

REVISED
City of Mission
Regular Meeting Agenda
Wednesday, February 21, 2018
7:00 p.m.
Mission City Hall

If you require any accommodations (i.e. qualified interpreter, large print, reader, hearing assistance) in order to attend this meeting, please notify the Administrative Office at 913-676-8350 no later than 24 hours prior to the beginning of the meeting.

CALL TO ORDER AND PLEDGE OF ALLEGIANCE

ROLL CALL

PUBLIC HEARING

1. SPECIAL PRESENTATIONS

2. ISSUANCE OF NOTES AND BONDS

3. CONSENT AGENDA

*NOTE: Information on consent agenda items has been provided to the Governing Body. These items are determined to be routine enough to be acted on in a single motion; however, this does not preclude discussion. **If a councilmember or member of the public requests, an item may be removed from the consent agenda for further consideration and separate motion.***

CONSENT AGENDA - GENERAL

- 3a. [Minutes of the January 17, 2018 City Council Meeting](#)

CONSENT AGENDA - Finance & Administration Committee

[Finance & Administration Committee Meeting Packet 2-7-18](#)

[Finance & Administration Committee Meeting Minutes 2-7-18](#)

- 3b. Surplus Property Resolution
3c. Replacement of Police Vehicle

CONSENT AGENDA - Community Development Committee

[Community Development Committee Meeting Packet 2-7-18](#)

[Community Development Committee Meeting Minutes 2-7-18](#)

- 3d. Contract for Arborist Consultant
3e. Purchase of MFAC Lounge Chairs
3f. 2018-2019 Nuisance Abatement Contractor

COMMUNITY COMMITTEE REPORTS

Approved Minutes from Board and Commission meetings are available on the City of Mission website under the "[Agendas & Minutes](#)" tab.

4. PUBLIC COMMENTS

5. ACTION ITEMS

Planning Commission

- 5a. Martway Mixed Use Development Preliminary Site Plan, 6005-6045 Martway St. ([page 4](#))
- 5b. Special Use Permit, 5655 Broadmoor Street ([page 170](#))

Miscellaneous

6. COMMITTEE REPORTS

Finance & Administration, Nick Schlossmacher

[Finance & Administration Committee Meeting Packet 2-7-18](#)

[Finance & Administration Committee Meeting Minutes 2-7-18](#)

- 6a. Magazine / Holiday Adoption Program Coordination ([page 193](#))

Community Development, Kristin Inman

[Community Development Committee Meeting Packet 2-7-18](#)

[Community Development Committee Meeting Minutes 2-7-18](#)

- 6b. 2018 Farmers Market Schedule ([page 197](#))

7. UNFINISHED BUSINESS

8. NEW BUSINESS

- 8a. Selection of Council Vice President
- 8b. Selection of Finance & Administration Committee Vice Chairperson
- 8c. Selection of Community Development Committee Vice Chairperson

9. COMMENTS FROM THE CITY COUNCIL

10. MAYOR'S REPORT

Appointments

Planning Commission

- Pete Christiansen, Ward IV

Parks, Recreation & Tree Commission:

- Amy Burkes, Ward IV

City Treasurer

- Debbie Long, Accounting Manager

11. CITY ADMINISTRATOR'S REPORT

EXECUTIVE SESSION

- Discussion of Current Litigation

ADJOURNMENT

City of Mission	Item Number:	5a.
ACTION ITEM SUMMARY	Date:	February 21, 2018
Community Development	From:	Danielle Sitzman

Action items require a vote to recommend the item to full City Council for further action.

RE: Martway Mixed Use development Preliminary Site Plan, 6005-6045 Martway Street

RECOMMENDATION: Uphold the recommendation of the Planning Commission for approval of the submitted preliminary site plan with the conditions noted.

DETAILS: The subject property is occupied by three small office buildings with a total footprint of approximately 34,000 square feet. They were constructed in the mid 1960's. In 2014, the land was platted for the first time into three lots known as the Martway Office Buildings Subdivision in anticipation by the then owner to offer them for sale. The applicant, Christian Arnold, recently purchased all three office buildings and is requesting a preliminary site plan approval for redevelopment of the site into a mixed use building consisting of retail/office and parking on the ground floor with apartments above.

The applicant is proposing a 4-story building containing apartments and retail space on a 1.8 acre infill site in the downtown on the south side of Martway Street roughly between Beverly Avenue and Dearborn Street. The project is bounded by the Rock Creek Trail along its northern border. The main building would be raised on a concrete podium to allow for parking beneath the structure, floodproofing, and clearance for fire district vehicles. The ground floor retail/office space would be comprised of two enclosed building sections flanking the entrances on the north side of the building. The remaining upper floors would contain 117 rental dwelling units. The preliminary plan submitted for review by the Planning Commission included the following total planned square footage by use:

	Use	Approx. Area 9.25.17 Version	<u>Approx. Area 12.18.17 Version</u>
Commercial	Office/retail	3,491 S.F. (ground floor)	<u>3,491 S.F.</u> <u>(ground floor)</u>
Residential	Apartments	155,908 S.F. (floors 2-5, 156 units)	<u>116,931 S.F.</u> <u>(floors 2-4, 117 units)</u>
	Total	159,399 S.F.	<u>120,422 S.F.</u>

Two public hearings were conducted and comments were received related to the building height, number of new residents, traffic, sky glow from site lighting, trespass of vehicle headlights, trash smells, noise, use of the outdoor pool and Victor X Andersen park, proximity of other multi-family housing in Johnson County, loss of vistas, loss of trees, provision of utilities, and off-street parking. A copy of the staff report and minutes from the September 25, 2017 and December 18, 2017 Planning Commission meetings are attached.

In addition, a neighborhood meeting was hosted by the applicant at the Community Center on

Related Statute/City Ordinance:	Including but not limited to 405.090, 440.160, 440.175
Line Item Code/Description:	NA
Available Budget:	NA

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Action items require a vote to recommend the item to full City Council for further action.

September 12, 2017. Property owners within 700' of the subject property were invited by a mailed invitation to attend. The event was also advertised on the City's social media accounts and website. Approximately 40-50 people attended the meeting. Issues discussed were similar to those expressed at the public hearing.

Planning Commission Recommendation

The Planning Commission, at their September 25, 2017 meeting, voted 8-0 to recommend denial of Case # 17-08 Martway Mixed Use due to concerns about the requested deviation in height.

The applicant revised the submittal based on comments received at the meeting and resubmitted the changes for consideration in December. The Planning Commission, at their December 18, 2017 meeting, voted 7-1 to recommend approval with conditions 1-10 as recommended by staff and the added conditions as shown in blue:

- 1) Approval of the requested deviation to rear yard setbacks to waive the requirement for a 25' setback along adjacent "R-1" zoned city property.
- 2) Approval of the requested deviation to height to allow a maximum building height of four (4) stories and or 56' 3" feet.
- 3) Approval of the requested deviation to waive the minimum lot area per dwelling unit to allow for the proposed design of 117 units or 116,931 square feet of residential development in a mixed-use building.
- 4) Approval of the requested deviation to waive the 6' parking lot setbacks along the west property line. Alternative screening of the area should be provided for consideration with the final site plan.
- 5) Approval of the requested deviation to waive the parking lot buffers for the the west boundary only with evenly-spaced tree islands installed.
- 6) Approval of the requested deviation to waive the site tree requirement based on parking spaces.
- 7) Approval of the requested deviation to waive the parking lot open space standard.
- 8) A revised final traffic study and final stormwater drainage designs must be submitted for review with the final site plan application. The appropriate data, text, maps, drawings and tables must be included per the Olsson Associates review comments dated September 20, 2017 and attached to this report.
- 9) Staff reserves the right to provide additional comments or stipulations on development plans until all traffic, circulation, ADA, storm drainage, and floodplain related concerns have been addressed.
- 10) Provide adequate right-of-way for the required streetscape elements. A minimum of 10' wide paved clear path is required for the Rock Creek Trail separated from the back of curb by a minimum 5' way planting zone.

Related Statute/City Ordinance:	Including but not limited to 405.090, 440.160, 440.175
Line Item Code/Description:	NA
Available Budget:	NA

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Action items require a vote to recommend the item to full City Council for further action.

- 11) Trash receptacle must be moved, or screened to not impact residence to the South West.
- 12) Light Pollution must be rectified to the satisfaction of staff before construction can begin.

