scott: There's no landscaping proposed in the four-foot strip. We could certainly suggest that to the applicant with the final approval of the building permit. They may be putting in a crosswalk or something, avoiding walking in the grass.

Comm. Braden: [inaudible].

MINUTES OF THE PLANNING COMMISSION MEETING

June 24, 2019

DRAFT

Comm. Troppito: Mr. Chairman, if you look at that layout, the traffic coming in front of the gym, there's an island that extends quite a ways to the west from that flow of traffic. So, as far as backing into traffic flow, they have to come and then move over that way to get there. I don't know if you can see it on that; I have it here.

Comm. Dukelow: You need to say it's 3 or wide through there. The driveway? The northwest drive is about three cars wide.

Mr. Scott: Probably about three cars wide.

Comm. Troppito: Down in that area, right where the gym is.

Mr. Scott: Yeah, the gym would be a little further south.

Comm. Troppito: Yeah and then most people continue straight unless they're going to turn right, so I don't think there's a lot of conflict there.

Mr. Scott: Yeah, they should be able to move in easily to one of these stalls. As opposed to swinging around and coming in at an angle. I just wanted to give you an update on that.

PC Comments/CIP Committee Update

Mr. Scott provided the Commission with an update on the KC Climate Action Coalition. There is summit on September 14, 2019 at Johnson County Community College. There will be a number of keynote speakers. If anyone is interested in attending, please let him know. Comm. Troppito commented that he plans to attend. He urges the Sustainability Commission to look at bringing two technology companies to the Kansas City area that deal with sequestration and CO-2. That is one of his interests in attending this meeting.

Mr. Scott then mentioned the First Suburbs Coalition Regional Housing Summit to be held on Friday, July 19th, 2019. The City of Mission is a co-sponsor of this summit, along with the city of Gladstone. He can register anyone who is interested in attending.

Staff Updates

Mr. Scott then presented a proposal for update of the City of Mission's Comprehensive Land Use Plan. He has researched what other cities are doing and looked at the American Planning Association and looked through their examples, etc. Updates proposed include land use, scope of services, analysis of demographics and emerging trends, and other items. He also mentioned trends in housing development, analysis of commercial development and emerging trends, transportation network and natural environment.

Mr. Scott then brought up the Community Satisfaction Survey set for this summer or fall. Comm. Troppito asked about ways of accessing a cross-section, not just online, but also through mail [inaudible].... He asked if it is broad based. Mr. Scott responded that the survey will be done by ETC out of Olathe. They do telephone and mail surveys, as well as social media. He said ETC is very cautious about getting the appropriate number of applicants to establish validity of results.

Mr. Scott then provided an update on the East Gateway. He says the developer has turned all their attention to Synergy, a new tenant. Their goal is to open by May or June of next year. Plans have been reviewed and the developer has been preparing the pad

MINUTES OF THE PLANNING COMMISSION MEETING

June 24, 2019

DRAFT

site. They are waiting for approval from Johnson County Wastewater because the sewer line on the site has to be moved. Mr. Scott expects a building permit to be issued in the next week or so. The developer has been interviewing architects. Mr. Scott anticipates reviewing plans for the food hall next winter. Neighbors Construction pulled a building permit last summer on the apartments but they have pulled out of the project. Fogel-Anderson is now going to build the apartments. Developers are working on financing. Comm. Davidson asked if it's normal to begin working on a project before construction financing is in place. Mr. Scott responded that it is not out of the ordinary.

Comm. Dukelow asked about the progress in the brewery that was planned. Mr. Scott responded that the developer is still moving forward.

ADJOURNMENT

With no other agenda items, **Comm. Casper moved and Comm. Dukelow seconded a motion to adjourn.** (Vote was unanimous). The **motion carried.** The meeting adjourned at 8:15 P.M.

Mike Lee, Chair

ATTEST:

Audrey McClanahan, Secretary

STAFF REPORT
Planning Commission Meeting June 24, 2019

AGENDA ITEM NO.: 1

PROJECT NUMBER / TITLE: Application # 19-03: Amendment of Preliminary and Final Site Development Plan Review - Tidal Wave Auto Wash (Case # 17 -11)

LOCATION: 6501 Johnson Drive

PROPERTY OWNER/APPLICANT: Petty Hardin
TW Mission Real Estate
124 East Thompson Street
Thomaston, Georgia 30286

STAFF CONTACT: Brian Scott, Assistant City Administrator

ADVERTISEMENT: 5/28/2019-The Legal Record newspaper

PUBLIC HEARING: Planning Commission meeting 6/24/2019

Background

The subject project is the site of the former Valero's gas and convenience store. A land use application (Case # 17-11) was filed with the City two years ago for the construction of an automatic car wash tunnel, office building and ancillary uses on the property. The preliminary development plan (PDP) was approved by the Planning Commission on March 26, 2018 and by the City Council on April 18, 2018. The final development plan (FDP) was approved by the Planning Commission on May 29, 2018.

The PDP and FDP provide for the construction of an automatic car wash tunnel at the back of the property and a two-story building at the front of the property. The lower level of the two-story building at the front would consist of stalls where the customer can vacuum their car and do any final drying with a hand towel after their car has gone through the wash tunnel. The second level of the building would be regional offices for the car wash company.

The applicant now desires to amend their PDP and FDP. In particular, the applicant desires to, make the two-story building at the front of the property a one-story building, eliminating the offices.

Property Information

The subject property is a former gas and convenience store located at 6501 Johnson Drive. To the immediate east is the Exact Performance auto repair facility. To the immediate west is the former Taco Bell restaurant (soon to be a Slim Chickens). The property is .76 acres in size.



Figure 1: Subject Property-6501 Johnson Drive

The subject property is zoned “CP-2B” Planned Retail and Service District. The proposed car wash use is an allowed use in this zoning district.

The subject property is also located in Block “Y” of the West Gateway Form Based Code (FBC) Overlay District. And, the Johnson Drive Design Guidelines are applicable to the property as well.

Original Proposed Development Project

The PDP and FDP that were originally approved provided for the construction of an automatic car wash tunnel at the back of the property and a two-story structure at the front of the property. The two-story structure would consist of open bays on the first level where customers could drive their vehicle in and vacuum it after having gone through the car wash. The second level would consist of office space for the car wash company.

Approved Site Plan

Ingress to the site would be on the east side of the property - a shared driveway apron with the property to the east, Exact Performance. A left turn lane along westbound Johnson Drive already exists, but the edge would be moved back a few feet to provide better turning radius. Vehicles would pull in and queue along the east side of the property line to access the wash tunnel at the back of the property.

Once through the wash tunnel, the vehicles would egress the property on the west side, or pull into one of the bays underneath the building at the front of the property for vacuuming or further cleaning.

The development would utilize a surface parking lot located in the interior of the lot. The City's traffic engineer, GBA, did review the site access plans, vehicle queues, turning templates, sidewalks, and traffic study and was satisfied with the design. Please see the attached site plan.

Storm water drainage is gradually west across the site. The proposed redevelopment would reduce the overall impervious surface area by 5%. Therefore, according to city ordinances, stormwater detention and BMP treatment are not required.

Approved Landscaping Plan

A detailed landscape plan was submitted with the FDP and approved. The plan indicated Shantung Maple, Velvet Pillar, and Everclear Laceback Elm to be planted along the front of the building on Johnson Drive. Each tree will be 2" caliber. Trees will be planted every 30 feet with tree grates incorporated into the sidewalk around each tree.

Plans also indicate that two trash receptacles (Black 36 gallon steelsites RB-36) will be incorporated into the streetscape along Johnson Drive as well as two bike loops (creative pipe inverted "U"). There will also be one black, metal bench (72 inches). In addition, plans indicate the installation of a decorative style of street light to be installed along Johnson Drive to match what the City intends for this section of Johnson Drive. Plans are in accordance with the Form Based Code and the Johnson Design Drive Guidelines.

Plantings throughout the rest of the site include Boxwood, Hooks Juniper, Switch Grass, Dwarf European Viburnum, and Periwinkle. Ground cover includes tall fescue and river rock. Please see attached landscape plan.

Approved Exterior Building Plans

Plans indicate that the Johnson Drive frontage of the proposed building will have 61% glazing (storefront windows). EIFS will only be 8% and is located along the top of the wall and parapet (or partial wall) at either end of the building. Stone veneer in a style known as "Southern Ledgestone" will be predominate on the front facade of the building. Split face block in an "Old Castle" style will go up the facade of the building approximately two feet from the ground on the front facade of the building. Please see attached elevations.

Form Based Code & Plan Review

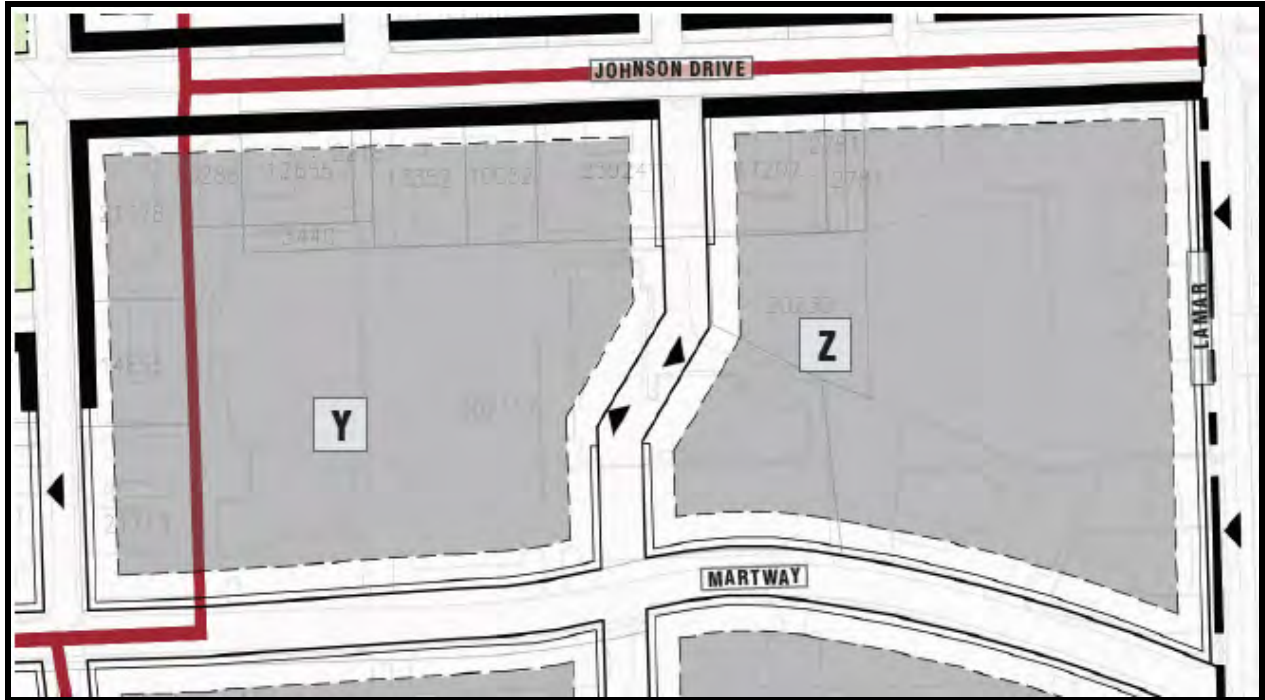
Sector & Regulating Plan

This property is subject to Form Based Code (FBC) for the West Gateway Study Area. It is located in the northern part of Block "Y" in the Martway Sector. The developer is not proposing to assemble any additional parcels into the project. In the event of a conflict between the provisions of the FBC and the City Code or Johnson Drive Design Guidelines, the FBC takes precedence.

Details from the current regulating plan for this entire block include the following:

- Contains a combination of small and large parcels, many with good visibility from Johnson Drive.
- Ground level retail uses are important facing Johnson Drive. Retail is required along the frontage facing Johnson Drive. Parking and service areas should be accommodated away from Johnson Drive.

- The entire block is large enough to accommodate structured parking which would be a good fit with the existing grade difference between Johnson Drive and Martway Street.
- The extension of Walmer Street to the south connecting with Martway Street would be required in conjunction with the development of the western half of Block Z.



Building Types Allowed

The following building types are allowed:

Townhouse type requires a minimum of two stories and a maximum of four. All floors to be residential.

Mid-rise building type requires a minimum of two stories and a maximum of four. Ground floor office or retail and upper floor office, retail or residential uses are permitted.

Parking Structure building type has no minimum or maximum floors. Ground floor commercial or office is required at principal and secondary frontages.

Low-rise building types are currently allowed in any sector, provided the site under development is no larger than $\frac{1}{2}$ acre. This site is $\frac{3}{4}$ of an acre. The total site area exceeds $\frac{1}{2}$ acre, but is less than 3 acres and would qualify for up to 60% of the gross square footage of development (in one or multiple buildings) to be Low-Rise building type. The applicant chose not to incorporate the extension of Walmer Street into their redevelopment plans. Such a street dedication may have reduced the area of this parcel below the $\frac{1}{2}$ acre threshold.

Application of the Form Based Code

The FBC was adopted in 2007 as a tool for implementing the West Gateway Vision Plan that had been created a few years before as a concept for the way future development in the west commercial area of the City should occur.

Whereas traditional zoning speaks to the permitted use(s) of a property, and specifically the regulations around those uses; the FBC speaks more to the character, look, and feel of the property through the built environment. In other words, it is not so much about the use as it is the building. The intent being that what is built on the site will last for many years through many generations of uses.

The developer is encouraged to work through their proposed development concept in consultation with the staff and the City's consultant so that the end product meets the desired outcome of the FBC. As an incentive for doing this, the proposed plan can be considered by the Planning Commission in one step instead of the more traditional two-step process - preliminary site development plan and final site development plan. And, once approved by the Planning Commission, the developer is eligible for a building permit with no approval from the City Council required.

Findings by staff are determined using the scoring system contained in the FBC. The FBC is structured hierarchically, understanding that certain elements are mandatory prerequisites, others are significant and others are minor.

During the development of the Code, it was reinforced that, in order to achieve the level of quality in the built environment as articulated in the West Gateway Vision Plan process, more attention should be paid to those elements that directly contribute to the public realm than to the individual elements of architectural design. Therefore, the review process is structured hierarchically, so that major elements are reviewed first. The early stages of review, including block configuration and building type, are more heavily weighted in the scoring process as they focus on elements that the FBC regards as inviolate or of significant importance. Conversely, lesser items may yield a lower score but not result in a finding for disapproval. All of the elements are important, but have different values to the public realm.

The threshold score for a recommendation of approval by staff as established by the FBC scoring system is 90 points or more, of a possible 100 points including the prerequisite passing scores in steps 1 and 2. Proposals which achieve this score should be considered to be in compliance with the intent of the FBC. They proceed on under the review steps outlined in the FBC. Development submittals which achieve a score of 89 points or lower will proceed through the more traditional approval process of the zoning code including full design review by the Planning Commission and City Council.

The FBC review that was conducted by staff and the City's consultant for this particular development project resulted in the original application receiving a score of 60 points, and thus failing several critical prerequisite steps. This was largely due to the proposed buildings failing to meet the required building type standards as provided for in the FBC for Block "Y." While the building fronting Johnson Drive did meet the requirement of two stories with a zero build line at the sidewalk, it failed in that the depth of the building was not enough, and the vacuum bays did not really meet the intent of ground floor retail.

Extensive discussion about the application of the FBC for this development occurred at the Planning Commission meeting on March 26, 2018. Please see the attached minutes.

It seems that the Planning Commission came to the conclusion that while the proposed development project did not receive a qualifying score under the FBC, it met the intent of the FBC in that the building fronting Johnson Drive was two-stories, and it did sit at the sidewalk,

and that it did incorporate those elements that were called out in the FBC and Johnson Drive Design Guidelines.

Planning Commission Recommendation

The Planning Commission, at their March 26, 2018 meeting, voted 8-0 to accept that all of the required findings of fact would be met through compliance with the following conditions, and therefore, recommended approval.

- 1) Complete information about percentages of EIFS and storefront glazing are to be provided for staff review and approval.
- 2) Along the Johnson Drive frontage, windows and doors shall meet the minimum 60% total coverage of the storefront and EIFS or stucco shall not be used within 8' of the ground nor comprise more than 25% of the first story.
- 3) Complete information regarding trash enclosures, retaining walls, other screening, pay canopy and pay kiosk in compliance with the Form Based Code standards shall be provided for staff review and approval.
- 4) Complete details regarding the site landscape and public streetscape including street lights, benches, trash receptacles and bike racks in compliance with the Form Based Code standards shall be provided to staff for review and approval.
- 5) The median break in eastern entrance driveway shall be relocated to allow vehicles to leave the wash tunnel queue before the pay station.
- 6) Complete details regarding the circulation of trash and other service vehicles on site shall be provided for staff review and approval.
- 7) Applicant and Developer agree to install a median break along the west edge of the entry drive into the car wash facility that aligns with the westbound parking lot aisle.
- 8) Applicant and Developer consent to the Commission conditioning approval of the plan on compliance with such streetscape improvements, as are required by applicable City regulations and as set forth in the Form Based Code for the West Gateway Study Area, Chapter 6 (Landscape Architectural Guidelines, Type II-Urban Blvd).
- 9) Applicant and Developer consent to the Commission conditioning the approval of the Plan upon the conveyance of right of way for sidewalks and landscaping along the site's Johnson Drive frontage in such dimensions as is required by City regulations and via conveyance language usually and customarily provided in similar circumstances by the City.
- 10) Applicant and Developer will consent to meeting with the Sustainability Commission and implementing mutually acceptable recommendations. Applicant and Developer are convinced that Developer operates the most environmentally sound and sustainable car washes in the country with emphasis on recycling and water conservation.
- 11) Applicant and Developer consent to the Commission making approval of the Plan

conditioned on delivery of Architectural Plans, with corresponding calculations which shall provide that windows and doors shall meet the minimum 60% requirement of the Mid-Rise building's northern face, in compliance with applicable City regulations and as set forth in the Form Based Code for the West Gateway Study Area, Chapter 5 (Architectural Guidelines).

- 12) Applicants and Developer will consent to the Commission conditioning approval of the Plan on delivery of requisite landscape details.

Proposed Amendment to the Preliminary and Final Development Plan

The applicant recently submitted a request (included in this packet) for an amendment to their PDP and FDP that would allow for the two-story building at the front of the property be a one-story building. The one-story building would be just the bays for vacuuming the vehicles. The office space originally proposed for the development project would not be included in the final development.

All other aspects of the proposed project as described previously would remain the same A approved with the final development plan.

Section 440.175 (4) and (5) of Mission Municipal Code states:

4. Once preliminary development plan approval has been granted, changes in the preliminary development plan may be made only after approval of a revised preliminary development plan. Changes in the revised preliminary development plan which are not significant may be approved by the Planning Commission and disapproval of such changes by the Community Development Director or his designee. Significant changes in the revised preliminary development plan may only be approved after rehearing by the Planning Commission and City Council, such rehearing shall be subject to the notice and protest provisions set forth in Section 440.140.
5. For purposes of this Chapter, "significant changes" shall mean any of the following:
 - a. Increases in the density or intensity of residential uses of more than five percent (5%).
 - b. Increases in the total floor area of all non-residential buildings covered by the plan of more than ten percent (10%).
 - c. Increases of lot coverage of more than five percent (5%).
 - d. Increases in the height of any building of more than ten percent (10%).
 - e. Changes of architectural style which will make the project less compatible with surrounding uses.
 - f. Changes in ownership patterns or stages of construction that will lead to a different development concept.
 - g. Changes in ownership patterns or stages of construction that will impose substantially greater loads on streets and other public facilities.
 - h. Decreases of any peripheral setback of more than five percent (5%).
 - i. Decreases of areas devoted to open space of more than five percent (5%) or the substantial relocation of such areas.
 - j. Changes of traffic circulation patterns that will affect traffic outside of the project boundaries.

- k. Modification or removal of conditions or stipulations to the preliminary development plan approval which are considered significant in the opinion of the Community Development Director or his designee.

Staff believes that the proposed amendment is significant and thus, believes that this should be considered through the process as outlined in the Code.

Planning Commission

The Planning Commission held a public hearing at its regular meeting on June 24, 2019 to take comments on Application #19-03. After taking public input and giving due consideration, the Planning Commission voted (8-0) to recommend approval to the City Council of Application # 19-03: Amendment of Preliminary and Final Site Development Plan Review - Tidal Wave Auto Wash (Case # 17 -11).

City Council Recommendation

The City Council will consider the recommendation of the Planning Commission pertaining to Application # 19-03: Amendment of Preliminary and Final Site Development Plan Review - Tidal Wave Auto Wash (Case # 17 -11) at its regular meeting on July 17, 2019.

TIDAL WAVE AUTO SPA

124 East Thompson Street
P.O. Box 311
Thomaston, Georgia 30286
Office - 706.647.0414 Fax - 706.647.0474

May 9, 2019

City of Mission
Planning Department
6090 Woodson St.
Mission, KS 66202

Re: Amendment to Final Development Plan

Dear Mr. Scott:

I appreciate your recent follow up to check on the status of our project.

We've spent the last several months putting together construction pricing. Even after getting multiple bids and value engineering as much as possible, we've concluded with the current design, the project is not economically feasible.

In effort to make the project viable, we've been diligently working with our engineers, architects, and sub-contractors to redesign the building fronting Johnson Drive.

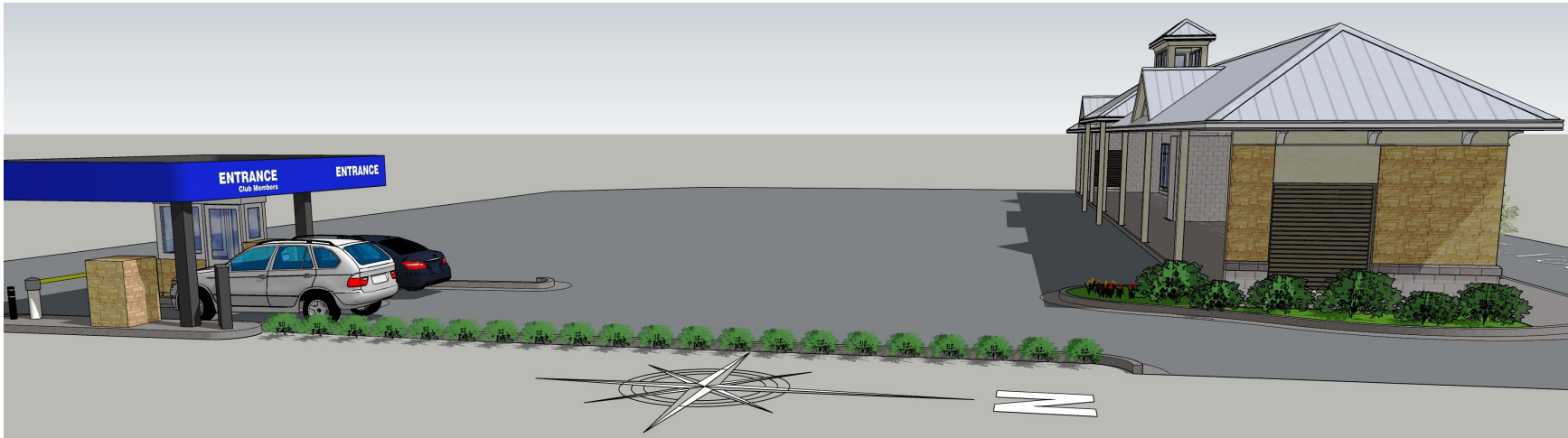
Per our conversation, Tidal Wave is respectfully requesting to amend the final development plan to allow for the re-designed building.

We ask to be put on the June 24th agenda to present the amended plan.
Please let me know if any additional items or information is needed from us.

Sincerely,



Pettey Hardin
Principal - K.C. markets



STAFF REPORT
Planning Commission Meeting May 29, 2018

AGENDA ITEM NO.: 3

PROJECT NUMBER / TITLE: Application # 17-11 Tidal Wave Auto Wash Final Site Development Plan Review

REQUEST: Site Plan review for the redevelopment of the former gas station site into a car wash

LOCATION: 6501 Johnson Drive

PROPERTY OWNER: CStore Investors Too, LLC
700 W 47th St, Ste 200
Kansas City, MO

APPLICANT: Steve Block
Block Real Estate Services, LLC
700 W 47th Ste, Ste 200
Kansas City, MO

STAFF CONTACT: Brian Scott, Assistant City Administrator

ADVERTISEMENT: 3/6/2018-The Legal Record newspaper

PUBLIC HEARING: Planning Commission meeting 3/26/2018

Property Information and Background

The subject property is located at 6501 Johnson Drive and has been used as a gas station / convenience store for many years. The property is currently zoned “CP-2B” Planned Retail and Service District, but is also within the West Gateway Form Based Code (FBC) Overlay District.



Figure 1: Subject Property-6501 Johnson Drive

The applicant, Mr. Steve Block, is a representative of the ownership group, CStore Investors Too, LLC of Kansas City, Missouri. They have a pending real estate contract to sell the property to Tidal Wave Auto Spa of Thomaston, Georgia. Upon purchase, Tidal Wave intends to demolish the existing structure(s) on site and develop a new, new automatic car wash tunnel and supporting services building totaling approximately 9,900 square feet. The proposed, new use is permitted with CP-2B zoned districts. However, because the property is within the West Gateway FBC Overlay District (identified as Block "Y" in the FBC), any new development would require conformance with the FBC.

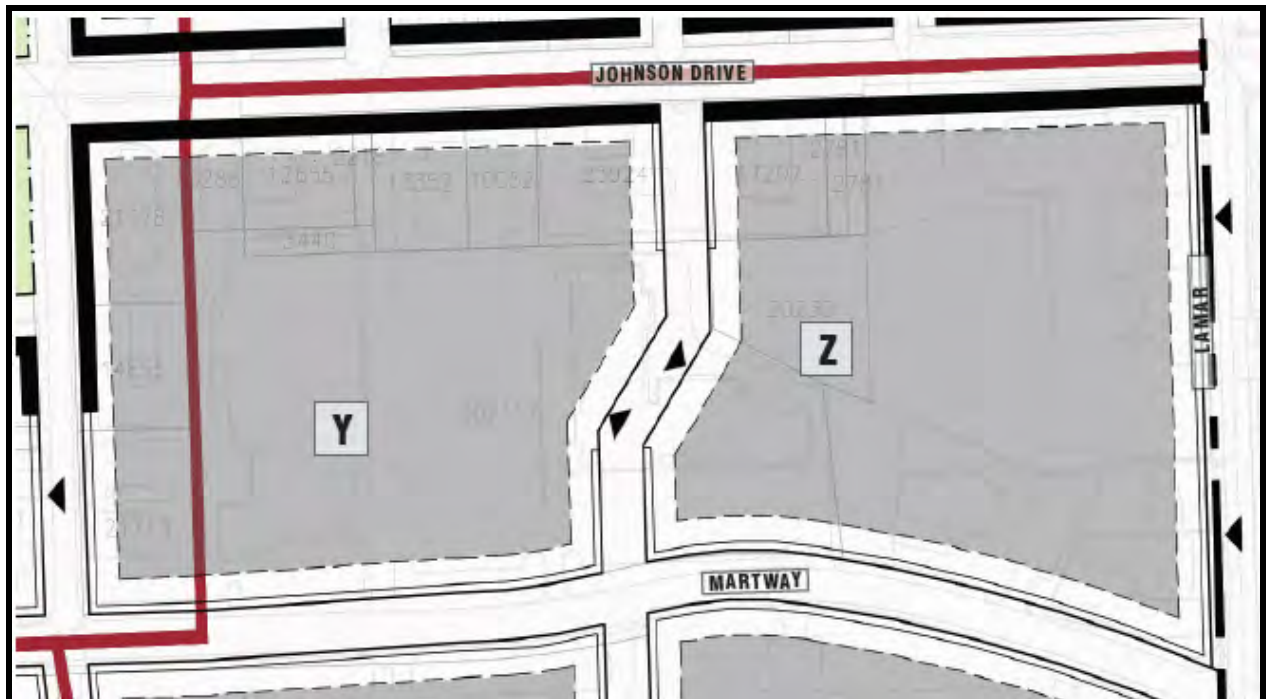
Form Based Code

Sector & Regulating Plan

This property is subject to the Form Based Code (FBC) as provided for in the West Gateway Overlay District. It is located in the northern part of Block "Y" in the Martway Sector. The applicant and developer are not proposing to assemble any additional parcels into the project. In the event of a conflict between the provisions of the FBC and the City Code or Johnson Drive Design Guidelines, the FBC takes precedence.

Details from the current regulating plan for this entire block include the following:

- Contains a combination of small and large parcels, many with good visibility from Johnson Drive.
- Ground level retail uses are important facing Johnson Drive. Retail is required along the frontage facing Johnson Drive. Parking and service areas should be accommodated away from Johnson Drive.
- The entire block is large enough to accommodate structured parking which would be a good fit with the existing grade difference between Johnson Drive and Martway Street.
- The extension of Walmer Street to the south connecting with Martway Street would be required in conjunction with development of the western half of Block Z.



Building Types Allowed

The following building types are allowed:

Townhouse type requires a minimum of 2 stories and a maximum of 4. All floors to be residential.

Mid-rise building type requires a minimum of 2 stories and a maximum of 4. Ground floor office or retail and upper floor office, retail or residential uses are permitted.

Parking Structure building type has no minimum or maximum floors. Ground floor commercial or office is required at principal and secondary frontages.

Low-rise building types are allowed in any sector of the FBC, provided the site under development is no larger than ½ acre. This particular site exceeds ½ acre, but is less than 3 acres and would qualify for up to 60% of the gross square footage of development (in one or multiple buildings) to be Low-Rise building type. The applicant has chosen not to incorporate the extension of Walmer Street into their redevelopment plans. Such a street dedication may have reduced the area of this parcel to below the ½ acre threshold.

Zoning of Surrounding Properties

North-"C-0" Office Building District, "C-1" Restricted Business District, "C2-B" Retail & Service District. Free standing medical office, fast food and coffee shop with drive-thru service.

East-"C-2B", Retail & Service District. Free standing auto repair shop.

South-"C2-A" Pedestrian Oriented Business District. Medical office, motor vehicle office, restaurant, fitness centers, discount stores and specialty retail located in a shopping center.

West-"C-2B", Retail & Service District. Free standing fast food restaurant.

Comprehensive Plan Future Land Use Recommendation

The future land-use map of the Comprehensive Plan designates the area as appropriate for mixed-use medium density re-development to include a pedestrian-friendly mix of housing, limited office and medium density retail to serve the residents of the surrounding neighborhood. It should serve as a transition zone between low to moderate density residential areas and higher intensity development.

Form Based Code & Plan Review

The West Gateway Form Based Code is a tool that assists in determining the appropriateness of development submittals to the City of Mission. Findings by Staff are determined using the scoring system contained in the FBC. The Form Based Code is structured hierarchically, understanding that certain elements are mandatory prerequisites, others are significant and others are minor. During the building of the Code, it was reinforced that, in order to achieve the level of quality in the urban environment that was envisioned from the vision plan process, more attention should be paid to those elements that directly contribute to the public realm than to the individual elements of architectural design.

Therefore, the review process is also structured hierarchically, so that major elements are reviewed first. This allows Staff and the City's consultants to provide the applicants with an opportunity to correct major flaws. Similarly, the early stages of review, including block configuration and building type, are more heavily weighted in the scoring process as they focus on elements that the Code regards as inviolate or of significant importance. Conversely, lesser items may yield a lower score but not result in a finding for disapproval. All of the elements are important, but have different values to the public realm.

The threshold score for a recommendation of approval by staff as established by the FBC scoring system is 90 points or more, of a possible 100 points including the prerequisite passing scores in steps 1 and 2. Proposals which achieve this score should be considered to be in compliance with the intent of the FBC. They proceed on under the review steps outlined in the FBC. Development submittals which achieve a score of 89 points or lower will proceed through the approval process of the zoning code including full design review by the Planning Commission and City Council.

Form Based Code review was conducted by the City's master developer and Staff. This application receives a score of 60 points and fails several critical prerequisite steps. This is largely due to all of the proposed buildings failing to meet the required building type standards. Buildings either failed to meet the minimum heights or were designed in a way that failed to reasonably accommodate the required building features. Additional points were lost as the applicant did not provide sufficient landscape, streetscape, or accessory structure information for review. Please see the attached letter from Core Design for additional details of this review.

The proposed development includes two buildings. One building is a two story, 6,699 square foot building fronting Johnson Drive to the north side of the property. The other is a 3,200 square foot building containing an automatic car wash tunnel along the south side of the property. The ground floor of the north building would consist of a series of drive-in parking stalls with vacuums for use by car wash customers. A small office would be on the second level and serve as the regional office and training facility for the company. The applicant has indicated that the depth of the north building is sufficient for use as retail space in the future if the proposed use as a car wash facility ceases.

Exterior building materials include split face block, stone veneer, standing seam metal, EIFS, metal awnings, and glass. Heavier materials such as block and brick are generally placed lower on the building walls. Partial information about percentages of EIFS and storefront glazing was provided. Windows and doors do not meet the minimum 60% requirement for storefronts in the north building along Johnson Drive. EIFS is used on 45% of the ground and upper floor of the north building. Dumpster, retaining wall, pay canopy and pay kiosk details were not provided in the initial review.

The development would utilize a surface parking lot located on the interior of the lot and accessed by two private driveways. A drive through lane for the wash tunnel is shown along the south side of the property. All traffic would enter the site via the easternmost driveway and exit to the west. The existing median on Johnson Drive will be altered to allow inbound and outbound turning movements. A median break is also shown after the payment kiosk to allow traffic to bypass the wash tunnel and enter the rest of the site.

The City's traffic engineer, GBA, has reviewed the site access plans, vehicle queues, turning templates, sidewalks, and traffic study and are generally satisfied with the design. GBA does recommend moving the median break to the west edge of the eastern drive entrance to allow vehicles to leave the wash tunnel queue before the pay station.

A landscape plan detailing streetscape improvements was not submitted with the initial review. The civil sheets generally show a sidewalk area meeting the FBC designs of 5' sidewalks with a 4' tree zones. Details of street trees and street furnishings like street lights, benches, trash receptacles and bike racks were not provided for initial review. All required streetscape

improvements shall be installed within the existing or proposed right-of-way. Final placement and configuration of these elements (street and pedestrian lighting system, site furnishings, trees, sidewalk paving, etc.) must be further coordinated with and approved by staff at the time of public improvement construction plans.

Storm water drainage is gradually west across the site. The proposed redevelopment would reduce the overall impervious surface area by 5%. Therefore, according to city ordinances, stormwater detention and BMP treatment are not required.

The Sustainability Commission has not yet reviewed this proposal for compliance with the Sustainability Scorecard tool. The scorecard contains a set of criteria intended to gauge the sustainability of new developments in Mission. It takes into account such factors as building materials, energy and water conservation, walkability, and light pollution. The score is then passed along to the City Council to help inform decisions about city incentives.

No development incentives are being requested by the applicant.

The project as submitted failed to receive a passing score in the FBC review as described above. Major flaws such as building types needed be addressed further and minor supporting details were not provided for review. Therefore, Staff recommended to the Planning Commission that recommend denial of the Preliminary and Final Site Development Plan Case # 17-11 for the Tidal Wave Auto Wash project.

Decision of The Planning Commission and City Council

An incentive for compliance with the FBC is that development projects can be sped through the development review process at a much quicker rate. Proposals which achieve a score of 90 or greater can be considered to be approved at the initial Preliminary Site Development plan review and do not require further design review by the Planning Commission (Final Site Development Plan Review) as would be the case with other development projects. The Planning Commission holds the authority for review and approval of form based code site plan applications. Those that do not receive a score of 90 or higher would be required to go through Final Site Development plan review. As with all Preliminary Site Development Plan Review, the City Council has final authority for review and approval.

The Planning Commission conducted a public hearing on March 26, 2018. At that time testimony from the City's Staff, the City's planning consultant for FBC, the applicant, and members of the public was taken. After due deliberations the Planning Commission voted 8-0 to accept that all of the required findings of fact as stipulated in the Staff's report would be met through compliance with the outlined below conditions, and therefore recommended approval of the development application.

The City Council meet in regular session on April 18, 2018 and voted 7-1 to uphold the decision of the Planning Commission.

Conditions to Be Met for Final Site Development Plan Review

The applicant has submitted materials for consideration of the Final Site Development Plan Review. Staff has reviewed the materials for conformance with the following conditions that were stipulated in the initial review and approval on March 26th. Staff's comments are in italics under each condition.

- 1) Complete information about percentages of EIFS and storefront glazing are to be provided for staff review and approval.

Approximate percentages of material coverings are summarized in the table below. Staff is satisfied that these meet the intent of the FBC.

North Building - Along Johnson Drive

Type of Finish	North	South	East	West
EIFS	8%	72%	32%	49%
Glazing	61%	20%	0%	13%
Stone Veneer	26%	6%	54%	27%
Split Face Block	5%	2%	14%	11%

South Building - Back of Property

Type of Finish	North	South	East	West
EIFS	39%	0%	23%	23%
Glazing	27%	67%	17%	17%
Stone Veneer	15%	22%	35%	35%
Split Face Block	20%	11%	26%	26%

- 2) Along the Johnson Drive frontage, windows and doors shall meet the minimum 60% total coverage of the storefront and EIFS or stucco shall not be used within 8' of the ground nor comprise more than 25% of the first story.

Plans indicate that the Johnson Drive Frontage of the proposed building will have 61% glazing (storefront windows). EIFS will only be 8% and is located along the top of the wall and parapet (or partial wall) at either end of the building. Stone veneer in a style known as "Southern LedgeStone" will be predominate on the front facade of the building. Split face block in an "Old Castle" style will go up the facade of the building. Approximately, two feet from the ground on the front facade of the building.

- 3) Complete information regarding trash enclosures, retaining walls, other screening, pay canopy and pay kiosk in compliance with the Form Based Code standards shall be provided for staff review and approval.

Information has been provided detailing trash enclosure, canopy, and pay kiosk. This information shows that material and style are in conformance with the FBC.

- 4) Complete details regarding the site landscape and public streetscape including street lights, benches, trash receptacles and bike racks in compliance with the Form Based Code standards shall be provided to staff for review and approval.

A landscape plan (sheet C-09) has been submitted indicating Shantung Maple, Velvet Pillar, and Everclear Laceback Elm to be planted along the front of the building on Johnson Drive. Each tree will be 2" caliber. Trees will be planted every 30 feet. Plans also indicate that tree grates will be incorporated into the sidewalk around each tree. Plantings through the rest of the site include Boxwood, Hooks Juniper, Switch Grass, Dwarf European Viburnum, and Periwinkle. Ground cover includes tall fescue and river rock.

Plans also indicate that two trash receptacles (Black 36 gallon steelsites RB-36) will be incorporated into the streetscape along Johnson Drive as well as two bike loops (creative pipe inverted "U"). There will also be one black, metal bench (72 inches). Details (pictures or drawings of these items were not provided).

In addition, plans indicate the installation of a decorative style of street light to be installed along Johnson Drive, but there are no pictures or drawings provided. A photometric plan (sheet E1.2) has been provided showing more standard parking light fixtures around the site (nothing on Johnson Drive), but no information about candlight is provided.

These later items will need to be confirmed and approved by staff at the time construction drawings are submitted to the City.

- 5) The median break in eastern entrance driveway shall be relocated to allow vehicles to leave the wash tunnel queue before the pay station.

Site plan sheet C-02 indicates that a median break has been created (note hash marks) to allow vehicles to exit the queue for the wash tunnel prior to reaching the pay kiosk.

- 6) Complete details regarding the circulation of trash and other service vehicles on site shall be provided for staff review and approval.

Site plan sheet C-02 also indicates circulation movements for trash pick-up. Developer has indicated that the truck will enter the western driveway and pull into the site and up to the trash enclosure. Once completed, the truck will leave through the eastern driveway.

- 7) Applicant and Developer agree to install a median break along the west edge of the entry drive into the car wash facility that aligns with the westbound parking lot aisle.

See response to condition 5.

- 8) Applicant and Developer consent to the Commission conditioning approval of the plan on compliance with such streetscape improvements, as are required by applicable City regulations and as set forth in the Form Based Code for the West Gateway Study Area, Chapter 6 (Landscape Architectural Guidelines, Type II-Urban Blvd).

See response to condition 4.

- 9) Applicant and Developer consent to the Commission conditioning the approval of the Plan upon the conveyance of right of way for sidewalks and landscaping along the site's Johnson Drive frontage in such dimensions as is required by City regulations and via conveyance language usually and customarily provided in similar circumstances by the City.

Site plans indicate appropriate setbacks for right-of-way for sidewalks and landscaping along the Johnson Drive frontage. These will be confirmed at the time detailed construction documents are submitted and corrections, if need be, made. Conveyance of right-of-way will take place through a separate mechanism and accepted by the City Council.

- 10) Applicant and Developer will consent to meeting with the Sustainability Commission and implementing mutually acceptable recommendations. Applicant and Developer are convinced that Developer operates the most environmentally sound and sustainable car washes in the country with emphasis on recycling and water conservation.

Applicant and Developer have been in contact with the Staff liaison to the Sustainability Commission to discuss submission requirements and times for presentation.

- 11) Applicant and Developer consent to the Commission making approval of the Plan conditioned on delivery of Architectural Plans, with corresponding calculations which shall provide that windows and doors shall meet the minimum 60% requirement of the Mid-Rise building's northern face, in compliance with applicable City regulations and as set forth in the Form Based Code for the West Gateway Study Area, Chapter 5 (Architectural Guidelines).

This has been indicated as such in the plans submitted. Staff will ensure this is still the case when final plans are submitted.

- 12) Applicants and Developer will consent to the Commission conditioning approval of the Plan on delivery of requisite landscape details.

See response to condition 4.

Additional Staff Comments

Though not specifically stated in the above conditions, it has been requested, and applicant and developer have stipulated to, the removing a portion of the traffic median in Johnson Drive so that the edge of the median aligns with the eastern edge of the east driveway. This will allow for

west bound traffic on Johnson Drive to make a left turn in to the site. Evidence should be shown in writing that the applicant and developer have gained approval with the property owner to the east for use of a shared driveway at the east entrance to the property.

The applicant and developer are proposing only one sign on the proposed building. This is to be located on the front of the building to the left. The sign is in accordance with the requirements of the sign code in that it cannot be more than 10% of the entire front facade. No other signs are indicated on the plans.

Attachments

- Item A - Street Rendering - 1
- Item B - Street Rendering - 2
- Item C - Tunnel Rendering
- Item D - Elevations of North Building
- Item E - Elevations of South Building (Car Wash Tunnel)
- Item F - Side Sections of Each Building
- Item G - Exterior Finishes
- Item H - Accessory Buildings
- Item I - Pay Canopy Detail
- Item J - Pay Canopy Picture
- Item K - Pre Canopy Detail
- Item L - Site Grading Plan
- Item M - Site Landscape Plan
- Item N - Ste Photometric Plan
- Item O - Site Circulation Plan

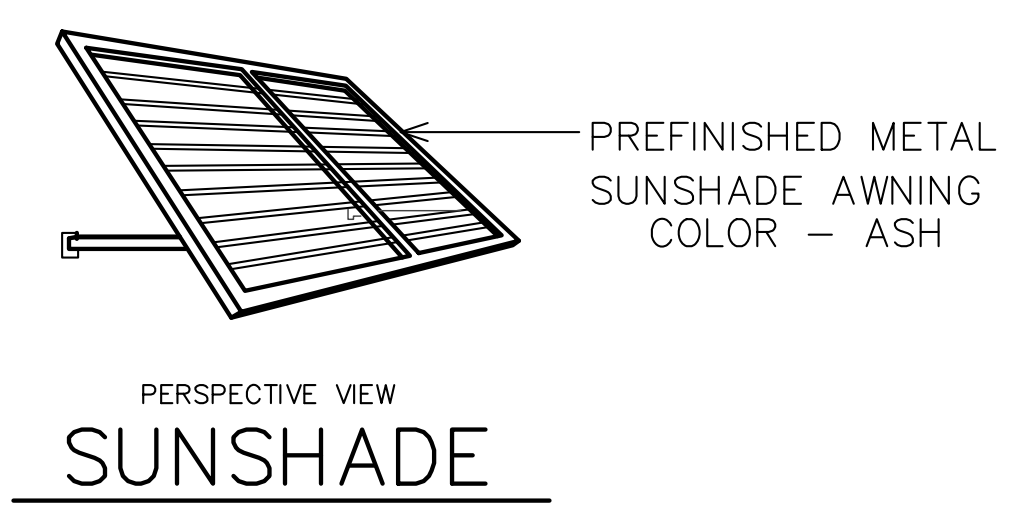
ALL GLAZING IS "CLEAR" GLAZING.



PREFERRED VIEW FROM SITE - SOUTH

SCALE: 3/16"=1'-0"

2562 SF WALL SURFACE EXCLUDING GABLES, ROOF, CUPOLA, & OPEN BAYS
 502 S.F. GLAZING (WINDOWS AND SF ENTRY)
 1834 S.F. EIFS OR HARD COAT STUCCO
 162 S.F. STONE
 64 S.F. SPLIT FACE BLOCK

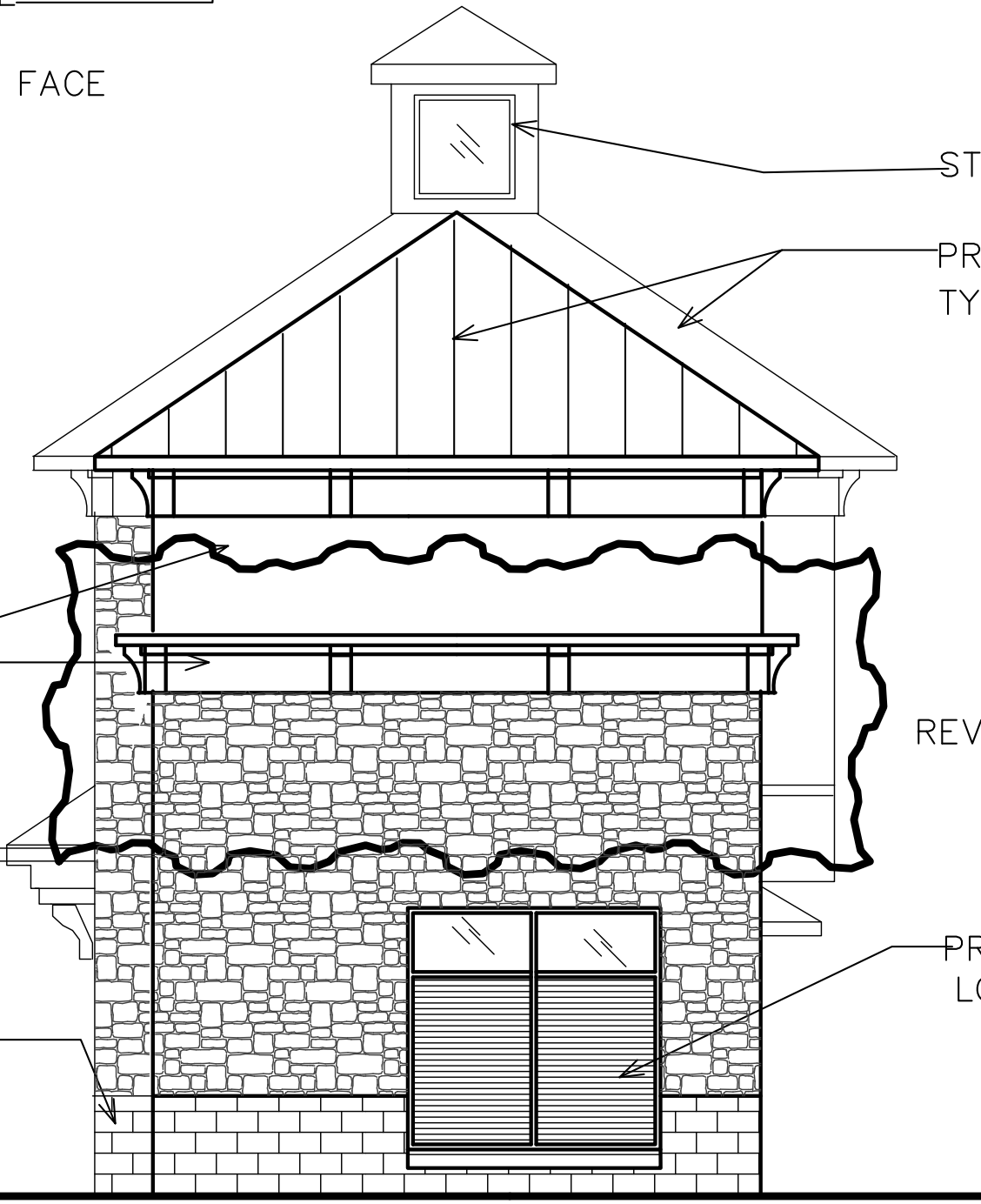
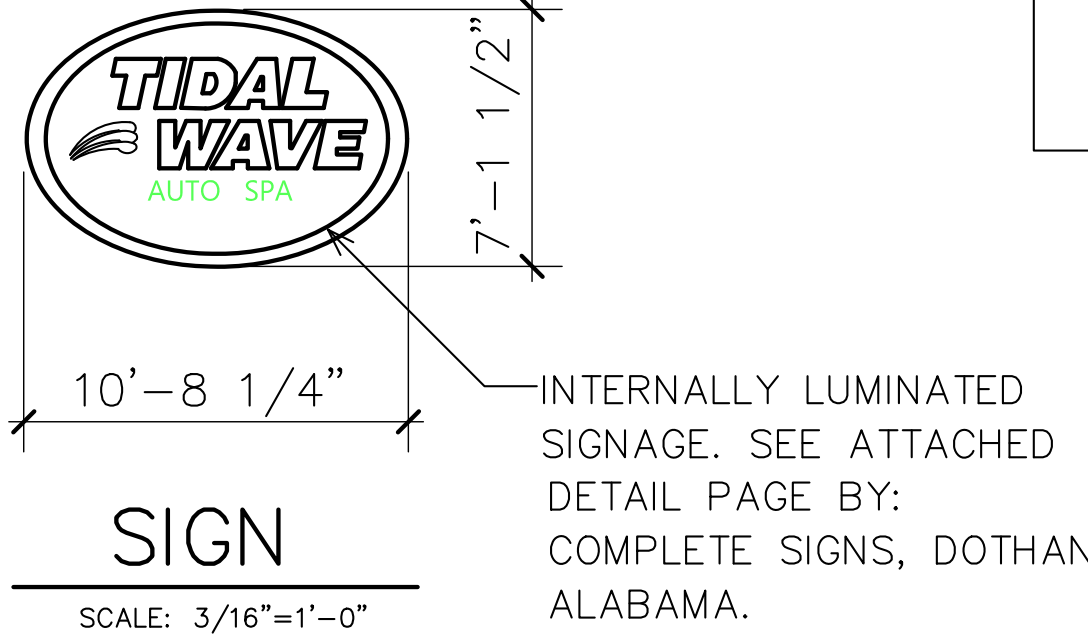


PREFERRED VIEW FROM JOHNSON DRIVE - NORTH

SCALE: 3/16"=1'-0"

OVERALL FRONT FACADE VENEER CALCULATIONS:
 3850 SF WALL SURFACE EXCLUDING GABLES, ROOF, CUPOLA.
 2337 S.F. GLAZING (WINDOWS AND SF ENTRY)
 313 S.F. EIFS OR HARD COAT STUCCO (LESS THAN 25% OF FRONTAGE AREA)
 1000 S.F. STONE
 200 S.F. SPLIT FACE BLOCK

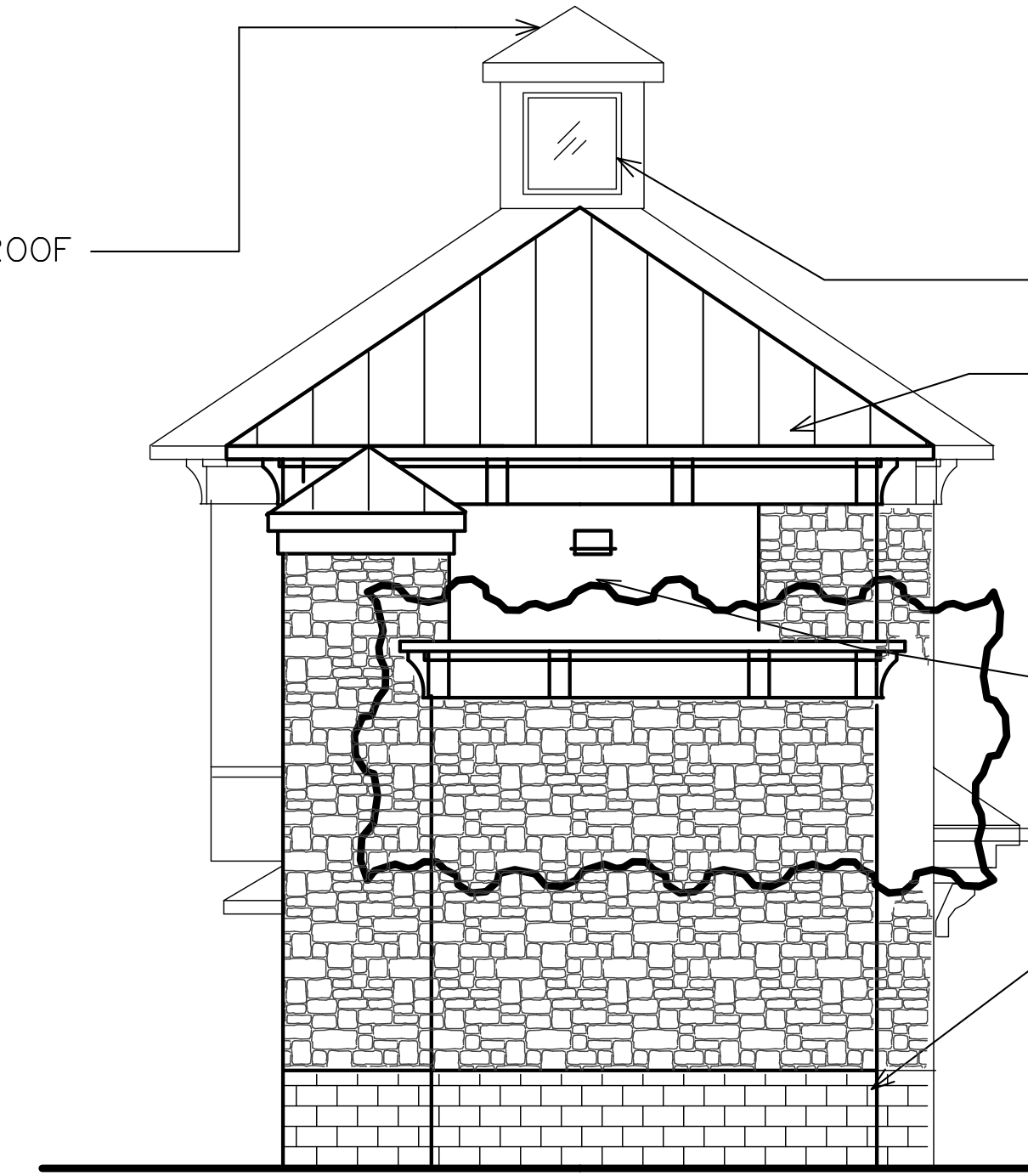
FIRST STORY FRONT FACADE VENEER CALCULATIONS:
 2,080 S.F. WALL SURFACE EXCLUDING GABLES, ROOF, CUPOLA.
 1393 S.F. GLAZING (WINDOWS AND SF ENTRY)
 100 S.F. EIFS OR HARD COAT STUCCO ACCENT (LESS THAN 25% OF FRONTAGE AREA)
 387 S.F. STONE
 200 S.F. SPLIT FACE BLOCK



PREFERRED SIDE ELEVATION WEST

SCALE: 3/16"=1'-0"

452 SF WALL SURFACE EXCLUDING GABLES, ROOF, CUPOLA.
 60 S.F. GLAZING (WINDOWS AND LOUVER)
 220 S.F. EIFS OR HARD COAT STUCCO
 122 S.F. STONE
 50 S.F. SPLIT FACE BLOCK



PREFERRED SIDE ELEVATION EAST

SCALE: 3/16"=1'-0"

514 SF WALL SURFACE EXCLUDING GABLES, ROOF, CUPOLA.
 0 S.F. GLAZING
 164 S.F. EIFS OR HARD COAT STUCCO
 280 S.F. STONE
 70 S.F. SPLIT FACE BLOCK

PROPOSED REVISIONS:
 THESE ITEMS DO NOT IMPACT THE OVERALL MATERIALS AND GLAZING AREA CALCULATIONS.
 THESE ITEMS DO NOT INCREASE THE ORIGINAL ARCHITECTURAL BUILDING FOOTPRINT.