Municipal Code

According to Section 440.175 of the Municipal Code, after the Planning Commission submits a recommendation, and the reasons therefore, the City Council may:

- 1. Approve and adopt such recommendation;
- 2. Override the Planning Commission recommendations by two-thirds (2/3) majority vote of the City Council; or
- 3. Return such recommendations to the Planning Commission with a statement specifying the basis for the City Council's failure to approve or disapprove.

CFAA CONSIDERATIONS/IMPACTS: Redevelopment of this property with a mix of uses including multi-family housing helps support a vibrant downtown by creating a market for a variety of sales and services. Efficient use of land by dense infill projects such as this helps support a transit system.

Related Statute/City Ordinance:	Including but not limited to 405.090, 440.160, 440.175
Line Item Code/Description:	NA
Available Budget:	NA

STAFF REPORT
Planning Commission Meeting September 25, 2017
Updated for December 18, 2017

AGENDA ITEM NO.: 2

PROJECT NUMBER / TITLE: Application # 17-08

REQUEST: Preliminary Site Development Plan for Martway Mixed Use Development

LOCATION: 6005-6045 Martway Street

APPLICANT: Christian Arnold, Clockwork Architecture + Design

PROPERTY OWNER: Martway Officeworks LLC
423 Delaware St, Ste 102
Kansas City, MO 64105

STAFF CONTACT: Danielle Sitzman

ADVERTISEMENT: 9/5/17 and 11/28/2017-The Legal Record newspaper

PUBLIC HEARING: Planning Commission meetings, 9/25/17 and 12/18/17



Property Information:

The subject property is occupied by three small office buildings with a total footprint of approximately 34,000 square feet. They were constructed in the mid 1960's. In 2014, the land was platted for the first time into three lots known as the Martway Office Buildings Subdivision in anticipation by the then owner to offer them for sale. The property is zoned Main Street District

2 “MS2”. It is located in the Downtown District and subject to the *Mission, Kansas Design Guidelines for the Johnson Drive Corridor*. “MS2” was assigned to this property at the time of the City initiated rezoning of entire downtown in 2006. The District was designed to reinforce and encourage the existing character within the core of the downtown.

Surrounding properties are zoned and developed as follows:

North/East/West: “MS2” Main Street District 2-municipal community center, multi-family housing, small office, auto-bank.

South: “R-1” Single-Family Residential District-Municipal Offices, Police Department, Outdoor City Pool, Parkland, single-family homes.

Comprehensive Plan Future Land Use Recommendation for this area:

The Comprehensive Plan indicates this area is appropriate for Medium-Density Mixed Use, Parks, and Office. This area should be composed of a pedestrian-friendly mix of mostly housing and limited office and retail uses at medium densities. Such districts typically serve as a transition zone between low to moderate density residential neighborhoods and areas of higher intensity commercial activity. This category primarily consists of an intermix of low to moderate density attached residential housing types, such as row housing, townhomes, condominiums, duplexes, triplexes, and fourplexes, and multiplex and apartment/condo dwellings. Residential densities may vary throughout the neighborhoods and are typically higher than low-density residential areas. The ground floor is appropriate for offices or limited retail stores with upper floors including housing units.

The proposed project is in conformance with the intent of the Comprehensive Plan to provide a mix of residential densities and uses located in proximity to the higher commercial intensity uses near Johnson Drive. It also addresses the Comprehensive Plan Goals of downtown floodplain redevelopment, supports multi-modal travel, and contributes to the economy of the downtown.

Project Background:

The applicant recently purchased all three office building properties. At this time the applicant, Christian Arnold of Clockwork Architecture + Design, is requesting a preliminary site plan approval for redevelopment of the site into a 5-story mixed use building consisting of retail and parking on the ground floor with apartments above.

Update: The revised plan is for a 4-story mixed use building with the same mix and arrangement of uses.

Plan Review

The applicant is proposing a 5-story mixed use building containing apartments and retail space on a 1.8 acre infill site in the downtown on the southside of Martway Street roughly between Beverly Avenue and Dearborn Street. The project is bounded by the Rock Creek Trail along its southern border. The main building would be raised on a concrete podium to allow for parking beneath the structure, floodproofing, and clearance for fire district vehicles. The ground floor retail/office space would be comprised of two enclosed building sections flanking the entrances on the northside of the building. The remaining upper floors would contain 156 rental dwelling units.

Update: The proposed building is now 4-stories with 117 dwelling units. All other site plan details remain the same. The preliminary plan submitted for review by the Planning Commission includes the following total planned square footage by use which has been

updated:

	Use	Approx. Area 9.25.17	<u>Approx. Area 12.18.17</u>
Commercial	Office/retail	3,491 S.F. (ground floor)	<u>3,491 S.F.</u> (ground floor)
Residential	Apartments	155,908 S.F. (floors 2-5)	<u>116,931 S.F.</u> (floors 2-4)
	Total	159,399 S.F.	<u>120,422 S.F.</u>

Planned District Deviations Requested

The Main Street District 2 is a planned zoning district and therefore eligible for consideration of deviations from the prescribed zoning standards. A planned district is a zoning technique that is intended to create additional flexibility in the application of zoning standards such as, but not limited to, setbacks and height. Conventional zoning, which relies on rigid dimensional standards, does not easily accommodate innovative development especially where mixed-use or infill projects are proposed. In addition, conventional zoning relief requires changing the zoning code standards on a project by project basis or through the consideration of variances. In the case of the former, changing zoning district standards often would create non-conformities as the new rules are then applied to all existing developed property within the same zoning district. On the other hand, variances are difficult to justify as the criteria used for evaluation rely on the demonstration of a unique hardship related to the physical characteristics of the property. The merits of a particular development concept alone are not a proper reason to grant a variance.

The adoption of planned zoning in Mission was a precursor to the development of other innovative zoning techniques such as mixed use zoning districts like the Main Street District 1 & 2 districts and other overlay zones. It is a valuable tool as it allows for deviations from conventional zoning standards on a case by case basis upon review of specific development proposals. The stated intent of the City of Mission's planned district code is to encourage quality development by permitting deviations from the conventional zoning district to encourage large-scale developments, efficient development of smaller tracts, innovative and imaginative site planning, conservation of natural resources, and minimum waste of land.

Many of the requested deviations discussed below relate to the special challenges of infill redevelopment. Infill refers to the development of vacant or underutilized parcels within previously built areas. These areas are already served by public infrastructure, such as transportation, water, wastewater, and other utilities.

Redevelopment describes converting an existing built property into another use. Ideally, redevelopment aims for better use of the property that provides an economic return to the community. In this case, conversion of several small offices in need of repair and renovation constrained by the nearby floodplain to a mixed-use development that combines residential and commercial uses.

Infill redevelopment optimizes prior infrastructure investments and consumes less land that is otherwise available. Infill redevelopment can result in:

- Efficient utilization of land resources
- More compact patterns of land use and development
- Reinvestment in areas that are targeted for growth and have existing infrastructure

- like the downtown
- More efficient delivery of quality public services such as transit

As a community where most land has already been developed, most, if not all, redevelopment in Mission will be infill redevelopment in nature. Therefore, in order to fulfill the long-range goals of the Comprehensive Plan to provide a mix of residential densities and uses located in proximity to the higher commercial intensity uses near Johnson Drive, redevelopment of the downtown floodplain, support of multi-modal travel, and enhancement of the downtown economy, additional flexibility is an important element of plan review.

The applicant is requesting the following deviations:

- 1) Update: This deviation is no longer needed as the required number of on-site parking stalls will be provided. The table below has been updated. No action is required on this item.

On-Site Parking. The “MS2” zoning standard requires a minimum of 4 parking spaces per 1,000 square feet of commercial gross floor area and 1 space per efficiency and one bedroom apartments. 2 spaces are required for two bedroom apartments (410.250). The proposed mix development contains the following mix on site:

Use	Number	Base Code Requirement	Proposed On-Site 12.18.17	Proposed Off-Site	Reduction
Retail	3,491 S.F.	14 spaces	0	0	0
Efficiency/One Bedroom Units	87 Units (18/69)	87 spaces		0	0
Two Bedroom Units	30 Units	60 spaces			
	Total	161	166	0	0

Use	Number	Base Code Requirement	Proposed On-Site 9.25.17	Proposed Off-Site	Reduction
Retail	3,491 S.F.	14 spaces	0	0	14
Efficiency/One Bedroom Units	116 Units (24/92)	116 spaces	166	10	20
Two Bedroom Units	40 Units	80 spaces			
	Total	210	166	10	34

The applicant is requesting a permission to provide 166 spaces on site with the option to lease

10 additional spaces from adjacent properties for a total reduction of 34 spaces

The applicant states in the project narrative (attached) that the full number of parking spaces will not be needed due to the anticipated 5% normal vacancy rate of the apartments and shared parking between the retail and housing uses which will have different periods of demand. In addition, the applicant proposes securing agreements for leasing nearby off-site parking spaces. The intent is to reduce the amount of land devoted to under utilized or unneeded parking and to allow for a more efficient use of land.