REV 1 IN LIEU OF SLOPING METAL ROOF AT WEST END 1 STORY AREA, PROVIDE FLAT ROOF AREA WITH PARAPET WALLS SCREENING MECHANICAL EQUIPMENT.

REV 2 IN LIEU OF UNCOVERED EQUIPMENT ENCLOSURE AT EAST END, PROVIDE ENCLOSED EQUIPMENT ROOM WITH PARAPET WALLS SCREENING MECHANICAL EQUIPMENT LOCATED ABOVE.

5/21/18

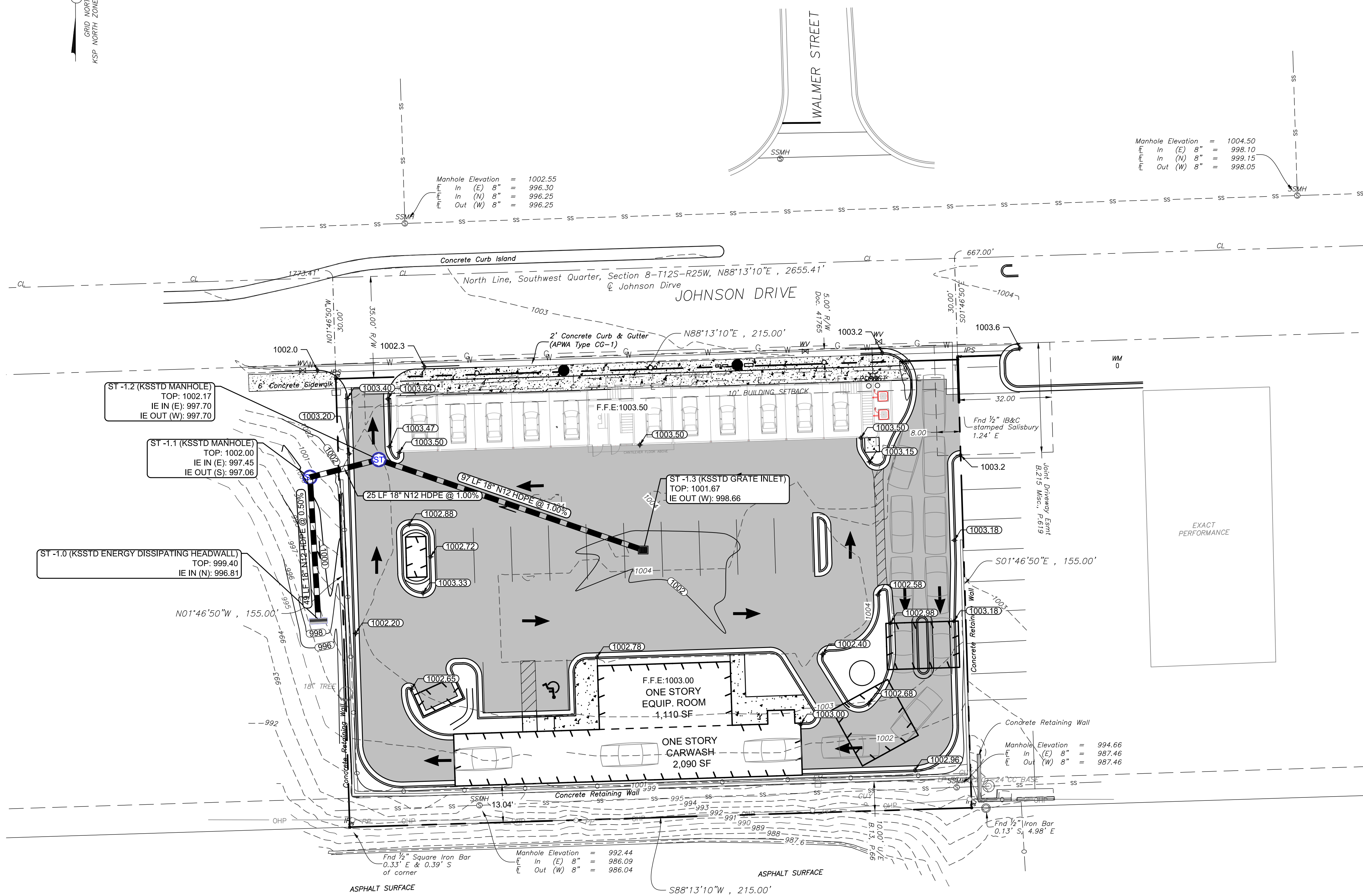
A NEW
 TIDAL WAVE AUTO SPA
 MISSION, KANSAS

ARCHITECT:
M. TODD ALBRITTON
ARCHITECT

202 EAST MAIN STREET
 THOMASTON, GEORGIA
 770-550-3275
 mtoddalbrittonarchitect@gmail.com

KANSAS REGISTERED ARCHITECT
 #6993

GRID NORTH
KSP NORTH ZONE NAD 83



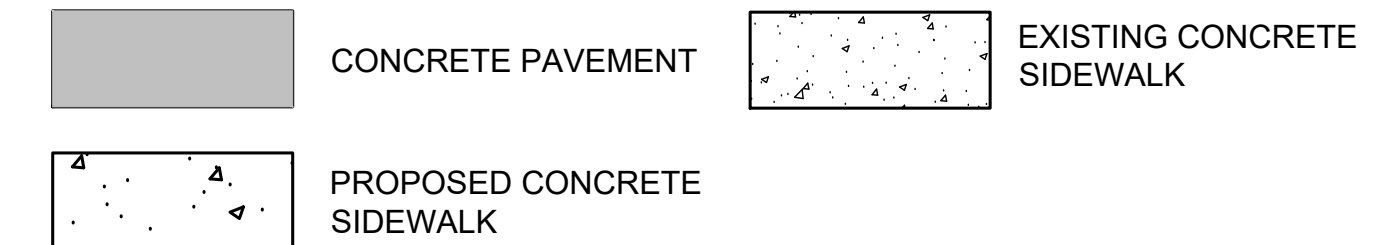
PAVING, GRADING, AND DRAINAGE NOTES

1. CONTRACTOR TO PROVIDE POSITIVE DRAINAGE AWAY FROM BUILDING.
2. ENTIRE SITE SHALL BE DRESSED TO UNIFORM, WELL DRAINED AND VISUALLY APPEALING SURFACE WITH A MINIMUM TOPSOIL LAYER OF FOUR INCHES.
3. MAXIMUM LONGITUDINAL SLOPE ON ALL ACCESSIBLE SIDEWALK SHALL BE 5.00%.
4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THE SITE IS ADA ACCESSIBLE AS REQUIRED BY FEDERAL, STATE AND LOCAL GOVERNMENT.
5. FINISH GRADE ELEVATIONS INDICATE TOP OF PAVEMENT / FACE OF CURB UNLESS OTHERWISE NOTED.
6. REFER TO DETAILS FOR PAVEMENT TYPICAL SECTION.
7. SITE SHALL BE GRADED UNIFORMLY FOR POSITIVE DRAINAGE AS SHOWN FROM THE ELEVATIONS, PROPOSED CONTOURS, AND THE DRAINAGE SLOPE ARROWS.
8. MAXIMUM SIDEWALK CROSS SLOPE IS 2%. MAXIMUM SIDEWALK LONGITUDINAL SLOPE IS 5%. MAXIMUM PAVEMENT SLOPE IN HANDICAP PARKING AREA AND AISLE IS 2%. MAXIMUM HANDICAMP RAMP SLOPE IS 12:1V.

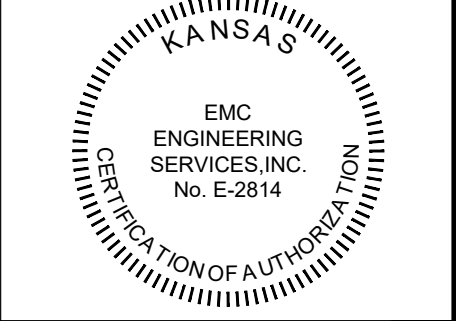
UTILITY NOTES:

1. THE LOCATION OF ALL EXISTING UNDERGROUND UTILITIES AS SHOWN HEREON ARE BASED ON ABOVE-GROUND STRUCTURES AND IS APPROXIMATE AND SHOWN FOR INFORMATIONAL PURPOSES ONLY. NO CERTIFICATION IS MADE AS TO THE ACCURACY OR COMPLETENESS OF THIS INFORMATION.
2. ADDITIONAL BURIED UTILITIES / STRUCTURES MAY BE ENCOUNTERED. NO EXCAVATIONS WERE MADE DURING THE PROGRESS OF THIS SURVEY TO LOCATE BURIED UTILITIES / STRUCTURES.
3. BEFORE EXCAVATIONS ARE BEGUN, GIVE THREE WORKING DAYS NOTICE TO THE UTILITIES PROTECTION CENTER AT KANSAS 811 PRIOR TO ANY EXCAVATION IN ORDER THAT UNDERGROUND UTILITIES MAY BE LOCATED AND PROTECTED.

HATCH LEGEND:



NO.	REVISION DESCRIPTION	BY	DATE
1	PRELIMINARY DEVELOPMENT PLAN	DEM	04/11/18
2	FINAL DEVELOPMENT PLAN	DEM	06/09/18



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 Ph: (229) 435-6133
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 www.emc-eng.com

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OFFICE LOCATIONS: ALBANY, ATLANTA, AUGUSTA, BRUNSWICK, COLUMBUS, SAVANNAH, STATESBORO, AND VALDOSTA

PAVING, GRADING, & DRAINAGE PLAN

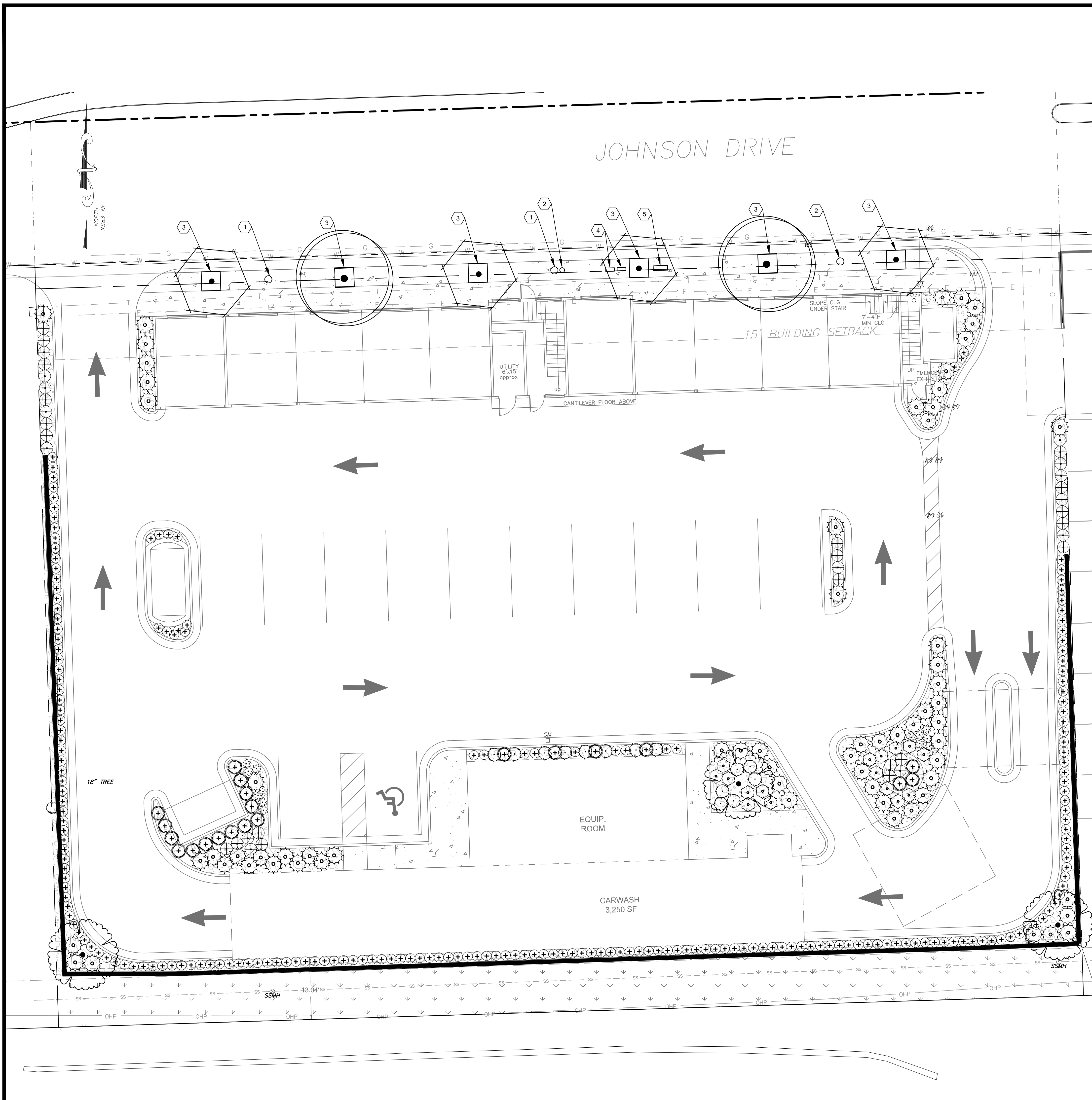
TIDAL WAVE AUTO SPA
 PROPERTY ID #: KP69500001 0003
 MISSION, JOHNSON COUNTY, KANSAS

Prepared for:
 SHJ CONSTRUCTION GROUP

PROJECT NO.:	17-6044
DRAWN BY:	DEM
DESIGNED BY:	DEM
SURVEYED BY:	CFSE
SURVEY DATE:	05/30/2017
CHECKED BY:	BHB
SCALE:	1" = 20'
DATE:	05/14/2018



Know what's below.
 Call before you dig.



PLANT SCHEDULE				
TREES	QTY	BOTANICAL NAME / COMMON NAME	CONT	CAL
	2	Acer truncatum / Shantung Maple	25 gal	2" Cal
	3	Malus x 'Velvetcole' / Velvet Pillar	B & B	2" Cal
	4	Ulmus parvifolia 'Everclear' / Everclear Lacebark Elm	25 gal	2" Cal
SHRUBS	QTY	BOTANICAL NAME / COMMON NAME	CONT	PLANT HT
	14	Buxus x 'Green Velvet' / Boxwood	5 gal	
	13	Hemerocallis x 'Little Business' / Little Business Daylily	1 gal	
	21	Juniperus chinensis 'Hooks' / Hooks Juniper FOR PLANTING SPECIFICATIONS SEE DETAIL 803 & 804, SHEET L2.1	B & B	6' Ht
	66	Juniperus virginiana 'Grey Guardian' / Greguard Juniper	5 gal	3' height
	35	Panicum virgatum 'Shenandoah' / Switch Grass FOR PLANTING SPECIFICATIONS SEE DETAIL 803 & 804, SHEET L2.1	1 gal	
	16	Viburnum opulus 'Nanum' / Dwarf European Viburnum FOR PLANTING SPECIFICATIONS SEE DETAIL 803 & 804, SHEET L2.1	5 gal	1.5' ht.
	211	Vinca major / Periwinkle	1 gal	
GROUND COVERS	QTY	BOTANICAL NAME / COMMON NAME	CONT	
	2,194 sf	Festuca arundinacea 'Heatwave+ Blend' / Heatwave+ Blend of Tall Fescue 5% Kelly Bluegrass 95% - 3 species Turf Fescue (Covenant II, Avenger, Garrison)	seed	
GROUND COVERS	QTY	BOTANICAL NAME / COMMON NAME	SIZE	DEPTH
	24 cy	Buffalo River Rock	1'-3"	3" deep

SITE DATA

	Data	Required	Provided
Site Area	0.76		
Site Trees			
1 tree/3,000 SF of landscaped open space	4,846.05	1.62	2
Street Trees			
1 tree/40 LF of street frontage	215	5.375	6
Parking Lot Trees			
1 tree/20 cars	12	0.6	1

- HARDSCAPE NOTES:**
- STREET LIGHTS 60' O.C. (SENTRY SCP LUMINAIRE / NEW YORK BLACK POST WITH TWO KEYSTONE RIDGE BASKET PLANTER PER POLE)
 - TRASH RECEPTACLE (BLACK 36 GALLON STEEL SITES RB-36 BY VICTOR STANLEY)
 - STREET TREES (30' O.C. WITH A NEEHAH FOUNDRY TREE GRATE #R-8726)
 - TWO BIKE LOOPS (CREATIVE PIPE: BLACK INVERTED "U" BIKE RACK)
 - BENCH (BLACK 72" SCARBOROUGH BY LANDSCAPE FORMS)

- LANDSCAPE NOTES:**
- THE CONTRACTOR SHALL VERIFY AND COORDINATE ALL FINAL GRADE WITH THE LANDSCAPE ARCHITECT AND OR DESIGN TEAM PRIOR TO COMPLETION.
 - LOCATION AND PLACEMENT OF ALL PLANT MATERIAL SHALL BE COORDINATE WITH THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
 - LOCATION OF ALL UTILITIES ARE APPROXIMATE, THE CONTRACTOR SHALL FIELD VERIFY LOCATIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION OPERATIONS.
 - REFER TO CIVIL DRAWINGS FOR ALL GRADING AND BERMING, EROSION CONTROL, STORM DRAINAGE, UTILITIES AND SITE LAYOUT.
 - PLANT QUANTITIES ARE FOR INFORMATION ONLY DRAWING SHALL PREVAIL IF CONFLICT OCCURS. CONTRACTOR IS RESPONSIBLE FOR CALCULATING OWN QUANTITIES AND BID ACCORDINGLY.
 - THE CONTRACTOR IS TO NOTIFY LANDSCAPE ARCHITECT AFTER STAKING IS COMPLETE AND BEFORE PLANT PITS ARE EXCAVATE.
 - TREE LOCATIONS IN AREAS ADJACENT TO DRIVES, WALKS, WALLS AND LIGHT FIXTURES MAY BE FIELD ADJUSTED AS APPROVED BY LANDSCAPE ARCHITECT.
 - THE CONTRACTOR SHALL REPORT SUBSURFACE SOIL OR DRAINAGE PROBLEMS TO THE LANDSCAPE ARCHITECT.
 - THE PLAN IS SUBJECT TO CHANGES BASED ON PLANT SIZE AND MATERIAL AVAILABILITY. ALL CHANGES OR SUBSTITUTIONS MUST BE APPROVED BY THE CITY OF MISSION, KANSAS AND THE LANDSCAPE ARCHITECT.
 - ALUMINUM LANDSCAPE EDGING TO BE USED ON ALL LANDSCAPE BEDS ABUTTING TURF AREAS AS NOTED ON LANDSCAPE PLANS.
 - LANDSCAPE CONTRACTOR IS TO BE RESPONSIBLE FOR WATERING ALL PLANT MATERIAL UNTIL THE TIME THAT A PERMANENT WATER SOURCE IS READY.
 - THE CONTRACTOR SHALL SHOW PROOF OF PROCUREMENT, SOURCES, QUANTITIES AND VARIETIES FOR ALL SHRUBS, PERENNIALS ORNAMENTAL GRASSES AND ANNUALS WITHIN 21 DAYS FOLLOWING THE AWARD OF THE CONTRACT.
 - CONTRACTOR SHALL PROVIDE FULL MAINTENANCE FOR NEWLY LANDSCAPED AREAS FOR A PERIOD OF 30 DAYS AFTER THE DATE OF FINAL ACCEPTANCE. AT THE END OF THE MAINTENANCE PERIOD, A HEALTHY, WELL-ROOTED, EVE-COLORED, VIABLE TURF AND LANDSCAPED AREA MUST BE ESTABLISHED. THE LANDSCAPED AREAS SHALL BE FREE OF WEEDS, OPEN JOINTS, BARE AREAS AND SURFACE IRREGULARITIES.
 - LANDSCAPE CONTRACTOR SHALL PROVIDE ROCK MULCH SAMPLE TO OWNER FOR APPROVAL.

NO.	REVISION DESCRIPTION	BY	DATE

BHC RHODES
Civil Engineering - Surveying - Utilities
7101 College Blvd., Suite 400
Overland Park, Kansas 66210
p. (913) 663-1900 f. (913) 663-1633
BHC RHODES is a trademark of Brueggli Horowitz & Company, P.A.

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CIVIL ENGINEERING
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OFFICE LOCATIONS: ALBANY, ATLANTA, AUGUSTA, BRUNSWICK, COLUMBUS, SAVANNAH, STATESBORO, AND VALDOSTA

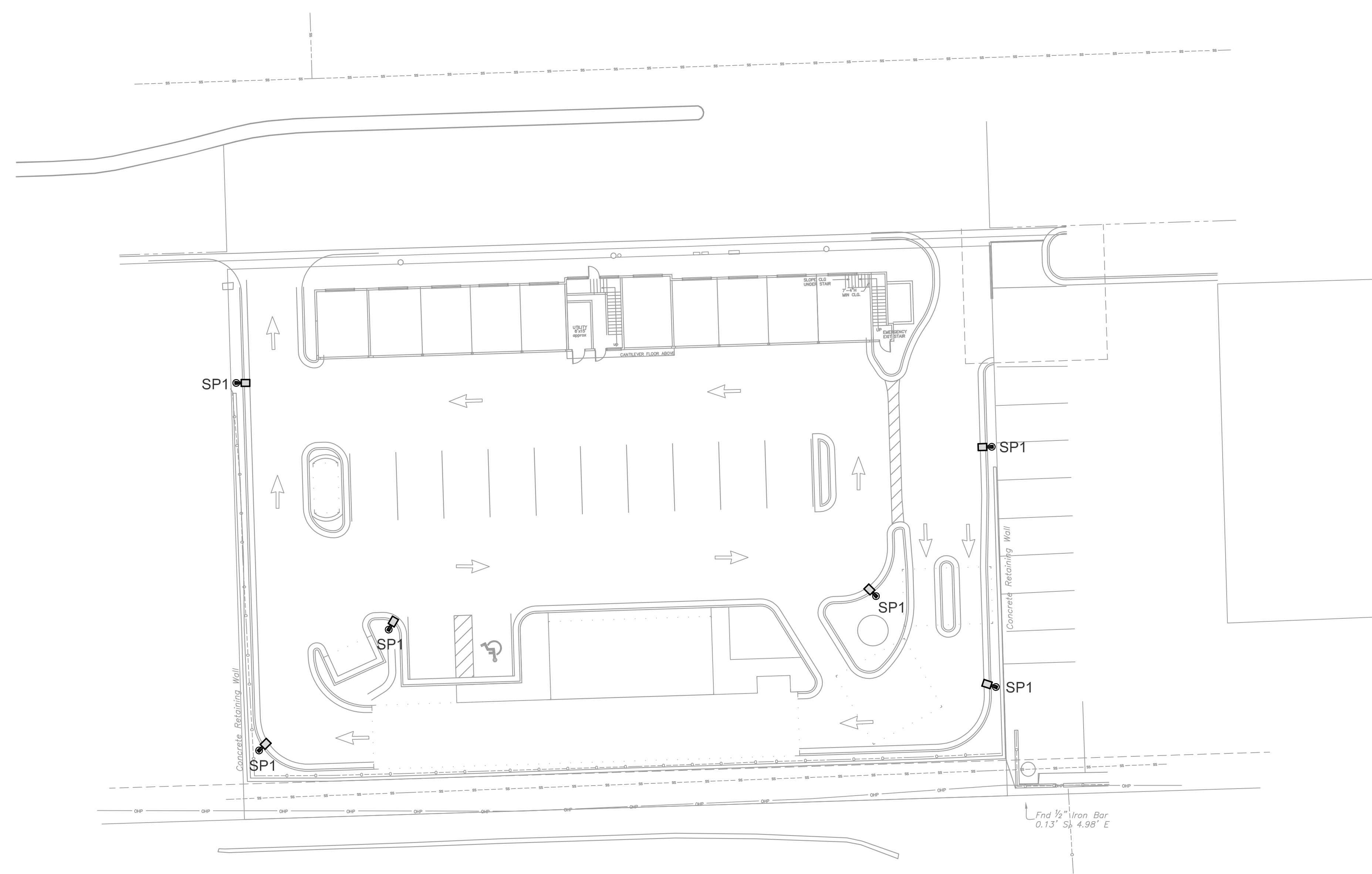
LANDSCAPE PLAN

TIDAL WAVE AUTO SPA
PROPERTY ID #: KP69500001 0003
MISSION, JOHNSON COUNTY, KANSAS
Prepared for:
SHJ CONSTRUCTION GROUP

PROJECT NO.: 17-6044
DRAWN BY: NAB
DESIGNED BY: NAB
SURVEYED BY: CFSE
SURVEY DATE: 05/30/2017
CHECKED BY: BHB
SCALE: 1" = 10'
DATE: 05/14/2018

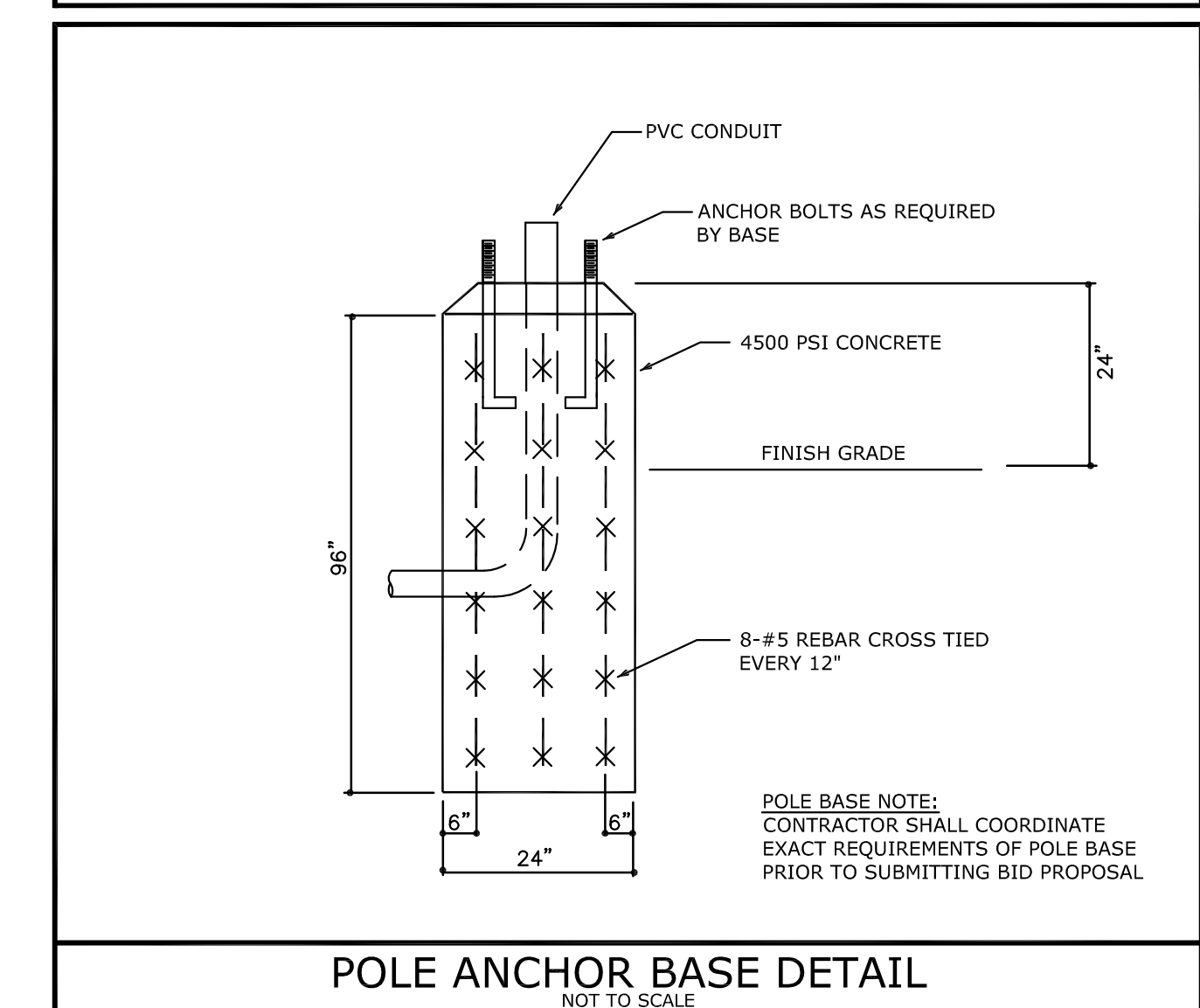
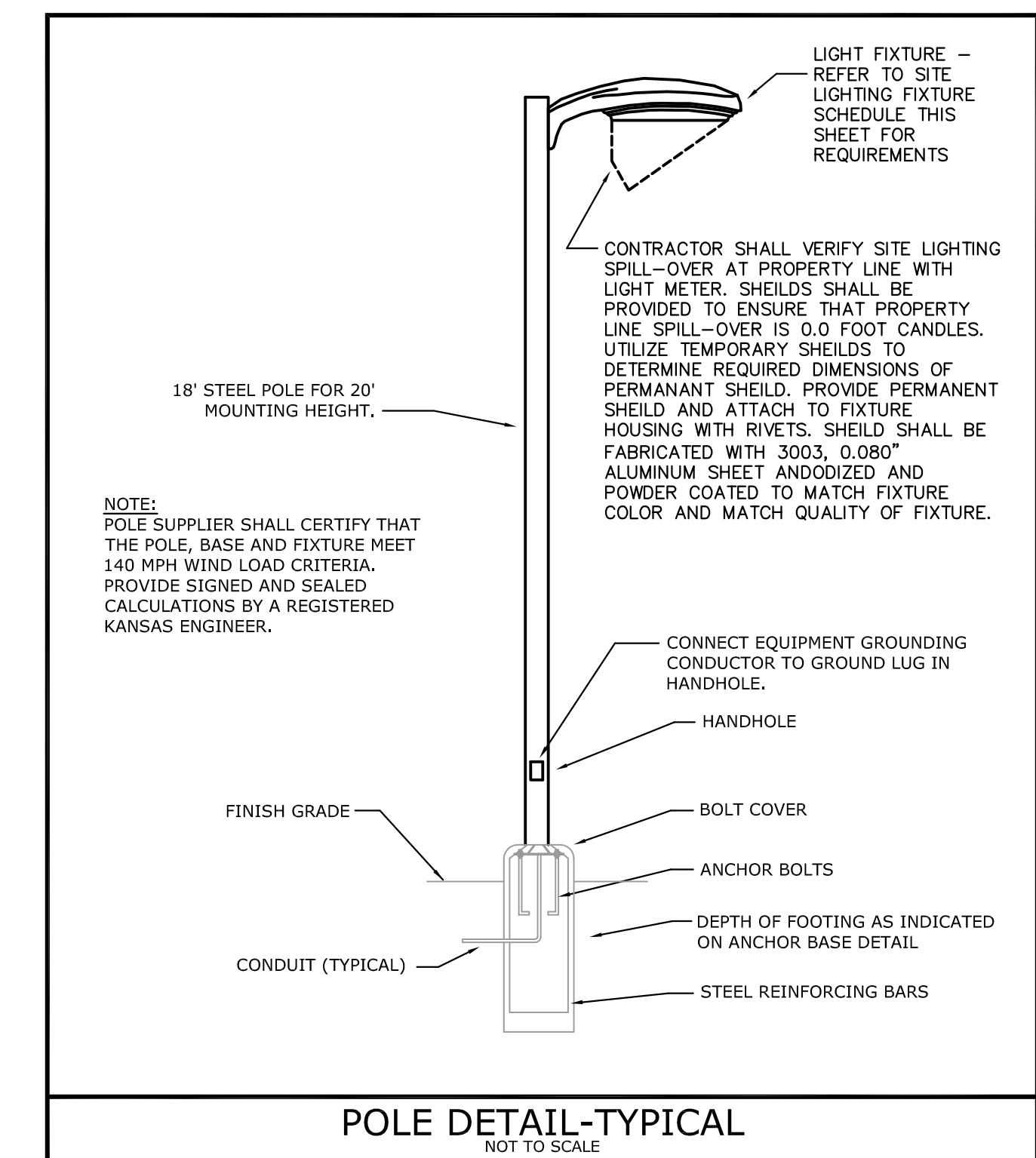
SITE LIGHTING FIXTURE SCHEDULE							
Symbol	Label	QTY	Manufacturer	Catalog Number	Description	Lamp	Wattage
	WM1	5	WILLIAMS OUTDOOR	WPTZ-L38-750	MEDIUM TRAPEZOID WALLPACK		3828.931
	SP1	6	Crow Inc	OSQ-A-xx-4ME-B-57K-ULxxxxx w/OSQ-BLSMF CONFIGURED FROM OSQ-A-xx-4ME-U-57K-ULxxxxx w/OSQ-BLSLF	Crow OSQ Series Area Luminaire, Type IV Medium w/ Backlight Shield, B Input Power Designator, 5700K	CONFIGURED FROM MDA	8950.428
							41.57

Statistics						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Parking Lot	X	- fc	- fc	- fc	--	--
Parking Lot to Boundary	+	- fc	- fc	- fc	N/A	N/A
Property Line	+	- fc	- fc	- fc	N/A	N/A

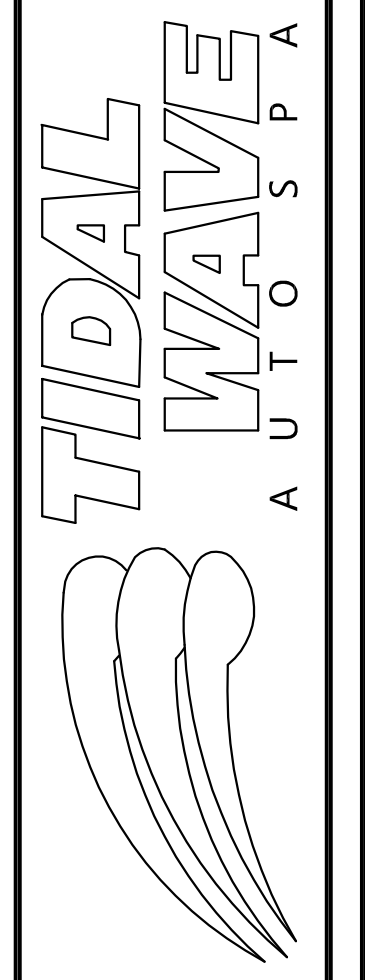


SITE PHOTOMETRIC PLAN

SCALE _____ 1"=20'-0"



TIDAL WAVE AUTO SPA
CAR WASH FACILITY
MISSION, KANSAS



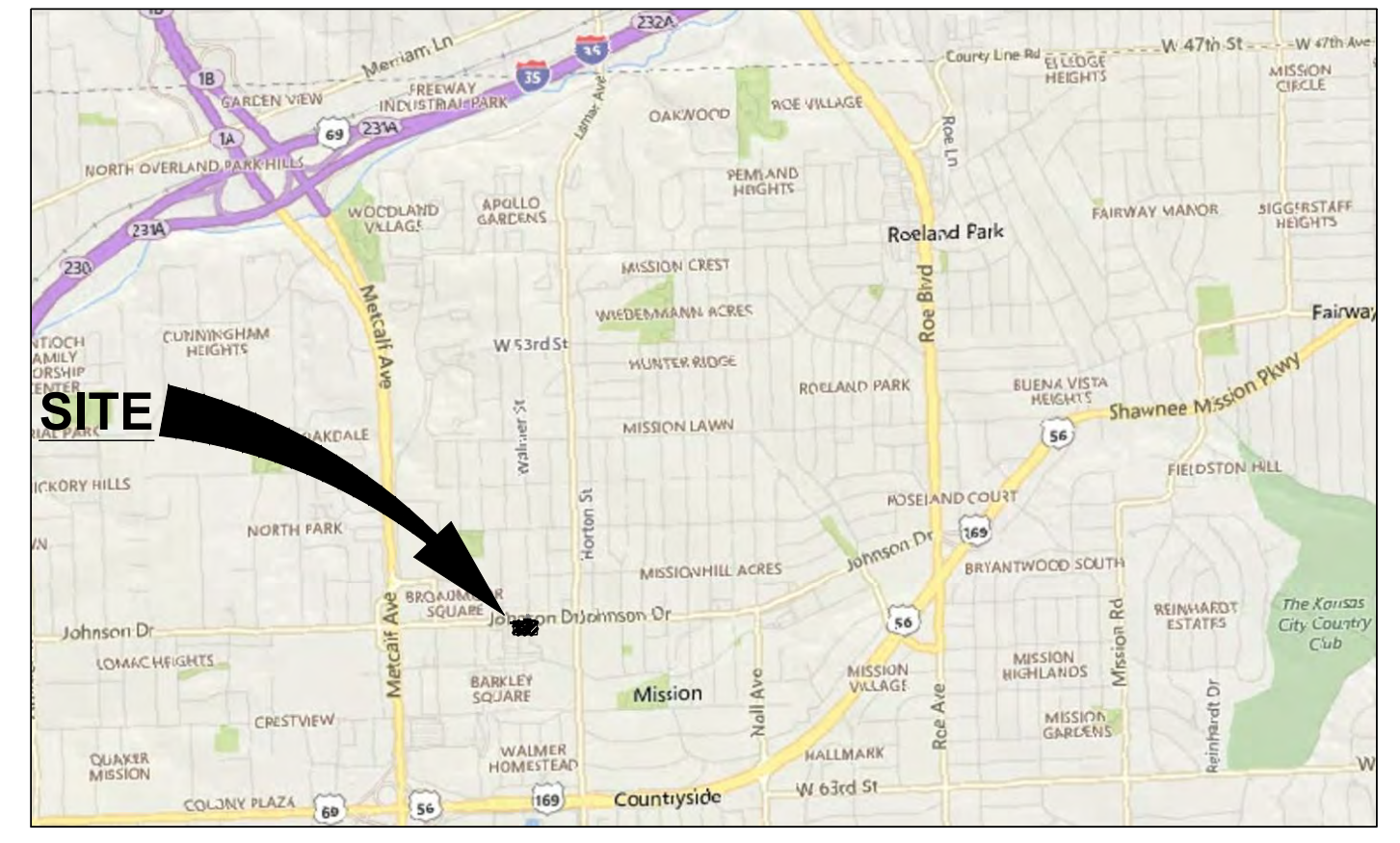
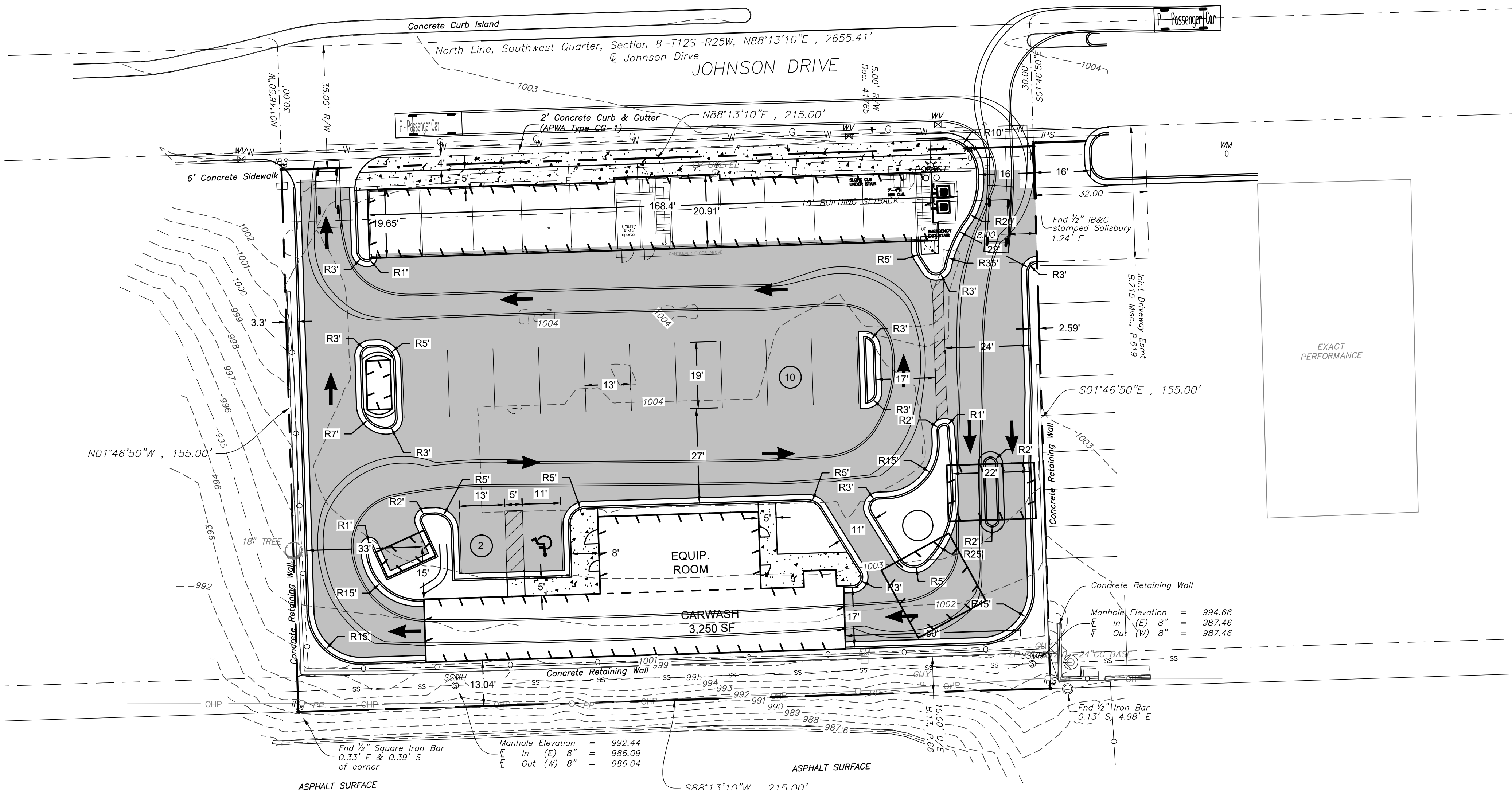
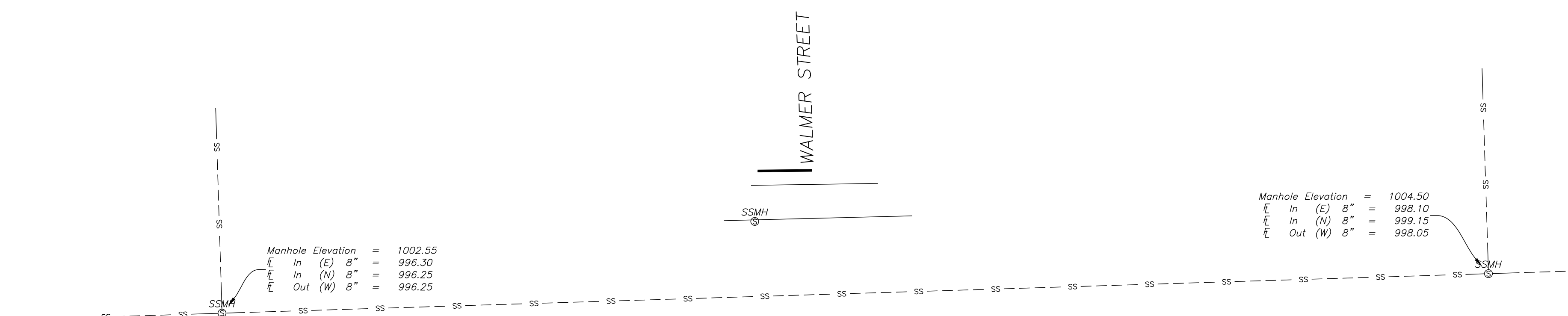
EA

SHEET: **E1.2**

3 OF 4

ROBERT L. CONNORS, P.E.
KANSAS LICENSE #23160
12738 HIGHWAY 92 E
WOODSTOCK, GA 30188
PROJECT MANAGER: STEPHEN EDWARDS
PHONE 678-466-7433

DATE: MAY 10, 2018 PROJECT DRAWN BY: SPE



VICINITY MAP
N.T.S.

OWNER INFORMATION

OWNER: CSTORE INVESTORS TOO, LLC
ADDRESS: 700 W 47TH ST, KANSAS CITY, MO 64118

APPLICANT INFORMATION

APPLICANT: TW MACON, LLC (d.b.a. TIDAL WAVE AUTO SPA)
ADDRESS: 124 THOMPSON STREET, THOMASTON, GEORGIA 30286
PHONE #: 770-271-5646

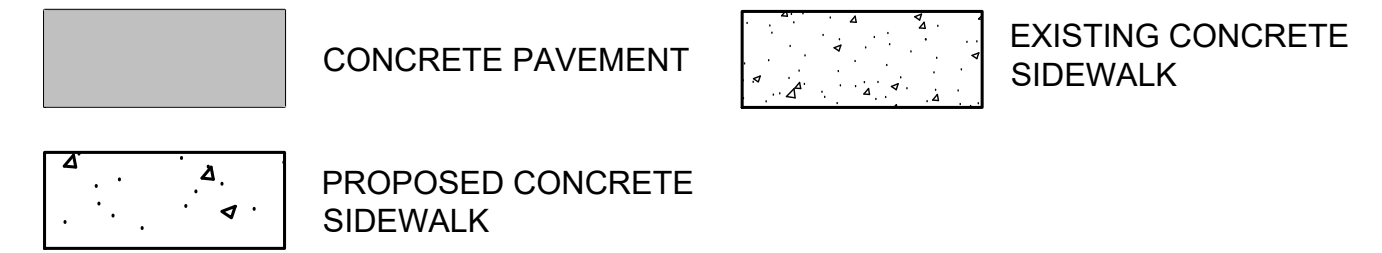
SITE INFORMATION

ADDRESS: 6501 JOHNSON DR., MISSION, KANSAS 66202
PROPERTY AREA: ±0.76 AC
PROPERTY ZONING: CP-2B
BUILDING SETBACKS:
FRONT: 0'-10"
SIDE: 0'
REAR: 0'

PARKING NOTE

PARKING REQUIRED: 3.5 SPACE PER 1,000 SF (3,250 / 1,000) X 3.5 = 12 SPACES
PARKING PROVIDE: 11 STANDARD SPACES + 1 HANDICAPPED SPACES = 12 TOTAL SPACES

HATCH LEGEND:



GENERAL NOTE:

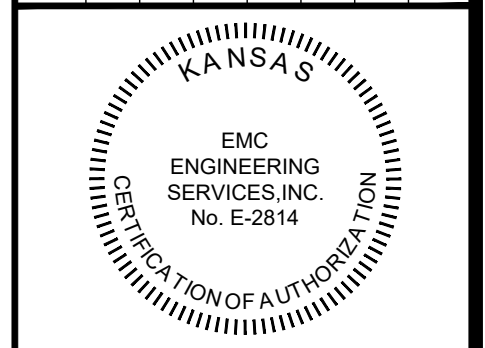
1. ALL DEVELOPMENTS SHALL CONFORM TO THE REQUIREMENTS OF THE CITY OF MISSION'S STORM WATER MANAGEMENT CRITERIA INCORPORATED BY REFERENCE HEREIN.

STORMWATER NOTES:

EXISTING CONDITION
PERVIOUS AREA = ± 0.05 AC (7%)
IMPERVIOUS AREA = ± 0.71 AC (93%)

PROPOSED CONDITION
PERVIOUS AREA = ± 0.08 AC (11%)
IMPERVIOUS AREA = ± 0.68 AC (89%)

NO.	REVISION DESCRIPTION	BY	DATE



GRAPHIC SCALE: 1" = 20'

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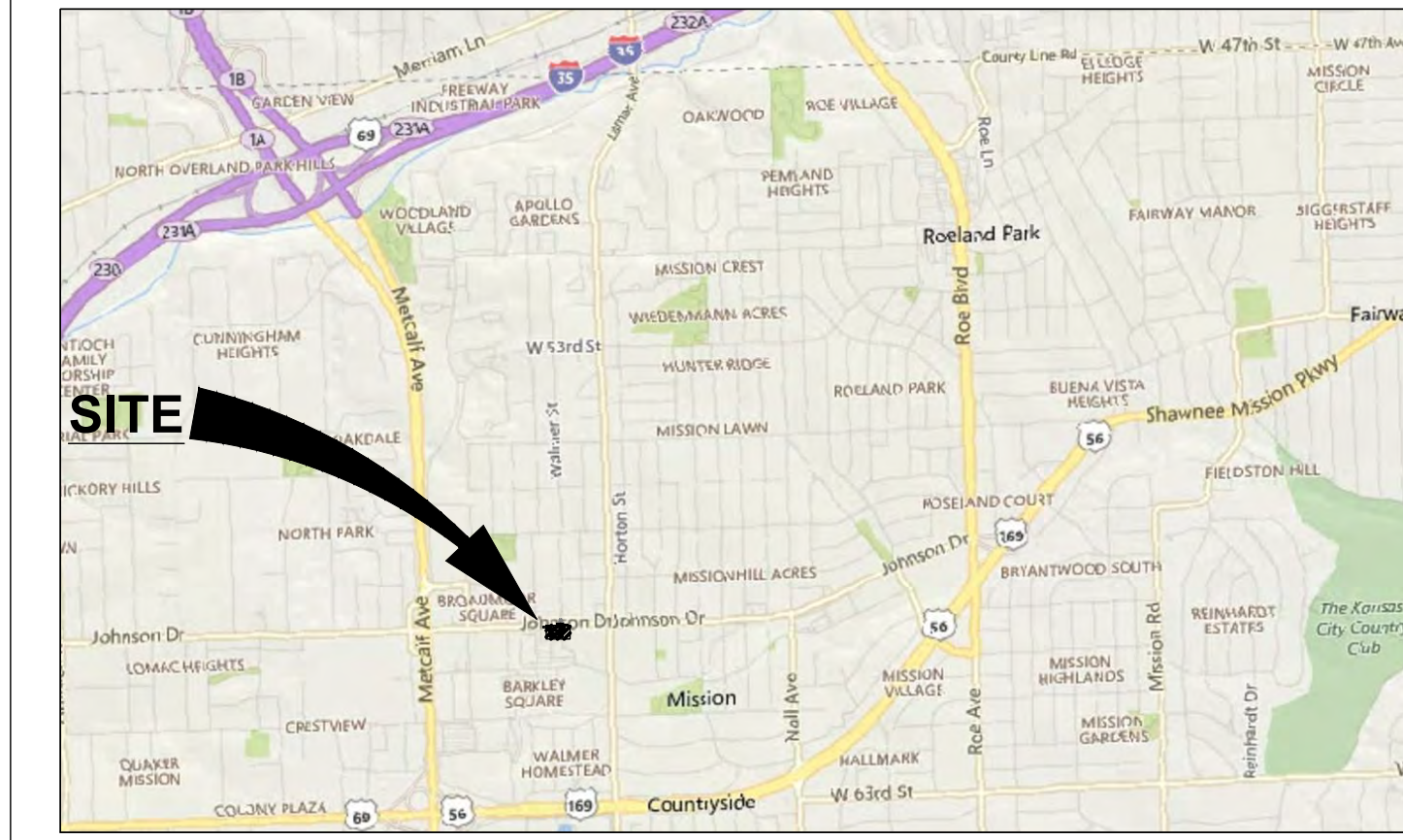
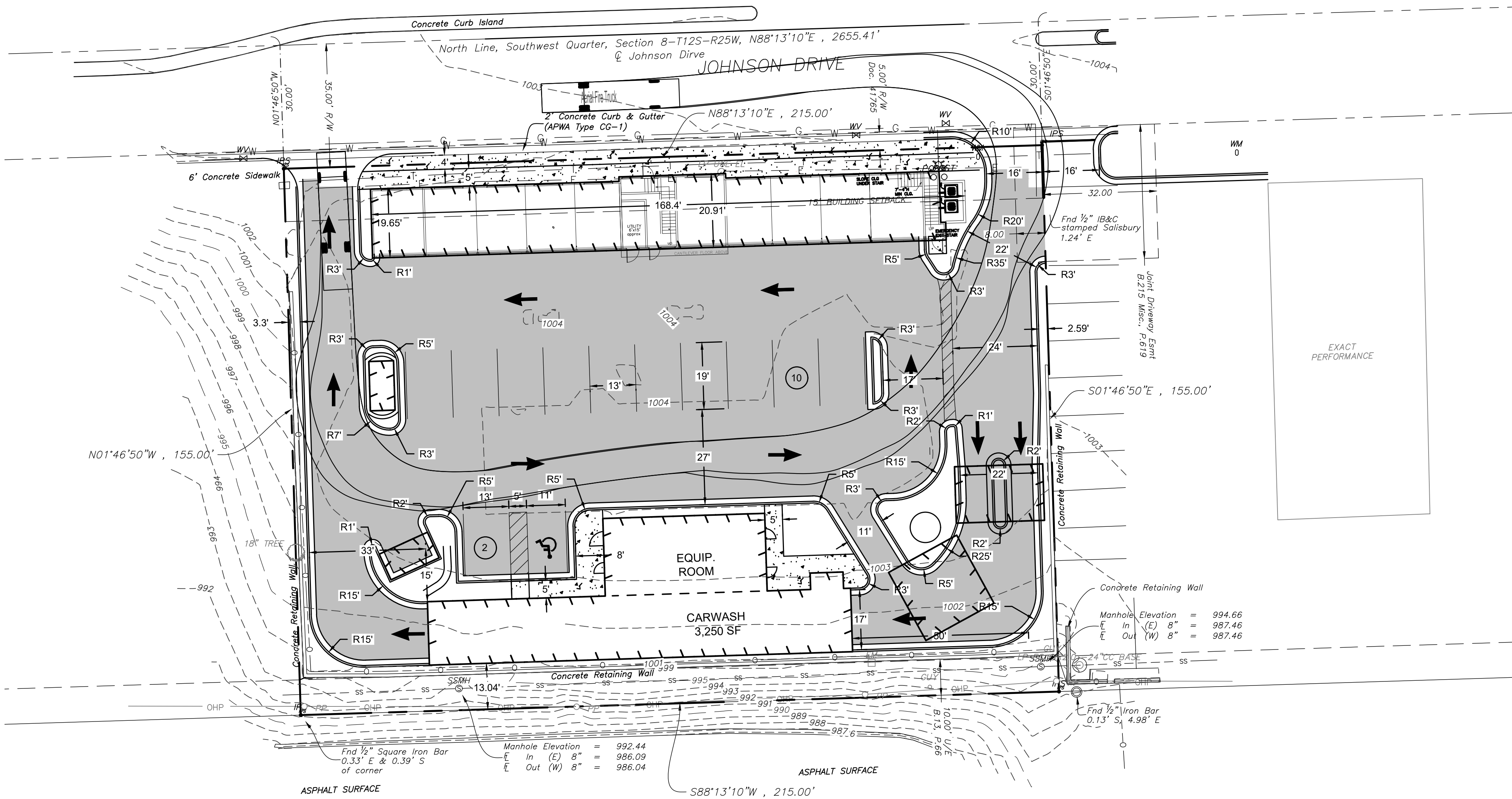
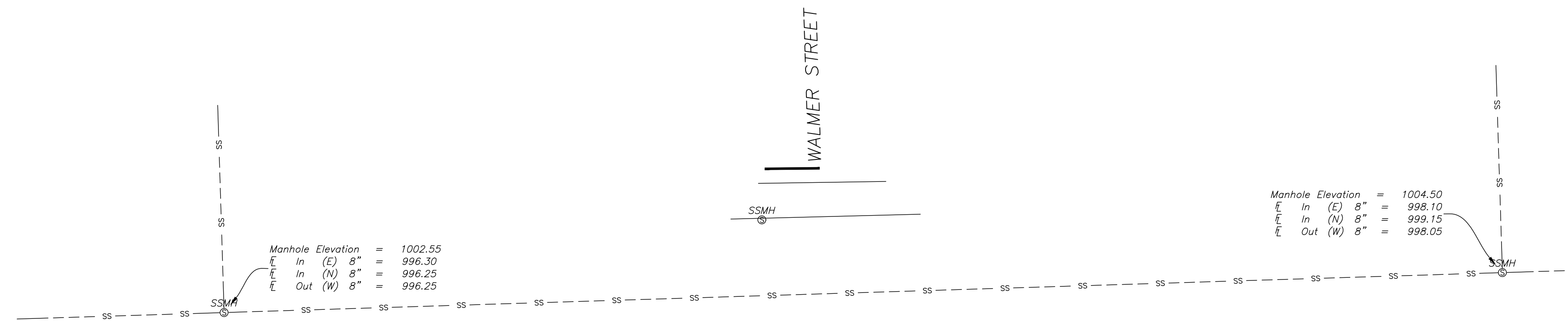
CAR AUTO TURN TEMPLATE
TIDAL WAVE AUTO SPA
PROPERTY ID #: KP6950001 0003
MISSION, JOHNSON COUNTY, KANSAS
Prepared for:
SHJ CONSTRUCTION GROUP

PROJECT NO.: 17-6044
DRAWN BY: DEM
DESIGNED BY: DEM
SURVEYED BY: CFSE
SURVEY DATE: 05/30/2017
CHECKED BY: BHB
SCALE: 1" = 20'
DATE: 04/11/2018



Know what's below.
Call before you dig.

SHEET
C-02
OF 5



**VICINITY MAP
N.T.S.**

OWNER INFORMATION

OWNER: CSTORE INVESTORS TOO, LLC
ADDRESS: 700 W 47TH ST, KANSAS CITY, MO 64118

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PROPERTY ZONING: CP-2B
BUILDING SETBACKS:
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SIDE: 0'
REAR: 0'

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---	--

HATCH LEGEND:

- CONCRETE PAVEMENT
- EXISTING CONCRETE SIDEWALK
- PROPOSED CONCRETE SIDEWALK

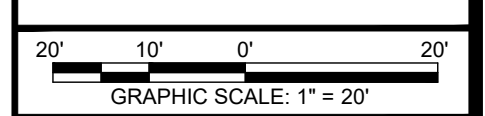
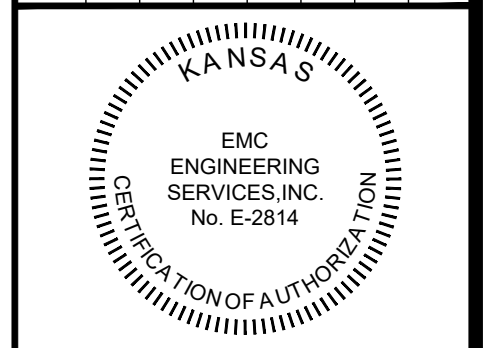
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NO.	REVISION DESCRIPTION	BY	DATE



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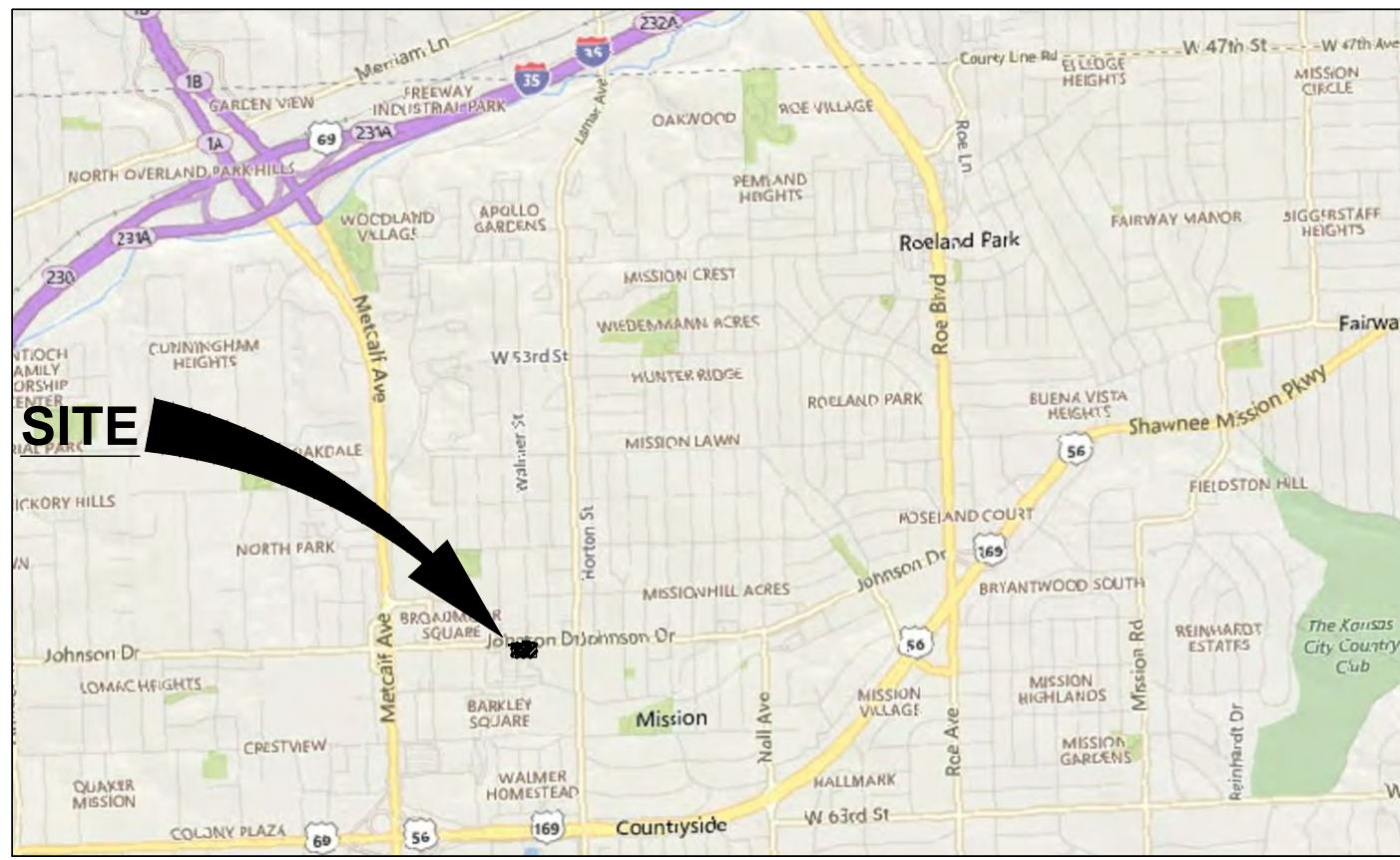
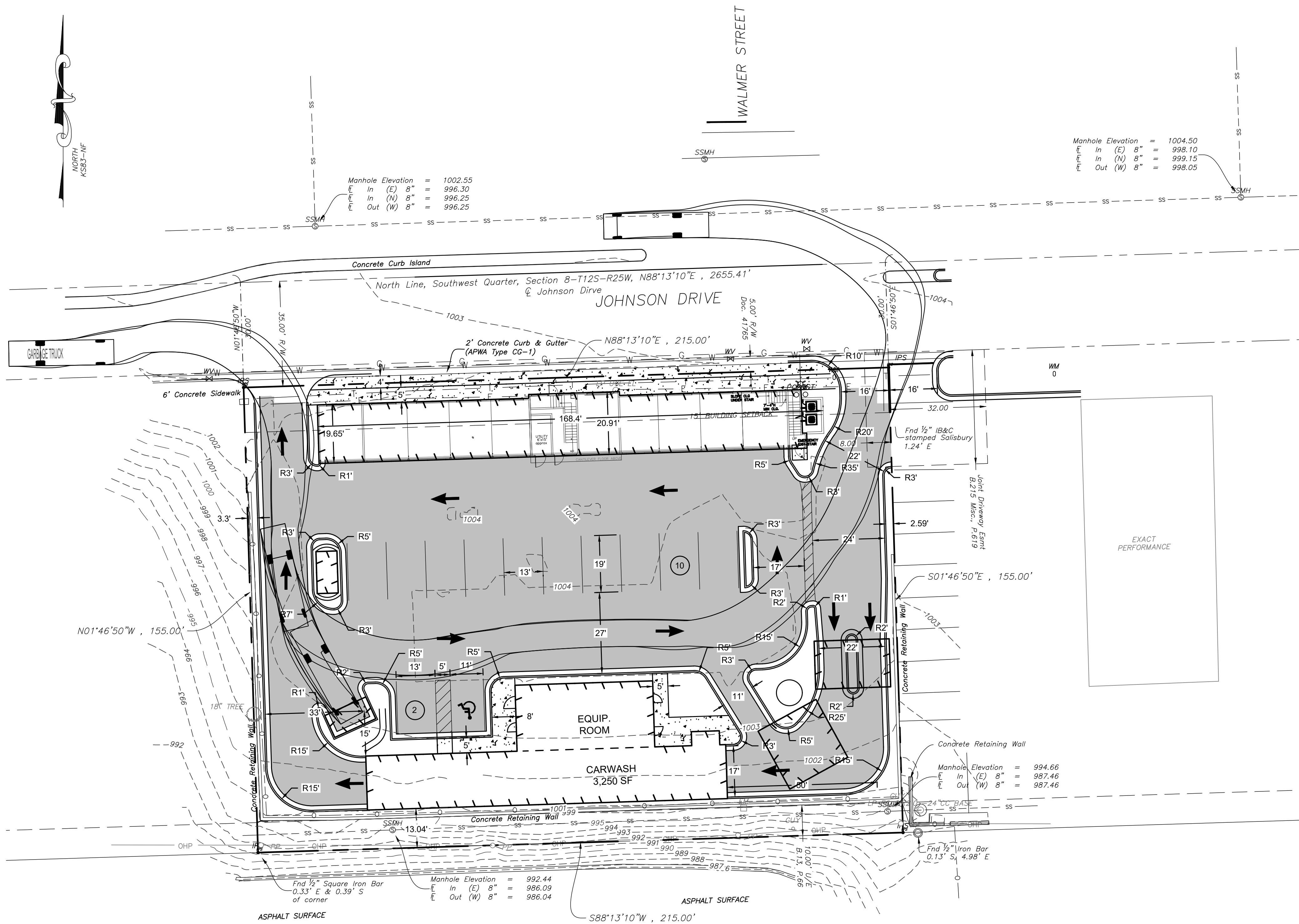
FIRE TRUCK AUTO TURN TEMPLATE

TIDAL WAVE AUTO SPA
PROPERTY ID #: KP6950001 0003
MISSION, JOHNSON COUNTY, KANSAS
Prepared for:
SHJ CONSTRUCTION GROUP

PROJECT NO.:	17-6044
DRAWN BY:	DEM
DESIGNED BY:	DEM
SURVEYED BY:	CFSE
SURVEY DATE:	05/30/2017
CHECKED BY:	BHB
SCALE:	1" = 20'
DATE:	04/11/2018



SHEET
C-03
OF 5



VICINITY MAP
N.T.S.

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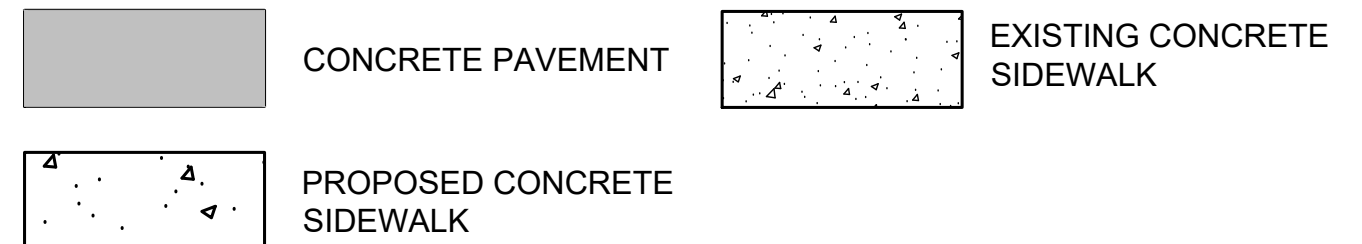
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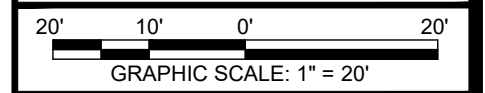
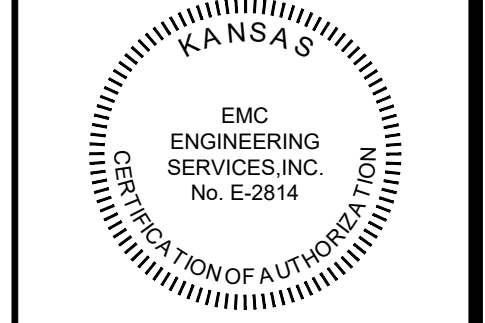
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OFFICE LOCATIONS: ALBANY, ATLANTA, AUGUSTA, BRUNSWICK, COLUMBUS, SAVANNAH, STATESBORO, AND VALDOSTA

GARBAGE TUCK AUTO TURN TEMPLATE

TIDAL WAVE AUTO SPA
PROPERTY ID #: KP6950001 0003
MISSION, JOHNSON COUNTY, KANSAS

Prepared for:
SHJ CONSTRUCTION GROUP

PROJECT NO.:	17-6044
DRAWN BY:	DEM
DESIGNED BY:	DEM
SURVEYED BY:	CFSE
SURVEY DATE:	05/30/2017
CHECKED BY:	BHB
SCALE:	1" = 20'
DATE:	04/11/2018



MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

The regular meeting of the Mission Planning Commission was called to order by Chairman Mike Lee at 7:00 PM Monday, March 26, 2018. Members also present: Stuart Braden, Brad Davidson, Robin Dukelow, Charlie Troppito, Frank Bruce, Burton Taylor and Pete Christiansen. Absent was Scott Babcock. Also in attendance: Danielle Sitzman, Planning & Development Services Manager; Brian Scott, Assistant City Administrator, Chris Cline, Core Design, Pete Heaven, Spencer Fane Attorney, and Ashley Elmore, Secretary to the Planning Commission.

Introduction of New Commissioners

Chairman Lee introduced the two new commissioners Burton Taylor and Pete Christiansen.

Approval of Minutes from the January 22, 2018 Meeting

Ms. Dukelow moved and Mr. Troppito seconded a motion to approve the minutes of the January 22, 2018, Planning Commission meeting.

The vote was taken (8-0). The **motion carried**.

Case # 17-08 Preliminary Site Plan – Martway Mixed Use

Chairman Lee: This has been before us before and has been sent back to us from the City Council to look at and discuss again. Staff?

Ms. Sitzman: Thank you, Mr. Chair. Joining us this evening is our attorney Pete Heaven. I'm going to let him start with a little direction to you all about the remand and what the process can be tonight.

Pete Heaven, Land Use Attorney, City of Mission, appeared before the Planning Commission and made the following comments:

Mr. Heaven: We have a little bit of an unusual process, and for the new planning commissioners, I wanted to step you through a remand. Under Kansas law, when a zoning matter comes before the Planning Commission, you make a recommendation to City Council. The City Council has the ability to either accept your recommendation, deny it, or remand the matter back to you for further consideration. A remand in Mission is a relatively rare event, so I wanted to step you through the process.

All the public hearings have now been held. This matter is now back before the Planning Commission. You may solicit information from the public or from the applicant. Basically, what the City Council has asked you to do is to look at three items in this particular proposal, that being height, density, and setback deviations. Now, as Danielle will explain to you, we've had some modifications to the application, and of the eight deviations that were first sought, there are only two left. The other six have been satisfied. I believe those have to do with density and the height of the building.

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

With that, if you have questions, chime in. I'm happy to answer them. Tonight is a reflection upon what you've been asked by City Council to do, which is reconsider your thoughts and ideas about height and density. That's our process.

Ms. Sitzman: Mr. Chair, I'll go through the staff report, briefly. As Pete said, this is Case #17-08, a preliminary site plan for property located at 6005-6045 Martway Street. The applicant has submitted revisions from their December 18, 2017, plan and made adjustments to the overall height. Our height standards are both by overall feet and by number of stories, so, they are requesting a deviation to the number of stories. They also have made adjustments to the number of dwelling units and to the massing of the building. The revisions do not contain changes that were significant according to our applicable code standards, and as such, they're before you tonight as a continuation of the case that came before you, and directed back to you as a remand.

Onto the points of consideration that were referred to you by City Council. The first one is setbacks. The revised plans that are before you tonight have removed any need for a request to a deviation for rear-yard setbacks. Here is a map showing the property and surrounding zonings. What's highlighted on the screen are the properties owned by the City. The two that are yellow are City Hall and the pool campus and the tennis courts. This little tract in pink is Tract A, which was discussed previously. This is also owned by the City. The pink areas are what are zoned MS-2, and the standard for setbacks in MS-2 is that there are no rear yard setbacks required unless MS-2 is adjacent to Residential R-1. So, along the portion of the property where they are adjacent to an R-1 District, they have changed the massing of the building to withdraw it from that 25-foot requirement. On the areas of the site that are adjacent to MS-2, there is no rear yard setback, they actually alter the massing a little bit to extend it. Where they took away in one area to extend out to the other to make up for the difference. It does still meet the rear yard setback requirements, which are zero for MS-2 adjacent to MS-2. That deviation is no longer required.

Regarding building height, as I said, there are two standards in our code regarding height. One is the number of feet in height, and the other is the number of stories in height. I've been asked a couple of times, why both? I recently went to see the largest one-story building in the United States, which is 526 feet tall. But it's one story, and they build rockets inside of it at the Kennedy Space Center. So, the thought in planning is that you need to specify both height and stories when setting limits. So, our code has three stories and 45 feet as the base code requirement. It met the overall height in their revised plans, or actually showed slightly less than the 45 feet, but they still have that space divided up into four stories. Basically, they trimmed off a few feet on each one of those floors to make the overall height still fit the four stories. So, there's still a deviation required to allow that one additional story for the height. This also impacts density, the number of dwelling units that were in that additional story. Let's talk about that a little bit.

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

The revised plan contains 27 fewer dwelling units and, therefore, increases the amount of lot area per dwelling in the calculation. The new unit count is 90 units, and the new density calculation is 807 feet per unit of lot area, or 53.98 units per acre. They're requesting a deviation to allow those 90 units, or approximately the square footage that they're showing. Of course, this is a preliminary site plan, so they're not tied to this exact floor plan. It can still flex a little bit in the number of units. That's why we're identifying both unit count and square footage, just to make sure that we don't need to come back and deal with these numbers again, between now and the final site plan. There's some additional ground floor space that's either to be utilized by the commercial tenants on the ground floor, additional storage for those businesses, or tenants as storage spaces. So, we've identified that as appurtenant ground floor space, allowing for a little bit of flexibility since they still haven't identified that space to use one way or the other.

There was a minor calculation error in the overall square footage of the lot in one of the previous versions. That's because we accidentally counted Tract A towards the land area of the development. We subtracted that out of the calculations and they've been re-run.

In your packet there's a density table that shows you how this specific density stacks up against other existing developments in similar zoning districts in the city of Mission, or in the downtown zoning district designation. It also compares this project to the zoning densities of the other current apartment construction going on in and around Johnson County, specifically highlighting projects in downtown Overland Park.

Staff reviewed the project again to make sure the other deviations had been taken care of and are no longer necessary. We went through the findings again and highlighted the exact findings that were being made for this case. We do have a recommendation for you tonight. Conditions 1, 2 - estimate in feet only - 4, 5, 6 7 and 10, from the Planning Commission's recommendation of December 18, 2017, have been withdrawn as they are now unnecessary. It is the opinion of staff that the proposed development, as revised, conforms with the Comprehensive Plan, meets the overall intent of the MS-2 zoning district, and complies with the required findings for section 405.090 and 440.160. Therefore, staff recommends the Planning Commission adopt the findings of fact contained in the staff report and recommend approval of the preliminary site development plan for this case to the City Council, with five conditions. The first two conditions relate to the deviations that are still required. The first one is approval of the requested deviation to height, to a maximum building height for mixed use. The second is approval of the requested deviation to waive the minimum lot area per dwelling unit to allow for the proposed design of 90 units, or 92,896 square feet of residential development and appurtenant ground floor space in a mixed-used building. The third and fourth conditions relate to the need to finalize some of the traffic studies and stormwater drainage reports, especially in light of the changes. So, a revised final traffic study must be submitted for review with the final site plan application. The appropriate data, text, maps, drawings and tables must be included per the Olsson Associates review comments dated September 20, 2017 and attached to this report. Staff reserves

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

the right to provide additional comments based on those new studies, or stipulations based on those to address traffic, circulation, ADA, storm drainage, and floodplain related issues. Fifth, there is a condition that came up through some of the public comments made at the Planning Commission hearing pertaining to light pollution. We carried that condition over to our recommendation tonight. It states that light pollution must be addressed to the satisfaction of staff before construction can begin. That concludes staff's report.

Chairman. Lee: Any questions? I assume the applicant is here.

Mr. Troppito: Pete, I assume, just for the record, that you're contending that this application meets the intent of the Comprehensive Plan, it meets all the zoning and code requirements?

Mr. Heaven: Yes, it does.

Mr. Troppito: One of the issues that was an original concern to me was hexavalent chromium in the building products. This is a question for the developer. I'd like you to state for the record that it has been resolved, and no other building materials to be used on this project contain hexavalent chromium. Besides shaking your head, would you confirm that for the record?

Christian Arnold, Applicant, appeared before the Planning Commission and made the following comments:

Mr. Arnold: Sure. We did investigate that product, and that product cut sheet that was submitted previously was for a residential product. A product that we would propose is a commercial product, so the safety data sheet will be submitted to the City. That product that you referenced is not in that at all.

Mr. Troppito: Thank you. One other question for Danielle. Recommendation - it's the last one. Light pollution must be addressed to the satisfaction of staff before construction can begin. I'll just state, I have a problem with that, in the sense that light pollution has been a major concern. The problem is it pushes it down the line to staff, and possibly an unknown staff member that we have no experience with. Why would this not be phrased to require the satisfaction of the Planning Commission, rather than staff?

Ms. Sitzman: It can certainly be rephrased that way. I think the element of allowing additional time to resolve it is because lighting and light levels is not a detail that is normally presented as part of a preliminary site plan. There would be a photometric study required at the time of a final site plan, and there are standards about foot candles, etc., that any staff would check at that point. But we could certainly reword that condition to say, "to the Planning Commission's satisfaction," or "at time of final site plan." That would be appropriate.

Mr. Troppito: Thank you.

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

Ms. Dukelow: I was going to ask Mr. Heaven for clarification on the, the plan that was remanded back to the Planning Commission. The plan that we previously recommended to City Council for approval, if I'm not mistaken, was the plan prior to a current plan that we are reviewing.

Mr. Heaven: That is correct. Yes.

Ms. Dukelow: That was the plan that we had seen in January, that was dated 11/26/17. Is that accurate?

Mr. Heaven: Yes.

Ms. Dukelow: Thank you. So, with regards to light pollution, I understand the photometric plan requirement - and this may be a question that we wait; this may be more appropriate for the applicant. I know that in previous meetings, the Commission has expressed concern about the headlights shining across the creek from the back parking area. I know that this is something that doesn't appear to have been addressed in this particular rendition of the plan. I just want to bring that up and make sure we address that through the course of this meeting.

Mr. Troppito: That was part of my concern.

Chairman Lee: Any other questions or comments?

Ms. Dukelow: This is probably a question for the applicant. I'm curious as to whether or not there will be bike storage for the residents.

Mr. Arnold: I can answer two of those questions at the same time. One, when we presented last time, this issue came up extensively at the City Council meeting. Once we looked at the topography of the site, the site is actually about 20 feet below the houses over there. So, we did a section study that was presented at the last meeting and showed that the tops of the houses were about in line with the top of the building because it is so far down. I think that has alleviated some of the concerns with headlights because they were so far down. Also, because we no longer have the parking lot pushed right up to the parking line - we're actually back six feet, which allows us to plant more vegetation along the back of the parcel, as well. So, when we last met, we said we were going to address these issues as we move through the process, and we have addressed these issues.

Bike storage? Yes. Because the first level is largely parking, there's ample storage for residents, as well as general bike storage. There's lots of space on the ground floor.

Chairman Lee: At this time, we will entertain a motion.

Mr. Braden: Mr. Chair, we have reconsidered the proposed height, density, and setback deviations within the Code as requested by the City Council, as well as the elimination of some of the originally requested deviations, and I believe the project should be returned to the City Council with our recommendation of approval. I therefore move we

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

adopt the suggested findings of fact and recommendations of Staff as contained in the staff report and recommend **approval** of the Preliminary Site Development Plan for Case # 17-08 Martway Mixed Use to the City Council with the following stipulations, as amended:

- 1) Approval of the requested deviation to height to allow a maximum building height of four (4) stories.
- 2) Approval of the requested deviation to waive the minimum lot area per dwelling unit to allow for the proposed design of 90 units or 92,896 square feet of residential development and appurtenant ground floor space in a mixed-use building.
- 3) A revised final traffic study and final stormwater drainage designs must be submitted for review with the final site plan application. The appropriate data, text, maps, drawings and tables must be included per the Olsson Associates review comments dated September 20, 2017 and attached to this report.
- 4) Staff shall have the right to provide additional comments or stipulations on development plans until all traffic, circulation, ADA, storm drainage, and floodplain related issues have been satisfactorily addressed.
- 5) Light pollution must be addressed to the satisfaction of Planning Commission upon submittal of the final site development plan.

Mr. Troppito: Second.

The vote on the motion was taken (8-0). **The motion to approve this application carried.**

Case # 17-11 Preliminary & Final Site Plan-Tidal Wave Auto Wash-Block Real Estate - Public Hearing

Ms. Sitzman: Also with us tonight is Chris Cline of Core Design. Chris has been working with us for many years. He is our on-call architect that helps us perform our form-based code reviews. I asked him to give you a quick refresher on the Form Based Code since we have several new members since the last time we had an application, which was four years ago. I know the staff covers much of the same information, but I thought it would be good to have a quick refresher from Chris. He is also here to answer any questions as we go through this process.

Chris Cline, on-call Architect for the City of Mission, appeared before the Planning Commission and made the following comments:

Mr. Cline: We've been working with the City of Mission with the Form Based Code since the beginning of developing the code. I wanted to take a minute to take you back to what went into the code and how we've been applying that code throughout the West Gateway District.

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

The West Gateway District is from Metcalf on the west side of town, basically from Shawnee Mission Parkway up to about Foxridge/54th Street. It's about 230 acres or so of development there. There are three things I want to touch on: The goals that were developed for creation of the Form Based Code, what the planning process was in creating the code and putting it in place, and then, some frequently-asked questions that get asked from time to time.

The goals for the code were to engage the community in the process, and create a form-based code that was consistent with the City's vision plan. The City had adopted a vision plan for this area similar to Vision Metcalf. This was done prior to Vision Metcalf. The same consultant actually worked on it. That vision called for engaging the community. There were lots of large charrettes and workshop meetings where folks were shown pictures of more of a suburban-type development that's out there today, or something that's a little more pedestrian-friendly and brings the buildings right up to the street. There was a strong movement and input and direction received from the community that they wanted something different in this part of town. They wanted something that wasn't as suburban, they wanted it urban, they wanted buildings up to the street, and to create a strong sense of streetscape and public realm. That's what we heard from engaging the community. Staying one step ahead was, if that's the type of development pattern that the City wanted to achieve in this area, how can we do that in a way that helped to guide developers to bring projects to the City that fit those guidelines? So, staying a step ahead was, let's put a code in place that really prescribed the type of development that's different than what's out there today, and hopefully encourage developers to bring projects that they could get approved faster. So, it's a proactive approach, and it's spelled out in the code.

In making the vision a reality, there were a lot of good ideas that were in the vision plan, but it didn't have all the details it needed to actually implement. We had to work through the details of taking those visions and turning it into a code that you could implement. In that process, we had 30-plus people on our advisory committee. It represented homeowners, property owners, commercial brokers, elected officials and community residents. A 10-month process. There were six meetings with the steering committee, lots of questions and things occurring throughout that process. We had three public meetings where folks came to look at the code as it was being developed and ask questions. Had two open house forums in March and July of 2007. And then, it went through a process where we got City Council and the Planning Commission together for a work session to walk through the bones of the plan, and then, took it to public hearings and adoption in October 2007. Really, took that vision plan, encouraging good projects by making them easier.

So, what that means is, in a lot of cases, a developer will bring a project and negotiate with staff on lots of things in a planned district project. And then, there are a lot of details that get worked out with staff, but sometimes the Planning Commission wants to talk about specific things in the project and make things better or different. Sometimes it

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

gets cumbersome. The intent for the Form Based Code was to put all of that work at the front end and hopefully bring projects before the Planning Commission that have already been through that process. If they receive a passing score, then it's up to the Planning Commission to decide, well, if they followed the rules, should we approve this plan? And if you approve it, it doesn't have to go to City Council. They can immediately go into final development and construction. So, it alleviates the process a little bit, but it takes a little more work on the front end.

There were some questions asked about this. Will infrastructure serve it? At the time, we were talking about adding density to this district, different than what's out there today. There's a lot of low-story buildings. If we added more density, would the infrastructure that's out there be able to support it? We did that study as part of the Form Based Code, and most of the infrastructure can't support that. There were some things that were proposed and put into the CIP.

Can the market support it? There was an extensive market analysis done that looked to the future and what the feasibility and projections were for new development. The good thing is, Mission is well positioned for future growth. We did study lots of different areas for different density development, types of development, to make sure that the code recommendations were realistic.

Is the code flexible? Prior to the code, I think there were a number of commercial properties that back up to some of the residential properties up on the north end of the district, primarily. The experience there has been that several commercial developments have come in; I believe residents would file a protest petition, there would be a big fight, a huge meeting at Planning Commission or City Council, and it was very difficult to try to work through that process. So, in development of the code and engaging the residents, we said, look, if we put this new code in place and put all these rules in place, if a developer follows those rules, should they be able to go ahead and get their project approved without a public hearing? So, if we looked through everything and scored it, and they get a passing score and the Planning Commission approves it, they're not rezoning the project. They're just getting their plan approved. Is that okay? And they said yes. If they follow those guidelines and give us a project like what we think, then yes, they were supportive of that. The City was supportive of it. Basically, it was put in as an overlay district, so it does not require someone to rezone the property. Therefore, there doesn't have to be a public hearing. So, if they get a passing score, they don't have to have a public hearing for a rezoning.

Mr. Troppito: Excuse me. You referenced "scores" several times. What's the score? Ninety? Eighty? Seventy?

Mr. Cline: I'll get to that. Existing businesses can still do business in the district, and we've had several cases where folks that have a non-conforming building can still make improvements to their building and continue to do business in the district. And then, developers, again, it avoids that protest petition process. It allows for a variety of

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

building types and uses. There are architectural and site design opportunities, and development phasing possibilities. This is one example of a larger site. You can see where access points are shown in those dark triangles. Buildings will be placed up next to the street. Surface parking could be placed behind. And then, eventually, more buildings could be built and that surface parking could become structured parking. So, there are opportunities in there for larger properties and other properties to phase things in over time, as well.

And then, when we talk about a scoring system, what we tried to do was to break things down since these were a new set of rules. We wanted to break it down so developers understood how to design their projects so that it fit the code requirements. There are a number of steps that you have to go through to look at each area -- the sector plan, the regulating plan, the building types, the setbacks, the early guidelines -- and then, looking at the building themselves, and the streetscape improvements. We put a score to each one of those so that, at the end of the day, if they submit the plans and they didn't get a passing score, instead of just saying, "You didn't pass, here's a list of 25 things that you didn't pass for," and they didn't really know, well, how important were some of these, and how not-important were other ones? So, we tried to put some kind of system in place that at least showed you what the relative level of importance was for each one of those items. They kind of go in a hierarchy fashion. You basically go through a four-step process. You look at the sector - in this case, that's Rock Creek. You look at the block in that area, which tells you what types of buildings are allowed in that block. You look at the building types themselves and it gives you some additional information about that building type. And then, you go through the architectural guidelines and the urban guidelines, which tell you where to place that building, how close to the street. And then, some of the streetscape improvements that need to be put in.

So, there is an extensive amount of information to get put in these plans, and there's usually a back-and-forth that happens with any applicant, where they may submit plans initially that don't have as much information and they don't get a passing score, but we give them a full listing of where they missed points, and where they could do better, and how they can improve their score when they re-submit and we score it again, and eventually, bring it before the Planning Commission.

Here are some examples of that: A bank proposal that was placed away from the building and was surrounding by parking. Eventually had the building pulled right up to the street corner and put all parking back behind. The Mission Crossing site. This was an initial proposal where the buildings were internally-oriented, pulled away from the streets. You can see how, in the concepts, the buildings started to move to the street corners, and eventually became a plan that looks pretty close to this, where the buildings all had that strong relationship with the street. Then, you can see what some of the renderings look like, and then, I've got a shot here, under construction. The last one is Cornerstone Commons, the grocery store and restaurants there on the corner.

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

And then, the little two-story building that's on the southeast corner of that particular site. Again, buildings address each one of the streets adjacent to parking on the inside.

The Form Based Code is kind of a new thing in the metro, and Mission was one of the first to put it in place. It was done in 2007, right at recession time. And even with all of that in place, there's been more economic development activity and development here in this part of Mission than anywhere else in Mission. There have been a number of projects that have come through - and you've seen some of them here - and followed the code and gotten approved.

So, thankfully, I've been able to help the City not only create the code, but to help implement it. I'd be happy to answer any questions.

Ms. Sitzman: Thank you. With that, I'll present the staff report in this specific case. This is Case #17-11, Tidal Wave Auto Wash, a preliminary and final site development plan. The combination of preliminary and final plans is required so that all of the details for a full score are presented and can be awarded. This is how we do all of our Form Based Code cases. We can do this with applications outside of the District, but you have told us in the past that you find that a little overwhelming. So, for other areas of town, other developments like the Martway Mixed Use application, you'll see a preliminary site plan go through the entire process, and then come back to do a final site plan. In this case, they are combined so we can get all the level of detail necessary to get a full picture of the project and complete the scoring process.

The property is the site of a former gas station and is currently zoned CP-2B Planned Retail and Service District. It's located in Block Y. As Chris explained, the Form Based Code divides all of the land in the district into separate blocks, so you will be referencing Block Y for this one. It's also located in the Johnson County Design Guidelines district. This site is about three-quarters of an acre in size. The proposed car wash is an allowed use in the underlying zoning district.

In the past, some of the other Form Based Code developments, like the Mission Crossing project, made use of the fact that this overlay zone allows for cumulative zoning and additional use flexibility. So, in the example of Mission Crossing, that property was not zoned for those uses originally. But, because they had a Form Based Code compliant project, the overlay zone allowed for those other uses to happen without the need for a rezoning process. In this case, the underlying zoning would allow a car wash.

The regulating plan for this particular sector in the Form Based Code identifies this property as part of Block Y where ground-level retail uses facing Johnson Drive are important. This is not in the Downtown District of Johnson Drive; however it does continue to reinforce that retail is important along that street. There is an extension of Walmer Street shown in the Sector Plan. The future extension of Walmer Street between Block Y and Z would be triggered at the time that Block Z actually

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

redeveloped. So, as that is not happening at this time, no proposed extension of Walmer Street is included in this proposal.

Both Block Y and Block Z are where the Mission West Shopping Center currently is with retail along its northern side. The Form Based Code is a long-range plan that envisions the eventual redevelopment of all of that area voluntarily by the owners of that property. The intent is to prepare for improvements to the street network, when that would eventually happen. Currently, that shopping center is in a really large block which is difficult to walk around on foot if you're a pedestrian. A goal of the future road extensions that are shown in the Form Based Code are really to make blocks smaller, more easy to navigate on foot by pedestrians. So, in summary, the Walmer Street extension not included in this plan, not required of this plan, but just noted as an element that's included for the long-range utility and usefulness of the district.

There are several building types that would be allowed in Block Y, anything from a townhouse development to a mid-rise building, a mid-rise building being something that's at least two stories tall. A parking structure-type building, which is really not just parking for cars, but envisions parking interior with retail wrapped around it. Also, low-rise buildings are currently allowed in any sector as long as the lot size of the development is less than a half acre. That gives additional flexibility for development of really small lots where it really wouldn't be feasible to do much of a large-scale development. As I said, this lot is larger than a half acre; it's about three-quarters of an acre. So, it does have to have a component of at least mid-rise development in it. For its lot size, it is allowed to have 60 percent of the gross square footage of development to be low-rise building type, but at least 40 percent of the development has to be something that's mid-rise, or larger building type.

The Comprehensive Plan helped inform the development of the Form Based Code. The Form Based Code is compliant with the Comprehensive Plan and encourages mixed-use median density redevelopment in this area. That would include housing, limited office, and medium-density retail in this situation.

Chris said that there is a score that is given to Form Based Code projects by staff. His review memo discusses how many points were available and how many points the project garnered through its design. As he said, there was a list of components that the scoring walks through, and they are hierarchically labeled on this list, number 1 being the most important, and having the most points required in order to score a passing grade at the end. The reason that this is done is because some elements of design are more important and have more impact on the public realm. The architecture of your building may not have as big an impact on what the public experiences near your development as where the building is placed on the lot does. So, things like where the building is placed on the lot is listed up front and the most key components of the scoring system. A passing score is a 90 out of 100 points. There are some prerequisite levels that go with this score, so you have to at least get all of the points in those early

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

categories. Otherwise, it's impossible to get to a 90. The later categories - 4, 5 and 6 on this list - are definitely points there to be had, but it's not as critical that an application receive all of the points in those categories. They could still get to 90 points without doing every thing called out there.

In the memo that follows this, you'll see that our scoring on this application came up with 60 points, which is not the 90 points required to receive staff's endorsement at this point. It fails in several critical prerequisite steps, which is why it could not get to that 90-point threshold. This is largely due to the fact that all of the proposed buildings fail to meet the required building type standards. So, they have a mix of a two-story building and a one-story building in their proposal. However, the one-story building is still shorter in height than it needs to be to meet the Form Based Code standard for a single-story building.

The north building was designed around the footprint of a car, and we had trouble deciding what kind of building type that it should be evaluated as. We didn't feel that a building that was only the depth of a vehicle was truly going to have a lasting value as a retail space. The code says a certain depth of space would be needed for reuse of that building as retail in the future. A goal of the Form Based Code is for reuse of these buildings over time and having flexibility to allow that. Staff's feeling was that to have a building footprint solely designed around the footprint of one car was difficult to justify as meeting the intent of the code for a mid-rise building type.

Therefore, failing that we looked at other building types to see what it might resemble more closely. It was a poor fit for a parking structure building type in the code also, but we went with that because it's the closest thing to a building designed around automobile in the building types. Of course, we let the applicant know that there were concerns about the design of their building which would affect their score. Like I said, we tried our best to score it with what they had submitted to us after staff comments were given to them.

There were other points that were not attained because the submittal was missing elements of the proposal, specifically things that would typically be included in a site plan such as landscape, streetscape, or the accessory structures. Things like trash enclosures, planting plans, street lights, benches - all of the elements that go into the streetscape plan.

As I said, the proposed development does include two buildings. One building is a two-story 6,699 square foot building. The other one is a one-story 3,200 square foot building. That works out to about 68 percent of the buildings being a two-story and 32 percent being a one-story, which meets the Form Based Code requirement. However, as I said, we had difficulty determining that the two-story building was a mid-rise building type and truly compliant with the Form Based Code.

The ground floor of the northern building would contain parking stalls for vacuuming vehicles and some office space up above on the second floor. The applicant has

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

indicated that the width of the northern building is sufficient to be used for retail space in the future if a car wash facility ceases. Again, the second story would be office and storage.

The Form Based Code says if there are not enough points earned within the prerequisite steps, we could stop review at that point. Staff feels that it's more important to give the applicant a chance to come before you tonight, so we went ahead and performed a review and scored the rest of their project so they could come with as much information as possible.

In our review of the exterior building materials we had some concerns about windows and doors not meeting the minimum of 60 percent requirement for storefronts. We also looked at some of the other exterior materials. They have proposed EIFS on the ground floor and upper floor of the northern building; 45 percent of the coverage of that space is EIFS. As you'll note, the Johnson Drive Design Guidelines actually put a limit on the amount of EIFS on building faces because it's a less-durable material. We certainly had concerns about how close to the ground it is located, where it can have a greater wear.

The development would utilize a surface parking lot located on the interior of the lot, behind the buildings, and would be accessed by two driveways. Basically, there is a one-way flow in and out through queuing up for the car wash tunnel, and then, circulating around the interior of the site, either for vacuuming bays or for parking for the office space, and then, existing in a one-way flow on the west side of the lot. There is an existing median along Johnson Drive that would need to be altered to allow inbound and outbound turning movements. There's also a median break interior to the site, so if you decide at the last minute you don't want to go through the car wash, there is a way to exit that queue. We've feel that the median break for getting out of the queue should be earlier in the flow, not after the pay kiosk.