Staff Notes-The number of parking spaces needed is related to the proposed uses of the site. In this case, primarily the number of apartment units. The City's parking ratios are based on conservative estimates of the average demand expected by a typical use. The intent is to ensure that the impact of vehicles generated by private activities such as housing and commercial activity do not overrun public facilities like the street network. The developer is proposing to provide parking ratios tailored to the character of their project. They indicate the number of apartments proposed is necessary to make the project financially feasible and sustainable over time. Costs unique to infill projects can come from demolition of existing structures, odd or obsolete site shapes and sizes, existing facilities like trails and street right-of-ways, and floodplains. In exchange for this allowance the project generates 44 additional bedrooms thus increasing the population density. Additional density is a more efficient use of land than a smaller scale development. Additional density and, therefore, additional rents offsets costs and results in potentially higher property values and a better quality project.

There are several well developed alternative modes of travel immediately available to the site which may reduce vehicle travel demand. This includes a network of sidewalks, the Rock Creek multi-modal trail, and several KCATA bus routes which travel between two enhanced bus stops at the community center and the Mission Transit Center hub on Johnson Drive.

The applicant's estimate of rates of parking demand for housing are similar to other observed conditions at similar apartment developments like those operated by EPC Real Estate. This would likely be sufficient to meet the needs for residential parking without building unnecessary stalls that would remain unused.

In regard to retail parking demand, the applicant's traffic study does not consistently identify the nature of the commercial space as either retail or office. Therefore the City's consulting engineer has asked for revisions to the study to clarify this. This is a relatively small total area of the building and is not anticipated to alter or to generate pass-by traffic. Pass-by traffic are those drivers who happened to be driving by on their way to something else and stop in because it is convenient before resuming their original trip. Also, it could be possible for the commercial tenants to share parking with the residential units as they operate at different peak hours. However, while the study appears to indicate traffic impacts will not require additional roadway improvements, without the correct data, staff would prefer to defer making a recommendation on the parking deviation. This deviation could be considered at the time of final site plan review when a revised traffic impact analysis report has been received and reviewed.

- 2) Rear Yard Setbacks. The "MS2" zoning standard requires properties adjacent to those zoned "R-1" Single-Family Residential District to provide a twenty-five foot (25) building setback between them. Otherwise no setbacks are required. (410.240). The applicant

is requesting permission to waive this setback.

In the project narrative the applicant indicates that the Rock Creek drainage tract, creek channel, and Victor X Andersen Park provide an equivalent if not larger setback from any surrounding single-family homes.

Staff Notes-The overall separation of structures intended by the code is a minimum of 45 feet (subject setback of 25'+ 20' rear yard setback of SF home). The only qualifying "R-1" zoned property adjacent to the proposed project is that of the City Hall building, outdoor pool, and Victor X Andersen Park. These areas are unlikely to redevelop into single family dwelling units and do not require a buffer from the proposed development which is a less intense use. Also, the city properties easily fit the definition of office or recreational zoning districts which if so designated would remove the need for any setback. The intent of the required setback has been met by the creek channel, Tract A, and the open space of the park. Granting this deviation allows for a more efficient use of land by removing an unnecessary buffer.

- 3) *Update: The applicant is requesting a maximum height allowance of 4 stories and / or 56' 3". This is one less story and 10' 9" shorter than previously proposed. Staff's notes on the project remain otherwise unchanged.*

Building Height. The "MS2" zoning standard limits a building's maximum height to 3 stories and or forty-five feet (45'). (410.240) The applicant is requesting a maximum height allowance of 5 stories and / or sixty-seven feet (67').

The applicant is requesting the height deviation so that additional apartment units can be included in the design. The project narrative explains that the building's height is also affected by a larger clearance on the ground floor to accommodate parking due to the floodplain and fire district access. The applicant points out the sloping topography which puts the site 10'-20' lower than many surrounding properties of similar height or of the nearest single-family homes.

Staff Notes-As stated earlier, the number of apartments proposed is necessary to make the project financially feasible and sustainable over time. Infill projects face additional site design challenges and costs. In exchange for this allowance, the project generates an additional 77,950 square feet of development. Half of this offsets the loss of ground floor development area due to the floodplain impacts. Additional density is a more efficient use of land than a smaller scale development. Additional density and therefore additional rents offsets costs and results in potentially higher property values and a better quality project.

- 4) *Update: The revised plan contains 39 fewer dwelling units and therefore increases the amount of lot area per dwelling. The new unit count is 117 and the new density calculation is 658 square feet/unit or 66.21 units per acre. The intent is to allow 117 units or approximately 116,931 square feet of residential development. The density table attachment has been updated. The project is now less dense than the Mission Trails project on Johnson Drive. Staff's notes on the project remain otherwise unchanged.*

Minimum Lot Area per Dwelling Unit. The "MS2" zoning standard requires 1,245 square feet of lot area per dwelling unit or a maximum of 35 units per acre (410.240). The applicant is requesting permission to reduce the lot area per dwelling unit to fit their proposed design to allow for the 156 units or 155,908 square feet of residential development in a mixed use

building. This is approximately 493 square feet or 88.64 units per acre. Note: the exact lot area or unit density calculation may fluctuate if the amount of land dedicated on the final plat for changes the site area. The intent is to allow 156 or approximately 155,908 square feet of residential development. This is not dwelling unit size.

The applicant states in the project narrative that the project has been designed in response to current market trends for increased density and to make the project economically feasible. They also indicate that the proposed density brings customers within walking distance of the main commercial district of the city.

Staff Notes-The proposed lot area per unit is comparable with many of the current apartment development projects underway in northeast Johnson County especially those in and around Downtown Overland Park (See attached density table). The baseline density contained in the "MS2" zoning district reflects the existing apartment development in the area which were constructed 35-60 years ago. All existing apartment complexes in the downtown predate the newly created zoning districts "MS1", "MS2" or "DND". If the baseline density was not altered, approximately 62 units would be allowed on site. Likely only 40 of these could be constructed due to the floodplain impacts to the ground floor because of the proximity to Rock Creek. That would result in a lot area per unit of 1,925 square feet which is lower than any other downtown multi-family property. Modern, market-driven, high quality infill requires flexibility to be built on this site.

- 5) Parking Lot Setback. The "MS2" zoning standard prohibits newly constructed paved surface parking areas from being closer than 6' from a street or property line (410.250). The applicant is requesting permission to waive this requirement for the west property line only.

The applicant states in the project narrative that the purpose of the request is to maximize on-site parking while avoiding placing incompatible features along the adjacent property. They point out that the adjacent development to the west also contains a surface parking lot. The applicant stated they will look for opportunities to create landscape buffers where feasible with the development of the final site plan.

Staff Notes-The intent of this code section is to provide screening and buffering from undesirable areas (surface parking lots) and the public way or adjacent properties. No side yard setback is required between the building and the west property line except for the parking lot. The proposed site plan otherwise meets the requirements for parking lot setbacks and the bulk of the surface parking lot is behind or under the proposed building which is a highly desired feature. A stipulation should be made that this deviation is for the west property line only and that alternate screening of this area should be provided for consideration with the final site plan.

- 6) Parking Lot Buffer. The *Mission, Kansas Design Guidelines for the Johnson Drive Corridor* requires parking lots abutting an interior property line to maintain a minimum of 4' of green space (3.2). The applicant is requesting permission to waive this requirement for the entire site.

Staff Notes-This requirement is similar to that of #5 but more strict in its applicability to all interior property lines regardless of what they abut. The proposed project is lined by the Rock Creek along the entire southern property boundary and a 6' buffer is shown along the east boundary. Granting the #5 deviation to the west boundary with stipulations will ensure proper

buffering of surrounding properties.

- 7) Site Tree. The supplemental landscaping requirements of the Municipal Code require site trees to be planting in the parking lot at a rate of 1 tree per every 20 parking spaces (415.090). The applicant is requesting permission to waive this requirement.

The applicant states in the project narrative that this deviation is requested to maximize on-site parking and that the location of the surface parking lot under and behind the proposed building screens and shades the parking area.