Our traffic engineer, George Butler Associates, has looked at the site access, the vehicle queues, and the turning templates. They've also looked at sidewalks and the traffic study. They are generally satisfied with those designs. However, they do recommend moving the median break to earlier in the queue flow. Also, there were no turning movements provided for service vehicles, so it is unclear how trash and other service vehicles would maneuver through the site. If it's a one-way flow, they are going to be either coming in through the same areas that vehicles would be, or moving counter to the flow, which is not desirable.

A landscape plan detailing streetscape improvements was not submitted. Generally, we feel like there's sufficient width being allocated for the streetscape improvements in the five-foot sidewalk and a four-foot tree zone, which would be compliant. However, we were unable to check all the other details that we needed to check for the planting of street trees and street lights, benches and trash receptacles, and bike racks. As with other developments, those elements would be required to be constructed by the

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

developer at the time that they do the project. Final placement and configuration of those elements must be confirmed before they could begin construction.

There would actually be a reduction in the impervious surface with redevelopment, so there is no stormwater treatment required at this time. The Sustainability Commission has not had a chance to review this application, so they've not made a recommendation to you. They have a separate scoring process that they go through. They have a sustainability checklist that they use, which would be a recommendation that would come to you from them.

Staff has included in the staff report suggested findings of fact, both for a preliminary and a final site plan. We feel that there are deficiencies in the site plan process, primarily due to absence of information. For example, the finding of fact that needs to be made for the final site plan has to do with landscaping and screening, which we were unable to make a determination on as the information was not provided. Also, we feel that because there was not a passing score achieved for the Form Based Code, that the findings that need to be made for consistency in good land planning and site engineering designs were also deficient.

According to Chapter 8 of the Form Based Code, applications are reviewed in that four-step process. They do need to comply with the requirements of the first two steps to automatically proceed. This is coming to you tonight without having met those prerequisites. You do have the authority to do the final review and approval of a Form Based Code application if it had garnered the 90-point score. This is what we would consider to be a non-compliant application, so it's back to you tonight for full review. The project as submitted fails to receive that passing score in the prerequisite steps. Staff feels that there are major flaws to the building types, and those should be addressed. And then, the minor supporting details should be provided for review as described.

Therefore, staff recommends the Planning Commission adopt the findings of fact contained in our staff report tonight and recommend denial of the preliminary and final site development plan for this case, for the Tidal Wave Auto Spa project. The applicant has requested to proceed with the meeting tonight with this failing score. They want to present their opinion on the project and its conformance to the code to you. You're certainly able to consider their opinion and consider making alternate findings of fact that you might determine based on what you've learned tonight. Included in your packet was some alternate motions that you could consider. Also, the applicant has actually provided a written statement, letting you know what they would be willing to agree to as conditions. If you were to take an action tonight to adopt alternate findings of fact and make a recommendation of approval, I would highly recommend that you do that with conditions, and consider those conditions provided by staff and the applicant. That concludes the staff report.

Chairman Lee: Thank you. Is the applicant here? Please introduce yourself.

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

Paul Schepers, Attorney, Seigfreid Bingham, appeared before the Planning Commission and made the following comments:

Mr. Schepers: I technically represent the owners of the platted 6501 Johnson Drive, who are technically the applicants who have submitted the preliminary development plan for your consideration. With me is the principal owner of Block Real Estates Services, the owner of that site, Mr. Stephen Block, who is sitting here. I also am here representing the developer, Tidal Wave Auto Spa. From that organization is Thomas Wells, an independent consultant with a company called Keystone, which has been working with Tidal Wave to navigate our plan through the process. Also present is Marty Murphy, the project manager for Tidal Wave on this particular project. Also, the principal of Tidal Wave Kansas City, Petty Hardin, who will be taking over after I finish my remarks, to tell you in greater detail what our vision is for 6501 Johnson Drive.

I listened very carefully to Mr. Cline's recitation of the history and the creation of the Form Based Code, and I will be here to testify that I've practiced law for 36 years, and this is my first encounter with a Form Based Code process. I agree with Mr. Cline. It's a very unique overlay or addition to the zoning ordinances that I typically see in Kansas and Missouri. My research indicates - and I can't guarantee this - that Mission, Kansas, may be the only municipality in the whole state of Kansas that's adopted Form Based Code. Before I even make this statement, I want to assure you that my purpose here this evening is not to shake my fist and threaten to sue you, because that's the last thing that my client wants to happen. But, I would point out to you that there is no Kansas case which has held that a Form Based Code - and in particular, the way the Form Based Code has been applied to my client's application - is authorized by the Kansas Zoning Enabling Act. There's no case that holds that on its face or as applied, it's constitutional.

So, there are some questions with regard to the lawfulness of the Form Based Code for use to deny an application for development like the one that my client has been presenting to the City of Mission. But, I'm not here to tell you I'm going to sue you. I'm very hopeful that at the end of our presentation, you will look at what's there at 6501 Johnson Drive, and look at what's going to be there when Tidal Wave Auto Spa completes their project. And, in particular, I'd like you to look and take into account everything that Tidal Wave Auto Spa has done to try the best that they possibly could to bring this project within the spirit of the Form Based Code, if not the letter of the Form Based Code.

In addition to Mr. Hardin, who will be making some comments and explaining the project to you, I have a gentleman who I think some of you are familiar with, who has experience with the Form Based Code that I did not have. I brought Dave Olson on board our team because he certainly has experience with the Form Based Code, having represented the developer who successfully obtained approval of a development plan in the same West Gateway area as is covered by the Form Based Code. I believe that Mr.

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

Olson's knowledge and experience with the Form Based Code entitled him to create a different Form Based Code scorecard than the one that Mr. Cline has created for our project. And at the close of this presentation, Mr. Olson will walk you through his own Form Based Code scorecard, which I'm pleased to tell you that Mr. Olson thought we had a 90 or a 91.

Again, the main area of disagreement, the main driving force behind the discrepancy with the grade we received from Mr. Cline, and the grade Mr. Olson gave us in his analysis of our compliance with the Form Based Code, rests virtually entirely with the two structures that are going to be located on the site. We received a failing Form Based Code score from Mr. Cline because he thinks that our two-story building that is going to front Johnson Drive is a parking garage. And if you read the Form Based Code, the definition of what a parking garage is, the first line states: *Parking structures or buildings which are specifically designed to store vehicles*. Not surprising. That's what I would have said a parking garage is. That's not what our two-story building is, at all. When you store your vehicle in a parking garage, you park it there, you get out, you go someplace, and then you come back and get in your car and drive away. That's not what's happening on the bottom floor of this two-story building. What's happening on the bottom floor of the two-story building is an extension of our retail operation. Because when you come into the Tidal Wave car wash, as part of your purchase price, you receive the right to use the vacuum cleaners, which everybody uses after they wash their cars.

We have designed the two-story building so that the vacuum cleaners will be on the first floor in a series of areas where you can pull in and vacuum your car, but you're not storing your car there. You're vacuuming it so it can be clean when you finally exit our facility. So, what we consider that building to be is a combination of offices on the top floor, and an extension of our retail operation on the bottom floor, which makes it a mid-rise structure. And if you grade that structure in accordance with the criteria you find in the code for mid-rise structures instead of the parking garage that's imaginary, you're going to get real close to a passing score. Because the main reason we failed the Form Based Code scorecard that Mr. Cline prepared is because our building isn't 40 foot deep. And parking garages under the Form Based Code are required to be 40 feet deep. There's no such requirement for a mid-rise structure. And our mid-rise structure is going to be plenty deep. If someday Title Wave goes off into the sunset and leaves that building there so that that bottom floor can be used for retail purposes, repurposed, if you will, for retail purposes, and if the Form Based Code had desired there to be some minimum depth of a mid-rise structure, why isn't it in there? It's not in there.

With respect to the car wash tunnel. Obviously, the Form Based Code doesn't have a couple pages that tell you what the criteria are in order to put an acceptable car wash tunnel on a site. There isn't any way to classify the car wash tunnel. If we were to classify it - as Mr. Cline has - as a low-rise structure, it still comes darn close to meeting the requirements in the Form Based Code. Not counting the cupola that's on top, which

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

I think you'll find very attractive, the height of that is about 21 feet. So, we're five foot short of what Mr. Cline says the height of the car wash tunnel ought to be. What's the purpose of the 26-foot car wash tunnel compared to a 21-foot car wash tunnel? There's no purpose to be served by that. The City of Mission isn't in any way benefitted by that. In fact, if Tidal Wave leaves, they're going to take all the stuff in that car wash tunnel and it's just going to be a shell. And the next person that comes along and tries to develop that site is going to tear it down. So, it isn't as if there's a reason why we need to be concerned about whether the car wash tunnel complies with the low rise building structure. I suppose if somebody decided they wanted to use that shell and make it some sort of retail operation, they could do that, and they could do it if it's 21 feet tall or 26 feet tall.

So, the Form Based Code, at least in our experience - and I'm not trying to cast aspersions on Mr. Cline, or anybody that had anything to do with the creation of the Form Based Code - but I'm here to guarantee you, it hasn't made this process easier for us. The Tidal Wave Auto Spa company is nationwide, and Petty Hardin and Thomas Wells have been before boards like yourself all over the country, and they've never encountered anything like a Form Based Code. Mr. Block has been in the real estate development business a long time. He's never encountered anything like this. And, I hate to say it, but it's based upon more of an imaginary vision of what somebody decided they thought the city of Mission ought to look like than the real-life situation that's out there.

I want to emphasize that despite my reservations about the enforceability, the wisdom, and the rationale behind the Form Based Code, we did everything we were told we needed to do to try and meet the Form Based Code requirements. Mr. Olson will be the last person to speak on our behalf, and he will present to you what he views to be the proper way to grade our project under the Form Based Code, and he'll draw upon his understanding and experience of the Form Based Code that he gained when working on that development that's just diagonally southwest of 6501 Johnson Drive.

When Mr. Block was approached by Tidal Wave, he was ecstatic that someone would be interested in buying this old abandoned gas station site that he owned, and he was convinced that when this development of the Title Wave Auto Spa was presented to the Planning Commission, and ultimately the City Council, they would see it the same way we do. Let me start by showing you where our site is. It's outlined in purple there. It might be easier to orient yourself if you were to glance at the color aerial photos I brought.

That little red circle with the point on it is 6501 Johnson Drive. Let's go to slides 2 and 3. This is the front view of the existing structure. This is the rear view. This is the drop-off that is directly behind the site. That fence that I'm standing next to is the southern boundary of 6501 Johnson Drive. And I'm here to tell you, it's not easy to walk up that grassy slope. From the cement ground of 6501 Johnson Drive down to here is 15 feet.

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

That 15-foot slope is contained in less than 6 feet of width. So, we've got a drop-off that's very, very dramatic, and that's going to become important when I explain to you the process that we've gone through so far to try to bring our project in as close to compliance with the Form Based Code as possible.

Go back to slide #2. This is what's there now. I'm old enough to remember when that was a Vicker's station. I grew up in Roeland Park, grew up driving up and down Johnson Drive. Most recently when we had a legitimate operator at that site, who was actually paying rent it was a Valero. That operation that was legit failed on Mr. Block in November 2015. We brought in a new operator to run that business. He had somewhat of a creative business plan that didn't really include paying rent. So, he operated it until April 2017 when Mr. Block finally put his foot down and said he had to go. No legitimate operator of a gas station and convenience store is interested in this site. The only real, viable use for this particular site, we are convinced, is the Tidal Wave Auto Spa that we're asking you to approve the development of today.

So, given what's there and what we think ought to be there with this two-story building on the front of Johnson Drive, winding around to the Taj Mahal of car wash tunnels, we were really thinking that when we showed up here and said we've got the greatest thing we could imagine to be done with this site, we were expecting to be patted on the back. That's not what happened. When we first showed up and were talking to staff, we had a car wash tunnel here with pretty nice landscaping and a way to get in and out, outdoor vacuums, etc. At that point, we were acquainted with the Form Based Code. And we were told, well, you're really not going to be allowed to do that because under the Form Based Code, we're going to need a structure, could have office, or retail use in that structure, but we had to have a building that fronts Johnson Drive. And if you could come up with a building that meets the Form Based Code - which we meant mid-rise structure - if you come up with a building and had it constructed on Johnson Drive frontage with a sidewalk there, for pedestrian access that isn't going to be there for years - If you could come up with this building, we think you're fine, and you can pass the Form Based Code, and we'll get you all developed, and everybody will live happily ever happy. Well, we did that. Drew up plans, paid a lot of money for architects to draw up plans for this two-story structure.

And, by the way, I should mention, the top floor of this structure has already been committed by Tidal Wave Auto Spa to be the offices for the Midwest region of Tidal Wave Auto Spa, a nationwide company. So, that second floor is going to be occupied from day one of the completion of this building. We couldn't come up with any kind of retail operator that we thought would be interested in this bottom floor. So, the idea was, well, you know, the vacuum cleaning part of our operation is retail. So, what we're going to do is have the greatest spot you could pull your car in and vacuum it out after we've finished cleaning it. There are 10 stalls for cars to come in to be vacuumed after they're finished. Petty will show the traffic flow that gets cars through the parking, through the site, through the car wash tunnel, and into those stalls. They don't have to put a coin in

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

them like the old-fashioned ones. It is part of what you get for the fee to have your car washed. So, we see that as retail. So, it's combination office and retail, and it sure as heck is a lot more of a combination office and retail than it is a parking garage.

Then I think, as Mr. Olson will elaborate, we've met all FBC requirements with respect to that. This car wash tunnel, if you view it as a low-rise structure, is technically six feet shorter than what Mr. Cline thinks it ought to be, but it's really more of an accessory to the retail operations being conducted there. It's full of state-of-the-art equipment. It's the greenest car wash in the United States, using almost all recycled water, and we're very proud of it. We think if it's viewed as an accessory structure, it's not subject to being graded under the Form Based Code. If you wanted to view it as a low-rise structure, it's five feet shorter than it maybe ought to be, maybe lose a point there, two or three points there - I don't know. But it's not going to make any difference, now or in the future, whether that's 21 feet tall or 26 feet tall.

So, we do this, and we think we've got it. We present it to the Planning staff, and what do we hear next? "Yes, you're getting close, but by the way, the Form Based Code calls for the extension of Walmer Street to go across Johnson Drive, and in order for you to be allowed to proceed with your plan, you're going to have to lop off 10-12 feet of the eastern edge of the site, because that's where Walmer is going to go." And if you look at the aerial photo, you'll see that if you extend Walmer in a straight line, you're running right through the eastern portion of our site. That's when I raised my hand and I called Pete, and said, "Pete, you can't do that. You've taken my site. You're condemning me. If you're going to do that, you're going to pay me for the whole thing, because there is no feasible use." Tidal Wave is gone. This Tidal Wave development is using virtually every square inch of this three-quarter-acre site in order to conduct an efficient business. When we were hit with carving a bunch of land and giving it up for an extension of Walmer, that's when I did have to shake my fist and say, "Pete, we can't do that. That kills this deal. And, you've taken my property."

So, after a period of time, Pete and the staff got back to us and said, "We don't think you need that. We'll run Walmer, not across your site; we'll veer it off to the east." I'm not sure how that happens, etc. Then go to the third slide. How are you going to get it down there to a level where it can go down to Martway? And, by the way, there are a couple buildings in between that we're going to have to tear down in order to get Walmer over to Martway. So, is that ever going to happen? I don't think so. I think it's cost prohibitive. Is it going to happen in my lifetime? Surely not. But, that's somebody else's problem now because we've agreed with the City - and the City's agreed with us, I should say - that Walmer's not an impediment to our development.

Now, we think we've got it. We come back and submit our preliminary development plan, thinking that we've met what they told us we needed to do to comply with the Form Based Code, and then we got a scorecard back that said we got a 57. It's like, what the heck? And reading that scorecard, this part hasn't changed. We went from a 57 to a 60

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

because of some tangential things. But we can't get a 90 as long as you're grading this as a parking garage. Because there's no way to put it out another 10 feet without destroying the whole traffic flow that you need to conduct a car wash operation.

So, when we got that 57 score back, we contacted Mr. Heaven and staff and said, "We give up. There's no way we're going to convince you that we should get a 90 Form Based Code." And we're told, and I think Danielle agrees, that if we can't make the Form Based Code, we can't use the expedited procedure in order to get approved without going before City Council. We said fine. Process this as you would any other preliminary development plan that's subject to the Form Based Code. Let's have our day before the Planning Commission, where we explain what we want to do, why it's a mystery to us that the City of Mission isn't jumping for joy about what we're trying to do. And then, we'll let City Council decide after we hopefully get Planning Commission approval of our preliminary development plan.

On the other hand, in the package we received along with our very disappointing Form Based Code grade card were several things that Danielle and her office thought we also needed to do. We then put together an enhanced supplement preliminary development plan that addressed many of those items raised. Other than, obviously, the Form Based Code scores that were attached to those two buildings, which made it impossible for us to get a passing grade there.

In addition - and I apologize for the late agreement to these things - again, this is apart from the Form Based Code provisions that Mr. Cline has applied to our buildings. We went together and came up with a list of conditions that we would be willing to meet in order to bring ourselves closer to compliance with the spirit of the Form Based Code, and which addressed many of the non-Form Based Code comments that we received from City staff. If you were to approve our preliminary development plan subject to these conditions, allow us to go forward in the normal process with the City Council, we're committed to satisfying these conditions.

So, we haven't thrown up our hands and said -- we did what we thought we could, what we were initially told we needed to do, to satisfy the Form Based Code. And I believe that the final development plans, if you look at the architectural plans and our site plan and some other information that Mr. Hardin is going to show you, I hope you'll come to the conclusion that this is pretty darn close to a totally-compliant Form Based Code development, but we're not asking for the expedited process. All we're asking is for you to approve the preliminary development plan and pass that on to City Council with your approval. Rest assured you're fully empowered to approve our preliminary development plan and pass it on to the City Council, despite what Mr. Cline's Form Based Code says we achieved. I believe you'll find Mr. Olson's comments enlightening. It boils down to, that's not a parking garage.

In closing, let me point out a couple of things. If we make that building 40 feet deep, the project is dead. You can't operate the car wash with the traffic flow if we make that thing

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

40 feet deep. It doesn't help the first floor area to be developed in the future, and it certainly doesn't have any impact on the new Tidal Wave Auto Spa division headquarters on the second floor. I think you're going to be very impressed with the aesthetics of what you're going to see when we show you what this development is going to look like. Our plan is going to bring much-needed revenue to the city of Mission. You saw the picture of what's there. Look at the assessed property value. You can look at what's going to be there. You can pretty well project the assessed property value. The property taxes are going up, and a lot of that is going into the City of Mission's coffers. Same thing with sales tax. Right now, you're getting zero. But as I read the sales tax statutes, you're going to get 1.6-plus cents of every dollar that's spent by someone going through the car wash. Again, we're bringing Tidal Wave divisional headquarters to Mission, Kansas, on Day 1. We're not asking for a penny of incentives. We're not asking for any property tax breaks. We're not asking for help to pay for any of this, except there is a mention in our conditions, which is common, that the City should pay for the electricity and controls that are necessary to operate the street lights that are going to go along that sidewalk between Johnson Drive and our two-story building. There won't be any need for litigation if we can get approval.

Most important, I would urge you to look at what is an extremely detailed traffic study that was performed by BHC Rhodes, who is probably the most reputable survey firm here in the Kansas City area. They have stated, in no uncertain terms, that our car wash operation is not going to have any kind of negative impact on traffic going up and down Johnson Drive.

At this point, I will turn it over to Petty, who is going to explain all the marvelous features that will be associated with the Tidal Wave car wash development, which we're asking you with great respect and humility to approve tonight.

Chairman Lee: Thank you.

Mr. Troppito: Is it appropriate to ask some questions now? I'm just wondering, you referred several times to this being the Midwest headquarters. How many jobs, and how many new jobs?

Mr. Schepers: There's not that many jobs in the car wash tunnel. It's a highly-automated operation. There will be a couple people working there. That's really a question for Mr. Hardin.

Petty Hardin appeared before the Planning Commission and made the following comments:

Mr. Hardin: We'll have 10 to 12 full-time employees at this particular location.

Mr. Troppito: Who is going to be the owner of this after approval? You'll be acquiring the land?

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

Mr. Murphy: Yes, sir. I'll be purchasing the land and am the principal owner of the car wash. Eighty-one percent, to be exact.

Mr. Troppito: And all the requisite environmental studies have already been done on that?

Mr. Schepers: Well, it's an old gas station, so there's going to be --

Mr. Troppito: That's why I'm asking.

Mr. Schepers: The tanks are going to have to go. We're not asking for any money to help pay for that. That's something that's going to have to happen.

Mr. Troppito: It hasn't happened yet.

Mr. Schepers: It hasn't happened yet.

Mr. Hardin: Upon approval of this, Mr. Block will be taking the tanks out at his expense.

Mr. Schepers: And that's probably a good thing, to get rid of them. Thank you very much for your attention.

Mr. Hardin: Thank you for hearing us tonight. I appreciate your time, and respect it, for sure. I'll just go through a few slides and tell you about who we are, what we do, where we started. I'll be as brief as possible because I know others want to speak, and I also see that there's other business owners in town that might want to speak at the public hearing. We first began washing cars out of Atlanta, Georgia, in 2004. We've been in business about 15 years. This location would be our 37th location. We have six other properties in the Kansas City metro area under contract and in various stages of development. We definitely want to build more locations, and we're excited about coming to Mission, which has been identified as having a good bit of opportunity. In 2009, we were voted Business of the Year by the Small Business Association. We're very serious about what we do.

A little bit about car washes of this magnitude. This concept is referred to as a spray-wash on the agenda there. It's not a self-service spray car wash. It's not a full-service detail wash, where you hand your keys over to folks and they detail the inside. The customer stays in possession of their vehicle. We've got four attendants on site most of the time. If it's a slow day, we could get down to as few as two. The customer stays in the vehicle. We assist with the payment process. They load their own vehicle onto a conveyor, keep their vehicle in neutral, and it pushes the customer through the tunnel. They come out clean and dry in roughly three minutes. As they exit the facility, they can opt to use our self-serve vacuum system, which is not individual canister vacuums. It's a centralized unit powered by a 25 horsepower motor, which we have in special enclosures that are not visible, and you're not able to hear them as well, the way we designed it. The customers have the option of whether they would like to vacuum their vehicle, or not.

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

Like Paul said, we would have regional headquarters upstairs. We thought about the aspect of retail and just felt like, knowing what's on the market, we didn't want to compete in that space. We're not landlords, we don't want to be, but in the effort to satisfy a Form Based Code in the city and the vision to build a two-story building, to make use of that ourselves we felt was the best thing to do.

There's roughly 27,000 of these type of conveyor car washes in the country. There's an industry magazine that rates us every so often, called *Modern Car Care*. We've consistently been in the top 50 car washes over the year. It's something we're proud of. We're very serious about operating clean, friendly environments. We have a similar vision as Chick-fil-a as far as quality of service that we offer, the friendly staff, etc. Our goal is not to be the most attractive car wash in a market. Our goal is to be the most attractive business in the market. And I can tell you, at the 30-some-odd locations we've done over the years, we have not built one to this magnitude. This is - in Paul's words - a Taj Mahal, and it will be. Our average car wash is somewhere in the neighborhood of \$3 million. We haven't formally bid this out yet, but we're pretty sure it's going to go north of \$4 million. So, we've got our due diligence in what we think that car wash can generate, and we feel certain that we can afford to design that and make it work. We're very proud of it. There's nothing in the country that looks like that.

Having said that, we want to be the most attractive business in the community, not necessarily the most attractive car wash. There are other car washes out there. It's not hard to be more attractive than them. We want to be just as attractive, if not more attractive than some of the later, newer buildings that have been developed recently in Mission. Like Paul mentioned, we're definitely clean, green, recycled water. The chemicals are not hazardous to the environment. We discharge into the sanitary sewer system. We are open 7 days a week. Hours are not set in stone. Sometimes on Sunday we may open a little later. But point being that we're not open after dark. So, in the wintertime, when it gets dark earlier, we may shut down at 5:30 or 6:00 o'clock. In the summertime, we may stay open as late as 8:30. People generally don't wash after dark.

We talked about the recycling of the water a little bit. I'll tell you, you can't recycle 100 percent of the water because when you use fresh water to wash a vehicle and you recycle that water, it's somewhat dirty. So, even though you've recycled it through a really good, sophisticated filtration system, you can't ever really get it back to that quality of truly-fresh water. So, we're able to use that recycled water, about two-thirds of the car wash, in the early process. For instance, high-pressure water that cleans wheels or undercarriage, things like that. You really can't use recycled water to mix with chemicals, and you surely don't want to use recycled water at the end of a car wash. But, somewhat clean water in the final rinse processes. So, you're not able to obtain 100 percent. But, if we didn't recycle water, we'd use about 47 gallons of fresh water per vehicle. Over the years, the equipment has come along, we've gotten better. We're able to now use 14 to 20 gallons of fresh water per vehicle, as opposed to some folks who said if you run a garden hose out on your driveway, you might use north of 50 gallons,

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

80 gallons - whatever it says. Point being, if someone washes their vehicle in their driveway, those chemicals and all that water is going down the storm drain, straight into streams and creeks. We capture all the water, recycle it, filter it through underground tanks, and discharge it into the sanitary sewer.

We definitely give back to the community. We're very big on donating to the 501(c) 3 organizations in each location. Site managers are responsible for finding who they want to donate to each year. The third Friday in September every year, at every location nationwide, we donate 100 percent of, not profits, not money we've made, or a portion of it, but every single dollar that comes in that day, we give to charities. The founder of our company has a disabled child, and that's kind of where this came from. It's really been effective over 15 years and been greatly appreciated. We definitely like to be partners in the community and good stewards of it.

This is our site plan. Johnson Drive is running east and west up this way. Customers will pull into here, the pay lane there. Pull under this canopy here, and there's two pay stations. It's more like an ATM style machine where the customers can pay. We have an attendant there at all times, assisting with payment. Once the customer pays, this is another canopy that we may or may not leave there, just because it's a little bit tight. If someone has ice on their vehicle, or some heavy-duty bugs, things that we know the automated equipment may not get off their vehicle, we can do some prep work here. We'll do it here, or we'll try to do it up front. Again, the customers stay in their vehicle. They've already paid. They put their vehicle in neutral. It pushes them through the tunnel, which takes about three minutes. As they exit the tunnel here, if they do not want to vacuum, they can leave straight out and go back on Johnson Drive. If they did want to vacuum, they take a right here and choose a vacuum in any of these spots. Or, this is an uncovered area, and if it's cold outside, people may want to be out in the sunshine. But if it's drizzly, or snowy, or what-not, and they want to be under the canopy, they can come under here, not a canopy, but a two-story building, and pick from one of the spots here. There's five spots over here, maybe six over here and five over here. This central area here is a stairwell, and we've got some vacuum equipment. The equipment that produces the suction will be housed inside the building, and it's piped out to both sides, which has the nozzles for each of those spots. After the customers finish vacuuming, they come out the same exit here. It's a consistent flow. It's one way in and it's one way out.

Staff had concern about trash or any other service vehicles that come. This is our proposed trash dumpster here. This exit is primarily during the daytime, 100 percent for customers or employees leaving. Trash comes at night. There's a gate here, and the trash guy can have a clicker and open that gate. He's coming after hours, so he's not going against traffic to pick up the trash. That's the general flow of the property.

Thomas Wells, Consultant, appeared before the Planning Commission and made the following comments:

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

Mr. Wells: The other concern that was raised by the City's civil consultant is creating another median break or access point off the entrance drive into the retail/office area. We don't have an issue providing that secondary break.

Mr. Scheppers: And that is listed on the material that I just handed out. We reluctantly caved on that point.

Mr. Wells: This is a front view of Johnson Drive, our two-story mixed use. As stated before, the upper level would have the Tidal Wave division offices, training room, material storage. The lower level would be retail of vending machines, products associated with the car wash tunnel, as well as the retail vacuum spaces in conjunction with the tunnel wash itself. That's a side perspective.

The materials that we have proposed - and this has been an evolution, as well. One of the points that was brought up earlier is providing information on the storefront facing Johnson Drive, north-face to meet requirements of the architectural guidelines of the Form Based Code. In essence, 60 percent of that building façade has to be glass, doors, windows, between the sidewalk and 18 feet up. And we have no problem making revisions to the architectural plans in order to meet that.

Another concern raised is the amount of EIFS or stucco where that is located. Again, we can make revisions to address those concerns. Typically, at the lower level, we have a split-face concrete masonry unit, and then a precast work table that sits on top of it, roughly about three feet up. And then, either a combination of stucco, EIFS or veneer stone that is above it. So, there's basically three primary products on the face, plus the glass, and then, we use an engineered, prefinished standing seam metal roof.

This is the exterior finishes. The one you can't see is the clear glass glazing. On the left is a representation of the split face masonry unit, and the color would be on the very lower level. The top-middle is the water table, water ledge that sits on top of the split face. And then, top left is a color representation of the hard coat stucco and EIFS that would be above that, and the stacked veneer stone that is used above that water table. Top-right is actually a change in the traditionally roof color Tidal Wave blue, in order to try and be more attractive along this style, which is one and only, you know, for the mid-rise two-story that's planned on the front.

This is floor plans. The top one is the lower level. You can see some of the vacuum stalls, and in the central portion, there is an ADA-accessible office. There will be some equipment in there, as well as vending services. Off to the far right is another stairwell and lower-level HVAC units. On the bottom section is Level 2 floor plan. Central stairway there. Emergency egress on the right side and two restrooms and office/storage/training rooms.

This is a perspective view of the accessory tunnel in the back that's considered the one-story. That's basically shielded from any views from Johnson Drive because our two-story mid-rise goes drive to drive. So, unless you look backwards or around the

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

corner. This is a similar architectural theme, with the split-faced concrete masonry unit, the precast water table, and then, the veneer stone on the tower, and then veneer stone columns and accents, accent on the mechanical room, and then some of the EIFS and hard coat stucco on the mechanical room, as well.

Again, this is just a view of the various materials that we went over on the previous slide. As Mr. Schepers alluded to earlier, the top of the cupola on this property is somewhere around 22 feet based on standard design. This is an actual vacuum enclosure. A lot of car washes, you'll see the large canisters nearby a space. A very noisy mechanical unit. This is standard for all Tidal Wave locations, to actually enclose that equipment in a vacuum enclosure so that we're able to control the noise. Here in a very urban and commercial area, it's not a major concern, but it still would be beneficial for our employees, our customers, our neighbors, and was brought up by Mr. Bennett, one of our neighbors to the east, to address that.

This is the express pay terminal - XPT - and this is the pay canopy. It has two lanes, it's basically like an ATM machine. You pick how many dollars you want to put in by the type of car wash you select.

Ms. Dukelow: Where on the site is the vacuum enclosure?

Mr. Wells: It's this building right here, in that landscape aisle.

Mr. Hardin: This uncovered island here, that enclosure would be there, and the other would be inside to service that portion.

Mr. Wells: We talked about the divisional office. In our initial meetings with staff, you know, having a functional two-story building up on Johnson Drive was going to be paramount to meeting the spirit and intent requirement of the Form Based Code. So, that quickly developed as a prime location, central location to a lot of the locations that we currently have in our development pipeline here in Mission. Quik Trip's division office is right around the corner. They didn't put it there just because there was an inexpensive office building. They put it there because it's centrally located to their stores and employees, and easy access. QuikTrip is another entity that Tidal Wave tries to emulate.

This particular site is .76 acres. We've got an 18-foot drop-off to the back. Part of Mission West shopping center that surrounds, is in the rear, wraps around, comes back onto the front. So, kind of land-locked on the west and the east. We've got Exact Performance to the south and west, and to the east we've got Exact Performance. We've been in dialog with Mr. Bennett, who owns Exact Performance. He doesn't have any plans to go anywhere. So, we're landlocked with what we're able to do and the size of the property that we have to work with. We're kind of a hybrid of a service and a retail-based operation, so we feel like from a pure retail standpoint, we've got better longevity. With Amazon and the Internet, pure retail is evolving quickly. We feel like the longevity of our business plan, even with Uber or Lyft, there will still be cars to wash.

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

Also, we feel there are ways that we could adapt this mid-rise building on the front, that at such time - 15, 20, 50 years - that Tidal Wave ceases to operate, there could be some adaptation of this building. But, at the same time, with the evolution of retail that's taking place, it's hard to guess what that adaptation might look like, or what we need to incorporate into the building now for some 15, 20, 30 year period out. This is one of our existing locations. It shows the intensity of landscaping. And then, the vacuuming canopy that we've eliminated on this one, so it will be open-air out front. You can see the mechanical room on the side. I'll turn it back over to Mr. Schepers.

Mr. Schepers: I will follow Mr. Olson to make sure I can respond to any questions you might have. But, at this point, I'd like to turn it over to Dave Olson, to express his own views on compliance of our plans with the Form Based Code, and in particular, with respect to those structures. Dave?

Dave Olson, Monarch Acquisitions, appeared before the Planning Commission and made the following comments:

Mr. Olson: It was about four years ago that I worked with staff, worked with the City, and we completed a development on Johnson Drive. It took a lot of work, but I won't go into a lot of details on that project. I want to boil it down. We have their staff report, eight pages, going through the rationale. So, being an engineer by trade, I've boiled it down to its essence. I've created a single-sheet scorecard. Look at what's possible, what's required. And in my opinion, what I think should have come out of the evaluation.

In the staff summary they talk about the code being a tool to evaluate the appropriateness of a project. So, we're sitting on a very small parcel, and you think about what's appropriate. I want to look at three things. I heard it mentioned earlier, but they're proposing - in my opinion - a two-story structure, office on top, retail on bottom. Certainly not a parking garage. And, they're proposing to build that second-story office space without any incentives. I know the last development that came before you went kicking and screaming about second story office space above retail. It would be difficult to lease. It's still vacant today.

That hits two of the three items that I want to put in your foremind. The third item is, I look at the rear building, the tunnel, as an accessory structure. I've also heard mentioned, as far as visibility, because of the almost complete frontage of the two-story building and the height of the two-story structure, you won't see the accessory structure. So, taking the scorecard - before I pass that out, again, I apologize. I got the numbers wrong on the actual score given to us from, from Chris. I somehow got it to 64 instead of 60. I'll pass that out. I want you to look back at the conditions that Mr. Schepers has provided, and as you look down the scorecard, I made brief notes on what the issues were that we didn't receive a passing grade. In each section, it starts out, you know, the rear building is not 28 feet high; under the regulating plan, the front building is interpreted as a parking deck and not 40 feet deep. The third item did not provide a traffic or turning template. With the access and introduction of the median break as

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

some of the conditions, I think we're satisfying the traffic turning template issues, and if in your mind you consider the rear building as accessory and the front building two-story structure office and retail, I think we get to 15 points in all three categories.

Under step 2, the building types, the basic essence of why we're not passing. Rear building is not 26 feet high, and the front building doesn't have commercial use on the ground floor. Which, again, is part of retail sales. If you're collecting sales tax on the services that are being sold, to me, it has to be considered commercial use.

Step 3, the Urban Guidelines. You don't have to get the perfect score under Step 3. There's some elements of - You don't have to get a perfect score. But, that was actually a category we did well in, and I didn't change or amend any of the scores, other than with parking area and using the conditions that we proposed, which were complying with the required streetscape, the landscaping that's required on the side and rear yards, and the one item that I suggested we add as a condition, which is the pedestrian connectivity. Tidal Wave chose not to agree to provide pedestrian connectivity through their parking and vacuum area, which I understand they're not really wanting to - if pedestrians don't have a car, they're not going to be using the facility. So, under Step 4, the architectural guidelines, the intent, the materials, the configuration and technique, again, considering the rear building as an accessory use subject to the 26 feet in height. And then, the north building storefront being 60 percent to the 18 feet level, they've agreed to do that. I think they've given you compliance. So, given that, I would estimate the score somewhere between 90 and 91. Thanks for your time.

Mr. Schepers: Ladies and gentlemen, I know it's late, and I fear we've tried your patience already. So, unless any of you have any questions, we'll submit the preliminary development plan and request that it be voted to be approved, subject to the list of conditions that we've agreed to abide by that are on the handout I gave you earlier today.

Chairman Lee: Any questions?

Mr. Troppito: Did staff review your list of conditions that you passed out tonight?

Mr. Schepers: I did not have a chance to finish that in time. I gave it to Pete before the hearing. I don't have any problem with staff taking time to look at it. But, I will say that each of those conditions were derived from comments that staff made in their recommendation, which they asked, which formed the basis for their disapproval of the plan. So, it's not as if we pulled those out of thin air. We took the staff comments and addressed as many as we could by agreeing to, what we interpreted those to be conditions to the staff's approval. So, they're not secret, but I didn't hand it out until just today.

Mr. Troppito: You mentioned that you wouldn't mind taking the time. Can you quantify that? Two weeks? Thirty days?

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

Mr. Schepers: Wouldn't mind taking the time to have the staff review it?

Mr. Troppito: I thought you were indicating that you would willing to provide extended time for the review of what you propose tonight. Is that correct?

Mr. Schepers: I'd rather just get the plan approved, but if Danielle is in a position to say she needs to review those, then who am I to say you can't review them because you had 30 minutes to look at it before the meeting today? So, yes.

Mr. Davidson: I have a question. Those offices above, as far as - and the four employees that are on site, or so, where would that office people and employees, where's the parking for them? And I understand that is a very tight project on three-quarters of an acre. I think that's a nice job, getting everything in according to how your national plan is. But, where would those vehicles be parking?

Mr. Wells: Two options. Obviously, depending on how busy the site is, some of the vacuum spaces could be used for employees while they are there. There are also plans that would be engaged to lease or rent off-site parking spaces so that employees wouldn't be taking up a space. And the idea of, you know, a lot of the training, team meetings, etc., will much likely be taking place after normal operating hours, which would then open up the entire facility for employees and team meetings.

Mr. Davidson: You're saying those office spaces would not be used during car wash hours?

Mr. Wells: Not to full occupancy, no. They would be operational people there during normal business hours, but as far as having team meetings and 100 percent occupancy of the office space upstairs, that would most likely be after operational hours.

Mr. Troppito: A question for staff. The varying conditions for approval presented tonight, do you feel you'll have adequate time to address these tonight? Or will you need more time?

Ms. Sitzman: Well, with a quick review of them while the presentations were going on, against the staff report, I feel like we could be comfortable with these tonight. I do have concerns about number 2 and the meaning of the "City agrees to provide power control equipment." I feel the topics raised cover the issues. I'm not sure that they're adequately addressed as worded.

Mr. Braden: Out of curiosity, what is Mr. Olson's relationship with this property, other than just -?

Mr. Schepers: I retained him as a consultant.

Mr. Braden: Is he getting paid?

Mr. Schepers: Yes. He loves the City of Mission, but not enough to waste his Monday evening here without being compensated.

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

Mr. Braden: In your wash bays, since it's enclosed on all three sides, is there any means - and this gets into the internal part of the building - for exhaust, for vehicle fumes? Has that been addressed?

Marty Murphy, Project Manager, Tidal Wave, appeared before the Planning Commission and made the following comments:

Mr. Murphy: With the open-roof system, there's no issue with exhaust. We have an open-roof system where at the end of the gables, it has a place for the exhaust to settle. That would be something your fire marshal would have to look into. We pass that everywhere we go. I've been developing these all across the United States. Every one that's been developed in the last few years, I've been there. We pass every inspection and review by fire marshals and by fire code standards everywhere we go. Once they see our architectural and our plans, we don't have any issues with that.

Mr. Braden: You said you haven't had one with offices on top.

Mr. Murphy: We've never had offices above the parking, which is our back stalls downstairs, but it's open on the back side. It's like an open garage on the back side. It's only enclosed on three sides. There's not an issue of exhaust. And it doesn't cover the car completely. The rear end of the car stays out. What we try to provide is shade for you at the doors, hit the side of the car. We don't provide shade for the entire car. So, your parking will actually stop at about the 14-foot mark, so you would only be able to enter into the back canopy or the two-story mid-rise building approximately 14 feet.

Ms. Dukelow: I have a few questions of staff. I have a question on Sheet A2.1. I could not figure out what these elevations are for. As far as I can tell --? Sheet A2.1 [*Looking for form.*] So, the top two elevations on that sheet, can anybody tell me how those line up with the plans.

Mr. Wells: Yes. This an accessory building located on the southern portion of the site. This is a single-story structure. The top one is one of the perspectives. You've got the tunnel, and the mechanical room. The middle elevation is the south side elevation.

Ms. Dukelow: Okay. I've got you now. Thank you for that clarification. And is all this glass along these elevations clear?

Mr. Wells: Yes, ma'am.

Ms. Dukelow: Also, I want to ask about the glass that fronts Johnson Drive. The intent is for that to also be clear?

Mr. Wells: Correct. It's required by the Form Based Code.

Ms. Dukelow: Yeah, I'm remembering that now. Thank you. Just a couple more questions. It appeared from one of the perspectives that we saw that there may be drinking fountains and restroom facilities also included in this accessory --?

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

Mr. Wells: Correct, in the mechanical room on the side of the tunnel, yes. There is an ADA restroom there, and there is a high-low drinking fountain provided on the side of the mechanical room. Another ADA requirement.

Ms. Dukelow: I also noticed on the site plan that there is an accessible parking stall at the back of that building.

Mr. Wells: Yes, on the southwestern portion of the site, the required ADA accessible, van-accessible parking space.

Ms. Dukelow: I didn't notice if there's an elevator provided in up to the office --?

Mr. Wells: At this point, no, there is not an elevator. Per the architect, based upon the size and occupancy load, etc., that is not required. But, I would have to defer to him on that.

Ms. Dukelow: Thank you. I have a question for staff, Mr. Cline, Mr. Heaven. I know that the sector plan shows extension of Walmer. But, with regards to that, I have several questions. One of them has to do with, how much of the property to the west of Horizons is owned by the district? I'm wondering about the feasibility of ever extending Walmer because the school district - I don't know who owns that property, but it may be the school district. And we all know that there's a dramatic slope. The other question I have is, looks like the fire station is not, is clearly out of the way of that. I really wonder about the feasibility, and would be interested in hearing more about that.

Mr. Cline: Sure. There are a number of places in the Form Based Code area where street realignments were considered, and this was one of them. Danielle touched on one of the primary reasons for that initially, was the large block size. There's a lot of property here to work with. The slopes, all that engineering has to be worked out as to where that alignment falls and how to make it connect with Martway. But, it would be feasible to do that in a number of different configurations. So, the intent behind the original code showing some type of connection here was to try to provide a mid-block connection between Johnson Drive and Martway. So, you can see there, I believe right where the Z is, I believe that parcel of property is part of the school property. It wraps around that commercial business there that's located just to the north.

Ms. Dukelow: So, that's shown directly along the west side of the district property.

Mr. Cline: Correct. And the intent was to line up with Walmer across the street and try to, since Johnson Drive is the busier street, to try to create a four-way intersection at that location. So, there was consideration made on that end to make that alignment at that location versus one of the other two blocks. Split the difference between the other two streets, to the east and west. You can see where Walmer lines up just to the north. Any anticipation as a part of all of the Form Based Code is that at some point, some of these properties may redevelop. So, if there was an opportunity in conjunction with

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

some other redevelopment that might occur to the south, all of those connections could be made and considered at that time.

Chairman Lee: How much of the cost of the car wash is reduced if I decide not to use the vacuum? What is the cost of going through the car wash if I opt not to use the vacuum? The reason I'm asking, you keep referring to the front being retail, which would be the vacuum. So, the value of that vacuum is how much in relationship to the cost of washing my vehicle? Which is really not being done in the accessory building. Isn't that where the retail actually is taking place?

Mr. Murphy: As you pay for the wash, that's part of the wash. It's not an option. If you need a percentage breakdown, I'd be glad to get you that information.

Chairman Lee: My point is that you refer to this front building as where retail at the first level is taking place. There is no retail being taken. If I can't pay for that service, where's the retail component?

Mr. Schepers: All I can tell you is that you pay for the right to use the vacuum when you buy your car wash.

Mr. Murphy: Yeah, there's retail sales and vending in that area.

Mr. Schepers: Yeah, there's going to be deodorizers, wipes...

[crosstalk]

Chairman Lee: Off the top of your head, what percentage of your business uses the vacuum?

Mr. Schepers: What percentage of the people who use the car wash? Use the vacuum?

[crosstalk]

Chairman Lee: Is that 10 percent of the sale? Five percent? Eighty percent?

[crosstalk]

Mr. Hardin: Of all of our revenues, how much is vending? It's not a huge portion.

[crosstalk]

Mr. Hardin: --- it's five percent, maybe?

Chairman Lee: So, you're saying retail that is taking place on the first floor is just that vended portion.

[crosstalk]

Mr. Schepers: As counsel, I would say that the retail service that you buy when you pay for your car wash includes the use of the vacuum.

Chairman Lee: I understand that.

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

[crosstalk]

Chairman Lee: -- there's no reduction if I opt not --

Mr. Schepers: The only reduction would be fewer people might buy car washes if they didn't get a complimentary vacuum.

Mr. Murphy: If we had a car wash, which in some big metro areas, I know some folks in Houston, Texas, where they had a very small site, much smaller than this, and they don't have vacuums. They have just a tunnel car wash, and they're cheaper because they don't have to buy as much property, buy the equipment. There's definitely a lot of dollars saved with electricity, running motors, that type of thing, associated with the vacuums. And it's a huge part of our business. If the vacuums were not available, probably 60 to 70 percent of the customers do use the vacuums. They can use them for five minutes, or 20 minutes. It's a very significant business model.

Chairman Lee: At this time, we're going to call the public hearing. If there is anyone who wants to get up and speak either in favor or against, this would be the time to do it.

Ben Bennett, Exact Performance, appeared before the Planning Commission and made the following comments:

Mr. Bennett: I'm to the east of the project. My concerns are traffic, noise, but I'm also tired of looking at, just a desolate piece of property next to me. So, I feel like it's kind of up to you guys to make the proper choice. I don't feel that a car wash is a bad decision. I don't know if there's a better decision out there, but I don't think it's a bad one.

My concern is Walmer Street. The comment I heard from Pete is, "it's moved to the east".

Mr. Heaven: No, that wasn't Pete who said that. I'm Pete, and I'll tell you this. That is a futuristic street plan, and one of the reasons we didn't press it in this case is we don't see it happening in the near future. It may be when your building comes down. Whenever that might be. The City has no interest in trying to condemn land or take buildings down and build streets.

Mr. Bennett: Or move businesses.

Mr. Heaven: Right. Absolutely.

Mr. Bennett: Yeah. Because I feel like I'm established, for 20 years. Our birthday is coming up this week. I feel like I contribute to this community.

Mr. Heaven: Absolutely. There is absolutely no intention --

Mr. Bennett: So, if there's any threat to my business, I want to know about it. Other than that, I'm fine. I hope he gets some business.

Chairman Lee: Anybody else who would like to speak, either for or against?

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

Sandi Russell appeared before the Planning Commission and made the following comments:

Ms. Russell: I'm a Mission resident, as well as a business owner. I have Twisted Sisters coffee shop on Johnson Drive. When you keep asking about the retail, isn't the cost of the charges for the car going through to get cleaned, is that not considered retail? I'm trying to get it clear in my head. So, they're paying money for a service, just like they would pay money for a service that they walked into. They're just driving in. So, I would consider that retail. I think it's an awesome-looking project against what's sitting there now, and has been there for years. We don't have much development on the west side, and that would be an awesome start. Unfortunately, Mission is kind of going through a phase now where people are moving out, businesses are moving out. So, for a business to come in, that would be awesome. And to have them not ask for any incentives, that's even more incredible. That's it.

Cathy Casey appeared before the Planning Commission and made the following comments:

Ms. Casey: I'm a business owner of two businesses in Mission, Casey's Auto Repair and Casey's Auto Repair on the Drive. I would just like to say, I challenge you to have the same landscaping that we offer at our shop. I agree with Sandi on everything she said. One thing I'm really troubled about is you're talking about Walmer going through. Why was it brought up? Why did he have to get a lawyer to come and talk to the City? Because it was brought up. Makes no sense to me. We have somebody who's going to have a beautiful building in Mission. Try and help them. Give them every chance to open up, instead of having them have to contact lawyers all the time to try and get something done. I would really like to hear that things are happening in Mission without having to get a lawyer to fight. That's what I'd like to say, and that's it.

Kevin Fullerton appeared before the Planning Commission and made the following comments:

Mr. Fullerton: I'm a resident of Mission, and I'm also a business owner. I own Springboard Creative down on Johnson Drive. One of the things I've been worried about, we have a Mission business partnership, of which I'm the president. We've been getting a lot of feedback from our businesses that our city has become very business-unfriendly. And as I sit here and look at the struggles they're having to go through to get in front of you all, to get in front of the City Council, that concerns me greatly as far as what other kinds of businesses are going to want to come to Mission.

It was mentioned earlier that we've got businesses that have been leaving. Bad timing. We've lost several. But, let's not make it harder for people to come into Mission, to start a business, to do it without asking for any incentives whatsoever. To me, all I had to hear was "no incentives," and that's great. And what they're doing, the way they've bent over backwards to get where we want them to be, has been amazing. So, I hope we can

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

bring in a great business like this to be part of the Mission business community. Thank you.

Mr. Schepers: I'd like to add for the record, not a single one of those people who just spoke was compensated in any way.

[*Laughter.*]

Chairman Lee: With that said, I'll close the public hearing.

Ms. Dukelow: Mr. Chairman, I have a question regarding next steps. I'm wondering if we - I don't know what, what would be the preferred way. If we choose to deny versus table, would that enable the applicant more opportunity to -? Would they still be able to take advantage of the expedited procedures that would be provided by the Form Based Code? Is that desirable? I mean, I'm just trying to figure out through these options what the outcomes of them would be for this particular project, on this particular site, for this particular applicant.

Ms. Sitzman: With the score they have currently from staff, they would still need to go to City Council for a final determination. If you wanted to make a motion to table this and direct staff to apply a different building type, if you were to tell us you have listened to comments tonight and you think the mid-rise building height should be evaluated, we can certainly come back to you with a revised score based on that direction. If they were to get a score that was 90 or better at that point, they could go back to the Form Based Code review that ends at the Planning Commission. That would address the expedited versus the normal review.

Ms. Dukelow: And if they were to, if we were to do a mid-rise building type review and they achieve the 90 points, then they would be here in a month?

Ms. Sitzman: Right.

Ms. Dukelow: I'm just trying to figure out how these things are going to impact, be impacted by schedule.

Ms. Sitzman: The next meeting is April 16th, a little earlier than usual. It's the third Monday. I think there is still time to prepare a review and have it before you at that meeting.

Ms. Dukelow: And then, the alternative would be, if we choose to do that, if we were to proceed and send it to City Council, staff would have time to review and go on --?

Ms. Sitzman: Right, and the meeting for City Council would be the Wednesday of that same week April 18th.

Ms. Dukelow: So, from a schedule standpoint, it really doesn't make a difference.

Mr. Schepers: Excuse me, Ms. Dukelow, if I might make this quick point with respect to that. My concern is that we've been what we thought was very close to the finish line several times, only to have it moved. So, if what happens, based upon your suggestion,

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

is we're back here on April 16th with the staff saying, "We've looked at it, it looks like a parking garage to us, no passing Form Based Code grade," now you've set us back even farther. And if that were the way it plays out, I would just prefer that you go ahead and approve it subject to our conditions and let the City Council make a final decision.