Staff Notes-The intent of this code section is to visually soften parking lots from the view from other areas, provide shade, ground water recharge, air purification, and enhance the quality appearance of the site. This development has proposed a building design in which parking is located under or behind the building. This is an acceptable or superior design and therefore buffering the parking area with site trees is not needed.

- 8) Parking Lot Interior Open Space. The supplemental landscaping requirements of the Municipal Code require site trees to be planting in the parking lot at a rate of 1 tree per every 20 parking spaces (415.110). The applicant is requesting permission to waive this requirement.

The applicant states in the project narrative that this deviation is requested to maximize on-site parking and that the location of the surface parking lot under and behind the proposed building screens and shades the parking area. Quality landscaping where feasible on the site will be explored with the development of the final site plan

Staff Notes-Again,the intent of this code section is the same as #7 above. This development has proposed a building design in which parking is located under or behind the building. This is an acceptable or superior design and therefore provided open space in the parking field is not needed.

Code Review: Standards of Development (405.090)

The Planning Commission, in the process of approving preliminary site development plans, may approve deviations upon a finding that all of the following conditions have been met:

1. The granting of the deviation will not adversely affect the rights of adjacent property owners.

-The requested deviations, with stipulations where noted, do not infringe upon the rights of other adjacent property owners to continue to reasonably use their own properties. The proposed development repeats a pattern already established in the neighborhood of ground floor retail or small office along Martway Street and multi-story multi-family housing.

2. That the deviation desired will not adversely affect the public health, safety, morals, order, convenience, prosperity or general welfare.

-The impacts of the deviations upon traffic, stormwater runoff, and the public streetscape are being examined and must be found to meet city requirements at the time of final site plan approval. At this time, it appears all impacts can be mitigated.

3. The granting of the deviation will not be opposed to the general spirit and intent of this Title.

-The requested deviations with stipulations as noted meet the spirit and intent of the code to encourage redevelopment which is in compliance with the comprehensive plan as discussed in the section above.

4. That it has been determined the granting of a deviation will not result in extraordinary public expense, create nuisances, cause fraud on or victimization of the public or conflict with existing federal or state laws.

-The proposed deviations will not create additional public expense, nuisances, or violate other laws.

Johnson Drive Design Guidelines

The Johnson Drive Design Guidelines provide a wide range of recommended and required design elements applicable to the development. These include streetscaping and the relationship of buildings and their exterior facades to public streets as well as building materials and screening. Many of these details are not required at the time of preliminary site plan review and will be fully evaluated with final site plans.

Staff Notes-Design Guidelines: Buildings are shown filling in the block parallel to the public street and extending the width of the property with parking behind or under the primary facade. Adequate room has **not** been reserved for streetscape elements to match the Martway Street streetscape and Rock Creek Trail already established. The proposed building materials and architectural style are reflected in the colored elevations and exterior renderings. A modern architectural theme is proposed. The intent of the Johnson Drive Guidelines is to encourage detailed and articulated building elevations that create interesting facades, complementary massing, human scale elements, and high quality appearance materials. It acknowledges that Mission benefits from a diversity of architectural styles and would not prohibit modern styles that are compatible in form and proportion to buildings with their immediate context on Martway Street. Specific details of all building elements including materials will be reviewed at the time of final site plan submittal. The applicant has provided comment on the building design in the project narrative.

Traffic Impact Analysis & Parking

Update: On site parking is no longer a concern as the required minimum number of stall are to be provided on site. In addition, with fewer dwelling units proposed, traffic generation will be reduced. An update to the traffic impact analysis will be required at the time of final site plan anyway, so these estimates can be revised then. Staff's notes on the project remain otherwise unchanged.

The proposed parking plan is discussed in the deviations section of the staff report. Access into the site is proposed from two access points along Martway Street. One will align with Beverly Avenue and one will be slightly offset from Dearborn Street. The off-set entrance is in the same location as an existing driveway and therefore not a new condition in the street network. Both driveways will enter into the ground floor parking area under the building.

Staff Notes-Traffic & Parking:The Johnson Drive Design Guidelines support structured parking and minimizing the amount of surface parking in redeveloping areas of the city. The applicant was required to provide a full traffic impact analysis including estimated traffic generation trips and the assignment of those trips to the various intersections surrounding the

site using standard traffic engineering practices. In addition to traffic volume, the impact to the performance of several intersections adjacent to the site were also studied and assigned a A-F grade.

The City's on-call engineers at Olsson Associates have reviewed the Traffic Impact Analysis and the proposed preliminary site plans. They are generally satisfied with the preliminary project design and the capacity of the road network to accommodate the proposed development but note a discrepancy in the trip generation method estimating traffic based on office or retail use on the ground floor. They recommend reserving the right to make further comment on the proposed parking until a revised final study is provided. Comments will be required to be resolved before the study or final site plan are accepted. Conditions regarding on-site vehicle and ADA circulation are included in the recommended approval below.

Stormwater Management

The subject property generally drains southeast into the adjacent Rock Creek channel located immediately south and flowing to the east. No details of the proposed future drainage collection, routes or discharged were provided. The proposed development results in a slight increase in impervious surface (approximately 3,418 S.F.) and has requested a waiver from stormwater management based on the adopted code provisions of APWA 5600.

The City's on-call engineers at Olsson Associates have reviewed a stormwater drainage memorandum and the preliminary site plans. They are generally satisfied with the preliminary project design but recommend reserving the right to make further comment until the final study is provided. Any further comments for the applicant to address will be required to be resolved before the study or final site plan are accepted. Conditions regarding drainage are included in the recommended approval below.

Floodplain

A portion of the Rock Creek regulatory 100-year floodplain exists on this site. Therefore the City's Floodplain Management Ordinance will regulate the development. Development will only be permitted through the issuance of a floodplain development permit under such safeguards and restrictions as may be reasonably imposed for the protection of the community. The City's on-call engineers have begun this review and will continue to evaluate the proposed construction for the proper floodproofing, site work, and regulatory permits. This is a process which occurs as site planning begins and concludes before building permit issuance. Conditions regarding this process are included in the recommended approval below.

On Site/Off-Site Public Improvements

The developer is responsible for the construction of public improvements along Martway Street such as sidewalk, street trees, irrigation, benches, bike racks, street lights, etc. Improvements to the barrier to Rock Creek may also be required. Any necessary off-site improvements identified in review of the final traffic and stormwater studies will also be the responsibility of the applicant.

Staff Notes-Public Improvements: A minimum 10' wide paved clear zone along Martway Street must be maintained for the existing Rock Creek Trail. The proposed 8' wide trail is insufficient to meet multi-modal trail standards. In addition, adequate space for a streetscape amenity zone (street trees, streetlights, signage, etc.) must be provided. This zone should be 5' wide at a minimum. Room for door sweeps for the ground floor commercial space should be accounted for outside of the trail as well. Additional details are needed with final plans to

ensure the Martway Street streetscape provides adequate dimensions. Additional street right-of-way dedication will be required with final plans and plats.

Signs

As a mixed use development, the subject property is encouraged to establish a private sign criteria as an alternative to the specific sign requirements of this district.

Staff Notes-Signs: The city's sign code indicates criteria shall be for the purpose of ensuring harmony and visual quality throughout the development. The size, colors, materials, styles of lettering, appearance of logos, types of illumination and location of signs must be set out in such criteria. Signs may wait to be addressed in this manner until final development plans are submitted. A preliminary proposal was provided. The sign criteria will be reviewed and approved at the time of final site plan review.

Sustainable design and construction practices

The Mission Sustainability Commission has developed a rating and certification system for development projects. The applicant has been invited to present the project to the Sustainability Commission. Once completed, the final scoring of the project will be provided to the Planning Commission at the time of Final Site Plan review.

Miscellaneous

A neighborhood meeting was hosted by the applicant at the Community Center on September 12th. Property owners within 700' of the subject property were invited by a mailed invitation to attend. The event was also advertised on the City's social media accounts and website. Approximately 40-50 people attended the meeting. Issues discussed included the building height and aesthetics of the project.

Update: In addition to the statutory requirement for notice of the public hearing to property owners within 200', property owners within 700' of the subject property were also mailed notice of the December 18th meeting.

Code Review: Consideration of Site Plans (440.160)

Site plans shall be approved upon determination of the following criteria:

1. The site is capable of accommodating the building(s), parking areas and drives with appropriate open space.

-The building, parking area, driveways, and open space have been designed to meet codes and guidelines within a planned district.

2. The plan provides for safe and easy ingress, egress and internal traffic circulation.

-There is adequate space on the site to allow for circulation of residents, customers, and the public with no impact to traffic on adjacent public streets. A traffic/trip generation study was submitted for review and any further comments can be addressed at final site plan review.

3. The plan is consistent with good land planning and site engineering design principles.

-The proposed project is in preliminary conformance with the Main Street District 2 zoning district with the deviations and conditions below and the *Mission, Kansas Design Guidelines for the Johnson Drive Corridor* for building placement and massing.

4. An appropriate degree of harmony will prevail between the architectural quality of the proposed building(s) and the surrounding neighborhood.

-The proposed project is subject to the design guidelines for the downtown district which will ensure architectural harmony as the final site plan is prepared. The design concept expressed at preliminary site plan indicates a modern architectural style similar to many similar mixed use developments occurring in Northeast Johnson County and the mid-century office buildings in the immediate neighborhood. Design elements of the surrounding buildings are shown in the exterior renderings.

5. The plan represents an overall development pattern that is consistent with the Comprehensive Plan and other adopted planning policies.

-The proposed mixed use building is consistent with the intent of the Comprehensive Plan to encourage greater density and mix of uses in the downtown District.

6. Right-of-way for any abutting thoroughfare has been dedicated pursuant to the provisions of Chapter 455.

-Any required right-of-way changes for this site to accommodate such things as public trails will be addressed with preparation of a revised final plat.

Staff Recommendation 9.25.17

The proposed development conforms with the Comprehensive plan, meets the overall intent of the "MS2" zoning district, and complies with the required findings for Section 405.090 and 440.160. Therefore, Staff recommends the Planning Commission recommend approval of the Preliminary Site Development Plan for Case # 17-08 Martway Mixed Use to the City Council with the following stipulations:

1. Deferral of consideration of the requested deviation to on-site parking until the time of final site plan approval.
2. Approval of the requested deviation to rear yard setbacks to waive the requirement for a 25' setback along adjacent "R-1" zoned city property.
3. Approval of the requested deviation to height to allow a maximum building height of five stories and or 67 feet.
4. Approval of the requested deviation to waive the minimum lot area per dwelling unit to allow for the proposed design of 156 units or 155,908 square feet of residential development in a mixed-use building.
5. Approval of the requested deviation to waive the 6' parking lot setbacks along the west property line. Alternative screening of the area should be provided for consideration with the final site plan.
6. Approval of the requested deviation to waive the parking lot buffers for the entire site.
7. Approval of the requested deviation to waive the site tree requirement based on parking spaces.

8. Approval of the requested deviation to waive the parking lot open space standard.
9. A revised final traffic study and final stormwater drainage designs must be submitted for review with the final site plan application. The appropriate data, text, maps, drawings and tables must be included per the Olsson Associates review comments dated September 20, 2017 and attached to this report.
10. Staff reserves the right to provide additional comments or stipulations on development plans until all traffic, circulation, ADA, storm drainage, and floodplain related concerns have been addressed.
11. Provide adequate right-of-way for the required streetscape elements. A minimum of 10' wide paved clear path is required for the Rock Creek Trail separated from the back of curb by a minimum 5' way planting zone.

Planning Commission Recommendation 9/25/2017

The Planning Commission, at their September 25, 2017 meeting, voted 8-0 to recommend denial of Case # 17-08 Martway Mixed Use due to concerns about the requested deviation in height.

Update

On September 29, 2017 the applicant indicated to staff they they would rework their proposal based on public comment for reconsideration by the Planning Commission. Revised plans were submitted for review.

Staff Recommendation 12.18.17

The proposed development conforms with the Comprehensive plan, meets the overall intent of the "MS2" zoning district, and complies with the required findings for Section 405.090 and 440.160. Therefore, Staff recommends the Planning Commission recommend approval of the Preliminary Site Development Plan for Case # 17-08 Martway Mixed Use to the City Council with the following stipulations:

1. Approval of the requested deviation to rear yard setbacks to waive the requirement for a 25' setback along adjacent "R-1" zoned city property.
2. Approval of the requested deviation to height to allow a maximum building height of four (4) stories and or 56' 3" feet.
3. Approval of the requested deviation to waive the minimum lot area per dwelling unit to allow for the proposed design of 117 units or 116,931 square feet of residential development in a mixed-use building.
4. Approval of the requested deviation to waive the 6' parking lot setbacks along the west property line. Alternative screening of the area should be provided for consideration with the final site plan.
5. Approval of the requested deviation to waive the parking lot buffers for the entire site.
6. Approval of the requested deviation to waive the site tree requirement based on parking

spaces.

7. Approval of the requested deviation to waive the parking lot open space standard.
8. A revised final traffic study and final stormwater drainage designs must be submitted for review with the final site plan application. The appropriate data, text, maps, drawings and tables must be included per the Olsson Associates review comments dated September 20, 2017 and attached to this report.
9. Staff reserves the right to provide additional comments or stipulations on development plans until all traffic, circulation, ADA, storm drainage, and floodplain related concerns have been addressed.
10. Provide adequate right-of-way for the required streetscape elements. A minimum of 10' wide paved clear path is required for the Rock Creek Trail separated from the back of curb by a minimum 5' way planting zone.

Planning Commission Recommendation 12/18/2017

The Planning Commission, at their December 18, 2017 meeting, voted 7-1 to recommend approval of Case # 17-08 Martway Mixed Use with the following conditions:

- 1) Approval of the requested deviation to rear yard setbacks to waive the requirement for a 25' setback along adjacent "R-1" zoned city property.
- 2) Approval of the requested deviation to height to allow a maximum building height of four (4) stories and or 56' 3" feet.
- 3) Approval of the requested deviation to waive the minimum lot area per dwelling unit to allow for the proposed design of 117 units or 116,931 square feet of residential development in a mixed-use building.
- 4) Approval of the requested deviation to waive the 6' parking lot setbacks along the west property line. Alternative screening of the area should be provided for consideration with the final site plan.
- 5) Approval of the requested deviation to waive the parking lot buffers for the the west boundary only with evenly-spaced tree islands installed.
- 6) Approval of the requested deviation to waive the site tree requirement based on parking spaces.
- 7) Approval of the requested deviation to waive the parking lot open space standard.
- 8) A revised final traffic study and final stormwater drainage designs must be submitted for review with the final site plan application. The appropriate data, text, maps, drawings and tables must be included per the Olsson Associates review comments dated September 20, 2017 and attached to this report.
- 9) Staff reserves the right to provide additional comments or stipulations on development plans until all traffic, circulation, ADA, storm drainage, and floodplain related concerns

have been addressed.

- 10) Provide adequate right-of-way for the required streetscape elements. A minimum of 10' wide paved clear path is required for the Rock Creek Trail separated from the back of curb by a minimum 5' way planting zone.
- 11) Trash receptacle must be moved, or screened to not impact residence to the South West.
- 12) Light Pollution must be rectified to the satisfaction of staff before construction can begin.

MINUTES OF THE PLANNING COMMISSION MEETING

September 25, 2017

The regular meeting of the Mission Planning Commission was called to order by Chairman Mike Lee at 7:00 PM Monday, September 25, 2017. Members also present: Jim Brown, Scott Babcock, Stuart Braden, Robin Dukelow, Dana Buford, Charlie Troppito and Frank Bruce. Absent was Brad Davidson. Also in attendance: Danielle Sitzman, City Planner; Brian Scott, and Ashley Elmore, Secretary to the Planning Commission.

Elections

Ashley Elmore was nominated as Planning Commission Secretary. The vote was taken (8-0). The **motion carried.**

Approval of Minutes from the June 26, 2017, Meeting

A motion was made and seconded to approve the minutes of the June 26, 2017, Planning Commission meeting. The vote was taken (8-0). The **motion carried.**

Case # 17-08 Preliminary Site Plan – Martway Mixed Use-Clockwork Architecture + Design-Public Hearing

Due to technical difficulties, recording started with the meeting already in progress.

Ms. Sitzman:public street, extend the width of the property with parking behind. We do have concerns about adequate room being left for the development of the streetscape elements along Martway Street. In our conditions on this application, we've addressed that. At this point, I would mention that the guidelines do encourage detailed and articulated elevations to create interest in facades. They allow for diversity for architectural styles. So, in this case, the applicant is proposing a modern architectural style. The design guidelines would not prohibit that. So, again, we'll get into some of those details with final site plan review.