Ms. Dukelow: I have another question of staff. I'm not familiar with the history of this project, so I'm wondering, how long has it been, really? How long have we been -?

Ms. Sitzman: I don't have the timeline in front of me, but there was a period of time where the applicant took some time to consider how they wanted to proceed. They took several months to consider this.

Unidentified: Fourteen months.

Ms. Sitzman: That sounds correct.

Mr. Braden: I have a question of staff. Again, can we just go over what the big heartaches are? I remember reading that there was a tough time telling what kind of building this is, so it was assumed a parking structure.

Mr. Cline: This one has been challenging because it doesn't neatly fit into any of the building type considerations. So, when I'm interpreting what's in the code and how to rank this project and score it appropriately, I had to look at the information that's here in the code about this type of use. So, a car being inside part of the building. When looking at mid-rise, or parking structure, or low-rise, all of them say accessory units prohibited. So, when we talk about this, the car wash in the back being an accessory use that was part of the score, accessory uses are not permitted with any of those building types. So, if you were to direct us, that this is a mid-rise building and score it that way, when I look at a mid-rise building type, it still doesn't allow for an accessory use. So, the structures in the back really need to be considered as one of these building types, which I consider it as a low-rise building. And it doesn't meet the 26-foot requirement. We've had other submittals in the past that have come through a few times before they finally meet the 26 feet.

Mr. Braden: How short are we of the 26 feet?

Mr. Cline: There was one case where it was four inches short and it failed. So, that precedent was set. The intent behind a low-rise building having such a tall profile was to create a sense of scale out here, and to make sure that the building fit the area a little better. I've been very fair and consistent in the way that we've scored that throughout that, and that's why that back unit being less than 26 feet doesn't qualify as a mid-rise building.

But, when I look at the mid-rise building, even when I get to the urban guidelines that are located in Chapter 4, page 3, it says: [Reading] *Depending on lot size and block configuration, parking may be provided under the building (below grade) - not on the ground floor - in a structured garage behind the building or in a well-designed surface lot*

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

behind the building. So, it provides three different options for how to address parking a vehicle as a part of the mid-rise building type, and none of those conditions say you can park directly underneath the building in the ground floor. So, when I look at what's written here, I don't consider this a mid-rise building based on this definition. So, the closest thing I could find in a building type to what's being proposed is a parking garage, and a parking garage, it says there has to be 40 foot of depth, a linear building, if you will, for retail use or some type of use out in front of the parking garage. That's where we came up with the 40 foot as a minimum depth. That's where it's referenced in the definition here for parking structure.

So, I've been trying to figure out how to adapt this to what's written and what's defined, and that's what I put in my report, is the way it stacks up with the way it's written right now.

Mr. Taylor: And I understand the challenge there, and I appreciate all the work that you all are doing to make this assessment. It occurs to me, I'm looking for other comparisons, types of businesses that might utilize this space and house cars. We have two others who spoke today who are mechanics or auto care companies, and I'm just curious, as a new member of the commission, how would those types of businesses qualify? Would they be parking structures? Can you speak to that at all?

Mr. Cline: Like I said, I'd have to see the floor plan and understand what it is --?

Mr. Taylor: It's not that simple?

Mr. Cline: Yes.

Mr. Taylor: Okay. The other question, I'm curious about the process. If we move forward and allow this plan, they would go outside of the Form Based Code and work with City Council. Not gaining any benefit. And they've spoken as though they accept that term.

Mr. Schepers: We came here resigned to the fact that we weren't going to get a passing score from Mr. Cline. Consequently, in my discussions with Mr. Heaven, we came to the conclusion that the path forward for us was to present this as any preliminary development plan outside of the Form Based Code areas where people presented, subject to your approval under the criteria that's set forth in the process for consideration of preliminary development plans. At that point, whether you vote it up or down, it goes to the City Council, which is really where we'd like to head.

Mr. Cline: I've done my best to be fair in the application of these standards to every application. I want the applicant and the City to know that I've tried to be very consistent and look for opportunities to score this as best I possibly can. I'm constrained by what's in this document just as much as anyone else.

Mr. Davidson: And I understand, you try to place a rubber stamp on something, you know, to keep uniformity throughout the city, through all these projects. And when we agree that we have this code on a property that we didn't want to say yes in this

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

situation and no in another situation. But I think these gray areas that the Form Based Code is based upon, it is a gray area. Meaning it's not a parking garage, but that's the closest definition you have in your descriptions. I think it comes to a point in time when the Planning Commission, it becomes our place in a gray area to say, do we like this project, or not? I'm talking about that single structure, the accessory building being six feet short from that 26-foot height, or what-have-you. But when you see that structure from the south and you have an elevation drop of 16 feet, you're looking at that structure, you know, 16 feet up in the air. And a lot of times you're not going to be able to tell if it's six feet taller than it is, or that kind of thing. So, I'm just saying, the logistics for this piece of property, those are the kinds of things that we as a Planning Commission have to use our common sense.

As far as the project, I think what is there and what they are proposing is quite, I mean, a wonderful use of the property. And I am familiar with cities that sometimes can be very difficult for businesses, and we don't want Mission to be like that. I hear that resident loud and clear, as well.

Mr. Bruce: Mr. Chairman, I'd like to jump in on what Brad was saying. We've heard from our business community this evening. There seems to be a general feeling that we're not supporting the business community. I think that ought to be one of our highest priorities as a group here, and also as a group at the City Council meeting, is to support, encourage and develop our business community.

Now, it looks to me like they have gone through a lot of exercise here to meet the requirements of the Form Based Code. Mr. Cline, I apologize, but it seems like we have a legalistic issue that is dividing us from accepting a viable development here because of some wording that says, either the rise, or on something else. So, my personal feeling is that I would like to see this move on to City Council and let them determine how strongly they want to support the Form Based Code when you have a project of this quality sitting as a potential along this eyesore of Johnson Drive. Thank you.

Mr. Heaven: If I could try to put this in perspective. The Form Based Code is a vision. It's kind of a general vision for what we want our city to look like. And the reason that we adopted the Form Based Code was to accelerate the process. If you meet our criteria, you get a passing grade, you basically go straight to the Planning Commission then you get your building permit. The idea was to promote business, not slow it down. But we do hit these gray areas. So, when we do hit a gray area, we circumvent the Form Based Code. The fact that you don't get a passing grade doesn't mean you can't do your project. It just means you can't have the accelerated process. So, you have every right to approve this project tonight, recommend approval to the City Council. You don't have to worry about the Form Based Code. It doesn't pass the Form Based Code. So, get over that. Let's not try to make it pass something it can't pass. And that's our fault. Our code doesn't deal with this.

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

I say, tonight, just make a decision. Decide whether you think the project is good and recommend it to the Council, or decide it may not be the best use and recommend denial. Really, the Form Based Code should be a guide, but - it's not deal-killer. It's something that would accelerate the process, and it doesn't qualify. So, I'd say go ahead and vote tonight if you can.

Mr. Braden: I have a question, and I know the Form Based Code kind of incorporates that, to not only speed up the process but give us an idea what this area is supposed to look like. In this case, maybe that's not sensible for this particular application. But what I want to make sure is that if we do go ahead and approve this, does that set a precedent for projects moving forward? That would be my main concern. Also, I would suggest that we find a definition in the Form Based Code that meets one of these kinds of structures. I think we need to look at that.

Mr. Heaven: Mr. Braden, I think the precedent you'll set tonight, if there is a precedent, is that you as a Planning Commission can rise above the Form Based Code and make decisions for the city. I don't think you're locking yourself into doing anything in the future that would be bad for the city. I think you're retaining your discretion to do what's good for the city.

Mr. Braden: Thank you, Pete.

Mr. Troppito: I have a question for Pete. Pete, you suggested we make a motion to approve this and move it to City Council. But, to what extent do you envision such motion would be including the conditions for approval that were passed out tonight?

Mr. Heaven: I believe in your packet, Danielle has given you a proposed motion with some conditions. One is to approve and send on. I have gone through what was prepared and handed out tonight by the applicant. I have no problem with it. I do have the same concern Danielle does. I don't understand exactly what it says, when we're going to agree to provide power and control equipment for street lights. I'd want to clarify that. But, I think with the conditions that Danielle suggested, and these conditions, in addition, I'm very comfortable with it.

Mr. Schepers: I have a question, Pete. Does the City not pay for the electricity for lights that are on public right-of-way?

Mr. Heaven: Yeah, but I don't know what you mean by control equipment. What is that?

[Overlapping comments.]

Mr. Schepers: The thing that turns it off and on. Like that photocell thing. The power and off-and-on thing.

Mr. Scott: You have to provide lights that conform to our design code. Beyond that, we pay for the power.

Mr. Schepers: Yeah. And we're applying for that. That's what we meant to say.

MINUTES OF THE PLANNING COMMISSION MEETING
March 26, 2018

Ms. Dukelow: Mr. Chairman, I will make a motion.

Ms. Sitzman: Ms. Dukelow, I'm sorry, you're going to have to read some conditions into the record, however, if you don't want to read all of the ones in the applicant's handout, you can simply say "conditions 1 through 6 as provided in the handout." We will provide a full record to City Council as to what that means.

Ms. Dukelow: Provided by applicant?

Ms. Sitzman: Yes. Although staff would recommend you simply strike the last part of number 2, starting from the word, "provided, however, the City agrees to provide power and control equipment."

Ms. Dukelow: Okay, so, the suggestion is to strike the last portion of condition #2 after the semicolon, which says, "The City agrees to provide power and control equipment for the street lights."

Ms. Sitzman: Correct.

Ms. Dukelow: Okay. Mr. Chairman, I move to adopt the following findings of fact and recommend to the City Council to approve Case #17-11, the Preliminary and Final Site Development Plan for Tidal Wave Auto Wash development, with the following stipulations. And these are as written by staff:

Prior to the issuance of a building permit:

- Complete information about percentages of EIFS and storefront glazing are to be provided for staff review and approval.
- Along the Johnson Drive frontage, windows are doors shall meet the minimum 60% total coverage of the storefront and EIFs or stucco shall not be used within 8' of the ground nor comprise more than 25% of the first story.
- Complete information regarding trash enclosures, retaining walls, other screening, pay canopy and pay kiosk in compliance with the Form Based Code standards shall be provided for staff review and approval.
- Complete details regarding the site landscape and public streetscape including street lights, benches, trash receptacles and bike racks in compliance with the Form Based Code standards shall be provided to staff for review and approval.
- The median break in eastern entrance driveway shall be relocated to allow vehicles to leave the wash tunnel queue before the pay station.
- Complete details regarding the circulation of trash and other service vehicles on site shall be provided for staff review and approval.

In addition to those conditions provided by staff, those will also include those conditions as provided by the applicant, and the amendment to the conditions provided by the applicant. We shall strike the last portion of item #2, in which the City agrees to provide power and control equipment for the street lights.

Mr. Troppito: Second.

MINUTES OF THE PLANNING COMMISSION MEETING

March 26, 2018

Chairman Lee: Call the roll, please.

The vote on the motion was taken (8-0). **The motion to approve this application carried.**

Mr. Hardin: I want to personally thank staff for working through the challenges so far with this project. I know it's been a gray area, and I appreciate your willingness to work with us. Our work has just begun, but I want to personally thank everyone involved in this. Thank you so much.

Mr. Schepers: I echo that. Thank you.

Planning Commission Comments/CIP Updates

Mr Braden provided the Planning Commission with an update on the CIP Committee's activities.

Staff Update

Staff provided an update on current and upcoming projects and events.

ADJOURNMENT

With no other agenda items, **Mr. _____ moved and Mr. _____ seconded a motion to adjourn.** (Vote was unanimous). The **motion carried.** The meeting adjourned at _____ P.M.

Mike Lee, Chair

ATTEST:

Ashley Elmore, Secretary



FORM BASED CODE SUMMARY PRESENTATION

April 4, 2018



WEST GATEWAY

REDEVELOPMENT AREA

FORM BASED CODE
SUMMARY PRESENTATION



REDEVELOPMENT INITIATIVES



WEST GATEWAY

REDEVELOPMENT AREA

FORM BASED CODE
SUMMARY PRESENTATION



FORM BASED CODE

1. Goals

- Proactive vs. Reactive

2. Planning Process

- HBA Tours / Examples

3. FBC – FAQ's

- Will the Infrastructure Serve It?
- Can the Market Support It?
- Is the Code Flexible?
- Using the FBC / Steps
- Other FBC's – Downtown OP



WEST GATEWAY

REDEVELOPMENT AREA

FORM BASED CODE
SUMMARY PRESENTATION



PLANNING PROCESS

- ADVISORY COMMITTEE MEETINGS

- 30+ / 6 meetings held
- 10-month process

- PUBLIC MEETINGS

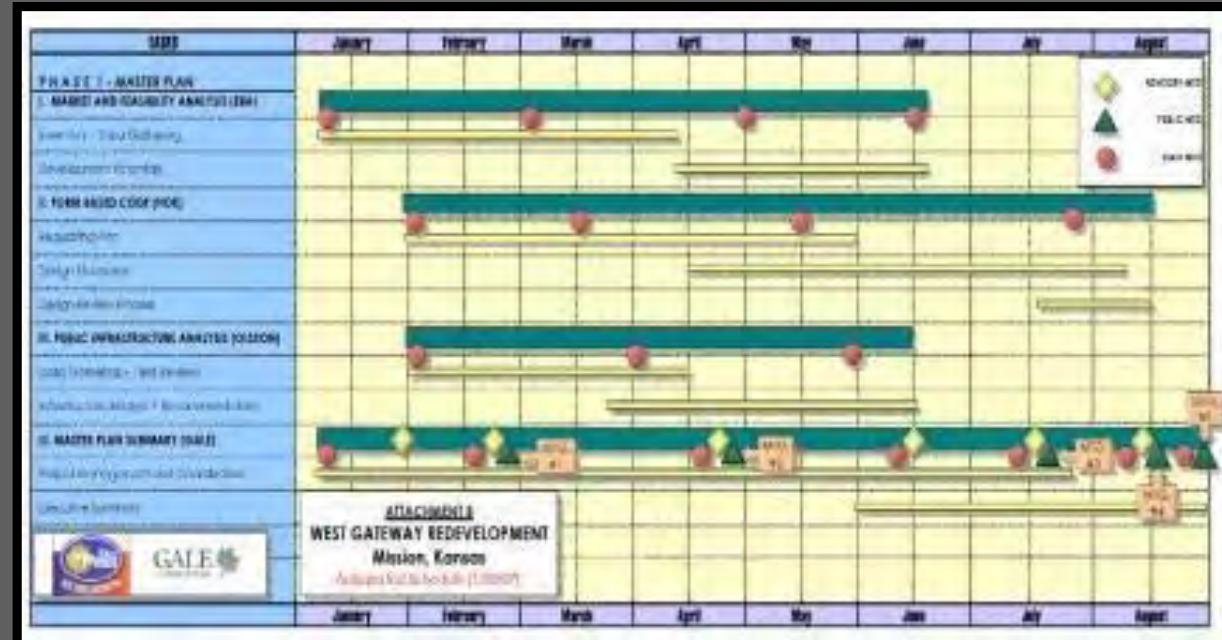
- 3 public meetings held

- OPEN HOUSE FORUMS

- 2 sessions held (March and July)

- PLANNING COMMISSION + CITY COUNCIL MEET

- Work Session
- Public Hearings
- Adoption (October 2007)



WEST GATEWAY

REDEVELOPMENT AREA

FORM BASED CODE SUMMARY PRESENTATION



GOALS

- ENGAGE THE COMMUNITY
 - Create a “Form-Based Code” consistent with the Vision Plan
 - Successfully guide all parties through the planning process
- STAY “ONE STEP AHEAD”
 - Pro-active approach to attracting and guiding redevelopment opportunities
 - Capitalize on superior location – develop a framework that leads to success
- MAKE THE VISION A REALITY
 - Provide guidelines for design flexibility and long-term sustainability
 - Attract high-quality mixed-use projects that benefit the entire community

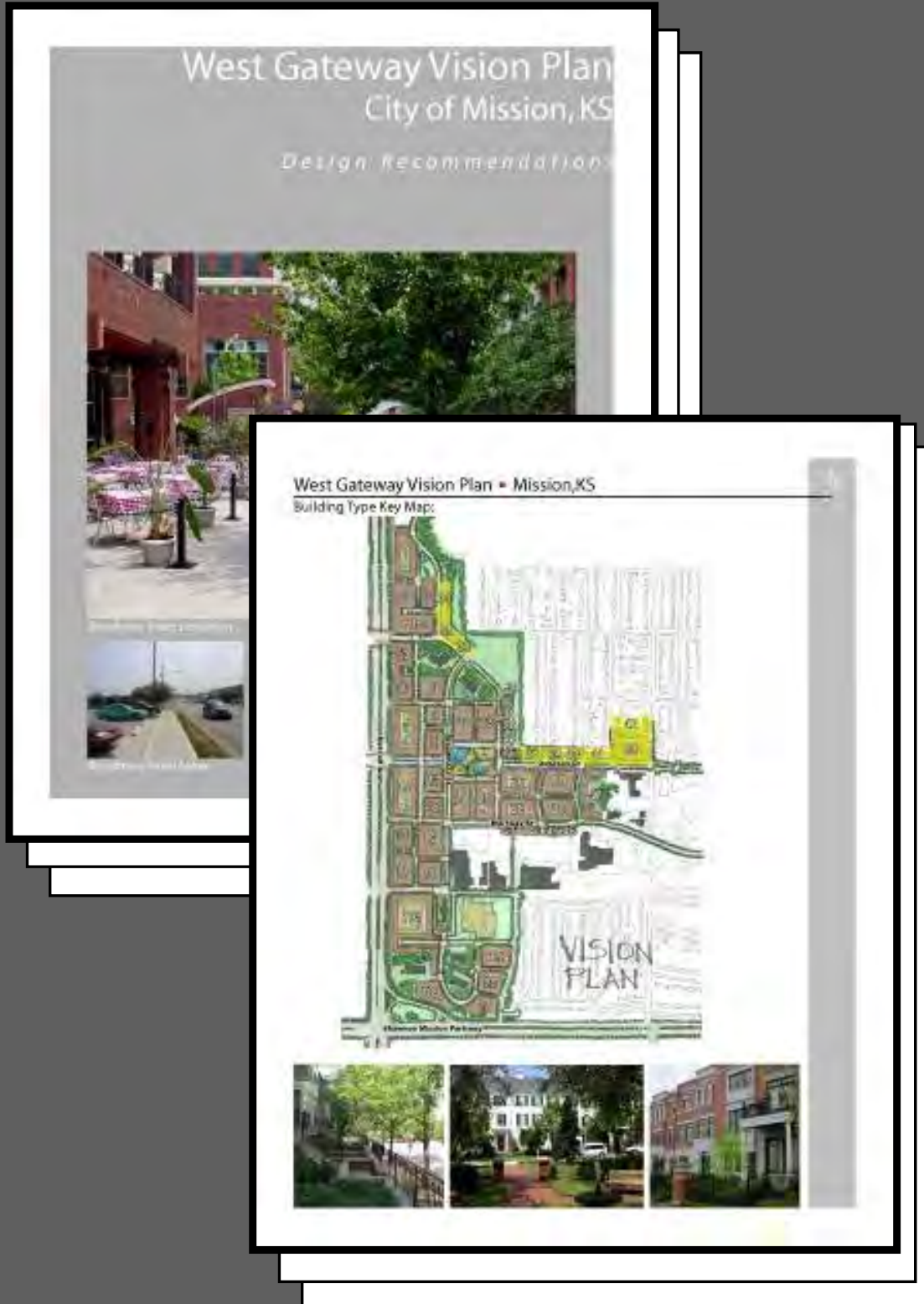


WEST GATEWAY REDEVELOPMENT AREA

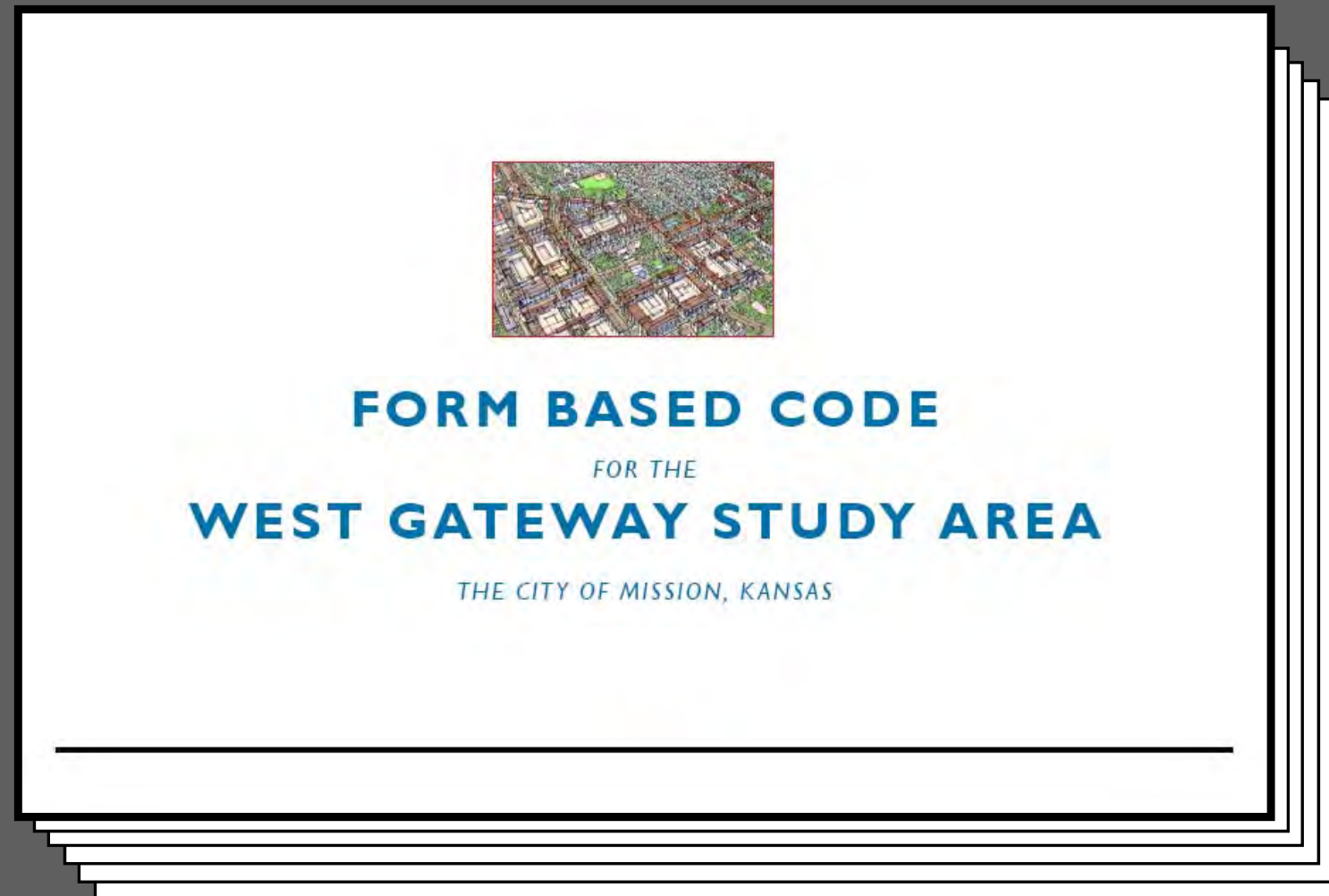
FORM BASED CODE
SUMMARY PRESENTATION



VISION PLAN



FORM BASED CODE



Encouraging "GOOD" projects by making them "EASIER"



WEST GATEWAY REDEVELOPMENT AREA

FORM BASED CODE SUMMARY PRESENTATION





Will the Infrastructure Serve It?

Review Included:

- Sanitary Sewers
- Water Lines
- Gas + Electric Lines



MAJOR FINDINGS:

- Each utility is capable of serving the area
- Some improvements needed - detailed report
- Recommendations were incorporated into the City's CIP

**WEST
GATEWAY**
REDEVELOPMENT AREA

FORM BASED CODE
SUMMARY PRESENTATION



Can the Market Support It?

- **Scope of Analysis:**
 - Comprehensive review of existing market conditions
 - Anticipate future market demand
 - Test feasibility of various buildings and development scenarios
 - Identify opportunities for appropriate public incentives
- **Feasibility + Projections:**
 - The Form Based Code is a realistic regulatory control
 - Mission is well-positioned for future growth



WEST GATEWAY REDEVELOPMENT AREA

FORM BASED CODE
SUMMARY PRESENTATION



Is the Code Flexible?

1. Residents

- Protects single family housing with building massing controls and linear parks

2. City

- Promotes desired redevelopment
- Overlay district
- No rezoning needed

3. Existing Businesses

- Allows maintenance of non-conforming building stock

4. Developers

- Considers transitional redevelopment with limited suburban construction (i.e. drive-thrus, 1-story buildings, surface parking)
- Avoids protest petition process



WEST GATEWAY

REDEVELOPMENT AREA

FORM BASED CODE
SUMMARY PRESENTATION



Is the Code Flexible?

1. Allows for Variety of Building Types + Uses
2. Architectural + Site Design Opportunities
3. Development Phasing Possibilities



WEST GATEWAY

REDEVELOPMENT AREA

FORM BASED CODE
SUMMARY PRESENTATION

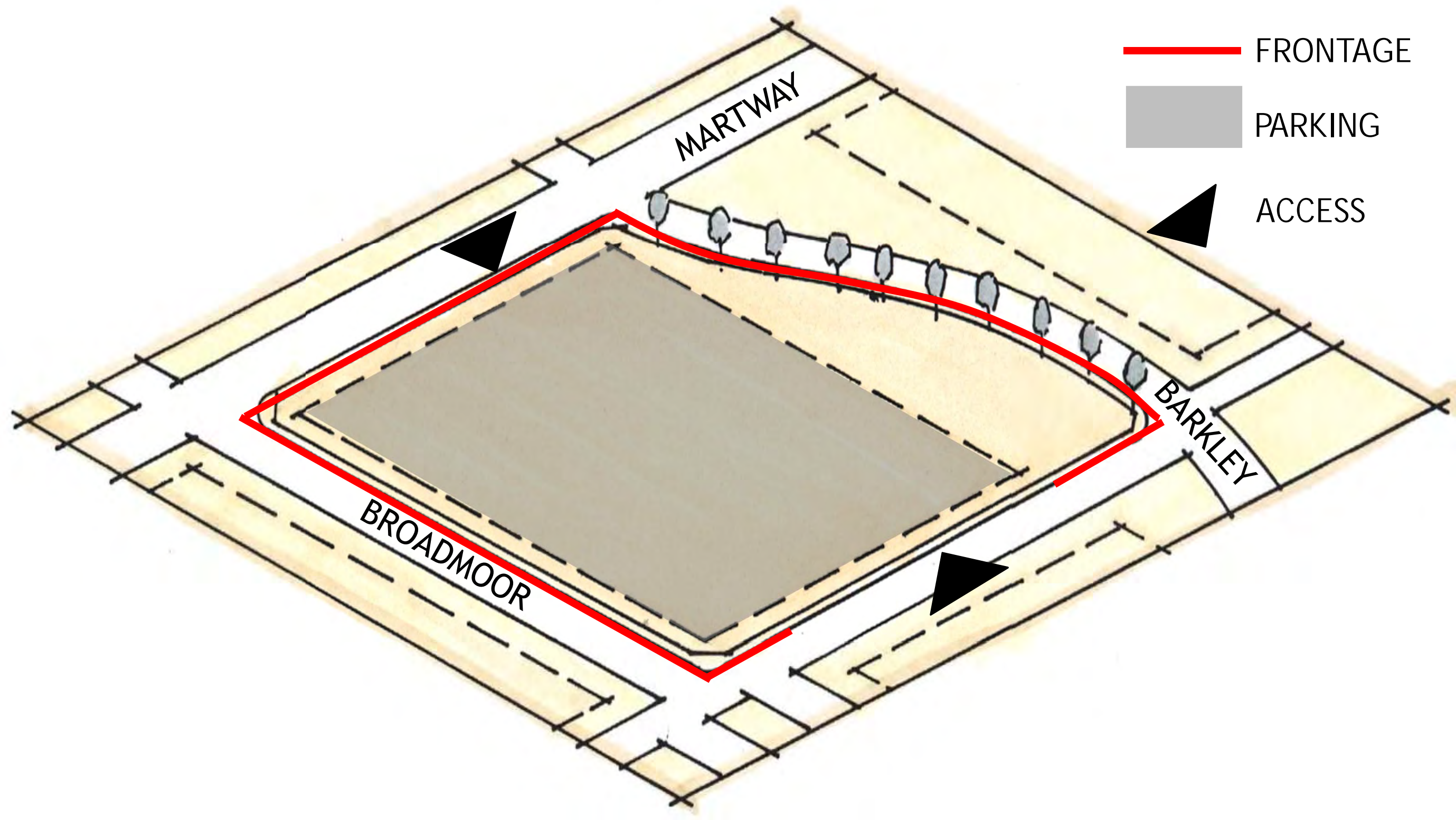


DEVELOPMENT PHASING POSSIBILITIES



WEST GATEWAY REDEVELOPMENT AREA

FORM BASED CODE
SUMMARY PRESENTATION



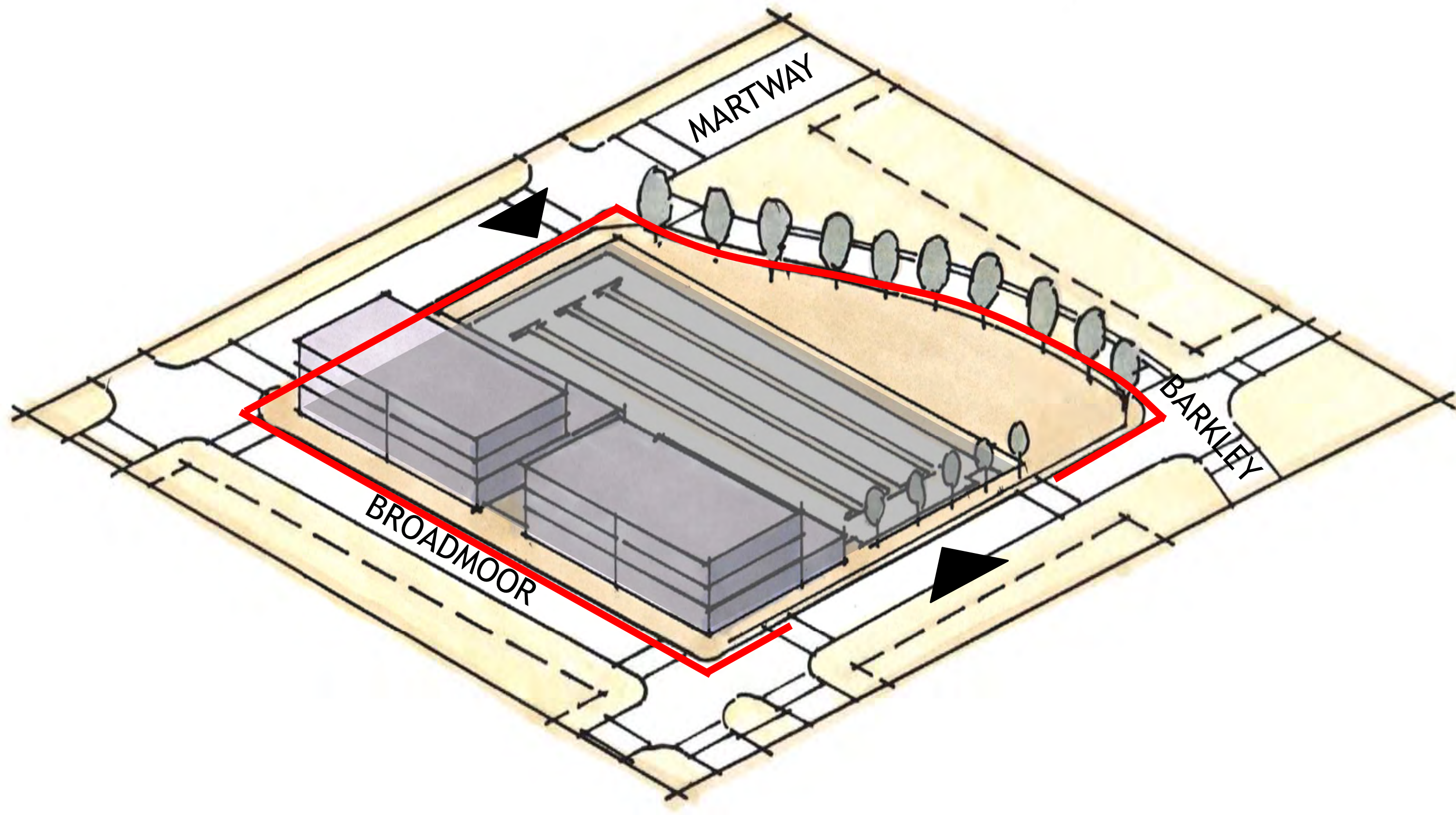
Example Block

DEVELOPMENT PHASING POSSIBILITIES



WEST GATEWAY REDEVELOPMENT AREA

FORM BASED CODE
SUMMARY PRESENTATION



Scenario 1

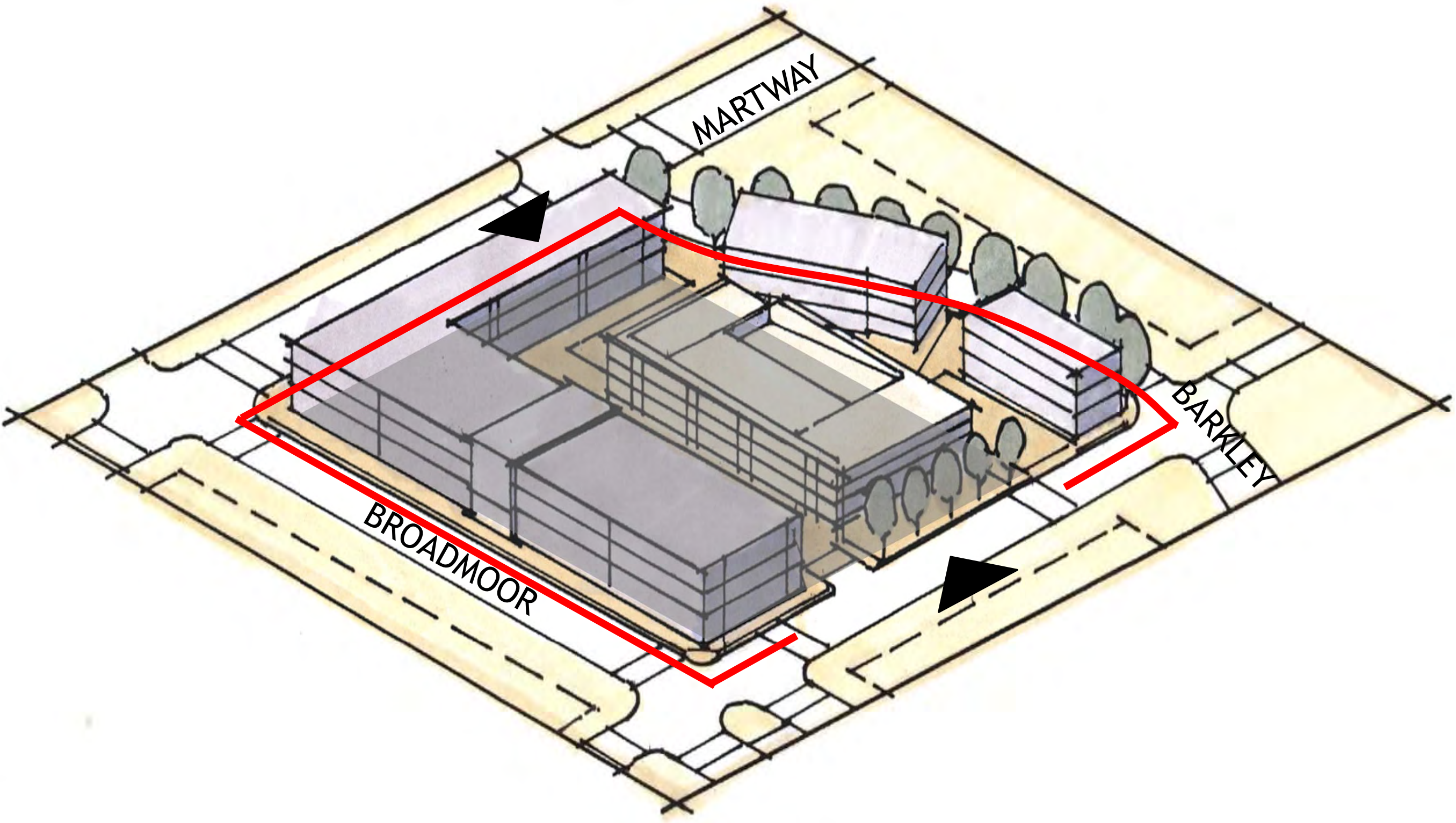


DEVELOPMENT PHASING POSSIBILITIES



WEST GATEWAY REDEVELOPMENT AREA

FORM BASED CODE
SUMMARY PRESENTATION



Scenario 2



USING THE FORM BASED CODE

Elements of the Code

90 Points = Passing Score

1. Sector Plan / Regulating Plan
45 Points Possible / 45 Points Required
2. Building Types
10 Points Possible / 10 Points Required
3. Urban Guidelines
30 Points Possible / 25 Points Required
4. Architectural Guidelines
15 Points Possible / 10 Points Required



**WEST
GATEWAY**
REDEVELOPMENT AREA

**FORM BASED CODE
SUMMARY PRESENTATION**



USING THE FORM BASED CODE



WEST GATEWAY REDEVELOPMENT AREA

FORM BASED CODE SUMMARY PRESENTATION

3 LOOK UP BUILDING TYPES IN BUILDING TYPES MATRIX

Resource: Building Types (Chapter 3)
 Building Types List: **Townhouse, Mid-Rise, High-Rise**

4 REVIEW URBAN + ARCHITECTURAL GUIDELINES FOR PERMITTED BUILDING TYPE

Resource: Urban Guidelines (Chapter 4) & Architectural Guidelines (Chapter 5)

BUILDING TYPE	FR
TOWNHOUSE	
LOW-RISE	
MID-RISE	

GALE COMMUNITIES theHOKPlanningGroup
FINAL REVISION

MISSION WEST GATEWAY FORM BASED CODE

USING THE FORM BASED CODE

The following pages identify the procedure to be followed when using the form based code

1 IDENTIFY PROPERTY LOCATION & NOTE ITS SECTOR

Resource: Sector Plan Map (Chapter 2, Page 1)
 Sector List: **Broadmoor, Westgate, Johnson Corridor, Rock Creek, or Martway Corridor**



SECTOR AND REGULATING PLAN
 CHAPTER 2: PAGE 1

2 DETERMINE BLOCK & NOTE PERMITTED BUILDING TYPES

Resource: Sector and Regulating Plan (Chapter 2)
 Block List: **G**
 Building Types List: **Townhouse, Mid-Rise, High-Rise**

MISSION WEST GATEWAY FORM BASED CODE

BLOCK F

- Higher density and intensity of uses are permitted on parcels located in this block.
- Higher residential density is permitted on parcels located in this block.
- Ground-level retail uses are encouraged on the site.
- The building massing along Broadmoor is intended to create a vertical edge to the street.
- 20' minimum front setbacks are required along the street frontage.
- Building type appropriate for the block is:

MID-RISE, HIGH-RISE, LINER

BLOCK G

- The block is located in higher density (residential) zoning.
- Commercial and business uses are permitted on parcels located in this block.
- The building massing along Broadmoor is intended to create a vertical edge to the street.
- Ground-level retail uses are encouraged on the street frontage.
- Building type appropriate for the block is:

TOWNHOUSE, MID-RISE, HIGH-RISE, LINER

BLOCK H

- Higher density and intensity of uses are permitted on parcels located in this block.
- Higher residential density is permitted on parcels located in this block.
- Ground-level retail uses are encouraged on the site.
- The building massing along Broadmoor is intended to create a vertical edge to the street.
- 20' minimum front setbacks are required along the street frontage.

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ROCK CREEK SECTOR

SECTOR AND REGULATING PLAN
 CHAPTER 2: PAGE 5



USING THE FORM BASED CODE

1 IDENTIFY PROPERTY LOCATION & NOTE ITS SECTOR

Resource: Sector Plan Map (Chapter 2, Page 1)
Sector List: Broadmoor, Westgate, Johnson Corridor, Rock Creek, or Martway Corridor.



SECTOR AND REGULATING PLAN
CHAPTER 2: PAGE 1



WEST GATEWAY REDEVELOPMENT AREA

FORM BASED CODE SUMMARY PRESENTATION



USING THE FORM BASED CODE



WEST GATEWAY REDEVELOPMENT AREA

FORM BASED CODE SUMMARY PRESENTATION

2 DETERMINE BLOCK & NOTE PERMITTED BUILDING TYPES

Resource: Sector and Regulating Plan (Chapter 2)
Block List: G
Building Types List: **Townhouse, Mid-Rise, High-Rise**

MISSION WEST GATEWAY FORM BASED CODE

ROCK CREEK SECTOR

BLOCK F

- Higher building and density is permitted in this block.
- Higher development allows for increased density in this block.
- Grouped multi-unit residential development is encouraged.
- Mid-rise and high-rise development is encouraged.
- Mid-rise and high-rise development is encouraged.
- Mid-rise and high-rise development is encouraged.
- Mid-rise and high-rise development is encouraged.

MID-RISE, HIGH-RISE LINER

BLOCK G

- The block is designed to accommodate parking structures in the middle of the block.
- Commercial and residential development is encouraged.
- The permitted multi-unit residential development is encouraged.
- Grouped multi-unit residential development is encouraged.
- Mid-rise and high-rise development is encouraged.
- Mid-rise and high-rise development is encouraged.
- Mid-rise and high-rise development is encouraged.

TOWNHOUSE, MID-RISE, HIGH-RISE, LINER

BLOCK H

- Higher building and density is permitted in this block.
- Higher development allows for increased density in this block.
- Grouped multi-unit residential development is encouraged.
- Mid-rise and high-rise development is encouraged.
- Mid-rise and high-rise development is encouraged.
- Mid-rise and high-rise development is encouraged.

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SECTOR AND REGULATING PLAN
CHAPTER 2: PAGE 5



USING THE FORM BASED CODE



4 REVIEW URBAN + ARCHITECTURAL GUIDELINES FOR PERMITTED BUILDING TYPE

Resource: Urban Guidelines (Chapter 4) & Architectural Guidelines (Chapter 5)

MISSION WEST GATEWAY FORM BASED CODE

TOWNHOUSE (OR ROWHOUSE)

Front setback: 5' - 10'

Side Street Setback: 5' - 20'

ARCHITECTURAL GUIDELINES

MATERIALS	CONFIGURATIONS	TECHNIQUES	BUILDING WALLS
<ul style="list-style-type: none"> • Solid wall of masonry • Brick • Stone • Concrete • Stucco • Wood • Metal • Glass 	<ul style="list-style-type: none"> • All buildings shall have a solid wall of masonry, brick, stone, concrete, stucco, wood, metal, or glass. • One building wall shall contain a door, window, or other architectural feature. • Buildings shall have a minimum height of 10 feet. • Buildings shall have a maximum height of 15 feet. • Buildings shall have a minimum lot coverage of 20%. • Buildings shall have a maximum lot coverage of 80%. • Buildings shall have a minimum front setback of 5 feet. • Buildings shall have a maximum front setback of 10 feet. • Buildings shall have a minimum side setback of 5 feet. • Buildings shall have a maximum side setback of 10 feet. • Buildings shall have a minimum rear setback of 5 feet. • Buildings shall have a maximum rear setback of 10 feet. 	<ul style="list-style-type: none"> • Buildings shall have a minimum height of 10 feet. • Buildings shall have a maximum height of 15 feet. • Buildings shall have a minimum lot coverage of 20%. • Buildings shall have a maximum lot coverage of 80%. • Buildings shall have a minimum front setback of 5 feet. • Buildings shall have a maximum front setback of 10 feet. • Buildings shall have a minimum side setback of 5 feet. • Buildings shall have a maximum side setback of 10 feet. • Buildings shall have a minimum rear setback of 5 feet. • Buildings shall have a maximum rear setback of 10 feet. 	<p>BUILDING WALLS</p> <p>INTENT</p> <p>Building walls shall be solid, masonry, brick, stone, concrete, stucco, wood, metal, or glass. Building walls shall be a minimum of 10 feet high and a maximum of 15 feet high. Building walls shall be a minimum of 20% lot coverage and a maximum of 80% lot coverage.</p>
<p>SIMPLE ENTRANCE</p>	<p>WALL FRONT ENTRANCE</p>	<p>MULTIPLE WALL ENTRANCE</p>	

WEST GATEWAY REDEVELOPMENT AREA

FORM BASED CODE SUMMARY PRESENTATION



USING THE FORM BASED CODE

LANDSCAPE GUIDELINES - THE RULES FOR THE PUBLIC REALM



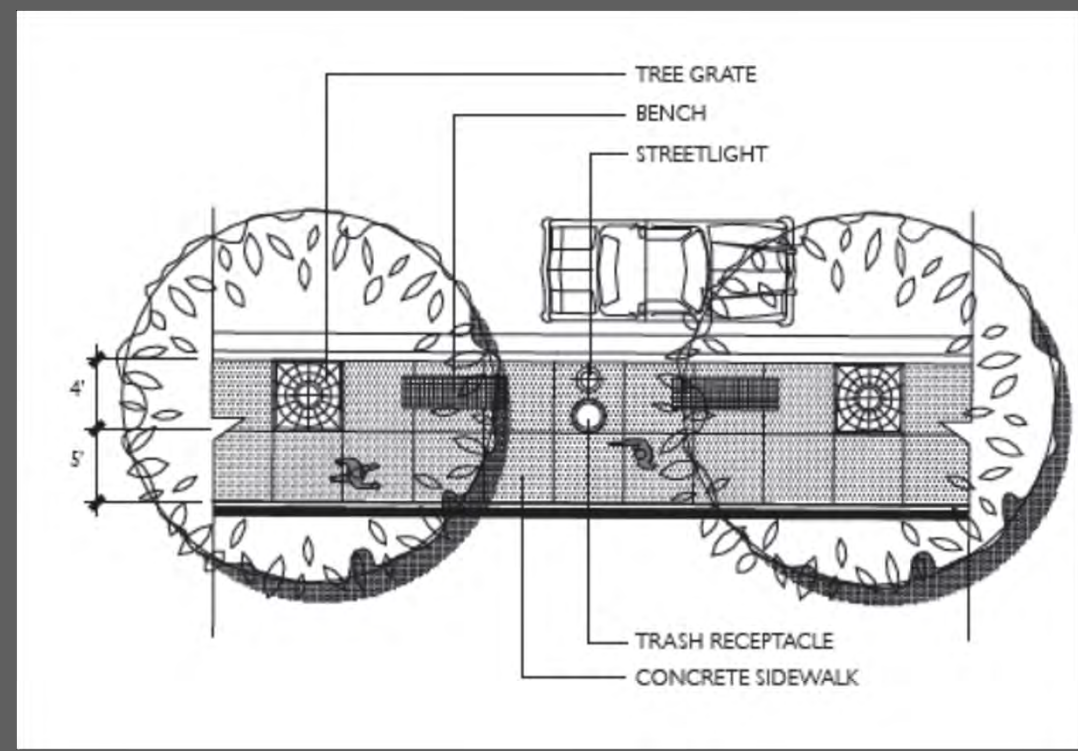
WEST GATEWAY

REDEVELOPMENT AREA

FORM BASED CODE SUMMARY PRESENTATION



+ TYPICAL PARKING LOT PLANT SCREEN



+ CREATIVE PIPE: INVERTED "U" BIKE RACK
 IJ-20, flanged surface mount, black polyester powder coat finish



+ NEENAH FOUNDRY COMPANY: TREE GRATE #R-8726
 48" square with 12" diameter expandable tree opening

MISSION WEST GATEWAY FORM BASED CODE

RIGHT-OF-WAY	56'
PAVEMENT	36'
TRAVEL LANES	2
PARKING	BOTH SIDES
SIDEWALK	6'
PLANTER TYPE	FTS
TREES	40' D.C. AVG.

STREET LAYOUT GUIDELINES
 TYPE 70 - BARKLEY STREET FRONT

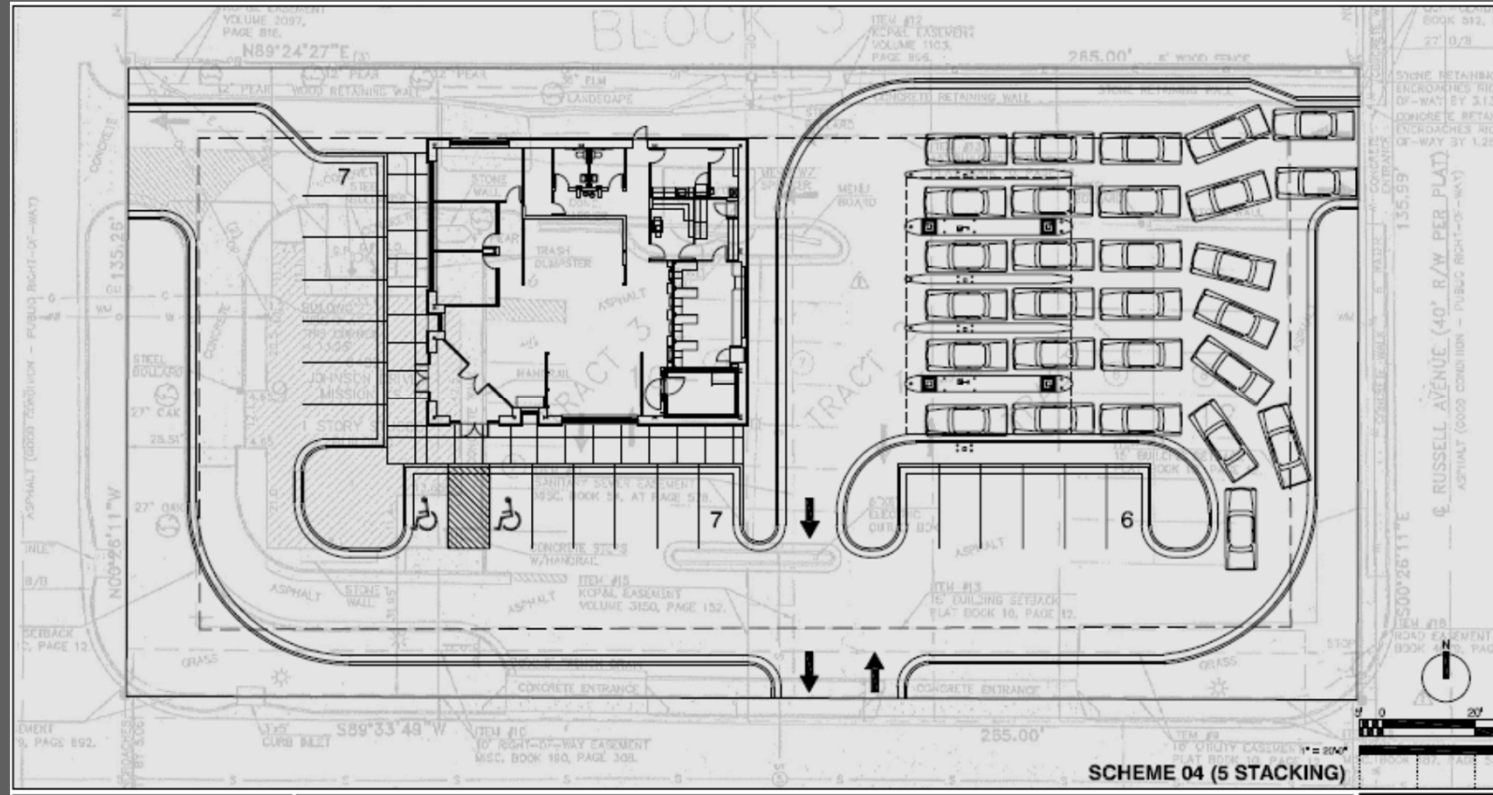
Section between 5th and Johnson Drive looking north

GALE | theHOKPlanningGroup
 FINAL REVISION - 08.07.07

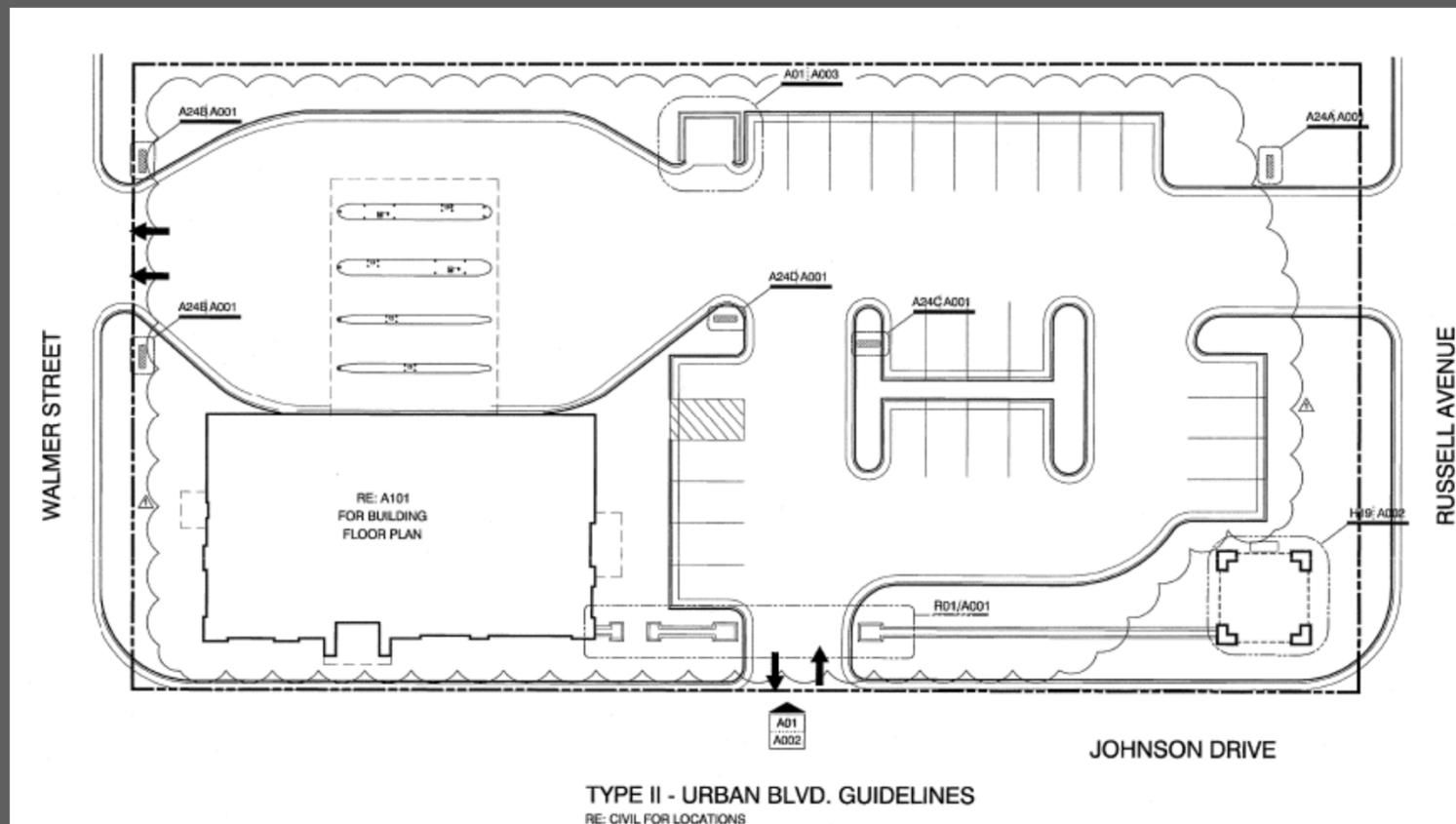
STREET LAYOUT GUIDELINES
 CHAPTER 7 - PAGE 8



Examples



Initial Plan - Conventional 1-story building, building set back from street



FBC Plan - 2-story building, at build-to line, pedestrian realm improvements



WEST GATEWAY REDEVELOPMENT AREA

FORM BASED CODE SUMMARY PRESENTATION



Mission Crossing



WEST GATEWAY

REDEVELOPMENT AREA

FORM BASED CODE
SUMMARY PRESENTATION



Mission Crossing



WEST GATEWAY

REDEVELOPMENT AREA

FORM BASED CODE
SUMMARY PRESENTATION



Mission Crossing



WEST GATEWAY

REDEVELOPMENT AREA

FORM BASED CODE
SUMMARY PRESENTATION



Mission Crossing



WEST GATEWAY

REDEVELOPMENT AREA

FORM BASED CODE
SUMMARY PRESENTATION



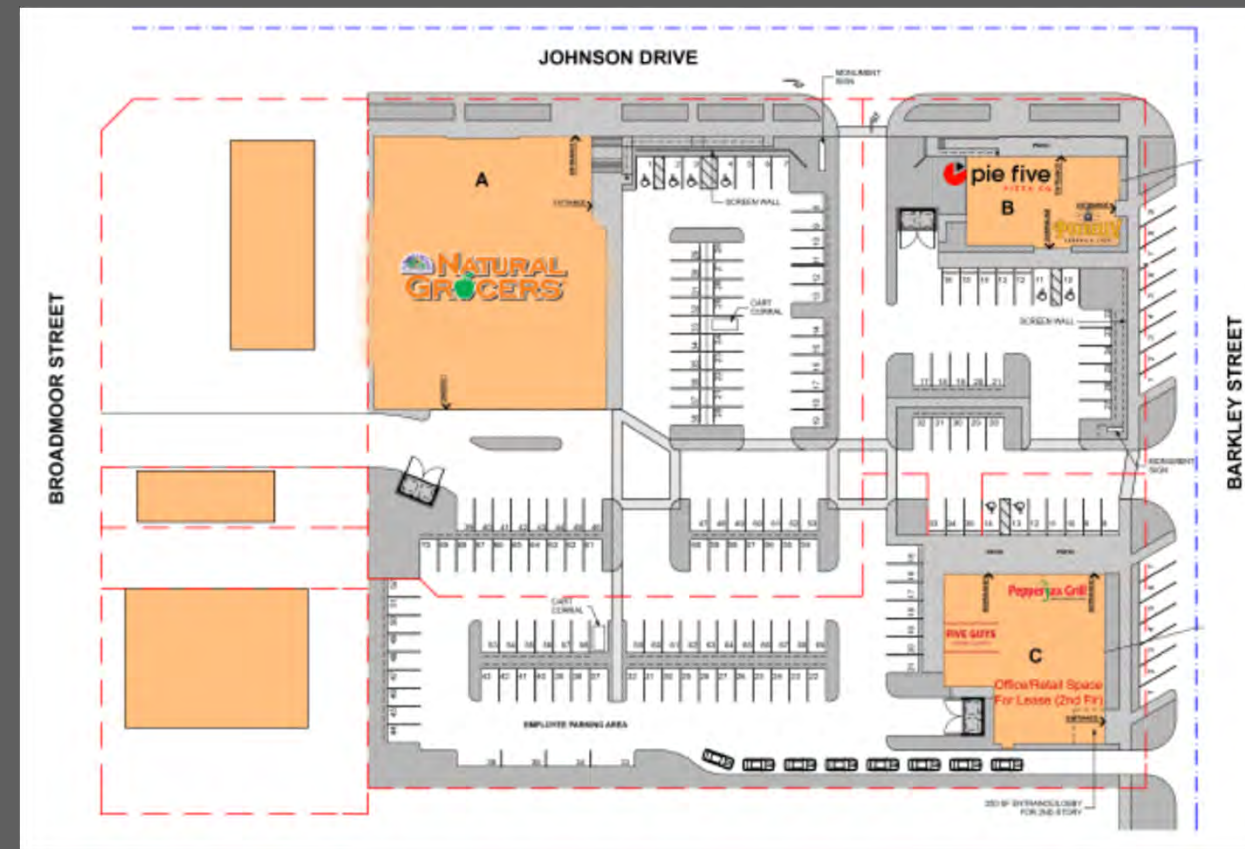
Mission Crossing



WEST GATEWAY REDEVELOPMENT AREA

FORM BASED CODE
SUMMARY PRESENTATION





Cornerstone Commons

Building Placement – All structures relate to and connect with adjacent streets

Pedestrian Realm – Streetscape improvements are provided on all adjacent public streets, including landscape plantings, site furnishings, lighting, etc.



WEST GATEWAY

REDEVELOPMENT AREA

FORM BASED CODE
SUMMARY PRESENTATION





WEST GATEWAY

REDEVELOPMENT AREA

FORM BASED CODE SUMMARY PRESENTATION

April 4, 2018

FORM BASED CODE
SUMMARY PRESENTATION





February 27, 2018

Ms. Danielle Sitzman, AICP
City Planner
City of Mission
6090 Woodson
Mission, KS 66202

RE: Tidal Wave Auto Spa - Development Plan Submittal

Dear Danielle,

As master developer of the West Gateway Redevelopment District for the City of Mission, it is our responsibility to conduct a review of development plans and applications within this area, and to provide comments relative to their compliance with the recommendations outlined in the City's Form Based Code (FBC).

It is our understanding that this applicant, Mr. Steve Block and/or TW Macon LLC, wishes to obtain approval for an amended and supplemented preliminary site development plan submittal for a ~0.76 acre site located on the southwest of the intersection of Walmer Street and Johnson Drive. As indicated in the submitted plans, the proposed improvements are intended to remove an existing single-use building with a large surface parking lot and redevelop the site to incorporate a proposed drive-through auto spa, a structure containing vehicle vacuum bays and commercial offices, and related site improvements.

Our review of the plans is provided below, including our interpretation and recommendations regarding their conformance with specific requirements outlined in the FBC. Since the site is larger than 0.50 acres, the FBC allows for up to 60% of the gross square feet of development to utilize low-rise building(s) in conjunction with at least 40% of other identified building types (in this case Townhouse, Mid-Rise, High-Rise, or Parking Structure).

This application includes two proposed structures - neither of which are consistent with the building types identified in the FBC. The smaller of the two structures (the car wash) does not meet the identified height requirement for a low-rise building type. The larger of the two structures (the vacuum bays with office space above) does not meet any of the building types identified in the FBC. Due to the ground floor of this structure proposed primarily for vacuum bay parking with no doors or full enclosure, we interpreted the closest FBC building type for comparative purposes to be a Parking Structure - and have used this for the analysis outlined herein.

Please note the FBC regulating plan also calls for Walmer Street to be extended south from the existing Johnson Drive intersection. If the dedicated street extension and related right-of-way were to have been incorporated, the remaining development site area could be less than 0.50 acres and would then qualify for use of a low-rise building as part of an alternative redevelopment scenario. However; this application does not include the proposed extension of Walmer Street as identified in the FBC.

That said, the City has requested our review of this application be provided under a scenario wherein Walmer Street is interpreted to not be designated for extension to the south as part of the FBC requirements. Our review is provided with this understanding.

These comments are provided in accordance with the “steps” outlined in Chapter 8 of the FBC, including the FBC scoring system.

Step 1 - Regulating Plan (45 Points Required to Pass, 45 Possible Points)

- *Block Configuration and Frontage Type:* In our opinion, the proposed development generally respects the hierarchy of frontage types identified in the FBC, with the primary (taller) of the two proposed structures placed along the existing Johnson Drive frontage. The regulating plan identifies building types appropriate for this block as Townhouse, Mid-Rise, High-Rise, and Parking Structure; however, this taller structure does not meet the definition of any of these appropriate building types. There is also no indication of retail uses proposed for the ground floor of this structure along Johnson Drive as required in the regulating plan.

The FBC also allows for the use of Low-Rise buildings in certain circumstances, and these buildings are required to be 26' in height. The smaller one-story auto spa structure located on the south portion of the site does not meet the 26' height requirement, and therefore does not meet the definition of the Low-Rise building type.

For these reasons, the full score for this portion of the plan review has not been achieved. This can be rectified by the applicant revising the design of these structures to comply with FBC building type requirements and continuing to place the tallest structure along the Johnson Drive frontage.

Score: 7/15 points

Parking: In our opinion, the proposed development generally meets the conditions identified in the FBC for placement of surface parking areas towards the interior of this development site; however, other parking proposed within a structure conflicts with provisions of the regulating plan. The taller structure proposed along Johnson Drive indicates vehicular parking in the ground floor space, with access provided from the interior of the site on the south side of this structure.

The use of the ground floor space of this structure for vehicular parking is not consistent with a mid-rise building type, and does not allow adequate space for retail uses along Johnson Drive as required in the FBC regulating plan. Because this building has parking included on the ground floor, it is interpreted to be reviewed as a Parking Structure building type; however, the size and configuration of this structure also doesn't meet the definition of a Parking Structure building type.

For these reasons, the full score for this portion of the plan review has not been achieved. This can be rectified by the applicant revising the design of the taller structure to comply with the FBC building type requirements.

Score: 7/15 points

Access: In our opinion, the proposed development adequately addresses site access issues identified in the FBC. Access to the site is provided near the

Walmer Street and Johnson Drive intersection with the proposed removal of a portion of the existing Johnson Drive median island. There are no provisions for a driver to be able to exit the line or to access parking for the proposed office space without first entering through the pay kiosk. A "right-out only" egress drive connection to Johnson Drive is also provided on the west edge of the site.

The applicant has provided an auto turn template using a passenger car to illustrate the ability to enter the site through the pay kiosk and exit to Johnson Drive on the west side of the site. However; it is unclear if trash and/or other service vehicles are anticipated to access the site using this entry drive through the pay kiosk, or whether they are anticipated to access the site through the single exit lane (west side) connecting to Johnson Drive. If they are anticipated to use this exit lane, it should be considered for widening to accommodate two-way traffic.

For these reasons, the full score for this portion of the plan review has not been achieved. This can be rectified by the applicant providing additional information regarding the ability of relevant vehicle types to access the interior portions of the site either through the pay kiosk lane or via alternative access points.

Score: 14/15 points

Total Score: 28/45 Points (NOT PASSED - 45 Points Required)

Note: According to Chapter 8 of the FBC, development submittals must comply with the requirements outlined in the regulating plan, and a score of 45 is required to automatically continue to the next review group in the process. At this point in the review process, the Community Development Department was notified of this issue/score, and it was determined that due to the nature and complexity of the overall project, the review should continue to be provided for the remaining steps of the FBC.

Step 2 - Building Types (10 Points Required to Pass, 10 Possible Points)

- *Building Type Matches Regulating Plan Text:* The taller structure is interpreted to be considered as a Parking Structure building type, but there is no ground floor commercial retail use along the Johnson Drive frontage as required in the FBC. The proposed height of the one-story structure (located along the south edge of the site) does not meet the minimum 26' height requirement to qualify for consideration as a Low-Rise building type.

For these reasons, the full score for this portion of the plan review has not been achieved. This can be rectified by the applicant revising the design of these structures to comply with FBC building type requirements outlined in the regulating plan text. Two example scenario concepts are provided below.

One concept could include revising the taller structure to meet the Parking Structure building type by expanding the width/depth of the habitable portion of the building to a minimum of 40' along the entire length of the building and 20' tall to accommodate a variety of uses, while including the proposed covered vehicle parking vacuum bays along the rear of the building with commercial, office, or parking spaces above in a second story.

Another concept could include be revising the taller structure to meet the Mid-Rise building type by expanding the width/depth of the building to a minimum of 40' along the entire length of the two-story building with habitable space on both levels, and an option to place the vehicular parking vacuum bays either behind the building or in a basement below a portion of the rear of this building by sloping the site grading for vehicular access to this basement condition.

These and/or other scenario concepts could be explored further in collaboration with the applicant to address specific programmatic and development requirements associated with these or other alternative approaches.

Score: 2/5 points

- *Building Type Matches Chapter 3 Definition:* The proposed taller structure proposed for the northern edge of the site does not contain the ground floor commercial retail along the Johnson Drive frontage as required in the FBC Parking Structure building type definition. The proposed smaller auto spa building structure on the southern edge of the site does not meet the minimum 26' height requirement to qualify for consideration as a Low-Rise building type.

For these reasons, the full score for this portion of the plan review has not been achieved. This can be rectified by the applicant revising the design of these structures to comply with FBC building type requirements as previously outlined herein. The auto spa structure needs to be increased in height to meet the 26' height requirement to qualify as a Low-Rise building type.

Score: 2/5 points

Total Score: 4/10 Points (NOT PASSED - 10 Points Required)

Step 3 - Urban Guidelines (25 Points Required to Pass, 30 Possible Points)

Intent: The development plan submitted by the applicant does not provide clearly outlined proposed building type designations for proposed structures. For a site this size, the FBC outlines "Low-rise buildings, built to low-rise building guidelines, may be developed as up to 60% of the gross square feet of development (in one or multiple buildings) ..." While the applicant has provided square footages of the proposed buildings, they do not meet the building type criteria as outlined below.

The proposed height of the one-story structure (located along the south edge of the site) does not meet the minimum 26' height requirement to qualify for consideration as a Low-Rise building type, and does not meet the intent.

The taller structure is interpreted to be considered as a Parking Structure building type, but there is no ground floor commercial retail use along the Johnson Drive frontage as required in the FBC. Above-ground Parking Garage structures must also include an inhabitable 20' high ground floor with a minimum depth of 40' along the entire length of the garage to accommodate a variety of uses. The proposed taller structure does not provide these features and does not meet the intent of this building type.

For these reasons, these structures do not comply with the written intent for these building types, and the full score for this portion of the plan review has not been achieved. This can be rectified by the applicant revising the design of these structures to comply with the intent of the FBC building type requirements.

Score: 1/5 points

- *Front Setback - Johnson Drive:* As the proposed taller structure is considered to be a Parking Garage building type for the purposes of this submittal, it appears the front setback on Johnson Drive has not been provided appropriately. Since the Walmer Street extension and its related secondary frontage line was removed from consideration as part of this review, the placement of the taller structure with an integrated Parking Structure along the primary Johnson Drive frontage was considered to be an acceptable approach. The front setback to secondary frontage lines is 40' minimum with the intent to require a 40' deep building use at street level adjacent to this corridor. This is not included on the submitted revised site plan.

In addition, the application has submitted some additional (yet very limited) information regarding proposed streetscape improvements along Johnson Drive including a few tree, light, bench, and trash receptacle locations - but these plans are still missing numerous notations, the types of lighting and site furnishings, and other details associated with providing the required quantity and configuration of streetscape improvements and related landscape plans that should be included for the front setback area along Johnson Drive. Street trees are required at 40' maximum spacing, and the plans indicate two trees - but three additional trees are required. One bench is shown, but two additional benches are required. One trash receptacle is shown, but one additional receptacle is required. Hanging baskets/planters are to be utilized with the lighting (2 per pole), but none appear to be indicated on the plans. Tree grates are to be utilized with the street trees, but none appear to be indicated on the plans. Three bike rack loops are required, but none appear to be indicated on the plans.

The sidewalk area is proposed to be 9' in width on the application, which matches the width as outlined in the FBC requirements.

Some additional information related to FBC lighting requirements for the Johnson Drive Corridor is provided below:

- *Street Lights:* use of the same street light fixtures utilized along the west side of Broadmoor south of Martway (Lumec or City approved equivalent) located on a taller single shared pole for use along Broadmoor at approximately 120' intervals.
- *Pedestrian Lights:* use of the same pedestrian light fixtures utilized along the west side of Broadmoor south of Martway (Lumec or City approved equivalent) located on shorter single poles along Broadmoor at approximately 60' intervals.
- Prior to ordering and installing any of the lighting or site furnishings associated with this project, additional design coordination with the City of Mission may be necessary to ensure the appropriate fixtures and furnishings are selected to be consistent with the City's final streetscape design recommendations and previous selections for use in the West Gateway area.

The development plan also includes references to landscape features and a proposed dumpster enclosure location in the southwest corner of the property, but no details or landscape plans have been provided for these items as part of the application.

For these reasons, the full score for this portion of the plan review has not been achieved. This can be rectified by the applicant revising the design of the taller structure along Johnson Drive to match one of the building types and placing it to correspond with the front setback outlined in the FBC, and also providing additional streetscape design, landscape, lighting and related Johnson Drive sidewalk area improvement details.

Score: 3/5 points

- *Side Street Setback:* Due to the Walmer Street extension being removed from consideration, there is no side street setback related to this proposal and the placement of structures appear to be generally acceptable.

Score: 5/5 points

- *Side Setback:* The locations of proposed structures along the east and west property lines generally meets the intent of the FBC relative to any side setback criteria, and thus complies. No landscape plans or details associated with proposed plantings along these areas have been submitted.

For this reason, the full score for this portion of the plan review has not been achieved. This can be rectified by the applicant providing appropriately detailed landscape plans.

Score: 4/5 points

- *Rear Setback:* The locations of proposed structures along the south property line generally meets the intent of the FBC relative to any rear setback criteria, and thus complies. No landscape plans or details associated with proposed plantings along these areas have been submitted.

For this reason, the full score for this portion of the plan review has not been achieved. This can be rectified by the applicant providing appropriately detailed landscape plans.

Score: 4/5 points

- *Parking Area:* The proposed development appears to meet the conditions identified in the FBC by placing the surface parking areas towards the interior of this development site. There are notations of landscape features and a trash enclosure located in the southwest area of the property, but no details associated with the proposed plantings and trash enclosure have been submitted. This enclosure needs to be architecturally compatible with the adjacent building's appearance and use of materials. There is also no provision currently for pedestrian sidewalks leading into the interior of the site and the proposed auto spa structure located in the southern portion of the site from the adjacent Johnson Drive corridor.

For these reasons, the full score for this portion of the plan review has not been achieved. This can be rectified by the applicant providing appropriately detailed landscape plans and a pedestrian sidewalk connecting to the auto spa structure.

Score: 3/5 points

Total Score: 20/30 Points (NOT PASSED - 25 Points Required)

Step 4 - Architectural Guidelines (10 Points Required to Pass, 15 Possible Points)

- *Intent:* The proposed architectural treatments generally comply with these requirements, and the revised plans provide indications of proposed architectural materials.

Due to the aforementioned one-story structure not meeting the required 26' building height to be considered a Low-Rise building type, the taller structure not meeting the definition of a Parking Structure building type, and the material comments outlined above, the full score for this portion of the plan review has not been achieved. This can be rectified by the applicant revising the design and providing additional details for these structures to comply with the intent of the FBC building type and architectural treatment requirements.
Score: 2.5/6 points

- *Materials:* It appears the initial materials indicated for use on the proposed taller structure and the one-story structure generally complies with these requirements. It appears the vast majority of façade material for the taller structure is proposed to be stucco/EIFS. The use of EIFS material in lower elevations of the building (within reach of people) can present some maintenance concerns, and more durable options could be explored for use on lower portions of the building while still complimenting the overall architectural appearance of the structure. The use of E.F.S. (similar to the proposed EIFS?) on the one-story structure presents similar durability concerns as noted above. There are also no materials or finishes specified for the trash enclosure or the concrete retaining wall, including whether there will be a railing installed atop this wall.

For these reasons, the full score for this portion of the plan review has not been achieved. This can be rectified by the applicant revising the elevations of these proposed structures, walls, and railings (if any are needed for safety) to reflect proposed materials and finishes, and by submitting samples of these materials for review.

Score: 2/3 points

- *Configuration:* It appears the proposed structures in some ways comply with these requirements; however, the aforementioned structure dimension and configuration changes associated with the taller structure are needed for it to be considered a Parking Garage building type, and the aforementioned structure height changes associated with the one-story structure are needed for it to be considered a Low-Rise building type. This will require substantial changes to the configuration of these structures - and additional review will be necessary at that time. Some additional observations on the application as submitted are noted below:

"Roofs":

- Provide calculations for the proposed cupola features to indicate they do not exceed 500 square feet in plan.

"Storefronts":

- Provide calculations indicating windows and doors of commercial establishments occupy no less than 60% of the total storefront, from sidewalk grade to a distance of 18' above the sidewalk grade.

The calculations provided appear to include the entire façade, and do not meet this 60% requirement.

"Awnings & Canopies":

- The use of awnings is permitted along the base of a building. Those proposed on any upper floors will be further considered in conjunction with any revised elevations that adequately address other issues outlined herein.

For the reasons stated above, the full score for this portion of the plan review has not been achieved. This can be rectified by the applicant revising the configuration of various components of the structures as outlined to comply with the FBC's configuration requirements.

Score: 1.5/3 points

- *Techniques:* It appears most of the structures generally comply with these requirements; however, the aforementioned structure dimension and configuration changes associated with the taller structure are needed in order for it to be considered a Parking Garage building type, and the aforementioned building height changes associated with the one-story structure are needed in order for it to be considered a Low-Rise building type. This will require substantial changes to the techniques used in the design of these structures - and additional review will be necessary at that time. Some additional observations on the application as submitted are noted below:

"Building Walls":

- Please indicate where any air conditioners and other utility elements are intended to be placed to eliminate their placement on a building wall facing the street, and indicate techniques proposed to properly screen them from public view.

For the reasons stated above, the full score for this portion of the plan review has not been achieved. This can be rectified by the applicant revising the design of these structures to reflect the comments outlined above to comply with the FBC's technique requirements.

Score: 2/3 points

Total Score: 8/15 Points (NOT PASSED - 10 Points Required)

The overall score for this proposal is 60 out of a possible of 100 points, with an overall total of 90 points required to pass. If you have any questions about these comments, please don't hesitate to contact me to review in further detail.

Best Regards,



Wm. Christopher Cline, ASLA
Core Design Development, LLC



City of Mission, KS
6501 Johnson Drive

Planning Commission

March 26, 2018

Preliminary Development Plan



Starbucks

Johnson Dr

Johnson Dr



Shawnee Missions
Horizon High School

MISSION WEST
Shopping Ctr

Lamar Ave





NOTHING BUTT CAKES

HONDA



Project History

- History of prior Use – Vacant for a year
- Site is in a zoning district that permits car wash facilities
- Form Based Code Objectives
- City's expression that a functional two-story building directly adjoining Johnson Drive was paramount to meeting the primary objectives of the West Gateway FBC
- 2-story renderings
- Traffic Study
- Amended Preliminary Development Plan submittal

COMPANY HISTORY

- Tidal Wave first began washing cars in 2004 in Atlanta, GA.
- Tidal Wave was selected by the United States Small Business Administration as the Small Business of the Year for Georgia in 2009.



COMPANY OBJECTIVES

- To be the most attractive business in the community.
- Provide the cleanest, greenest and easiest car cleaning service.
- Maintain each location in a clean and beautiful manner with superior customer service.



OPERATIONS

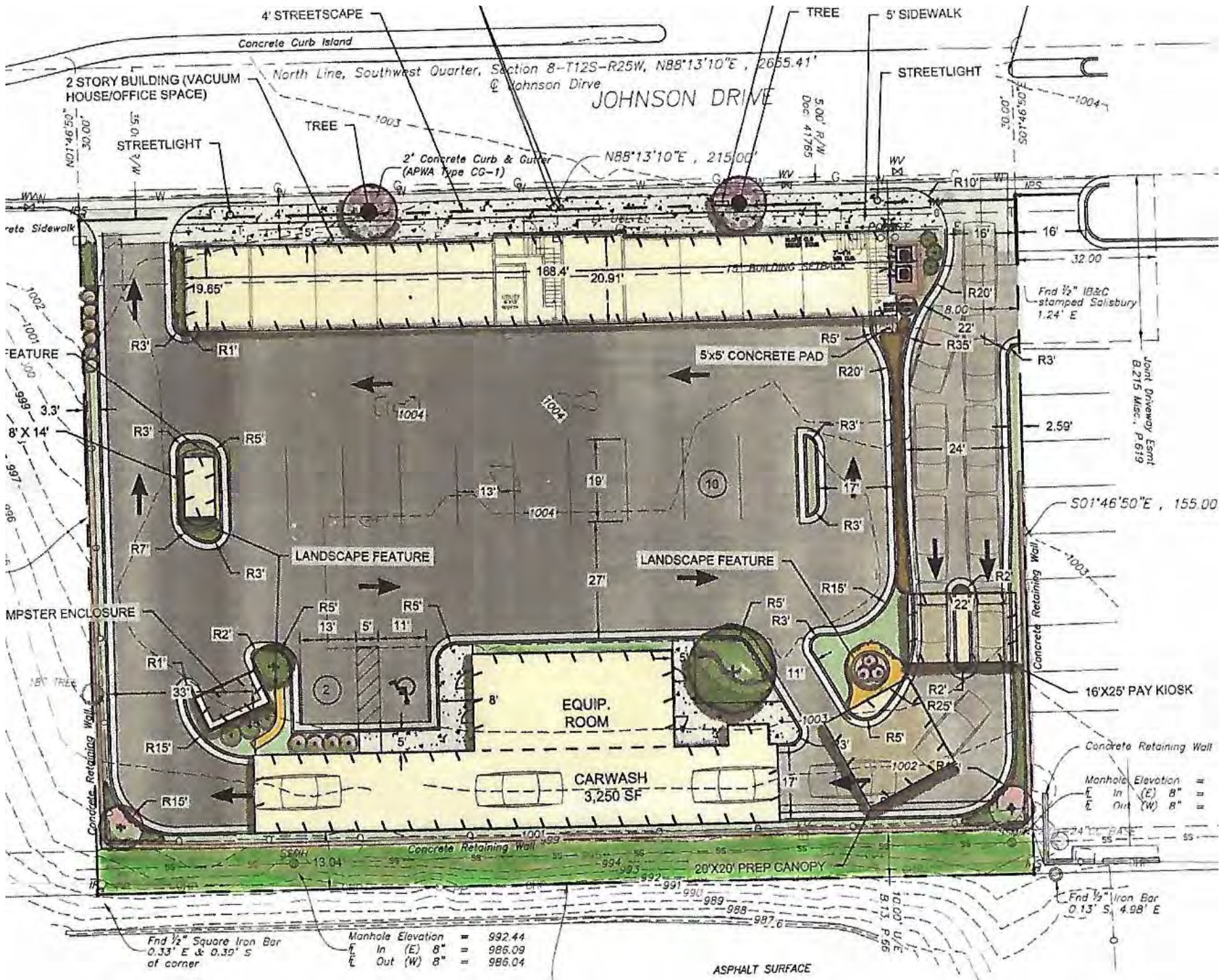
- Open seven days per week, 8 AM to 8 PM.
- Recycled water used at all locations.
- Water conservation promoted by reducing the amount of 'fresh' water needed to provide a quality exterior wash vs. car washes at home. 14-20 gallons vs. 80-140 gallons.
- Waste water filtered prior to entry into the sanitary sewer system.



COMMUNITY

- Tidal Wave partners with local schools, athletic teams and their booster clubs, service organizations, churches and other 501(c)(3) organizations.
- The third Friday of September is designated as 'Charity Day' with 100% of ALL proceeds donated to special needs charities.





Fnd 1/2" Square Iron Bar
0.33' E & 0.39' S
of corner

Manhole Elevation = 992.44
E In (E) 8" = 986.09
E Out (W) 8" = 986.04

ASPHALT SURFACE





Building Material List – Mid-Rise



VIEW FROM JOHNSON DRIVE

3850 SF WALL SURFACE EXCLUDING GABLES, ROOF, CUPOLA.
1800 S.F. GLAZING (WINDOWS AND SF ENTRY)
1750 S.F. EIFS OR HARD COAT STUCCO
300 S.F. SPLIT FACE BLOCK

TIDAL WAVE AUTO SPA

EXTERIOR FINISHES



EIFS/STUCCO
SW 9102 QUINOA



WATERTABLE
BASCO PRECAST PRODUCTS
COLOR - BUFF



METAL ROOFING
COLOR - KHAKI



WATERTABLE
SPLIT FACE BLOCK
OLD CASTLE

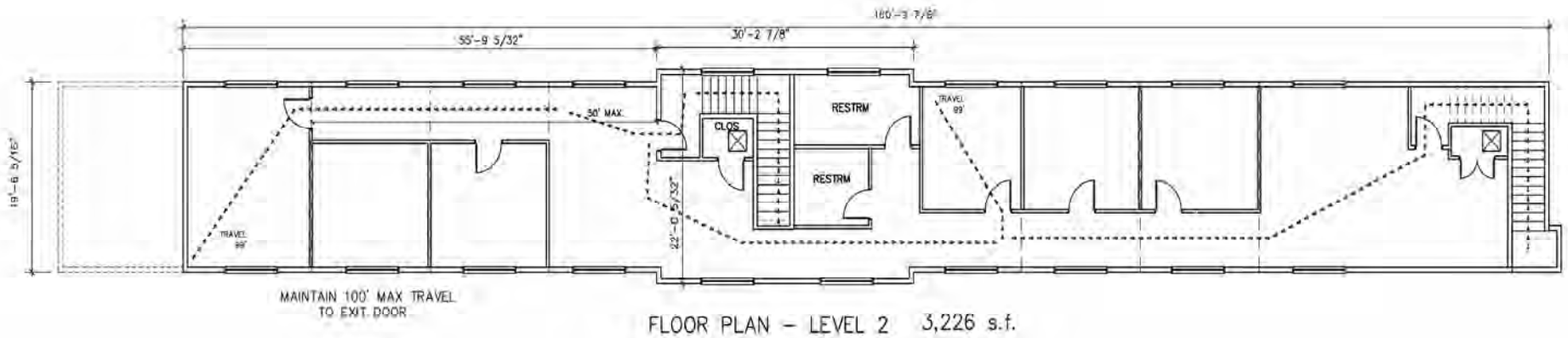
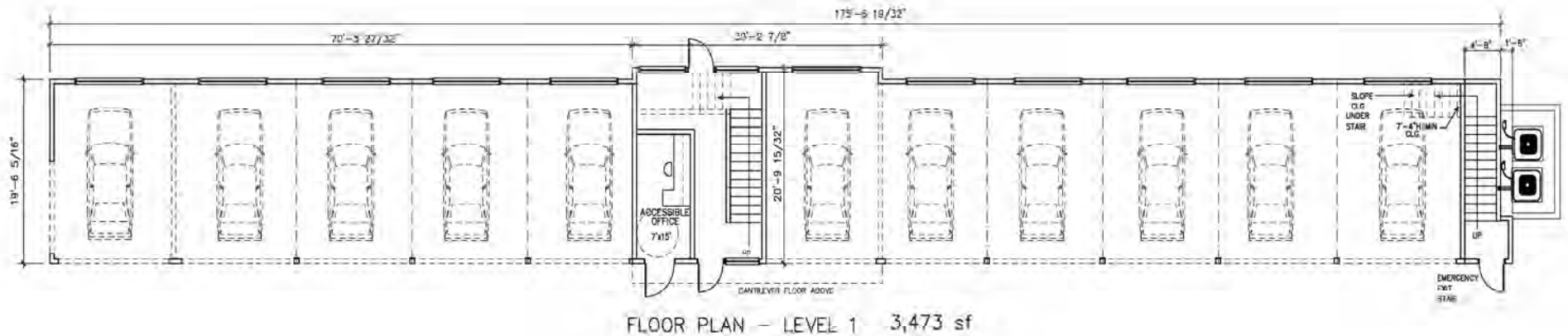


BORAL PROFIT
SOUTHERN LEDGESTONE
COLOR - BUCKS COUNTY



GLAZING
CLEAR GLASS

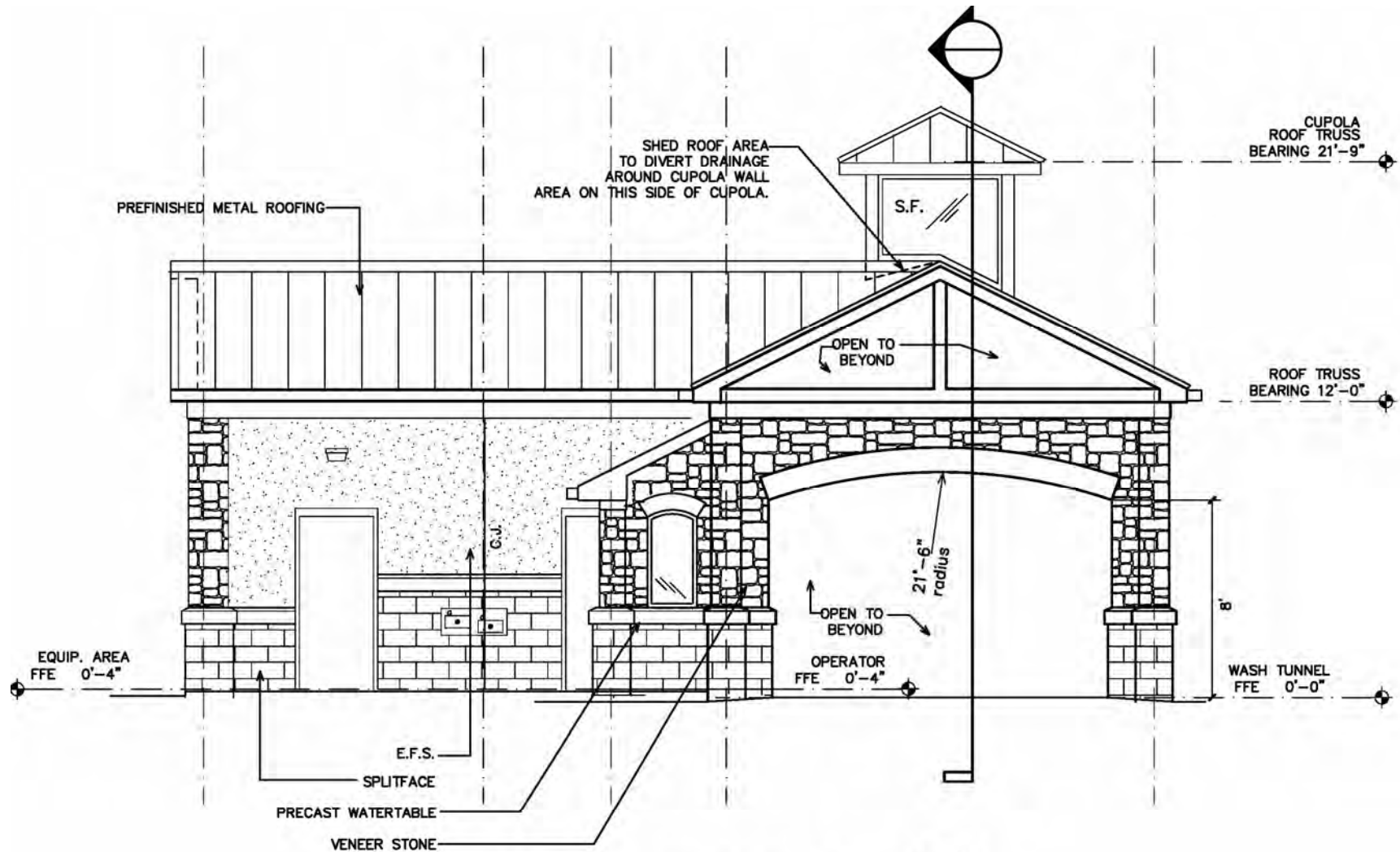
Floor Plan - Mid-Rise



Tunnel Entrance Perspective



Building Material List – Tunnel



REAR ELEVATION (ENTRY)

SCALE: 1/4"=1'-0"

5
A2.1

Vacuum Enclosure

(Height 11 Ft.)



Xpress Pay Terminal (Height 4.5 Ft.)



Project Future

- Planned for Tidal Wave Auto Spa's Divisional Office
- Site is on an "Island"
- Serviced Based Operation
- Future Retail Adaptation
- Building Type Designation (Mid-Rise vs. Parking Structure)





THANK YOU

Form Based Code Score Card

w. stipulations
proposed

	Possible Score	Required	Actual	Perceived
Step 1: Regulating Plan:				
Block Configuration and Building Type	15	15	7	15
Parking	15	15	7	15
Access	15	15	14	15
Total Step 1:	45	45	28	45

Notes:

Rear Building is not 26 feet High

Front Building is interpreted as parking deck and is not 40 feet deep.

Did not provide two way traffic or turning template.

Step 2: Building Types:

Building Type Matches Regulating Plan	5	5	2	5
Building Type Matches Chapter 3	5	5	2	5
Total Step 2:	10	10	4	10

Notes:

Rear Building is not 26 feet High

Front Building does not include commercial use on ground floor.

Step 3 Urban Guidelines:

Intent	5	5	1	1
Front Setback - Johnson Drive	5	5	3	5
Side Street Setback	5	5	5	5
Side Setback	5	5	4	5
Rear Setback	5	5	4	5
Parking Area	5	5	3	4
Total Step 3:	30	25	20	25

Notes:

Rear Building is not 26 feet High

Front Building is interpreted as parking deck and is not 40 feet deep.

Did not provide required Streetscape Details.

Did not provide required landscape plans along side and rear yard.

Did not provide required landscape details and pedestrian sidewalks for connectivity.

Step 4: Architectural Guidelines

Intent	6	6	4	3
Materials	3	3	3	3
Configuration	3	3	3	3
Techniques	3	3	2	2
Total Step 4:	15	10	12	11

Notes:

Rear Building is not 26 feet High

Front building does not provide 60% storefront to 18 feet aff.

Total Project Score:	100	90	64	91
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City of Mission	Item Number:	2.
ACTION ITEM SUMMARY	Date:	June 28, 2019
Administration	From:	Martha Sumrall

Action items require a vote to recommend the item to full City Council for further action.

RE: June 5, 2019 Community Development Committee minutes.

RECOMMENDATION: Review and accept the June 5, 2019 minutes of the Community Development Committee.

DETAILS: Minutes of the June 5, 2019 Community Development Committee meeting are presented for review and acceptance. At the committee meeting, if there are no objections or recommended corrections, the minutes will be considered accepted as presented.

Draft minutes are linked to the City Council agenda packet so that the public may review the discussion from the committee meeting in advance of the Council action on any particular item.

CFAA CONSIDERATIONS/IMPACTS: N/A

Related Statute/City Ordinance:	
Line Item Code/Description:	
Available Budget:	

MINUTES OF THE MISSION COMMUNITY DEVELOPMENT COMMITTEE

June 5, 2019

The Mission Community Development Committee met at Mission City Hall, Wednesday, June 5, 2019 at 6:30 p.m. The following committee members were present: Pat Quinn, Hillary Thomas, Arcie Rothrock, Nick Schlossmacher, Debbie Kring, Kristin Inman, Ken Davis and Sollie Flora. Mayor Appletoft was also present. Councilmember Thomas called the meeting to order at 6:30 p.m.

Also present were City Administrator Laura Smith, Assistant City Administrator Brian Scott, City Clerk Martha Sumrall, Chief Ben Hadley, Assistant to the City Administrator Emily Randel, and Street Superintendent Brent Morton.

Acceptance of the May 1, 2019 Community Development Committee Minutes

Updated minutes of the May 1, 2019 Community Development Committee were provided to the committee. These included two minor changes recommended by councilmembers. There being no objections or additional corrections, the updated minutes were accepted as presented.

Resolution Approving 2020-2024 CARS Program

Mr. Morton reported that each year, a resolution is approved that identifies Mission's 5-year CARS road improvement plan. Johnson County then selects projects for funding which includes up to 50% of the project's construction and construction inspection costs. Mission's eligible streets are Lamar (Foxridge to 67th), 51st (Lamar east to City limit), Foxridge (56th to Lamar), Johnson Drive (Metcalf to Roe), Roe (Johnson Drive to 63rd), Nall (Johnson Drive to 67th), Martway (Metcalf to Roeland), Roeland Drive (Johnson Drive to SMP), and Broadmoor (Johnson Drive to Martway). He provided information on the projects identified for the next five years, which include:

- 2020 - Lamar (Shawnee Mission Parkway to Foxridge) - Project will include UBAS treatment and the addition of bike lanes.
- 2021 - Foxridge Phase II (51st to Lamar) -Project includes full depth street replacement, curbs and gutters, and new stormwater infrastructure due to significant stormwater issues in the area. Estimated cost is approximately \$5 million.
- 2022 - Johnson Drive (Lamar to Roe) - Project includes UBAS treatment, ADA improvements and new striping.
- 2023 - Johnson Drive (Metcalf to Lamar) - Project includes full depth street replacement, stormwater interceptor and other stormwater improvements, sidewalks, pavement markings, street signs, ADA improvements, street and traffic light improvements. Total estimated cost is \$11 million

- 2024 - Roe Avenue (Johnson Drive to 59th Street) - Mill and overlay and spot replacement of curb and sidewalks where needed. This project is in conjunction with another Roe Avenue project further south.

Mr. Morton stated that adoption of this resolution does not lock in funding for these projects, and changes can be made in the future as we have possible additional funding through SMAC, STP, etc.

Councilmember Kring asked if there are any plans to bury KCPL lines as these projects move forward. Mr. Morton stated there is not as KCPL usually does not bury lines unless it is new construction. Ms. Smith stated we would need to do this at the City's expense.

Ms. Smith provided the committee with a handout on the 2019 CARS Program 80% rule and discussed why Mission is requesting a particular amount for Johnson Drive. The CARS program will match 50% of the City's funding, excluding SMAC, STP, etc. funding. She stated that approximately \$16 million is available annually and our apportionment estimate is based on the City's population and property valuation. She stated that Mission is doing well under this formula and that although in theory we could ask for \$16 million, it is important and makes the program work when all work together to share funding.

Councilmember Davis recommended that the resolution adopting the Five Year City/County Street Improvement Program for the City of Mission for 2020-2024 be forwarded to Council for approval. All on the committee agreed. This will be a consent agenda item.

Councilmember Inman asked if Fairway will be paying for part of the improvements to Roe in 2024. Mr. Morton stated they will further south where Mission shares the street Fairway, but the section from Johnson Drive to 59th all belongs to Mission.

Hardwood Floor Resurfacing

Ms. Smith provided information on the recommended resurfacing of the hardwood floors at the Community Center, which is included in the 2019 Parks and Recreation CIP. The floors are recoated yearly as part of the Center's annual maintenance program, but this full resurfacing will sand the floors completely, including removing the current striping prior to refinishing. This has not been done since the facility was built. There are three areas with hardwood floors in the Community Center - south gym including two racquetball courts, north gym, and the exercise/aerobics room on the second floor. Parks and Recreation solicited bids for the project and three were received, with Von Lintel Refinishing and Flooring being the lowest and most responsive. Ms. Smith stated that Von Lintel has done our annual resurfacing in the past. The project will take approximately two weeks as it is a five step process and will be conducted during the annual maintenance closure of the Center in August. The proposal from Von Lintel for this project is in an amount not to exceed \$40,262.

Councilmember Quinn recommended that approval of the proposal from Von Lintel Refinishing & Flooring, Inc. in an amount not to exceed \$40,262 be forwarded to Council for approval. All on the committee agreed. This will be a consent agenda item.

Councilmember Kring asked what our annual maintenance cost is for maintaining the floors. Ms. Smith stated it is approximately \$10,000.

Task Order for Design of Rock Creek Channel Improvements

Ms. Smith stated following discussion at the May 1st CDC meeting, staff was directed to pursue a design task order and financing options that would allow the City to proceed with the entire Rock Creek Channel project. GBA has prepared a task order for this project that would design the entire channel between Roeland Drive to just east of Nall. GBA has been involved in this project for some time, providing the preliminary engineering study and working with the City and Roeland Court Townhomes Association on a solution. The total project costs are estimated at \$4.2 million with the City's portion being \$4,854,928 and the Roeland Court Townhomes portion \$400,809, which would be paid through their recently established CID. This proposed task order covers survey, design, project meetings, bidding of the construction contract, and construction period services and is in a total amount not to exceed \$694,500. Ms. Smith stated the estimated timeline anticipates 90-120 days for design, followed by bidding and construction starting this winter. Funds for the design are currently available in the Stormwater Utility Fund.

Councilmember Davis recommended the Task Order with George Butler Associates for the design of the Rock Creek Channel Improvements and repairs to the parking and common areas of the Roeland Court Townhomes in an amount not to exceed \$694,500, paid from the Stormwater Utility Fund, be forwarded to Council for approval. All on the committee agreed, but this will not be a consent agenda item.

50th & Dearborn Storm Sewer Repairs

Mr. Morton provided information on the storm sewer failure at 50th and Dearborn. This is a corrugated metal pipe collapse that was discussed by the committee in January and at that time approval was given for GBA to design and provide bid phase services for this project. Slip lining the pipe is a bid alternate for this project as it would allow the work to be completed without closing the street, which is a one way in and one way out. The engineer's estimated to repair the failed storm sewer and additional infrastructure is \$166,505, and the bid alternate to slip line the creek channel is an additional \$18,470 for an estimated project total of \$184,975. He stated that funds are available in the Stormwater Utility Fund for this project.

Ms. Smith provided a handout with the updated Stormwater CIP information. Funding has been increased to \$350,000. Councilmember Davis asked if the CIP Committee has discussed this project as it is important to have them included in the process. Ms. Smith stated they have not, but it will be included on their agenda for June 10th.

Councilmember Kring asked if the street inventory includes additional information on what is underneath the streets as we are seeing more sinkholes. She would like to be sure we are being proactive in addressing these issues. Ms. Smith stated additional information on the current asset inventory will be provided under Department Updates on this agenda.

The committee recommended that this project move forward as an action item on the July 10 Community Development Committee agenda.

Other
Department Updates

Update on Asset Inventory - BHC Rhodes

Ms. Smith stated during the RFQ process for on-call engineering firms, BHC Rhodes impressed the interview panel particularly on their asset management and inventory process. Over the last few months they have been working with staff in gathering data and establishing Mission's asset inventory. Members of the team working on Mission's project attended the meeting to provide an overview on the project.

Mr. Morton introduced Dave Nolte, Matt Broll and Joe Hunninghake with BHC Rhodes. Mr. Nolte stated they have been working since January to gather data from Mission and Johnson County AIMS to see what stormwater infrastructure Mission currently has and its condition. The data collected will allow Mission to know the size of stormwater pipes and materials used, when installed and inspected so that we can be more proactive in maintaining this infrastructure going forward. He provided several maps to the group that identified the "estimated risk" associated with current stormwater structures and a map of stormwater pipes indicating those that have data and those that do not.