As I mentioned, there is a need for some revisions to the full traffic impact analysis. That is a condition that we included in the case before you tonight. There was also an analysis done of the stormwater impacts to the development. Again, our on-call engineers and Olsson & Associates have reviewed the stormwater drainage and find that it to be generally satisfied. There are potentially a few more comments that may happen with the final study, so we would reserve an opportunity to make comment on any of the final study information that's provided for stormwater.

As I've mentioned a couple times, this site does include a flood plain. There can be development in a flood plain, but there are extra regulations associated with it. Basically, this applicant would go through a floodplain permit process, and we would take a look at the design of the building where it is impacted by flood plain. There are certain flood-proofing standards that would have to be met, and Olsson & Associates would help to review that. The applicant is aware of this and already thinking ahead to that stage.

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If there are any off-site improvements such as the construction of sidewalks, street trees, bike racks, street lights, etc., the developer will be responsible for the construction. We do review those as the project goes along. Like I mentioned, we are somewhat concerned about the amount of land being allocated on the north side of the building to accommodate public sidewalks; in this case, the Rock Creek Trail. Because it is a multi-modal trail, it needs to have a slightly wider width than our typical commercial district sidewalk. We require that to be a 10-foot-wide, paved path because we try to work within the federal standards for trails. We do that because in the future, we like to capture federal funds to help us build future trails, and we get credit for existing trails that are compliant and build a network. So, we want to continue to be able to consider the Rock Creek Trail a part of our trail network when we make future applications. Of course, there needs to be room for the street trees and street lights, etc. We typically ask for a 5-foot-wide zone, and if there are door sweeps that open into that area, those need to be planned for, as well. Additional street right-of-way dedication will be required with the final plans and plats. We also let the applicant know that they should consider our private sign criteria. Because this is a mixed-use building, the sign code is probably not going to be a good fit as-is.

The applicant has been invited to present their plan to the Sustainability Commission, which has a scorecard that they go through with applicants and provide them with a score. There was also a neighborhood meeting hosted by the applicant at the Community Center earlier this month. We provided notice to property owners not just within the 200-foot required notice area, but to a much broader area. We had 40 to 50 people attend. Issues discussed that night included building height and the aesthetics of the project. Included in the packet is also the findings of fact included for consideration and final site plans.

I would conclude tonight with staff's recommendation. The proposed development conforms with the Comprehensive Plan as described; it meets the overall intent of the MS2 zoning district, and complies with the required findings for the planned zoned district and site plan sections of our ordinance. Therefore, staff recommends the Planning Commission recommend approval of the preliminary site development plan for Case #17-08 Martway Mixed Use to the City Council, with the following stipulations. The first eight have to do with deviations. The first one is the deferral of consideration of the deviation to onsite parking. Conditions 2 through 8 are for the approval of the requested deviations as described. I would make an amendment to the fifth one, which has to do with waiving the parking lot setbacks along the west property line. That should also include a stipulation that alternate screening of the area should be provided for consideration with the final site plan. It was mentioned in the staff report but I did not get that exact language into the condition.

And then, the final three conditions have to do with getting results from the final traffic study, reserving the right to provide additional comments or stipulations based on what those say. And then, providing adequate right-of-way for the required streetscape elements. I would remind you, Mr. Chair, this is a public hearing. Also in your packet besides the density table was a summary of all of the rental property submissions that we did for City Council not too long ago. It lists the property, it's location, when it was constructed, its value over time, and the rent rates

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charged, as best as we could ascertain. Also included in your packet was the applicant's response and narrative, and traffic and stormwater engineering folks' memo on the proposed development; a copy of the traffic impact analysis; the stormwater drainage plans; and the site plans. That's it for the staff report.

Chairman Lee: Thank you, Danielle. Is the applicant here this evening?

Christian Arnold, Clockwork Architects, appeared before the Planning Commission and made the following comments:

Mr. Arnold: Danielle asked that I do a brief presentation. She did a phenomenal job of capturing the data and numbers. So, this is a version of the presentation that was made to the general public a couple of weeks ago.

One of the things that we wanted to talk about is what we feel is unique for a project like this. A lot of the housing demand that we're seeing comes from the two large population groups – the Boomers and the Millennials. It's what is commonly referred to as "renters by choice," people that really don't want to invest in a home any more, or people who are tired of the maintenance and lawn care, etc., and they want to simplify their lives. We see that being a huge population that is driving the demand for projects like this.

The second piece of it is that 50 percent of people in this study actually prefer a walkable community. That's what we love about the city of Mission. It's very authentic, it's real, it has great services that have been developed along Johnson Drive, and to be able to have a critical mass and some density to this area, that we continue to support those businesses that we feel are very critical. The investment that's been made along Johnson Drive has been phenomenal, and we are huge proponents of small business, that those continue to be viable. But, it really needs more people. It's not really feasible to think about, you know, buying an entire block of single-family homes, taking those down and coming back with a structure. So, typically in development, you look for opportunities for buildings that have kind of lived out their useful life, they need a lot of repair; that's where those opportunities happen.

I grew up in a small town. Most of our projects are in small towns, and the number one thing that I hear from business owners on the service side is that they want more people, more customers, more patrons. The fact that we're right along Rock Creek Trail, we think is a huge advantage in trying to boost the walkability. If you've looked, there is a website where you can actually do a walkability score. Mission ranks higher than downtown Overland Park, which we think is phenomenal. I think it's just the scale, it's a fairly compact city, and we think that has a lot going for it. That's why it's getting great ratings, and wanting Millennials to live here, largely due to the affordability issue.

Again, just to hit the high points, we look for projects that have already started with a lot of public investment, and we look at ways to leverage the private dollars to advance those. So, the fact that we're right across from City Hall, the community center, Rock Creek Trail, the mixture of this area is very walkable along Johnson Drive, are all huge advantages. The fact that the park is to the south creates a great buffer to the single-family residences there. It's over 300 feet to the 60 residences. So, although the top floor is likely going to be visible above tree tops, there is a nice

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distance between those to create that buffer.

Again, as Danielle said, the site presents some challenges due to the floodplain issues. We have hopefully mediated those through this elevated design where we're parking underneath. The buildings are built up on pillions. When we first started looking at the feasibility of renovating those existing office buildings, it's very limiting because of the amount of redevelopment that can occur to anything that's in that flood zone. That's when we started to look at Option B, which was getting everything out of that flood plain.

As I mentioned, we're not including the buffer to the park. We've also tried to create a very sensitive site design where it pushes the building up towards Martway, again, giving as much buffer as possible to all of the surrounding areas. And then, tucking the parking underneath, screening it, and having it be along the south side. We are heavily landscaping the south side of the site where Rock Creek is, so, hopefully it will provide a very sensitive solution. Our plan is to build with high-quality materials. A lot of the projects that we see of this scale are being built out of wood construction. We are proposing metal construction. We think it creates a higher-quality product for our building, and hopefully it's something that will reduce our operational costs over the long term. This ends up being a very institutional-quality building, long-lasting, very sustainable because there's a lot less waste that comes off of the materials. It's highly insulated, as well as very low sound transmission between units. So, our desire is to create very high-quality products.

The challenge with that is the rents, obviously. We want to be an affordable solution that not only works for fixed income individuals later in their life, but also professionals that are getting out of school and have student loans, etc. So, it is a bit of a balancing game between the aesthetics and quality, but we feel like putting it into the structure and the core systems is where to invest. Overall, we're looking at 156 residences at an approximate cost of about \$30.1 million. That building is in the existing Rock Creek TIF, so the plan is to do a lot of the flood zone mediation as planned through the master planning process.

We did some sight line studies. This is a view looking south across the small park to the north of our parcel. You do see the building topping up over the tree tops at a couple locations. This is a composite view looking northwest, so you see the aquatics center in the foreground. You can see a little bit of the building popping up there. One of the things that's a little misleading is it's really down in the valley, about 20 feet below 61st Street, and Johnson Drive, as well.

This is a view looking along Rock Creek, the floodway, with the chain link being the tennis courts. This is the area that is most visible due to the minimal tree cover existing there. Sometimes it's hard to understand the scale of a building until you compare it to what's out there. The top diagram shows the Mission Trails project on Johnson Drive. The middle diagram shows Mission Square in the middle. The last building is the proposed Martway structure. So, you can see how the slope to the ground tapers down. It ends up being about the same height as Mission Square and a little bit shorter than Mission Trails. The bottom two elevations show the comparable massing, which is consistent with the project that is going through the approval process right now. It is also consistent with the intent of the zoning board because it would allow for a project of this size. Again, if the flood plain weren't there, the whole thing would probably

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be sitting on the ground and more similar to the Mission Trails project. Once we started elevating it out of the floodplain, and in order to keep with the fire department on the clearances, we started to push it a little higher.

This is a view looking southeast along the trail and Martway Street. This is a view looking southwest. And then, some of the architectural examples of buildings and projects that we're referencing that would be comparable in nature and size and quality. Any questions that I might answer?

Mr. Babcock: What are you looking for, for dollar per square foot cost?

Mr. Arnold: We are interviewing property management companies right now. Our goal is to charge around \$1.50. So, we're collecting comps. Some of the neighboring projects like – the idea that, some of the other projects coming on line, you know, in downtown Overland Park, as well as some of the proposed projects.

Mr. Babcock: Are you guys set on a, like a modern architecture for this building?

Mr. Arnold: We're trying to respond to the market. We looked at the area to see, you know, you look at vacancy rates, occupancy rates, and see what is desirable. Sometimes design is subjective, and it's hard for us to anticipate what the user or the renter of this building is looking for. Through our research, we are finding that they are attracted to a more progressive architecture. I don't think it has to be. I think some of these projects are a bit transitional. Some have a variety of materials. If I looked at the project that's proposed in Gateway, it's probably more progressive. And, the project that's proposed at Mission Trails is maybe less progressive. So, we're trying to hit the middle where it maybe would appeal to a wide spectrum of people.

Mr. Babcock: On the bottom floor, what is the planned use for that?

Mr. Arnold: I think the most viable solution there is office. We're somewhat open at this point, but if I put on my small-business-owner hat, I would probably want to be on Johnson Drive if I was a strong retail business, just because of the foot traffic and the car traffic. So, the likelihood is it probably makes more sense as office space. But, I think it's a little early to tell. The parking study that we did, we looked at it both ways.

Mr. Babcock: That's the reason I was asking.

Mr. Arnold: Yeah. It's probably more of a demand from the retail point of view, less from the office point of view. So, you put those two things together and I would speculate that office makes the most sense.

Ms. Dukelow: I have a couple comments and questions. It's great to bring more people in, and I appreciate the resilience of the approach, because being in the floodplain, we understand that cars could actually get a little wet. So, that's all great. And the proximity to pool and park are excellent. I have a couple of comments, though. I'm wondering if it's possible to consider pedestrian connectivity between – Just looking at the site plan, it looks like it would be a possibility at the east end of the site where there is a sidewalk. The west end looks like it would be a little tighter, although you don't have the grade challenges there. So, that's something I

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think would really enhance the project, would be connectivity for the community, if there was a foot trail –

Mr. Arnold: I'm glad you pointed that out. We have visited with staff about that possibility and what needs to be done in terms of, whether it's a small pedestrian bridge, or completing some of the Rock Creek TIF remediation work as a part of that, as a section of it. So, I like the idea of connecting, and hopefully we can work through that, of how that would work to connect off-property.

Ms. Dukelow: That would really enhance the area, I believe.

Unidentified: Danielle, could you put the site plan up on the screen so we can follow along with this discussion?

Ms. Sitzman: Sure.

Ms. Dukelow: I was actually looking at C200 because it does show the grading, and it shows more of the site than the landscaping. The landscaping plan shows, is really well-developed for this specific area, but I'd like to talk about the areas that are south of Rock Creek. I understand that the site is very tight, and the south side of the parking would be really, really hard to screen on the northside of Rock Creek. However, I think that we could do some landscape screening, if possible, in Anderson Park. So, we would basically have the same screening requirements, but maybe put those outside of the property line, so to speak. That would help the neighborhood and the park. Because it's going to be quite a change. And Rock Creek currently has quite a bit of vegetation in it, but it's coming out of the, you know, the concrete and everything south, and more than half of it is on the north side. There are a few really nice trees in Anderson Park that I think if we would enhance that edge, add some landscaping trees along that edge, it would make the park much more desirable. So, those are my thoughts on stipulations number 6 and 7. So, I don't know how we would work with that. Like, if we said, okay, we're not actually going to waive the [inaudible] requirements, we're just going to ask you to put them somewhere else, or exactly how all that will work, I'm not sure. But I'm pretty sure it would make it more palatable.

Mr. Arnold: Yeah. Thank you.

Mr. Troppito: I have a number of questions for you, Christian, and staff, as well. The number of deviations concern me, the sheer number. In the past, we've recommended projects to City Council, and City Council has questioned whether it still meets the intent of the zoning code. So, if you obtain a legal opinion from Pete specifically addressing this, how soon can you get it?

Ms. Sitzman: We consulted with Pete before these cases came to you. So, as you'll recall, we made some changes to the planned zoning district in anticipation of having deviations come before us for consideration. So, we did our homework ahead of time. Pete is satisfied with the types of analysis that would happen if those deviations, that they would probably change the findings of fact to be more specific to development, rather than being based on the findings of fact that we would use for a variance, which was an inappropriate set of criteria to be using on these kinds of projects. I think the number of deviations that you're seeing here are partly due to more diligence on staff to point out exactly what's going on in your applications, and also on the

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applicant wanting to get everything out front, instead of doing a little bit at the preliminary and maybe asking for a few more changes when they get to final site plan. They were very thorough in their analysis of our ordinances and letting us know exactly what they anticipated needing to have additional flexibility with. I think staff is satisfied. A lot of the deviations were the same thing, so several of them have to do with landscaping. They go back to parking lot design and things like that. So, I don't think this is an unreasonable number of deviations to request for this type of complex redevelopment.

Mr. Troppito: So, you have discussed this with Pete, so I would presume there would be no problem getting something in writing from Pete before the final site plan.

Ms. Sitzman: We could certainly have a legal opinion memo with the final site plan.

Mr. Troppito: Thank you. On parking, on page 34 of the report, where it says it's been graded such that no parking stall would pond over 7 inches based on FEMA floodplain depths. And that's based on a 100-year flood?

Mr. Arnold: Yes, that's correct.

Mr. Troppito: Do you think that's adequate?

Mr. Arnold: That's what our consulting engineers are telling us, yes.

Mr. Troppito: Well, the reason I raise the question is it seems like it would a 500 year flood. What about flood insurance?

Mr. Arnold: We have not reached out to any insurance agencies yet. Most of the buildings, by elevating them out of the flood plain, and then, all the finished spaces are out of the floodplain, our hope is - .

Mr. Troppito: That's based on a 100-year flood, correct?

Mr. Arnold: That's right. The only thing that will be in the floodplain will be in surface parking. We're optimistic that there won't be a premium associated with the insurance, but if there is, it will be carried by the developer.

Mr. Troppito: You will have that determined by your final plan and present to us what the options are?

Mr. Arnold: Sure. We can do that.

Mr. Troppito: Thank you.

Mr. Arnold: And you're just wanting to know what the insurance plan would be?

Mr. Troppito: Yes. What mechanism is going to protect the cars. On page 44, it discusses the hazardousness from Nichiha Fiber Cement. Is that correct?

Mr. Arnold: That's correct.

Mr. Troppito: Now, on page 44, there is a material safety data sheet for that. And it says it contains hexavalent chromium. And also on page 46, it says that the amount is a small amount.

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Now, in my memory, hexavalent chromium is toxic at levels that meets the definition of parts per billion. So, what's "small?" I'd like that clarified. I'd also like to clarify from somebody who is credible to make the opinion, such as an industrial hygienist, or professional environmental engineer. I'm concerned about the ability for hexavalent chromium to leach out over time in the ground. I'd like to see that addressed.

Mr. Arnold: Okay. Because the materials and the exact manufacturer of them is usually something that would be submitted at final, it limits our options prior to that submission. Honestly, there may be a switch in manufacturer, or something.

Mr. Troppito: If there is, I would like to see it.

Mr. Arnold: Okay. I appreciate your thoroughness. That's impressive.

Mr. Babcock: I'm going to talk about parking areas. This is also a comment for staff. We've got a guideline as far as units, and I see you say we're going to have a 5 percent vacancy rate, so we don't need as many. Okay, so, 5 percent, if I don't include those, that should be 10 spaces. Well, we don't need it for retail. Okay, I'll buy that. That's 24, and you're asking to short it by 34. I mean, I don't necessarily see the logic behind going over your own logic. So, how do you justify that?