The committee discussed Johnson County's risk assessment values for stormwater which is in AIMS and assists with SMAC funding, and the SMAC program to update this data with Mission being approved for funding to inventory our stormwater structures. There is a County-wide asset management program that we will supplement with our data as we have it. Discussion continued on the map of stormwater pipes and those that have data associated with them (green) and those that do not (red). It was noted that creeks are not included on the map. The team was also encouraged to select different colors for mapping as those who are color blind are unable to tell the difference between the two colors.

Mr. Morton provided information on the inspection process, stating that if they are able to see approximately 50 ft. of the pipe, there is no need to video it. Most of our stormwater pipes are relatively short runs and this will save money in the end. Once complete data is available in our asset management program, you will be able to click on a site and see the risk rating, pictures

of the pipe, length and size, etc. This will be available with ARC view as well as through Google Earth files with overlays.

Mr. Nolte stated the next phase of the project is streets, curbs, and gutters. Mission's Stantec data will be used and this will assist with our 10-year street program in prioritizing projects and estimating costs. Although the Stantec data is approximately two years old, Mr. Nolte stated that a street's condition does not change that much in just a few years.

Mr. Morton stated this project is focusing on stormwater issues and prioritizing projects as there is funding available for these projects. Councilmember Thomas thanked the BHC Rhodes team and city staff for their work on this project, noting that to date there are 11,000 entries made into the inventory.

This item was informational only and no action was taken.

Other

Ms. Smith provided a brief update on The Gateway Project, noting a recent article in the Kansas City Business Journal. She stated that old liens against the project mentioned in the article were satisfied in April. Construction has begun in prepping the pad for the Cinergy building. There are no longer construction trailers on site as the contractor will be officing out of a building across the street from the project. It is anticipated that work will begin on site again by June 24th. They continue to work with Johnson County Wastewater and once they sign-off on the plans, Mission can issue a building permit. Plan review for the Cinergy building is underway, and the developer anticipated having a contractor in place for the food hall and office building by July. Burns and McDonald has pulled a right-of-way permit to move electrical.

Councilmember Thomas asked if there was any concern with the columns for the apartments that are exposed. Mr. Scott stated that a third-party structural engineer is looking at these and working with the City's building official. He stated there is a concern with shifting ground, but that the rust visible is just on the surface and not an issue.

Councilmember Davis asked if there is an update on the Hodges planters. Ms. Smith stated the committee met several weeks ago and she will provide a report on the work of the group at the July committee meetings.

Meeting Close

There being no further business to come before the Committee, the meeting of the Community Development Committee adjourned at 7:15 p.m.

Respectfully submitted,

Martha Sumrall
City Clerk

City of Mission	Item Number:	3.
ACTION ITEM SUMMARY	Date:	July 3, 2019
Public Works	From:	Brent Morton

Action items require a vote to recommend the item to full City Council for further action.

RE: Foxridge Drive and Woodson Stormwater Repairs

RECOMMENDATION: Approve the contract with SheDigsIt, LLC (SDI) for repairs to two storm drainage infrastructure failures in an amount not to exceed \$73,307.

DETAILS: This April, Council authorized a task order with GBA to provide survey, design and bid phase services for two stormwater infrastructure failures. Sinkholes had formed adjacent to these failures causing unsafe conditions for the public. This contract will address failures at the following locations:

5501 Foxridge Dr - A corrugated metal pipe failed underneath the stormwater inlet on the east side of Foxridge Dr. This caused the subgrade under the parking lot to erode and has compromised the structural integrity of this location.

5939 Woodson St - The CMP stormwater pipe in this area failed causing a sinkhole adjacent to the street and beneath the parking lot/yard of the business at this location.

The sinkhole stormwater repair projects were advertised and bids were opened on July 8th, 2019. There were two bidders, with SDI being the lowest and most responsive.

Sinkhole Stormwater Repairs - Bid Opening Results July 8th, 2019	
SDI, LLC	\$73,307
G-B Construction, LLC	\$124,447

Due to the more immediate concerns and issues raised by the CMP failures/sinkholes, this action item is being included in the packet prior to the bid opening. This will allow the Council to award a contract and move forward with repairs without losing another month. If the bids are not acceptable to staff/Council, the item will be continued to the August 7 CDC Committee meeting.

CFAA CONSIDERATIONS/IMPACTS: N/A

Related Statute/City Ordinance:	
Line Item Code/Description:	
Available Budget:	\$150,000

C:\14184\04\Civil\3D\Production Drawings\Storm Plans\14184_04D1400.dwg Layout: Plan -- Monday, June 24, 2019, 9:02am -- Copyright 2019, George Butler Associates, Inc. Architect 00212, Professional Engineer 000133, Landscape Architect 000025, Professional Land Surveyor 000039

4' dia. Conc. Concentric MH
 Top EL = 980.02'
 FL out West = 964.75' (8" VCP)
 FL in SE = 965.30' (8" VCP)

15' Electric Easement
 Bk. 199, Pg. 624

9801 Renner Boulevard
 Lenexa, Kansas 66219
 913-492-0400
 www.gbateam.com

DATE:	5/31/19
DESIGN BY:	DWC
DRAWN BY:	DS
PROJECT NO.:	14184.04
SHEET NO.:	1
TOTAL SHEETS:	1

5501 Foxridge Drive
2019 Storm Repairs
 City of Mission, Kansas

NO.	DATE	REVISIONS	BY	APPROVED

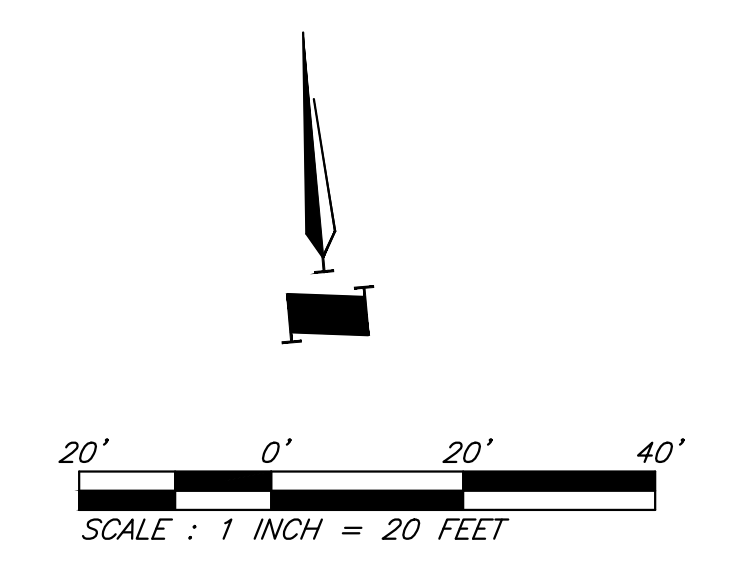
Legend

- Asphalt Repair
- Sod
- Curb

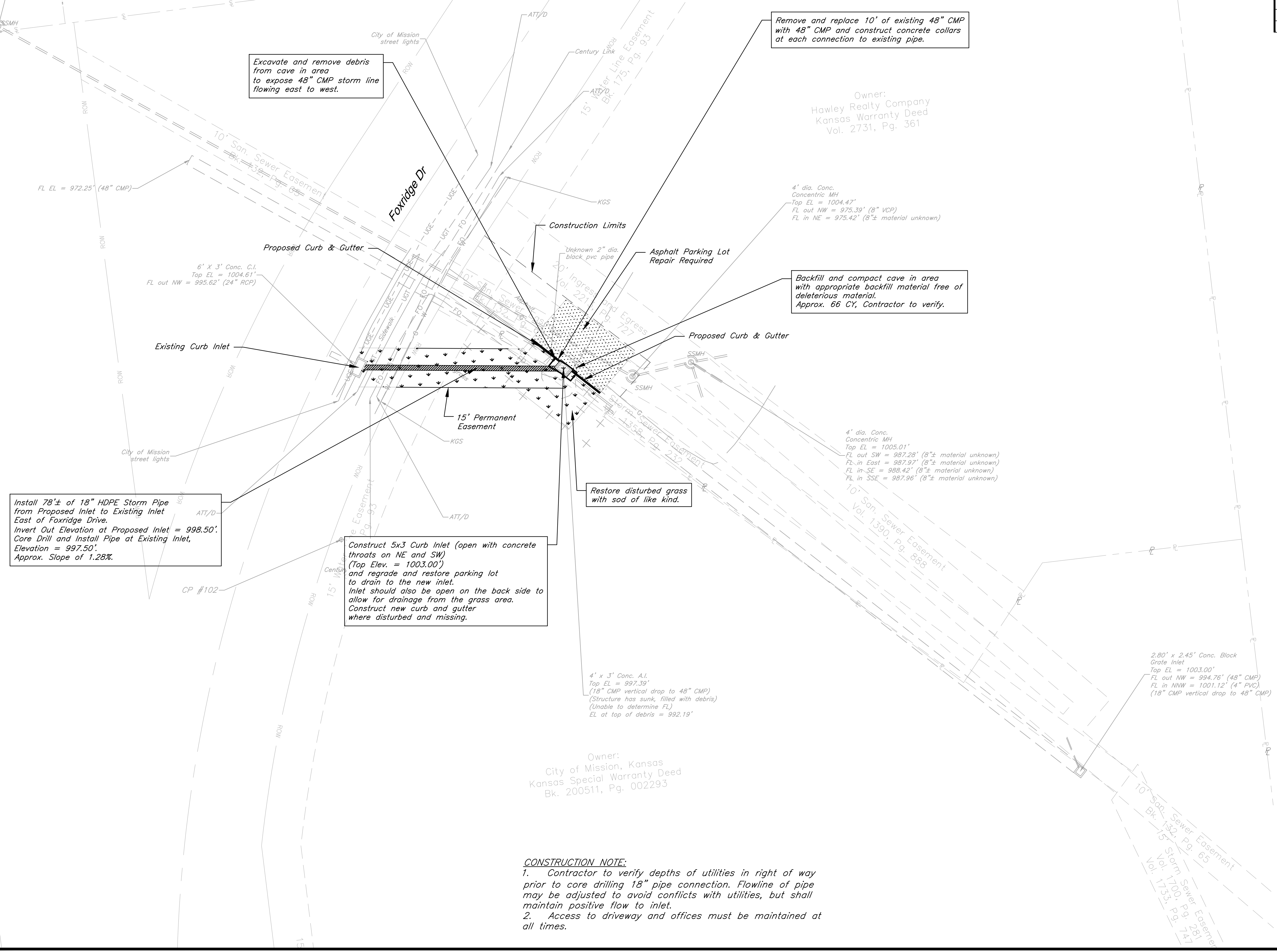
ITEM NO	DESCRIPTION	UNIT	QUANTITY
1	5'x3' CURB INLET	EA	1
2	18" HDPE	LF	78
3	CURB & GUTTER - TYPE B	LF	20
4	48" CMP	LF	10
5	ASPHALT PARKING LOT REPAIR	SY	74
6	BACKFILL CAVE IN AREA	CY	66
7	SOD	SY	170

* NOTE: The quantities above are for Contractor reference only. Bid items and measurement and payment are dictated by the Contract Documents. Quantities above are approximations only as site conditions may have changed.

Coordinates Shown Hereon:
 Modified State Plane (Project Ground Coordinates), NAD83
 1501 - Kansas North, U.S. Feet
 Vertical - NAVD88, U.S. Feet
 0.999922643
 To get to State Plane:
 Coordinates x CAF = State Plane
 CP #102 - 1/2" Rebar with GBA cap
 Coordinates:
 N: 270793.91'
 E: 2259106.02'
 EL: 1005.26'
 CP #103 - 1/2" Rebar with GBA cap
 Coordinates:
 N: 271270.93'
 E: 2259390.61'
 EL: 1026.30'
 BM #11 - Chiseled "+" on West bonnet bolt of fire hydrant on East side of Foxridge Drive, South of ATT building
 Coordinates:
 N: 271286.68'
 E: 2259396.69'
 EL: 1028.81'



5501 Foxridge Dr



Excavate and remove debris from cave in area to expose 48" CMP storm line flowing east to west.

Remove and replace 10' of existing 48" CMP with 48" CMP and construct concrete collars at each connection to existing pipe.

Owner:
Hawley Realty Company
Kansas Warranty Deed
Vol. 2731, Pg. 361

4' dia. Conc. Concentric MH
Top EL = 1004.47'
FL out NW = 975.39' (8" VCP)
FL in NE = 975.42' (8" material unknown)

Backfill and compact cave in area with appropriate backfill material free of deleterious material. Approx. 66 CY, Contractor to verify.

4' dia. Conc. Concentric MH
Top EL = 1005.01'
FL out SW = 987.28' (8" material unknown)
FL in East = 987.97' (8" material unknown)
FL in SE = 988.42' (8" material unknown)
FL in SSE = 987.96' (8" material unknown)

4' x 3' Conc. A.I.
Top EL = 997.39'
(18" CMP vertical drop to 48" CMP)
(Structure has sunk, filled with debris)
(Unable to determine FL)
EL at top of debris = 992.19'

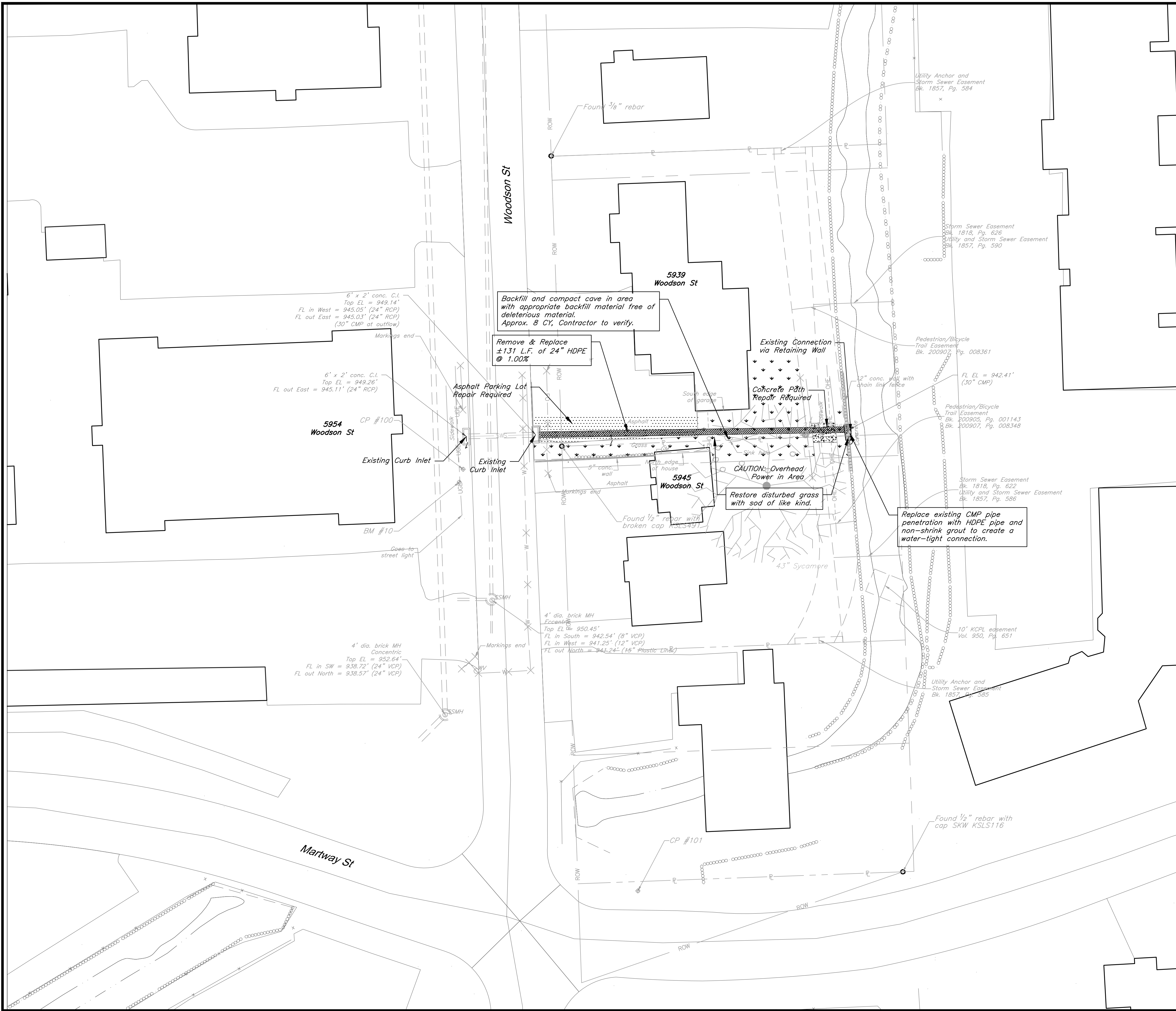
2.80' x 2.45' Conc. Block
Grate Inlet
Top EL = 1003.00'
FL out NW = 994.76' (48" CMP)
FL in NNW = 1001.12' (4" PVC)
(18" CMP vertical drop to 48" CMP)

Install 78'± of 18" HDPE Storm Pipe from Proposed Inlet to Existing Inlet East of Foxridge Drive.
Invert Out Elevation at Proposed Inlet = 998.50'.
Core Drill and Install Pipe at Existing Inlet, Elevation = 997.50'.
Approx. Slope of 1.28%.

Construct 5x3 Curb Inlet (open with concrete throats on NE and SW) (Top Elev. = 1003.00') and regrade and restore parking lot to drain to the new inlet. Inlet should also be open on the back side to allow for drainage from the grass area. Construct new curb and gutter where disturbed and missing.

CONSTRUCTION NOTE:
 1. Contractor to verify depths of utilities in right of way prior to core drilling 18" pipe connection. Flowline of pipe may be adjusted to avoid conflicts with utilities, but shall maintain positive flow to inlet.
 2. Access to driveway and offices must be maintained at all times.

Owner:
City of Mission, Kansas
Kansas Special Warranty Deed
Bk. 200511, Pg. 002293



GBA 50 1988	DATE:	5/31/19
	DESIGN BY:	DWC
	DRAWN BY:	DS
	PROJECT NO.:	14184.04
	SHEET NO.	TOTAL SHEETS
	1	1
9801 Renner Boulevard Lenexa, Kansas 66219 913.492.0400 www.gbateam.com		

5939 Woodson Street
2019 Storm Repairs
 City of Mission, Kansas

NO.	DATE	REVISIONS	BY	APPROVED

- ### Legend
- Asphalt Repair
 - Sod
 - Concrete Repair

5939 WOODSON STREET STORM REPAIR QUANTITIES			
ITEM NO	DESCRIPTION	UNIT	QUANTITY
1	24" HDPE	LF	131
2	CONCRETE PATH REPAIR	SY	9
3	ASPHALT PARKING LOT REPAIR	SY	74
4	BACKFILL CAVE IN AREA	CY	8
5	SOD	SY	218

*** NOTE:** The quantities above are for Contractor reference only. Bid items and measurement and payment are dictated by the Contract Documents. Quantities above are approximations only as site conditions may have changed.

Coordinates Shown Hereon:

Modified State Plane (Project Ground Coordinates), NAD83
 1501 - Kansas North, U.S. Feet
 Vertical - NAVD88, U.S. Feet

CAF: 1.00007546
 The coordinate values of all features have been scaled from a base point, known as CP #100.

To get to State Plane:
 Scale drawing by the reciprocal of the above referenced CAF (0.999924546);
 holding point #100 as the base point.

CP #100 - 1/2" Rebar with GBA cap on West side of Woodson Road in front of apartment #5954
Coordinates:
 N: 267793.39'
 E: 2262832.21'
 EL: 949.66'
Ties:
 1) West 1.83' to East edge of sidewalk
 2) East 2.87' to back of curb
 3) South 20.23' to North face of light pole

CP #101 - 1/2" Rebar with GBA cap at NE corner of Woodson Road and Martway Street.
Coordinates:
 N: 267617.60'
 E: 2262905.08'
 EL: 956.91'
Ties:
 1) SE 9.32' to NW corner of curb inlet
 2) South 2.66' to North edge of sidewalk
 3) West 16.60' to East face of light pole

BM #10 - Chiseled "U" cut on East edge of concrete sidewalk in front of apartment #5954 located on West side of Woodson Road.
Coordinates:
 N: 267788.02'
 E: 2262830.58'
 EL: 950.11'
Ties:
 1) NE 5.63' to CP #100
 2) South 14.97' to North face of street light

SCALE: 1 INCH = 20 FEET

5939 Woodson St

City of Mission	Item Number:	4.
ACTION ITEM SUMMARY	Date:	July 10, 2019
Public Works	From:	Brent Morton

Action items require a vote to recommend the item to full City Council for further action.

RE: 50th and Dearborn Street Stormwater Repairs

DETAILS: In January, the City Council approved a task order with GBA for the design, and bid phase services for a storm sewer failure at 50th and Dearborn Street. The engineer's estimate to repair the failed storm sewer and additional infrastructure is \$166,505, and the bid alternate to slip line the creek channel is an additional \$18,470, for an estimated project total of \$184,975.

Due to the age and condition of the stormwater infrastructure in this neighborhood, a portion of corrugated metal pipe (CMP) under the driveway of the residence at 5028 Dearborn collapsed late last fall. Sinkholes formed adjacent to the failures causing unsafe conditions for the public. Public Works installed a steel plate, as a temporary solution, to provide access to the residents while a permanent solution could be evaluated.

Because of the condition of other pipes in the area, some of which travel underneath the roadway, the limits of the project were expanded to ensure that the repairs would address longer-term stability for the roadway. This was of particular concern as Dearborn is a dead end street, providing just one way in and one way out of the neighborhood.

The design includes removal, replacement, or abandonment of existing stormwater pipe in this area, as well as a bid alternate to slip line the storm culvert under Dearborn Street. The benefits of slip lining the street crossing include allowing for the roadway not to be disturbed by culvert repair, no traffic interruptions during installation and potential damage to underground utilities is avoided.

This project was not specifically planned or budgeted in the 2019-2023 Stormwater CIP. Because of the size and scope of the proposed project, it was recommended to provide the CIP Committee with an opportunity to review. The CIP Committee supports moving this project to construction in 2019, and staff is asking for Council approval to move this project forward to the bidding phase.

General maintenance funds are available in the Stormwater Utility Fund to complete the project.

CFAA IMPACTS/CONSIDERATIONS: N/A

Related Statute/City Ordinance:	NA
Line Item Code/Description:	22-61-407-05
Available Budget:	\$184,975

WEST 50TH AND DEARBORN STORM REPAIRS

MISSION, JOHNSON COUNTY, KANSAS

SECTION 5, TOWNSHIP 12S, RANGE 25E

INDEX OF SHEETS

Sht. No.	Description
1	Cover Sheet
2	Plan View
3	Profile View
4 - 6	City Standard Details

UTILITY COMPANIES

Fiber	Level 3 Communications (877) 366-8344
Fiber	Johnson County DTI (913) 486-5018
Fiber	Google Fiber (913) 486-5018
Electric	KCP&L (800) 788-9140
Gas	Kansas Gas Service (800) 778-9140
Telephone	AT&T (HIGH PRIORITY FACILITIES IN AREA) Steve Barron (314) 420-1973
Telephone	Surewest / Everest (800) 788-9140
Cable	Time Warner Cable (800) 778-9140
Public Works	City of Mission 4775 Lamar Ave Mission, KS 66202 (913) 676-8375
Sanitary Sewer	Johnson County Wastewater 4800 Nall Ave Mission, KS 66202 (800) 778-9140
Water	WaterOne 10747 Renner Boulevard Lenexa, KS 66219 (913) 895-1800
Locates	Kansas One Call System, Inc.

UTILITY NOTES

The information concerning the location of underground utilities shown hereon, which are not visible from the surface, has not been verified in the field. These locations are not to be construed as accurate or exact. Before beginning any construction or excavation, please call Kansas One-Call System, Inc. at 1-800-344-7233.

CONSTRUCTION NOTES

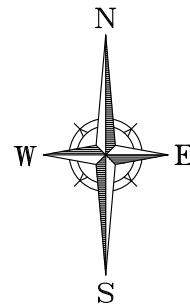
- Contractor to verify existing pipe dimensions, positions, and elevations prior to shop drawing submittal. Any deviations from these plans shall be noted to the engineer during shop drawing review.
- Notify property owners prior to beginning work and disrupting driveway access.
- Before starting excavation activities, the Contractor shall notify all utilities involved, and shall request their cooperation during construction to avoid breaking utility lines. The utility shall be notified immediately should a break occur in a line during construction under this contract. Any lines so broken by the Contractor shall be repaired according to the utility company's standards at the expense of the Contractor. The Contractor shall be held responsible for contacting all utility companies for field location of all underground utility lines, whether shown on these plans or not, prior to any excavation.
- The Contractor shall provide protection to prevent undermining or damage to the structural integrity of all utility poles, fences, road signs, or other items that parallel or cross the site and make arrangements with owning entity to provide temporary support or protection during construction.
- Existing private sanitary sewer facilities serving buildings may not be shown. The Contractor shall be held responsible for any damages done to these facilities during construction, and shall maintain continuous service to all buildings.
- Construction activity (including demolition, clearing, storage of equipment and materials, installation of materials and equipment, and employee parking) shall be restricted to street right-of-way and City of Mission-owned property, permanent and temporary easements, and along with other areas secured by Contractor by private agreement. Engineer shall be informed of Contractor's private arrangements for construction easements and shall be supplied a copy of the written agreement.
- Use of the Site: Limit use of the premises to work in areas indicated. Do not disturb portions of the site beyond the areas in which the work is indicated.
- Traffic control shall conform to the requirements of the City of Mission. No road closures will be approved as part of this project. Parking may be prohibited during construction. Through traffic will be maintained at all times on all streets being where work is occurring, unless otherwise agreed to by the City of Mission.
- Roads and Driveways: Keep roads and driveways clear and available to the residents and emergency vehicles at all times. Do not use these areas for parking or storage of materials. Provide temporary surfacing as necessary to maintain access. Schedule deliveries to minimize space and time requirements for storage of materials and equipment on-site.
- Contractor shall identify where on-street parking must be temporarily prohibited. Contractor shall provide temporary signs and direct the installation. Contractor shall provide labor and equipment for installation and subsequent removal of temporary signs. Temporary parking prohibitions shall be posted at least 48 hours in advance of the effective date of parking prohibition.
- All excavation shall be Unclassified Excavation.
- Contractor shall restore the project sites to conditions equal to or better than those existing prior to entry. All existing items and construction removed or damaged during the performance of the Work, regardless of location and whether or not indicated on these Contract Drawings, shall be repaired or replaced as detailed in the Contract Specifications unless otherwise indicated on these Contract Drawings.
- Pavement restoration shall involve restoration of paved surfaces with similar materials in accordance with the City of Mission Standards as currently adapted.
- All disturbed areas shall be restored with sod to match existing.
- Backfill in street or alley right-of-way and under pavement or within 4 feet horizontal of pavement shall be flowable fill (60 to 90 psi).
- Backfill in areas other than street or alley right-of-way shall be acceptable excavated material.

PERMITS

The Contractor shall be responsible for obtaining all required construction permits, paying all fees, and otherwise complying with all applicable regulations governing the work and the cost shall be subsidiary to other bid items.

Contractor shall utilize Best Management Practices to control erosion and sediment migration into storm drainage structures or channels. Contractor shall evaluate drainage patterns and provide erosion control devices necessary. Contractor shall keep 150 LF of 9" wattle on site for temporary erosion protection during construction. The wattle may be moved at the direction of the Owner or Engineer for temporary erosion control during construction. The wattle may be removed or left in place at the direction of the Owner or Engineer when the site is 70% revegetated.

The Contractor shall halt construction immediately and contact the Kansas State Historical Society should artifacts of questionable historical relevance be discovered during excavation.



GENERAL NOTES

This project shall be constructed in accordance with the Specifications and Contract Documents. All work in public easements and Right-of-Way and all erosion control work must comply with the latest edition of the Overland Park Design and Construction Standards Volume 2, Construction Specifications 2015 Edition or as modified by the City of Mission.

The Contractor shall have one (1) signed copy of the plans and one (1) copy of the appropriate construction specifications at the job site at all times.

The Contractor shall carefully preserve all monuments, reference points, stakes, and bench marks set for project control, and in case of destruction of same through carelessness or negligence on the part of the Contractor, he will be charged with the resulting expense of replacement and responsibility for any mistakes or loss of time caused thereby.

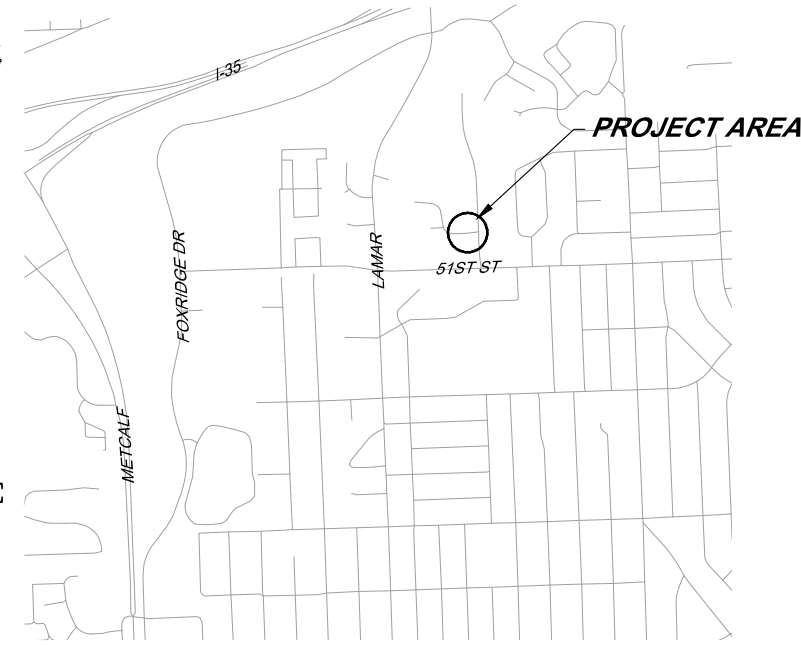
The Contractor is responsible for the protection of all property corners and section corners. Any property corners or section corners disturbed or damaged by construction activities shall be reset by a registered land surveyor licensed in the State of Kansas, at the Contractor's expense.

Table of Quantities shown on these plans are for reference for the Base Bid item. The Base Bid bid item may require more items to fully restore the project areas. Items necessary for full restoration of the project area shall be subsidiary to the Base Bid item.

Driveways, sidewalk, pavement and other areas inside and outside the construction limits damaged by the contractor shall be restored to a condition equal to or better than that existed before damage occurred at the Contractor's expense.

Structure placement coordinates shown on plans are for center of structure.

Pipe lengths shown on plan are horizontal distance from center of structure to center of structure.



VICINITY MAP

1" = 1000'

PROJECT SURVEY

Modified State Plane (Project Ground Coordinates), NAD83
1501 - Kansas North, U.S. Feet
Vertical - NAVD88, U.S. Feet

CAF: 0.9999226
To get to State Plane:
Coordinates x CAF = State Plane

BM#10 - Chiseled "L" cut in SW corner of headwall on North side of 50th Street, ±296' West of Dearborn Street

N- 274157.8483
E- 226228.9175
Z- 943.37

CP#100 - 1#2" Iron Bar w/ GBA control cap

N- 274137.7400
E- 226228.5288
Z- 947.27

CP#101 - 1#2" Iron Bar w/ GBA control cap

N- 274144.1741
E- 226249.1787
Z- 966.712

DEVELOPED AND OWNED BY:
CITY OF MISSION
4775 LAMAR AVENUE
MISSION, KS 66202
913-676-8380
Brent Morton

PREPARED & SUBMITTED BY:
GEORGE BUTLER ASSOCIATES, INC.
9801 RENNER BOULEVARD
LENEXA, KANSAS 66219-9745



PROJECT ENGINEER: Katie H. Stucky P.E., CFM, ENV SP

DATE:



9801 Renner Boulevard
Lenexa, Kansas 66219
913.492.0400
www.gbateam.com

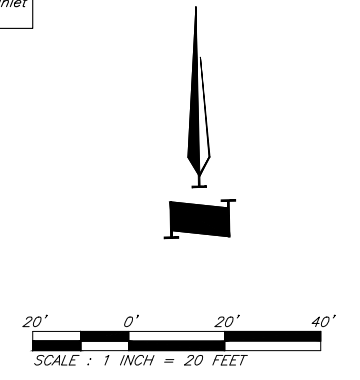
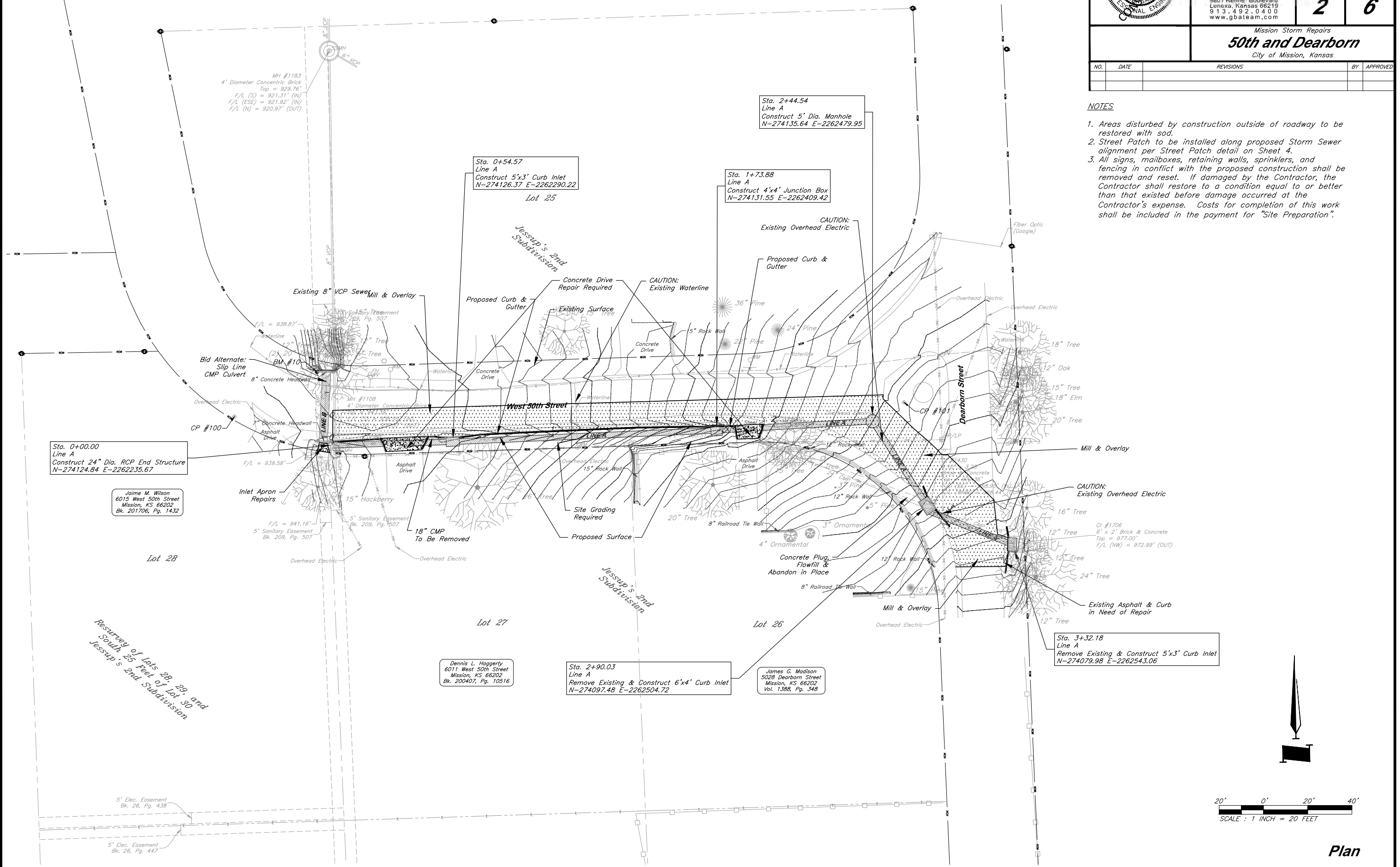
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		DESIGN BY: KHS				
		DRAWN BY: DS				
		PROJECT NO.: 13186.23				
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SHEET NO.	TOTAL SHEETS					
2	6					

Mission Storm Repairs
50th and Dearborn
 City of Mission, Kansas



NO.	DATE	REVISIONS	BY	APPROVED

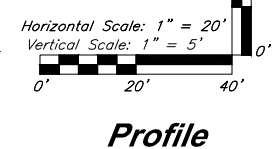
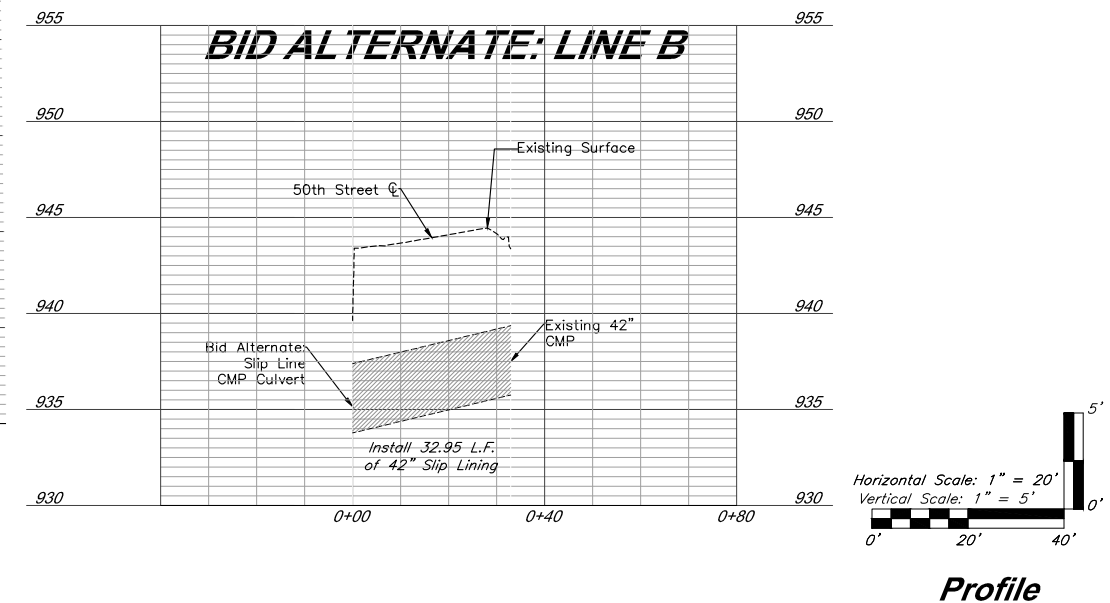
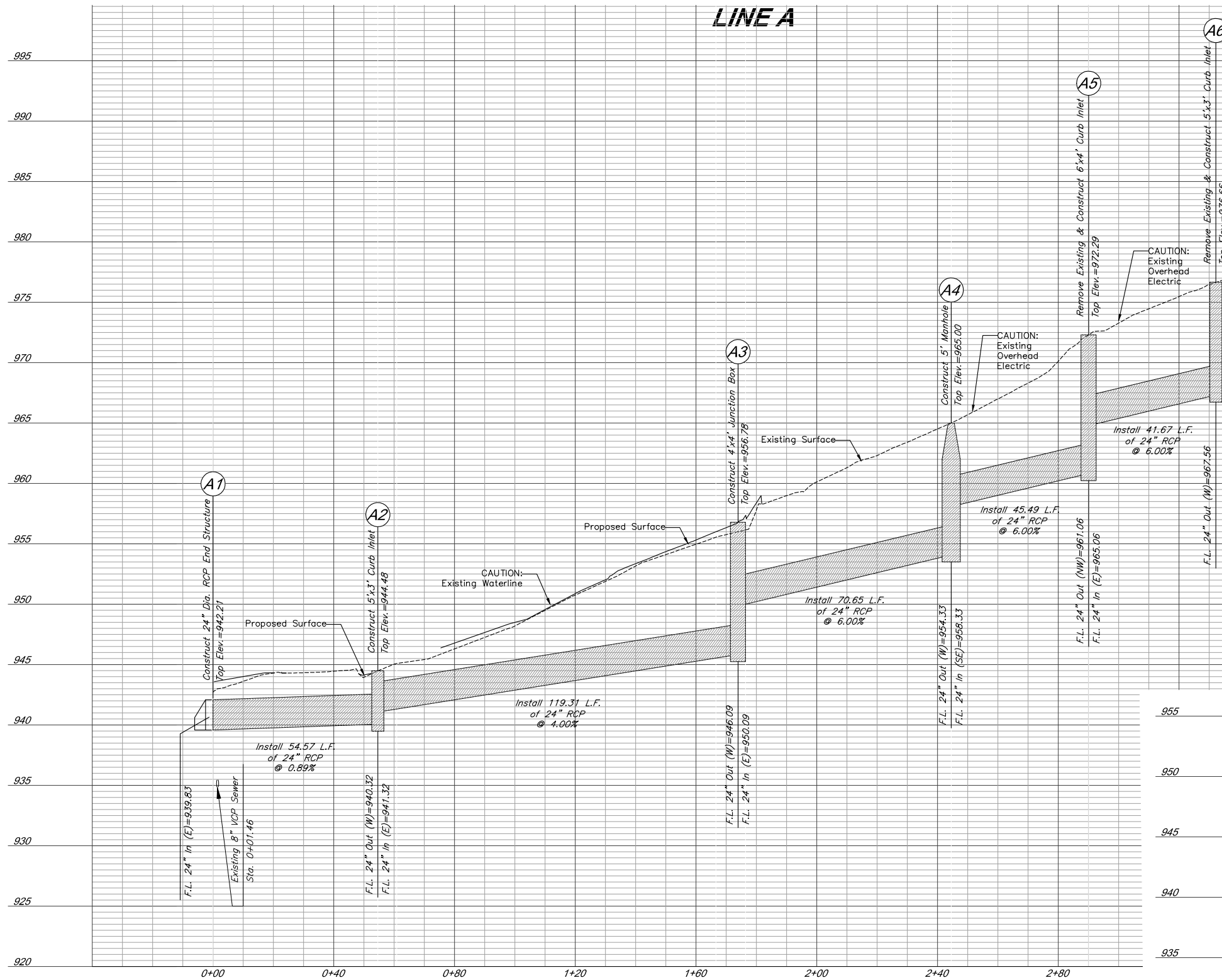
- NOTES**
1. Areas disturbed by construction outside of roadway to be restored with sod.
 2. Street Patch to be installed along proposed Storm Sewer alignment per Street Patch detail on Sheet 4.
 3. All signs, mailboxes, retaining walls, sprinklers, and fencing in conflict with the proposed construction shall be removed and reset. If damaged by the Contractor, the Contractor shall restore to a condition equal to or better than that existed before damage occurred at the Contractor's expense. Costs for completion of this work shall be included in the payment for "Site Preparation".



Plan

C:\13186.23\Draw_30\Production Drawings\Storm Plans\13186_2301402.dwg Layout: Profile -- Wednesday, April 03, 2019, 2:36pm -- Copyright 2019, George Butler Associates, Inc. Architect 00212, Professional Engineer 000133, Landscape Architect 000025, Professional Land Surveyor 000039

				DATE: 4/3/19 DESIGN BY: KHS DRAWN BY: DS PROJECT NO.: 13186.23
		SHEET NO.: 3	TOTAL SHEETS: 6	
Mission Storm Repairs 50th and Dearborn City of Mission, Kansas				
NO.	DATE	REVISIONS	BY	APPROVED



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		DATE: 4/3/19		
		DESIGN BY: KHS		
9801 Renner Boulevard Lenexa, Kansas 66219 913.492.0400 www.gbateam.com	PROJECT NO.: 13186.23	TOTAL SHEETS: 6		
	SHEET NO.: 4			
Mission Storm Repairs 50th and Dearborn City of Mission, Kansas				
NO.	DATE	REVISIONS	BY	APPROVED

Standard Type "A" Curb

Standard Type "B" Curb

Standard Type "C" Curb

Types "A" - Dry Curb

Types "B" - Dry Curb

Type "C-1" Curb
(Use w/ Type "A" Curb)

CONCRETE CURB & GUTTER
Not to Scale

CONCRETE CURB & GUTTER
Not to Scale

CONCRETE CURB & GUTTER
Not to Scale

Expansion Joint

Contraction Joint

JOINT DETAILS
Not to Scale

Year 2018 Edition
 OVERLAND PARK
 DEPARTMENT OF PUBLIC WORKS
 STANDARD DETAILS
 CONCRETE CURB WITH ASPHALT PAVEMENT
 (COLLECTOR AND RESIDENTIAL STREETS)
 DATE: 09/28/09

NOTES
1. Standard Type "B" Curb

PLAN
Not to Scale

SECTION A-A
Not to Scale

DRIVEWAY ENTRANCE ON NON-CURBED STREETS
Not to Scale

RESIDENTIAL DRIVEWAY
Not to Scale

Year 2018 Edition
 OVERLAND PARK
 DEPARTMENT OF PUBLIC WORKS
 STANDARD DETAILS
 RESIDENTIAL DRIVE DETAIL
 DATE: 09/28/09

Asphaltic Concrete Street Repair
Type I
(Concrete Base w/ Asphaltic Concrete Surface)

Asphaltic Concrete Street Repair
Type II
(Full Depth Asphaltic Concrete)

Portland Cement Concrete Street Repair
**
-- May be Used Only on Existing Concrete Streets

Pipe Underdrain Repair
Type I
(Concrete Base w/ Asphaltic Concrete Surface)

Pipe Underdrain Repair
with OP Special

Repair Under Curb, Sidewalk or Driveway

Utility Investigation Excavation Repair

--- to repair areas under 1' x 1'

STREET REPAIR
No Scale

Year 2018 Edition
 OVERLAND PARK
 DEPARTMENT OF PUBLIC WORKS
 STANDARD DETAILS
 STREET REPAIR DETAILS
 DATE: 09/10/98

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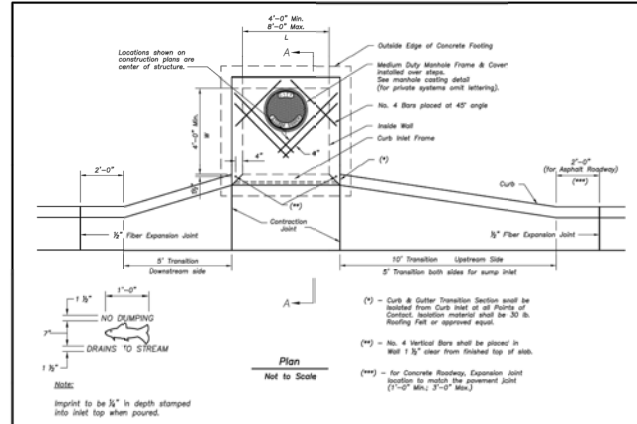
9801 Renner Boulevard
Lenexa, Kansas 66219
913.492.0400
www.gbateam.com

DATE: 4/3/19
DESIGN BY: KHS
DRAWN BY: DS
PROJECT NO.: 13186.23

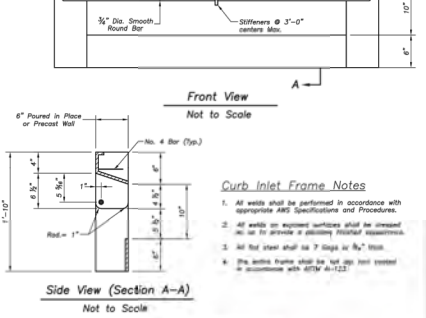
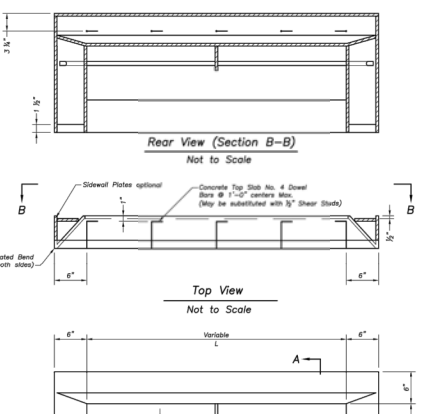
SHEET NO.	TOTAL SHEETS
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Mission Storm Repairs
50th and Dearborn
City of Mission, Kansas

NO.	DATE	REVISIONS	BY	APPROVED

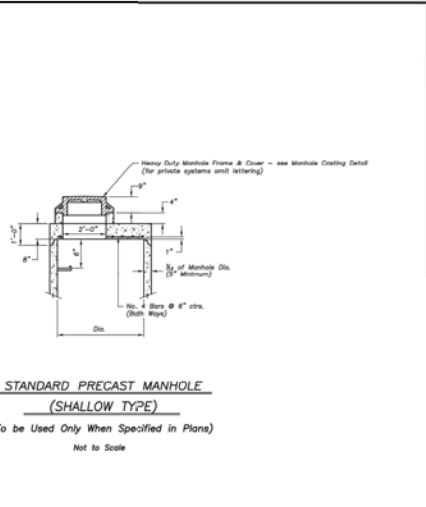
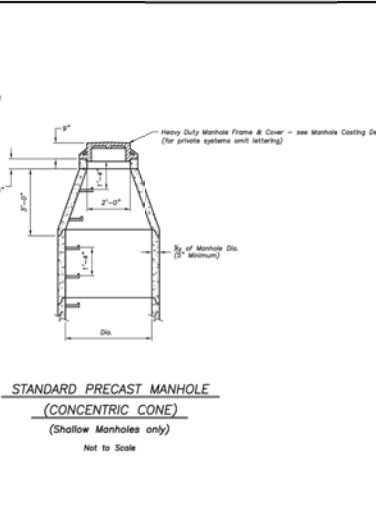
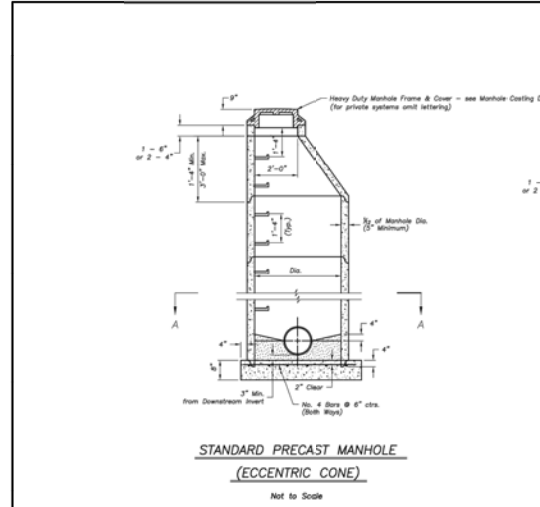
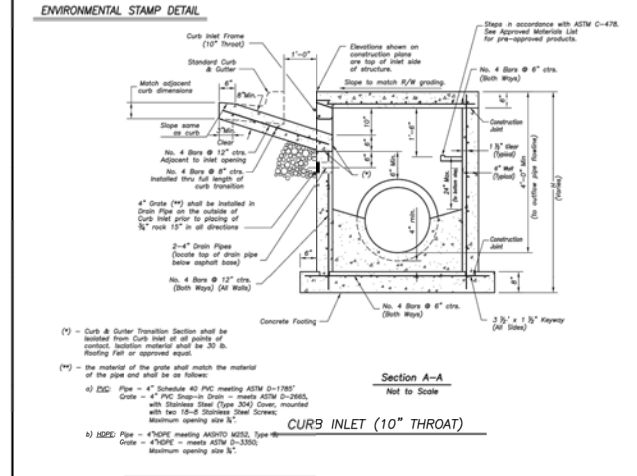


- Curb Inlet Notes**
- General**
- Overland Park Municipal Code (OPMC) and Overland Park Design and Construction Standards Manual (OPDCSM) are incorporated, except as otherwise noted.
 - All storm sewer structures shall be pre-cast or poured in place. If pre-cast structures are used, the base shall be poured in place and the wall shall be set exposed to a height 2" above the finish top elevation, or as directed by the City Engineer.
 - Pre-cast shop drawings are to be approved by the City Engineer for publicly financed or administered projects. Pre-cast shop drawings for privately financed projects are to be submitted to the Engineering Services Division of the Planning and Development Services Department.
 - Do not scale these drawings for dimensions or clearances. Any questions regarding dimensions shall be brought to the attention of the City Engineer prior to construction.
 - On-grade inlets shall conform to the street grade and ramp slope shall be level.
 - The first dimension listed in the construction notes is the "W" dimension. The second dimension is the "M" dimension. The concrete thickness and reinforcement shall be for faces with ("LxW") and ("MxW") less than or equal to 20. For faces with either of these conditions greater than 20, a special design is required.
- Concrete**
- Concrete used in this work shall be KCMBCMC, as approved by the Kansas City Metropolitan Waterworks Board, and shall meet the requirements of the OPMC.
 - Reinforcing steel shall be placed with non-reinforced concrete inserts to provide smooth flow.
 - Reinforcing steel shall be placed with 3/4" triangular meshing.
 - Reinforcing steel shall be new billet, minimum Grade 40 as per ASTM A618C, and shall be bent.
 - All dimensions relative to reinforcing steel are to centerline of bars. 2" clearance shall be provided throughout unless noted otherwise. Tolerances of +/- 3/16" shall be permitted.
 - All top surfaces not shown shall be a minimum of 40 bar diameter in length.
 - All reinforcing steel shall be supported on fabricated steel bar supports @ 3'-0" maximum spacing.
 - All steel shall be securely placed and securely tied in place prior to placement of bottom slab concrete. Sliding of sheets into trench or partially hardened concrete will not be acceptable.
- Construction**
- The bottom slab shall be at least 24 hours old before placing elevated concrete. All elevated forms shall remain in place a minimum of 24 hours after sidewalks are poured before removal. After removal, the slab shall be immediately treated with moisture curing compound.
 - All curb inlet tops are to be constructed after final curb along with the curb.
 - Pipe connections to pre-cast structures shall have a minimum of 2" of concrete around the entire pipe within 2' of the outside wall of structure. A minimum of 2" - maximum of 6" clearance is required between the outside pipe wall and the edge of structure.
 - Tamped bedding shall be used around structures, except under paved areas. Tamped bedding shall be finely graded J40 excavated material free from debris, organic material and stones, compacted to 95% maximum density as determined by ASTM D1556, Method 1.0.5.
 - Removable Flexible FIB is required under any portion of paved right of way, including the area within bedding or future public street pavement and under existing or future curb and gutter, median, asphalt path, concrete sidewalk.



CURB INLET FRAME (10" THROAT)

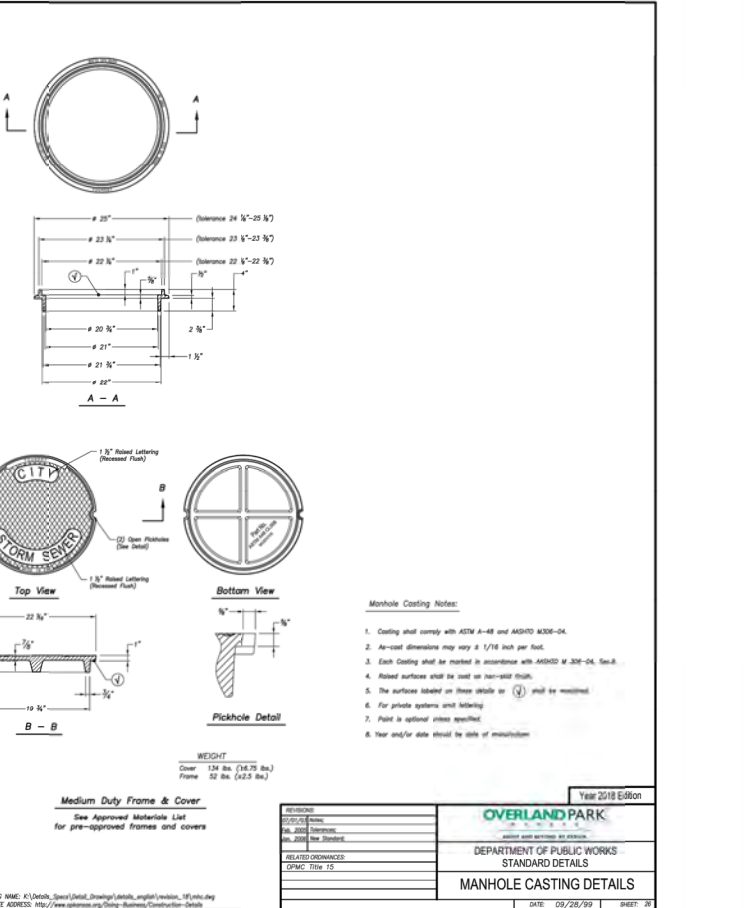
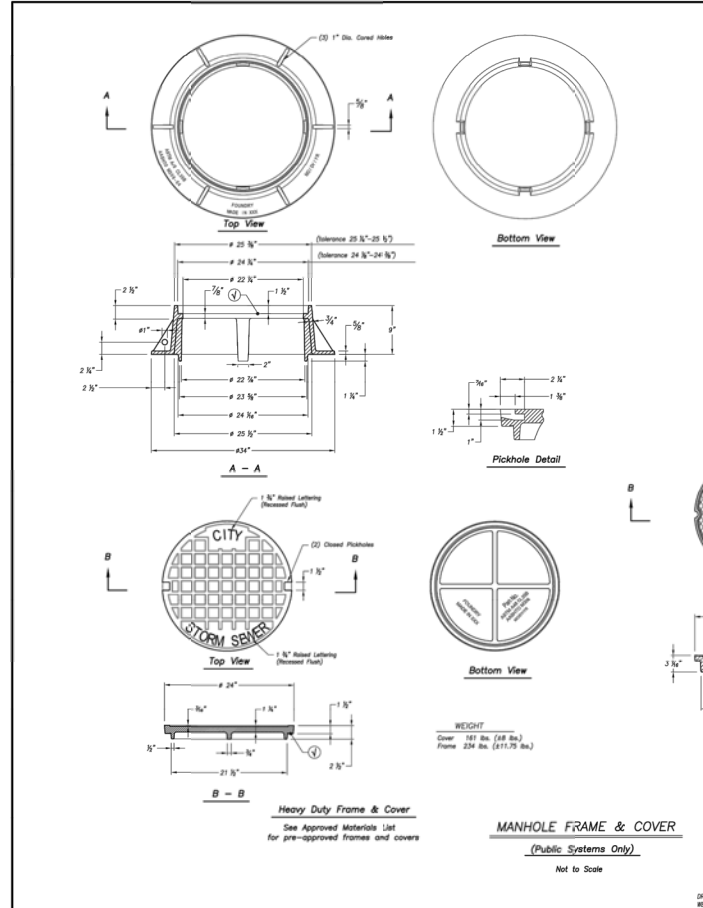
<p>REVISIONS</p> <p>DATE: 05/28/99</p> <p>YEAR 2018 Edition</p>	<p>OVERLAND PARK</p> <p>DESIGN AND CONSTRUCTION STANDARDS</p> <p>DEPARTMENT OF PUBLIC WORKS</p> <p>STANDARD DETAILS</p> <p>CURB INLET DETAILS</p>
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- Precast Manhole Notes**
- Overland Park Municipal Code (OPMC) and Overland Park Design and Construction Standards Manual (OPDCSM) are incorporated, except as otherwise noted.
 - The inside diameter of the manhole shall be 4'-0" for pipe diameters from 12" thru 24". All manhole bases (pre-cast or poured-in-place) shall have No. 4 reinforcing bars placed at 8" centers both ways.
 - All manhole rings and covers shown in plans shall be heavy duty.
 - Standard cast iron manhole steps or steel cone, plastic coated steps may be used. See Approved Materials List for pre-approved steps.
 - Reinforcement in all sections shall equal or exceed A.S.T.M. C-478 specifications.
 - Moisture resistant to be used at all section joints. D-rings may be used for joints below the cone section, but the cone section itself shall not use D-rings.
 - Approved gasket or concrete mortar to be used around pipe in smooth-outs.
 - Flow connections to pre-cast structures shall have a minimum of 2" of concrete around the entire pipe within 2' of the structure.

PRECAST MANHOLE DETAIL

<p>REVISIONS</p> <p>DATE: 06/01/99</p> <p>YEAR 2018 Edition</p>	<p>OVERLAND PARK</p> <p>DESIGN AND CONSTRUCTION STANDARDS</p> <p>DEPARTMENT OF PUBLIC WORKS</p> <p>STANDARD DETAILS</p> <p>PRECAST MANHOLE DETAIL</p>
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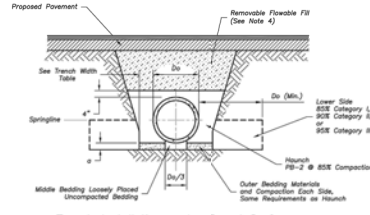


- Manhole Casting Notes**
- Casting shall comply with ASTM A-48 and ASTM A-248.
 - Re-cast dimensions may vary a 1/16 inch per foot.
 - Curb casting shall be treated in accordance with AASHTO D-207-04, Sec. 4.
 - Rebar surface shall be rusted or treated as per AASHTO D-207-04, Sec. 4.
 - The surface labeled on these details as (1) shall be reinforced.
 - For private systems and bidding.
 - Paint is optional unless specified.
 - Year and/or date should be side of manhole.

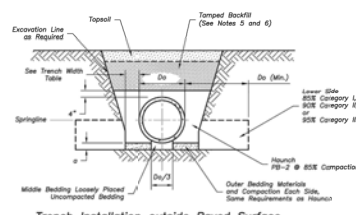
MANHOLE FRAME & COVER

<p>REVISIONS</p> <p>DATE: 09/28/99</p> <p>YEAR 2018 Edition</p>	<p>OVERLAND PARK</p> <p>DESIGN AND CONSTRUCTION STANDARDS</p> <p>DEPARTMENT OF PUBLIC WORKS</p> <p>STANDARD DETAILS</p> <p>MANHOLE CASTING DETAILS</p>
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C:\13186.23\City Standard Details\Storm Plans\13186.23\20201.dwg Layout: City Standard Details (3) --- Wednesday, April 03, 2019, 2:36pm --- Copyright 2019, George Butler Associates/Architect 00212, Professional Engineer 000133, Landscape Architect 000025, Professional Land Surveyor 000039



Trench Installation under Paved Surface



Trench Installation outside Paved Surface

RCP Installation notes:

- Overland Park Municipal Code (OPMC) and Overland Park Design and Construction Standards Manual (OPDCSM) are incorporated, except as otherwise noted.
- Category I material shall be crushed stone or siliceous gravel meeting the requirements of Section 1107 of the Standard Specification, Appropriate Designation PB-2.
- Compaction and soil symbols - i.e. "BSE Category I" - refers to Category I soil material with minimum standard Proctor compaction of BSE.
- Removable Flexible Fill is required as shown on the Trench Installation detail in any paved portion of the right-of-way including the curb, gutter and other median, ramp, parking lot, concrete sidewalk, etc. and other existing concrete, asphalt or other paving material under any future driveway approach, when the location is shown. Flexible Fill shall be considered subsidiary to the pipe installation.
- Temporary backfill shall be used at all other locations unless otherwise noted. Temporary backfill shall be fine-grained soil containing material free from debris, organic matter and stones, compacted to BSE maximum density as determined by ASTM D1557, Method "A".
- Granular fill may be substituted for all or part of temporary backfill. Granular fill shall be AASHTO M-28, as specified in the Standard Specification, compacted to a minimum of 95% of maximum density within 0.25 inches of surface. Temporary backfill shall be used in the top 6" of trench 6" below surface. Topsoil shall be used in the top 6" of trench.
- When the existing grade is not at least 18" above the proposed top of pipe, the trench shall be excavated to 18" below the top of pipe prior to excavation for storm sewer installation, or as approved by the City Engineer.
- Soil in outer bedding and haunch areas shall be compacted to at least the same compaction as specified for the majority of soil in the backfill zone.
- The trench width shall be wider than shown if required for adequate space to obtain the specified compaction in the haunch and bedding zones.
- For trench walls that are within 10 degrees of vertical, the compaction or firmness of the soil in the trench walls and lower side zone need not be considered.
- For trench walls with greater than 10 degree slopes that consist of embankment, the lower side shall be compacted to at least the same compaction as specified for the soil in the overall zone.
- See street repair details for additional backfill requirements under street crossings, curbs, sidewalks and other structures in right-of-way.

TRENCH WIDTH TABLE

Pipe Size (inches)	Minimum Trench Width (inches)
12 - 18	6
21	6.5
24	7
27	8
30	8.5
33	9
36	10
42	11
48	12.5
54	13.5
60	15
66	16
72	15.5
78	15.5
84	16
90	16.5
96	17
102	17.5
108	18

TABLE OF FILL DEPTHS BELOW PIPE

D	Soil	min. in	max. in
12 - 18	Soil	3"	6"
60" & smaller	Soil	3"	6"
66" & larger	Soil	6"	12"

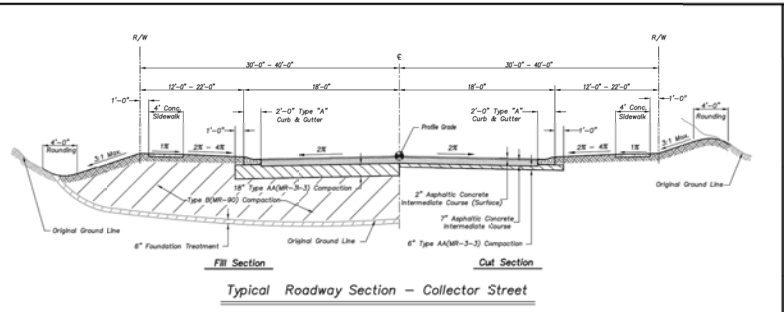
RCP INSTALLATION

No Scale

Year 2018 Edition

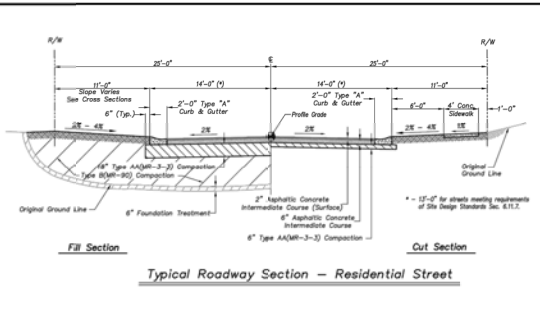
OVERLAND PARK
DEPARTMENT OF PUBLIC WORKS
STANDARD DETAILS
RCP INSTALLATION DETAIL

DATE: 09/28/99 SHEET: 3



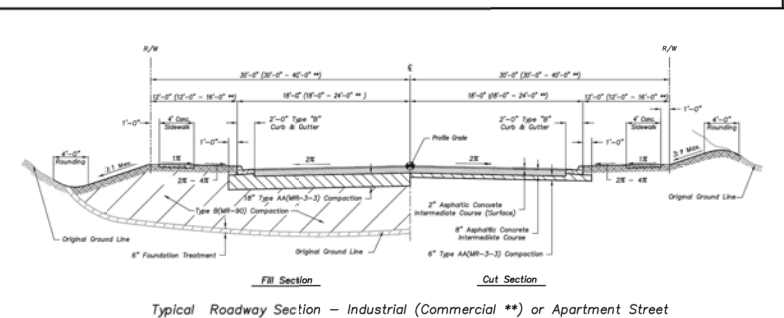
Typical Roadway Section - Collector Street

- Notes:
- The Overland Park Municipal Code (OPMC) and Overland Park Design and Construction Standards Manual (OPDCSM) are incorporated, except as otherwise noted.
 - Sidewalk and Earthwork requirements shall be as specified in OPDCSM.



Typical Roadway Section - Residential Street

- Notes:
- The Overland Park Municipal Code (OPMC) and Overland Park Design and Construction Standards Manual (OPDCSM) are incorporated, except as otherwise noted.
 - Sidewalk and Earthwork requirements shall be as specified in OPDCSM.



Typical Roadway Section - Industrial (Commercial **) or Apartment Street

- Notes:
- The Overland Park Municipal Code (OPMC) and Overland Park Design and Construction Standards Manual (OPDCSM) are incorporated, except as otherwise noted.
 - Sidewalk and Earthwork requirements shall be as specified in OPDCSM.

Year 2018 Edition

OVERLAND PARK
DEPARTMENT OF PUBLIC WORKS
STANDARD DETAILS
ASPHALT STREET SECTION

DATE: 09/28/99 SHEET: 4

DATE: 4/3/19
DESIGN BY: KHS
DRAWN BY: DS
PROJECT NO.: 13186.23
SHEET NO.: 6
TOTAL SHEETS: 6

9801 Renne Boulevard
Lenexa, Kansas 66219
913.492.0400
www.gbateam.com

Mission Storm Repairs
50th and Dearborn
City of Mission, Kansas

NO.	DATE	REVISIONS	BY	APPROVED

WEST 50TH AND DEARBORN STORM REPAIR QUANTITIES

ITEM NO	DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	EXTENSION
1	MOBILIZATION	LS	1	\$ 15,000	\$ 15,000
2	CLEARING, GRUBBING & DEMOLITION	LS	1	\$ 36,000	\$ 36,000
3	EROSION & SEDIMENT CONTROL	LS	1	\$ 10,000	\$ 10,000
4	TRAFFIC CONTROL	LS	1	\$ 6,000	\$ 6,000
5	5' JUNCTION MANHOLE	EA	1	\$ 6,000	\$ 6,000
6	6'x4' CURB INLET	EA	1	\$ 6,500	\$ 6,500
7	5'x3' CURB INLET	EA	2	\$ 5,000	\$ 10,000
8	4'x4' JUNCTION BOX	EA	1	\$ 999	\$ 999
9	24" RCP END SECTION	EA	1	\$ 1,500	\$ 1,500
10	24" RCP	LF	333	\$ 145	\$ 48,285
11	CURB & GUTTER - TYPE B	LF	214	\$ 40	\$ 8,560
12	STREET PATCH	SY	0	\$ 25	\$ -
13	ASPHALT MILL & OVERLAY	SY	567	\$ 25	\$ 14,175
14	DRIVEWAY APPROACH	SY	21	\$ 60	\$ 1,260
15	FLOWABLE FILL - ABANDONED PIPE	CY	12	\$ 125	\$ 1,500
16	SODDING & FERTILIZING	SY	121	\$ 6	\$ 726
STORM REPAIR IMPROVEMENTS TOTAL					\$ 166,505

WEST 50TH AND DEARBORN - BID ALTERNATE SLIP LINE CMP CULVERT

ITEM NO	DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	EXTENSION
BA1	SLIP LINE	LF	34	\$ 530	\$ 18,020
BA2	INLET APRON REPAIR	SY	18	\$ 25	\$ 450
SLIP LINE IMPROVEMENTS TOTAL					\$ 18,470

City of Mission	Item Number:	5.
ACTION ITEM SUMMARY	Date:	July 10, 2019
PUBLIC WORKS	From:	Brent Morton

Action items require a vote to recommend the item to full City Council for further action.

RE: BHC Rhodes Engineering and GIS Services for Asset Management (Streets)

RECOMMENDATION: Approve a task order with BHC Rhodes to perform services related to asset management, condition inventories, and conceptual program costs for Mission residential street network in an amount not to exceed \$9,000.

DETAILS: During the initial planning phases for the City's comprehensive Street Maintenance Program, all streets were given a Pavement Condition Index (PCI) score. PCI measures the condition of the pavement surface and the smoothness of the road. A numerical rating is assigned to each section of road, with 0 being the worst and 100 being the best.

At the time the program was developed, the Council set a goal of maintaining all streets in Mission at a PCI rating of 70 or higher. Using the PCI scores, street sections were assigned the treatment best suited for the road's current condition. This information was then used to develop budget estimates to aid the City is reaching its goal of touching/treating every residential street in the City within 8-10 years.

During the initial years of the street program staff discovered that about 50% of the residential streets targeted for a mill and overlay treatment could not structurally handle that treatment and would require full-depth reconstruction, which came with a substantial price tag.

Recognizing the original street program would need to be completely re-evaluated and reprioritized, the City worked to complete an updated pavement condition rating and geotechnical analysis. In 2019, the city signed a task order with BHC Rhodes to start compiling all asset information into one GIS location with the goal of building long-term planning, budgeting, and asset management tools. Initial work with BHC Rhodes focused on the stormwater system. The City is now ready to turn its attention to streets, in order to develop a new residential street maintenance program to determine which treatments will provide the best results for long term street preservation.

CFAA CONSIDERATIONS/IMPACTS: NA

Related Statute/City Ordinance:	
Line Item Code/Description:	01-20-207-03
Available Budget:	\$40,000



May 31, 2019

Ms. Laura Smith
City Administrator
City of Mission
6090 Woodson Street
Mission, Kansas 66202

**Re: Proposal for Engineering & GIS Services
Asset Management (Streets)
City of Mission, Kansas**

Dear Ms. Smith:

Brungardt Honomichl & Company, P.A. (BHC RHODES) is pleased to submit the following proposal for engineering and GIS services to the City of Mission.

PROJECT UNDERSTANDING

Based on the information provided, we understand that you are seeking engineering and Geographic Information System (GIS) services related to asset management regarding "street conditions", capital programming for street maintenance and improvements, and related tasks for the City of Mission.

SCOPE OF SERVICES

BHC RHODES proposes to provide professional services for the City on a time and expense basis that may involve the following activities requested by City staff focusing on "streets":

1. Data Input & Analysis
 - 1.1. Summarize provided data and prepare list of missing/incomplete/outdated information to be collected under subsequent City efforts
 - 1.2. Meet with City staff to ensure expectations of datasets are clearly understood
 - 1.3. Layout plan for future data collection and assembly/completion of final street-related datasets
 - 1.4. Assist City staff with preparation of RFQs/RFPs for future data collection/inventory/rating work
 - 1.5. Assist City staff and officials in evaluating available data, creation of ranking scenarios for potential street improvements, and provide conceptual program costs to assist in City funding decisions
2. Deliverables (to be requested by City)
 - 2.1. Written summary of provided data and incomplete/outdated system information related to streets that should be completed

- 2.2. Compiled version of available street-related data that can be assembled electronically in ESRI GIS format, along with exports of information to KMZ file format.
- 2.3. Conceptual cost estimates for identified City street replacement/reconstruction/enhancement to assist City in developing an overall program for infrastructure maintenance and upgrades.
- 2.4. Attend meetings with City staff and/or elected officials to discuss findings, criteria, needs, and alternatives.
- 2.5. Possible electronic and/or paper copies of specific maps to be identified by City.

ADDITIONAL SERVICES

The services provided for this project are limited to those listed in the Scope of Services and budget amount approved by the City. Any additional services will be performed at an hourly rate or a lump sum basis as agreed to prior to initiating the additional service.

FEEES

BHC RHODES will perform the Scope of Services described above utilizing the attached hourly rates (Exhibit B) with maximum limit of **\$9,000.00**. Reimbursable expenses such as mileage, printing, etc. will be billed inclusive to the maximum limit.

SCHEDULE

BHC RHODES proposes to commence work upon your acceptance of this proposal, written authorization to proceed, and receipt of reference documents. We will establish a mutually agreed upon schedule for the work as well as production of project deliverables prior to beginning the work.

PROVIDED BY CLIENT

Client will provide:

1. Any additional existing data, reports, analysis, input associated with the focus on street assets.

AGREEMENT

This proposal along with attached Exhibit A (Terms and Conditions) and Exhibit B (Standard Hourly Rates) represent the entire agreement between the Client and BHC RHODES. Your signature below will serve as acknowledgement of your acceptance of this proposal and the attached terms and conditions. Please return one signed copy of this proposal to us.

Thank you for the opportunity to provide this proposal for engineering and GIS services. We look forward to continuing to build our working relationship. If you have any questions concerning this proposal, please contact me at your convenience.

Sincerely,



Randall J. Gorton, PE, PTOE
Vice-President | Public Works Segment Lead



David Nolte, PE
Project Manager

Proposal Accepted By: _____

Type/Print Name: _____

Date: _____

Attachments: Exhibit A Terms and Conditions, Exhibit B Standard Hourly Rates

EXHIBIT A

Terms and Conditions

1.0 - COMPENSATION AND TERMS OF PAYMENT

Client shall pay Consultant for performance of services in accordance with fees presented in the proposal for professional services, which is attached hereto and incorporated by reference as part of this agreement. Consultant shall submit invoices every four weeks based on completion of the individual work items described in the Fee Schedule. Payment shall be made within 30 days after receipt of invoice.

If the Client is in breach of the payment terms or otherwise is in material breach of this Agreement, the Consultant may suspend the performance of services upon five (5) calendar days' notice to the Client. The Consultant shall have no liability to the Client, and the Client agrees to make no claim for any delay or damage as a result of such suspension caused by any breach of this Agreement by the Client. Upon receipt of payment in full for all outstanding amounts due the Client, or curing of other such breach which caused the Consultant to suspend services, the Consultant shall resume services and there shall be equitable adjustment to the remaining project schedule and fees as a result of such suspension.

2.0 - CHANGED CONDITIONS

If, during the term of this Agreement, circumstances or conditions that were not originally contemplated by or known to the Consultant are revealed, to the extent that they affect the scope of services, compensation, schedule, allocation of risks or other material terms of this Agreement, the Consultant may call for renegotiation of appropriate portions of this Agreement. The Consultant shall notify the Client of the changed conditions necessitating renegotiation, and the Consultant and the Client shall promptly and in good faith enter into renegotiation of this Agreement to address the changed conditions. If terms cannot be agreed to, the parties agree that either party has the absolute right to terminate this Agreement in accordance with the Termination provision hereof.

Client may request changes to the scope of services by altering or adding to the Services to be performed. If Client so requests, Consultant will return Client an Agreement for Additional Services detailing the additional scope and fees. Additional terms and conditions may be added or changed only by written amendment to this Agreement signed by both parties.

3.0 - GENERAL OBLIGATIONS OF CONSULTANT

Consultant shall exercise the same degree of care, skill, and diligence in the performance of the Services as is ordinarily provided by members of the same profession currently practicing under similar circumstances in the performance of such services. All of the Services shall be performed by qualified personnel.

Consultant shall exercise usual and customary professional care in its effort to comply with all rules or regulations of the federal, state, or other government body or any administrative agency pertaining to the performance of the work hereunder.

4.0 - ACCEPTANCE

Client agrees that execution of this Agreement is a material element of the consideration Consultant requires to execute the Services, and if Services are initiated by Consultant prior to execution of this Agreement as an accommodation for Client at Client's request, both parties shall consider that commencement of Services constitutes formal acceptance of all terms and condition of this agreement. Additional terms and conditions may be added or changed only by written amendment to this Agreement signed by both parties. In the event Client uses a purchase order or other form to administer this Agreement, the use of such form shall be for convenience purposes only and any additional or conflicting terms it contains are stricken. This Agreement shall not be assigned by either party without prior written consent of the other party.

5.0 - GENERAL OBLIGATIONS OF THE CLIENT

The Client shall provide payment to the Consultant as provided in 1.0, Compensation and Terms of Payment.

The Client shall monitor the performance of the Consultant's work and shall notify them of any concerns and/or modifications required to the Services.

The Client shall make available to the Consultant any documents, drawings, electronic files, specifications, files or other information necessary in the execution and completion of the Services. The Client shall furnish, at the Client's expense, all information, requirements, reports, and instructions required by this Agreement. The Consultant may use such information, requirements, reports, and instructions in performing its services and is entitled to rely upon the accuracy and completeness thereof.

6.0 - OWNERSHIP OF DOCUMENTS

All reports, drawings, specifications, computer files, field data, notes, and other documents and instruments prepared by the Consultant as instruments of service shall remain the property of the Consultant. The Consultant shall retain all common law, statutory and other reserved rights, including the copyright thereto, subject to laws and regulations.

Brungardt Honomichl & Company, P. A.
Engineering & GIS Services
Asset Management (Streets)
City of Mission, Kansas

7.0 - INSURANCE

Consultant shall carry and maintain throughout the performance of the Services insurance acceptable to the Client in the following amounts:

1. Workers Compensation, including occupational disease.
(Statutory Limits)
2. General (Public) Liability
Bodily Injury \$1,000,000
Property Damage \$1,000,000
3. Automobile Liability (hired, owned, non-owned)
Bodily Injury \$1,000,000
Property Damage \$1,000,000
4. Professional Liability \$1,000,000

Upon Client's request, the Consultant shall provide the Client with certificates of insurance evidencing the coverage in effect. After such policies become effective, none of such policies shall be canceled by the insurance company except after ten days notice in writing to the Client.

8.0 - INDEMNIFICATION

The Consultant shall indemnify the Client and hold him and his officers harmless from any damage, expense, and liability or claim therefore on account of any injury, including death, resulting therefrom, or damage sustained by any person or persons (including the Consultant's employees) by reason of any negligent act, omission or neglect on the part of the Consultant's employees.

The Client shall also indemnify the Consultant and hold him and his officers harmless from any damage, expense, and liability or claim therefore on account of any injury, including death resulting therefrom, or damage sustained by any person or persons (including the Client's employees) by reason of any negligent act, omission, or neglect on the part of the Client's employees.

Neither the Client nor the Consultant shall be obligated to indemnify the other party in any manner whatsoever for the other party's own negligence.

9.0 - LIMITATION OF LIABILITY

To the maximum extent permitted by law, the Client agrees to limit the Consultant's liability for the Client's damages to the sum of \$25,000 or the Consultant's Fee, whichever is greater. This limitation shall apply regardless of the cause of action or legal theory pled or asserted.

10.0 - TERMINATION

Either party may terminate this Agreement at any time by giving the other party five days written notice of such termination. Immediately upon receipt of Notice of Termination, the Consultant shall discontinue Services and incur no further obligation or expenses. The Consultant shall be paid for all work completed prior to the effective date of such termination.

The Consultant shall not assign, transfer, or sublet this Agreement or any interest herein without the prior written consent of the Client.

11.0 - NON-DISCRIMINATION

There shall be no discrimination against any person employed pursuant to this Agreement in any manner forbidden by law.

12.0 - STATUS

The Consultant shall, during the entire term of this Agreement, be construed to be an independent contractor, and in no event shall any of its personnel be construed to be an employee of the Client.

13.0 - GOVERNING LAW AND JURISDICTION

The Client and Consultant agree that this Agreement and any legal actions concerning its validity, interpretation, and performance shall be governed by the laws of Kansas. It is further agreed that any legal action between the Client and the Consultant arising out of this Agreement or the performance of the services shall be brought in a court of competent jurisdiction in Kansas.

Exhibit B
Standard Hourly Rates (2019)

Title	Rate
Principal Program Manager	\$170.00
Project Manager	165.00
Sr. Traffic Engineer	165.00
Sr. Project Engineer	165.00
Traffic Engineer	135.00
Utilities Manager	135.00
Project Engineer	135.00
Resident Project Engineer	135.00
Design/Staff Engineer	110.00
Staff Engineer	110.00
Sr. Designer	110.00
Designer	95.00
Senior Eng. Technician	115.00
Engineering Technician	95.00
GIS Supervisor	120.75
GIS Analyst 1	78.75
GIS Analyst 2	89.25
GIS Analyst 3	99.75
Intern	55.00
Clerical	50.00
Sr. Land Surveyor	150.00
Survey Manager	150.00
Project Surveyor	125.00
Sr. Survey Technician	110.00
Survey Technician	75.00
Drafter	70.00
Crew Chief	95.00
Crew Member	75.00

Reimbursable Expenses:

<u>DESCRIPTION</u>	<u>UNIT</u>	<u>PRICE</u>
A. Passenger Vehicle	Per mile	IRS rate
B. Survey Vehicle	Per mile	\$0.70
C. Telephone/Cellular/Long Distance		Actual Cost
D. In House Reproduction	Sq. Ft.	\$0.15
F. Freight & Postage		Actual Cost
G. Total Station Equipment fee	Per Hour	\$15.00
H. GPS Equipment fee	Per Hour	\$30.00
I. Robotic Total Station	Per Hour	\$40.00
J. Laser Scanning	Price per Project	

City of Mission	Item Number:	6.
ACTION ITEM SUMMARY	Date:	June 28, 2019
Public Works	From:	Brent Morton

Action items require a vote to recommend the item to full City Council for further action.

RE: Foxridge (Lamar to 51st Street) Engineering Services Contract

RECOMMENDATION: Approve a task order with GBA for Foxridge engineering services in an amount not to exceed \$49,831.80.

DETAILS: The Foxridge project (Lamar to 51st Street) is included in the City's Capital Improvement Program (CIP) for construction in 2021 at an estimated cost of \$5,008,584. In addition to the roadway improvements, the project includes a number of stormwater improvements to address ongoing groundwater problems and the installation of sidewalks to improve pedestrian safety. Both the stormwater and sidewalk improvements could trigger the need for the City to acquire additional right-of-way.

Staff is recommending Council approval for the survey portion of the design contract. By separating out this work and accelerating the schedule (2019 vs. 2020), staff anticipates it will allow for the project to move more efficiently through the easement acquisition process. This should help not only with design phase considerations, but will assist in more accurate cost estimates, and would allow more time for the easement acquisition to be accomplished.

The attached task order covers surveying, property research, conceptual hardshell design, estimates for right-of-way needs and project meetings in an amount not to exceed \$49,831.80.

CFAA CONSIDERATIONS/IMPACTS: The project seeks to improve connectivity and meets the objective that the City plan and construct sidewalks to maximize use and safety for all users.

Related Statute/City Ordinance:	N/A
Line Item Code/Description:	Capital Improvement Fund
Available Budget:	\$50,000

June 28, 2019

Mr. Brent D. Morton
Public Works Superintendent
4775 Lamar Ave.
Mission, KS 66202

SUBJECT: Foxridge Drive Phase II – Conceptual Survey and Engineering Services (51st Street to Lamar Avenue)

Dear Brent:

As requested, GBA has prepared this letter proposal to provide the following on-call survey and engineering services for Foxridge Drive Phase II, as requested by the City of Mission staff:

1. Preliminary survey including property research, full topography and basemapping for the use in conceptual design.
2. Conceptual hardshell design of the Foxridge Drive Phase II improvements. The purpose of this design will be to determine a conceptual cost estimate and right of way needs for budgeting purposes.

These services would be provided as a separate task order under GBA's current Master Agreement for on-call engineering services with the City, which became effective on January 1, 2019.

SCOPE OF SERVICES

To complete the required services for Foxridge Drive, it is anticipated that GBA personnel will need to provide the following engineering services:

- 1) Preliminary Survey
 - a) Research - titlework ordering and filing (13 Parcels)
 - b) Establish horizontal and vertical control
 - c) Locate section corners, property lines and highway R/W
 - d) Survey storm and sanitary inverts
 - e) Topographic survey (0.85 Miles)
 - f) Office basemapping with title work (13 parcels, 3 Plats)
- 2) Conceptual Hardshell Design
 - a) Project setup
 - b) Generate conceptual horizontal and vertical alignment
 - c) Develop typical section
 - d) Meet with City to discuss future geometry
 - e) Generate conceptual template and cross sections
 - f) High level wall analysis
 - g) Develop ROW taking areas
 - h) Develop conceptual cost estimate for comparison of City's estimate
 - i) Meet with City to discuss findings

j) Address City comments

SCHEDULE

Unless circumstances arise beyond reasonable control, GBA personnel will commit to completing these survey and engineering design services by September 30, 2019.

PROPOSED FEES

GBA will complete the project work tasks described in the above listed Scope of Services on an hourly rate plus expenses basis. Invoices will be submitted for all work completed during each previous month, including an itemized task summary as required by the City staff, and will become due and payable within thirty (30) days.

Our anticipated fees to provide these survey and engineering services are expected to be approximately forty-nine thousand eight hundred thirty-one dollars (\$49,831.00).

We appreciate the opportunity to serve you and the City on this very important project. If you agree with the project scopes, schedules, and fees, please sign the following project authorization form and return the original back to GBA at our attention.

Respectfully submitted,

GEORGE BUTLER ASSOCIATES, INC.



James R. Jarrett,

Principal / Vice President



P.E.

Bryan Blizzard, P.E.

Associate / Project Manager

I hereby authorize George Butler Associates, Inc. (GBA) to perform the tasks in the above listed Scope of Services. I acknowledge and agree with the listed project schedule and fee. I further agree to pay the monthly invoices from GBA for the services provided within thirty days of receipt.

Authorized by: _____

Title: _____

Date: _____

CLASSIFICATION	PRI	SRASC	ASC	SRSP	Proj Lead	Senior AES	Project AES	Design AES	Staff AES	Prj tech	ST3	SRPLS	PLS	SP2	CLI	TOTAL	GPS	Mileage	Mileage	Additional	TOTAL	TOTAL	
	\$276	\$232	\$195	\$180	\$170	\$160	\$140	\$110	\$108	\$105	\$100	\$130	\$125	\$170	\$95	HOURS	Units	Personal	Survey	Expenses	EXPENSES	COST	
Survey																							
Project Coordination, Research, Titlework Ordering/Filing, Utility Locating(13 Parcels*400/Parcel)										4		3	8			15					\$5,200.00	\$5,200.00	\$7,010.00
Horizontal and Vertical Control														10		10			\$34.80			\$34.80	\$1,734.80
Section Corners, Property and Highway R/W														16					\$34.80			\$34.80	\$2,754.80
Storm and Sanitary Inverts														24		24				\$52.20		\$52.20	\$4,132.20
Topography (0.85 Miles)														64		64	\$2,400.00		\$174.00		\$2,574.00	\$13,454.00	
Basemapping with Title work (13 parcels, 3 Plats)										96		1	4			101					\$0.00	\$10,710.00	
																0						\$0.00	\$0.00
Survey Subtotal	0	0	0	0	0	0	0	0	0	100	0	4	12	114	0	214	\$2,400.00	\$0.00	\$295.80	\$5,200.00	\$7,895.80	\$39,795.80	
Conceptual Hardshell																							
Project setup							2		4							6						\$0.00	\$712.00
Generate conceptual horizontal and vertical alignment					2		4									6						\$0.00	\$900.00
Develop typical section					2		2									4						\$0.00	\$620.00
Meet with City to discuss future geometry					2		2									4						\$0.00	\$620.00
Generate conceptual template and cross sections							6		16							22						\$0.00	\$2,568.00
High level wall analysis					2		4		2							8						\$0.00	\$1,116.00
Develop ROW taking areas					2		2		4							8						\$0.00	\$1,052.00
Develop conceptual cost estimate for comparison of City's estimate					2		4		4							10						\$0.00	\$1,332.00
Meet with City to discuss findings					2		2									4						\$0.00	\$620.00
Address City comments							2		2							4						\$0.00	\$496.00
																0						\$0.00	\$0.00
Conceptual Hardshell Subtotal	0	0	0	0	4	0	30	0	4	0	0	0	0	0	0	76	\$0.00	\$0.00	\$0.00	\$0.00	\$0.00	\$0.00	\$10,036.00
GRAND TOTAL FOR DESIGN SERVICES	0	0	0	0	4	0	30	0	4	100	0	4	12	114	0	290	\$2,400.00	\$0.00	\$295.80	\$5,200.00	\$7,895.80	\$49,831.80	

City of Mission	Item Number:	7.
DISCUSSION ITEM SUMMARY	Date:	June 28, 2019
ADMINISTRATION	From:	Emily Randel

Discussion items allow the committee the opportunity to freely discuss the issue at hand.

RE: Facility Conservation Improvement Program

DETAILS: The Kansas Corporation Commission (KCC) offers the Facility Conservation Improvement Program (FCIP) to achieve energy conservation through energy performance contracts. Cities may take advantage of the program's single procurement energy performance contracts that include the cost of an energy study, audit, improvement or equipment design and costs associated with the implementation of approved improvements.

The Mission Sustainability Commission hosted Lynn Retz, Energy Program Director in the Energy Division of the KCC and David Carter from the Kansas State University Kansas Energy Program at their meeting in June. The two shared information about the FCIP and the technical support that would be available to guide the City of Mission through an energy audit and conservation efforts at the Sylvester Powell, Jr. Community Center and potentially other City-owned facilities. Given the City Council priority of an energy audit at the Community Center in 2019, the program seems promising for further investigation. The program eliminates the need for issuing a request for qualifications and uses KCC's list of pre-approved Energy Service Companies (ESCOs).

KCC staff is updating the pre-approved list and their program materials in June and July. Following those updates, if the City Council is supportive, City of Mission staff will reach out to the KCC staff to initiate participation in the program.

The program steps include:

- Check references of updated list of ESCOs.
- Schedule walk-throughs with selection of ESCOs
- ESCO presentations
- Consideration of the selection of an ESCO by the Community Development Committee, City Council
- Investment grade audit performed
- KCC staff review audit report
- Consideration of recommendations from report, development of contract
- Implementation

CFAA IMPACTS/CONSIDERATIONS: N/A

Related Statute/City Ordinance:	n/a
Line Item Code/Description:	n/a
Available Budget:	n/a

FCIP



The Facility Conservation Improvement Program (FCIP), administered by the Energy Division, facilitates the implementation of energy conservation and other improvements in state, municipal, county and educational facilities.

How does FCIP work?

Using FCIP, energy conservation is achieved through an energy performance contract with an energy service company, known as an ESCO. In brief, it is a project approach that uses energy savings to pay for the cost of new energy efficient equipment and systems over time. More specifically, it involves a single procurement contract that:

- covers everything from initial design and engineering through installation, startup, and measurement and verification of outcomes;
- identifies energy savings improvements sufficient to pay for all costs associated with implementing the project (i.e., equipment, materials, labor, fees, bonds, permits, and debt service) over the life of the contract.

Who can participate?

Kansas Statutes Authorize Energy Performance Contracting for Political Subdivisions and State Agencies

KSA 75-37,125 allows political subdivisions and state agencies to enter into a contract or lease-purchase agreement for an energy conservation measure, which is defined as an energy study, audit, improvement or equipment designed to provide energy and operational cost savings at least equivalent to the amount expended for the study/audit/improvement/equipment over a period of not more than 30 years after the equipment or improvement is installed or becomes operational.

What are the benefits?

In a conventional approach to replacing energy equipment, multiple contracts and often multiple firms, may be involved in designing a project, purchasing equipment, installing equipment, and commissioning. Even under a design/build contract, which integrates these processes under a single company, once the project has been accepted, the long-term operational risk lies with the customer.

In contrast, in energy performance contracting, one or more Energy Service Companies (ESCOs) will conduct a preliminary assessment of the energy-savings potential of your facility. If the potential seems promising, you select an ESCO to conduct a comprehensive Investment Grade Audit that identifies all energy efficiency opportunities for the facility. You work with the ESCO to select the energy improvements that best meet your needs and budget, resulting in a contract that specifies scope of project, compensation, liability, accountability, guarantee of savings, and post installation monitoring.

Because energy performance contracting pays for new equipment with the savings from reduced energy usage, the best candidates for a project are facilities with higher energy bills and outdated, energy-inefficient equipment.

How can I get involved?

FCIP is working to prepare a new contract for pre-approved ESCOs that will go out for bid later in 2018 and this will include program guidance. In the meantime, political subdivisions and state agencies would need to follow the appropriate procurement processes to select an ESCO for any energy performance contract project. However, Energy Division staff in partnership with Kansas State University – Engineering Extension staff would be available to answer questions and provide assistance, including oversight assistance during the Investment Grade Audit and Energy Performance Contract process. FCIP is a fee-funded State program, fees would apply for oversight assistance.

City of Mission	Item Number:	8.
DISCUSSION ITEM SUMMARY	Date:	July 10, 2019
ADMINISTRATION	From:	Brent Morton

Discussion items allow the committee the opportunity to freely discuss the issue at hand.

RE: Update on Speed Limit and Traffic Signage - 51st Street, east of Lamar Avenue

DETAILS: At the March Community Development Committee meeting, a resident voiced concern about the speed limit (25 mph) on 51st Street, east of Lamar Ave and requested a speed limit increase. During the discussion, the Council also expressed traffic safety concerns for this roadway section, and GBA was tasked with an intersection sight distance evaluation and speed limit recommendation.

GBA has completed an assessment of this corridor and does not recommend an increase in speed. They did recommend additional signs and changing the placement of existing signs. The attached memorandum from GBA details the specific changes and updates. The signage installation was completed by the Public Works Department on June 28.

CFAA IMPACTS/CONSIDERATIONS: N/A

Related Statute/City Ordinance:	NA
Line Item Code/Description:	NA
Available Budget:	NA



DESIGN MEMORANDUM

To: Mrs. Laura Smith (City Administrator)
From: David J. Mennenga, P.E., PTOE
Date: May 2, 2019
Subject: 51st Street (Lamar Avenue to Maple Avenue) – Intersection Sight Distance Evaluations

As requested, GBA's traffic engineers recently completed in-field measurements of the intersection sight distance (ISD) conditions provided to side street drivers along the 51st Street corridor in Mission, KS. Between the intersections with Lamar Avenue and Maple Avenue, 51st Street is generally a two-lane roadway with a posted speed limit of 25 miles per hour (mph). A school-related speed zone is in effect for the pedestrian-actuated crossing signal at the Woodson Road intersection during the hours of 7:20 – 8:20 a.m. in the morning and 2:50 – 3:50 p.m. in the afternoon. On-street parking is prohibited along the entire length of this study roadway segment.

Measurements of the existing ISD conditions for all intersections along the 51st Street study segment were made using the methodology prescribed by the American Association of State Highway and Transportation Officials (AASHTO) in the latest edition (i.e., 7th ed. published in 2018) of "*A Policy on Geometric Design of Highways and Streets*." This procedure utilizes a driver's eye height of 3.5 feet for the side street vehicle at a point located 14.5 feet back from the edge of the traveled way, as well as for the driver of an approaching vehicle in the near-side lane for right-turning maneuvers and in the far-side lane for left-turning maneuvers. Based upon the currently posted 25-mph speed limit on 51st Street, the AASHTO requirements for adequate ISD conditions to be provided are as follows:

- Stopping Sight Distance (for crash avoidance) = 155 feet
- Case B1, Side Street Left Turn from Stop = 280 feet
- Case B2, Side Street Right Turn from Stop = 240 feet

The following table summarizes the results of the in-field measurements completed by GBA personnel and indicates whether a given location provides satisfactory ISD conditions for the side street drivers looking each respective direction (i.e., east or west) from the stopped location. It should be noted that in cases where more than 500 feet of ISD was apparently available, no specific measurements were made.

Location	Looking West	Adequate ISD?	Looking East	Adequate ISD?
Southbound Vehicles at Drive for “The Retreat at Mission” (STOP sign)	Past Lamar Avenue signal	✓	260’	✓
Southbound Vehicles at Dearborn Street (Signalized)	>500’	✓	245’	X
Northbound Vehicles at Woodson Road (STOP sign)	290’	✓	>500’	✓
Southbound Vehicles at Outlook Road (STOP sign)	440’	✓	>500’	✓
Northbound Vehicles at Outlook Road (STOP sign)	520’	✓	410’	✓
Southbound Vehicles at Reeds Road (STOP sign)	>500’	✓	295’	✓
Northbound Vehicles at Reeds Road (STOP sign)	>500’	✓	>500’	✓
Northbound Vehicles at Maple Avenue (STOP sign)	>500’	✓	>500’	✓

As noted in the table above, there are only two existing locations where the current ISD conditions were determined to be marginal and/or deficient based upon GBA’s field reviews:

- For southbound vehicles exiting “The Retreat at Mission” driveway, there is just enough sight distance available (i.e., 260 feet) looking to the east since left-turning vehicles will only need to cross the westbound lane as they make their turning maneuvers. Still, it is recommended that the City install a “T-intersection Ahead” warning sign (MUTCD designation W2-2) near the crest of the hill to the east of this driveway to advise approaching westbound drivers on 51st Street that turning vehicles could be present. Also, there is an existing School Crossing sign (MUTCD designation S1-1) located just east of this driveway that somewhat blocks the southbound drivers’ vision looking to the east. We would recommend that this sign be relocated approximately 50’ east from its current location to provide better line-of-sight for drivers exiting the apartment complex driveway.

- Drivers looking to the east from the southbound Dearborn Street intersection obviously had previous well-documented sight distance limitations that resulted in the installation of the existing traffic signal controls. Therefore, there are no additional traffic control measures required to address the sight distance deficiencies at this location.

During the field investigations, GBA personnel did note the absence of one “Traffic Signal Ahead” warning sign (MUTCD designation W3-3) for the eastbound vehicles approaching the pedestrian-actuated school crossing signal at the Woodson Road intersection. We would recommend that this sign be installed for the eastbound direction on 51st Street just to the east of the Dearborn Street intersection, about 200’ prior to the existing school speed zone sign assembly.

Based upon our completed traffic engineering reviews, we believe that the currently posted 25-mph speed limit is appropriately set, from an intersection safety perspective. At this time, no vehicle speed / classification traffic counts have been completed for this study corridor. We understand that there has been some citizen input requesting that the speed limit along 51st Street be raised by the City. It should be noted that for a speed limit increase to 30-mph along 51st Street, the AASHTO requirements for adequate ISD conditions would correspondingly increase as follows:

- Stopping Sight Distance = 200 feet
- Case B1, Side Street Left Turn from Stop = 335 feet
- Case B2, Side Street Right Turn from Stop = 290 feet

Even this 5-mph increase of the posted speed limit could in turn create unsafe sight distance conditions at two additional locations along the 51st Street study corridor (highlighted with **red** text in the table on Page 2): for northbound vehicles looking west from the Woodson Road intersection, and for southbound vehicles looking east from Reeds Road. For this reason, we would not recommend any change to the posted 25-mph speed limit on 51st Street, and instead believe that a reasonable level of enforcement should continue to be employed by the City of Mission police to regulate vehicle travel speeds along this corridor.

cc: KGM, file



Southbound Apartment Drive looking to the West



Southbound Apartment Drive looking to the East



Southbound Dearborn Street looking to the West



Southbound Dearborn Street looking to the East



Northbound Woodson Road looking to the West



Northbound Woodson Road looking to the East



Southbound Outlook Road looking to the West



Southbound Outlook Road looking to the East



Northbound Outlook Road looking to the West



Northbound Outlook Road looking to the East



Southbound Reeds Road looking to the West



Southbound Reeds Road looking to the East



Northbound Reeds Road looking to the West



Northbound Reeds Road looking to the East



Northbound Maple Avenue looking to the West



Northbound Maple Avenue looking to the East