Mr. Arnold: One of the things that we try to do in fill developments, most people don't find it very enjoyable to walk across or next to large surface lots. When we zoomed out of this parcel, you know, we're surrounded by largely empty surface lot. So, we reached out to our neighbors and property owners all around us. There's over 200 privately-owned surface spaces, so the thought was to not build more of what we try to screen. So, by tucking the parking under the building, behind, landscaping around it, we're trying to conceal that parking. In doing so, we don't want to create more parking somewhere else, or create more of a demand. That was the thought process, was to not over-park it. And also, you know, use it based on industry standards that we've seen, and other projects in the area that we've seen, and how they perform. Just because highest and best use in a great community is not surface parking, in our opinion.

Mr. Babcock: Okay. The other comment is for staff. I'm seeing a consistency of five stories, and basically our code is for three. So, we're doing variances each time. And I think when Mission Trails put forth their plan, I mean, I get the fact that there is a precedent set. We've got Mission Bank at one end; we've got Mission Bank at the other end. We've got Script Pro. We have precedence for height that you want to build. And I generally can accept that. But, if we're going to allow to build to that height, then we should change our code to allow to build to that height, instead of doing variances each time. We're supposed to be the keepers of the code, and we're breaking our code each time.

Ms. Sitzman: For clarity, it's not a variance, it's a deviation. I understand your concern about --.

Mr. Babcock: Excuse me. Lack of use of the appropriate term.

Ms. Sitzman: I know you're concerned about the number of them. I would point out that simply building into one zoning district doesn't give you the flexibility to look at design. So, I'm not sure that's saying a higher number of stories would always be appropriate everywhere, by every

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design. So, if staff sees value in the planned district and the deviation process, that you can look at specific projects in specific context. We can certainly, at any point, if the Planning Commission wants to consider changing zoning code standards, engage in that process. But so far, I'm not sure that the planned district is necessarily broken. It does make for a longer staff report. I'm sorry. [*Laughter.*]

Mr. Babcock: That has nothing to do with it. It's just I tend to be a rule-follower, and it's making me break the rules every time someone comes in and wants to build a five-story building.

Mr. Braden: Mr. Chairman, I have a question on the glazing. You were asking to build a bulkhead, which I see accounts for 5 percent of what you could have done. But, even with that, you would have only been at 60 percent total -

Mr. Arnold: Oh, yeah. The first floor here, you're referring to?

Mr. Braden: Yes. I'm just wondering, why can't we reach that number?

Mr. Arnold: Yeah, the bulkhead does reduce the percentage. We're open to as much glass along that side as possible. If you look at the drawing, it's kind of deceiving. It looks like it would be so much more, percentage-wise. We do think we can potentially give more of a continuous strip. We were also just trying to introduce some kind of interest, whether it's a mass to give the building a little bit of weight. If not, I mean, I don't think it's going to be overly strong without it. If you said you wanted all of that to be solid glass -

Mr. Braden: Does the parking lot open to --?

Mr. Arnold: Yes. It screens down below, so that the planter and vegetation there. It's open-air above.

Mr. Braden: I think the only other thing I would say is, it's just my personal opinion that [inaudible] with that being such a long [inaudible....] narrow strip that still looks pretty monolithic to me. It would help if there was maybe more interest or something to kind of break up that long building in the middle. That's just a personal opinion.

Mr. Brown: I have a couple questions. I notice that in this plan, and addressed in the staff report, that the building is not currently shown as being set back

Unidentified: [*Inaudible comment.*]

Mr. Brown: And your response to that would be some sort of concern about massing and the neighborhood to the south, which I don't understand because you're asking for a two-story deviation, and yet, you're using massing as your excuse for not stepping the building back. So, I'm having a hard time with that math.

Mr. Arnold: If I understand your question, I think it was a building zone of each side, and then, well, all the buildings along Martway seem to have that stepping. So, we were just trying to be more consistent with those buildings. Mission Square doesn't have the stepping, you know, the smaller office buildings don't have the stepping. It creates some, obviously from a [0:37:18], that's where usually [inaudible] where water gets in, so we try to avoid it, where possible. And

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because of our tight site, obviously. We're trying to get a lot in here without pushing all the way back towards the park and residential properties. So we're trying to kind of hold a lot of building massing, too, to Martway Street.

Mr. Brown: I'm concerned about the height and the surrounding neighbors to the south. I know there was an explanation given that there is a park as a buffer there, but that park is just a wider space. I mean, those houses are elevated from near projects, so they're getting the full view, unless there is something planted there, like Robin was talking about. To create the appearance of the building from those, you know, homes [inaudible]. So, I'm concerned about that, and I'd like to see something like that materialize before we see it again.

Mr. Arnold: Okay.

Mr. Brown: The parking on the left side and on the lower part there on Martway, I'd like to see that look like the rest of the office space, or the business space on the other side of the building. I don't want to look out there and see the cars. I want it to look like it's part of the building. Just on the Martway side. The rest of the, you know, parking from the back and stuff, I don't know if that's going to be an issue. But for where pedestrians are walking down the front of the building, I don't want to be walking along a building front and having a nice look at, you know, engagement with the public way, and all of a sudden, I have an opening, I see all these colors. I prefer not to experience that. And, I would like to see that streetscape lined, as was addressed in the staff report.

And then, a question. Along the creek, are we doing anything back there to make improvements to the floodway? Is there a new retaining system in place?

Ms. Sitzman: As Mr. Arnold indicated, this is in an already-designated TIF district because of the anticipation of flood plain impacts. Several years ago, the City engaged in a study to look at what would be required to take properties out of the flood plain along the Rock Creek corridor. We've made some of those improvements. This particular segment of the channel is not in any of our immediate capital improvement plans to be resolved. So, I believe the best alternative at the time we last did the study was to, to put it into a channel like we've done elsewhere. So, along where the farmer's market is, to the east of there, to the Gateway site, a similar treatment to that. So, it is something that the City has in their long, long-term plans, but nothing immediate to resolve this at the time this project would be happening. So, unless it's part of – as Mr. Arnold indicated – some part of their proposal, some sort of development agreement to step up those flood plain improvements, it's not something we have planned

Mr. Brown: Thank you. The intention of your design as it relates to the 100-year flood elevation, are you planning a flow-through design on the parking? Or, are you putting a wall there to try and keep the water out? I know you've said no ponding over seven inches, but is the water free to move in and out, so it's not restricted and not going to damage the building -?

Mr. Arnold: Tom can you speak to that part of it?

Tom: [CFS Engineer] We have the seven-inch [inaudible] ponding.

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Mr. Arnold: Yes, but I think the idea is to allow the water to move in and out, right?

Tom: Yes.

Mr. Arnold: No barriers, free flows. I think the intent is that if there was a barrier, that creates the ponding.

Mr. Brown: My last question regards something I didn't understand, which was the alignment of the streets in the traffic study. Can you explain where we landed with that? Which one is getting aligned, which one is not, and what is the significance and importance of doing so or not doing so?

Mr. Arnold: We have aligned the drive access to –

Mr. Brown: Beverly?

Mr. Arnold: It wasn't previously. We were going to re-use the existing one, and it was five or 10 feet, too. The consulting engineer suggested that we align that so we have another access point to align to Dearborn. That's off our property, so we're not able to align that one. So, we've aligned one but not the other.

Mr. Babcock: Along the lines of Ms. Dukelow's suggestion, what permissions need to be in place for them to do landscaping across the creek on the City's property? I mean, you can't answer right now, but I'm asking the question out loud, in public. How would they go about getting permissions to do that, and how would that be navigated? And then, who is going to be responsible for the maintenance of those agreements that would need to be in place?

Ms. Sitzman: That would typically be something a development agreement might cover. So, it's like any other offsite improvement that a developer would have to make. If for some reason they were being required to make traffic improvements at an intersection, same kind of mechanism. So, an offsite landscaping buffer improvement would be something we would capture there. And, we would want to make sure that it is tied into the final site plan approval. That's something the Planning Commission felt needed to be a condition. That's where we would place it as a condition. It sounds like he is amenable to discussing that.

Mr. Babcock: Is that also something that the tree board should have a say in?

Ms. Sitzman: Absolutely. The tree board would be involved in that decision. Parks & Recreation and the tree board is all combined now. Site plans are circulated to those groups as they come along. We would make sure they were okay with that specific condition.

Mr. Babcock: Danielle, I don't remember, what's the current say about the, is this considered -? This is central, isn't it?

Ms. Sitzman: This is in the downtown district.

Mr. Babcock: Downtown. That's what I was asking. So, what's the style of architecture supposed to be? I know, like, down in the Gateway, we're saying it's supposed to be modern.

Ms. Sitzman: It's all covered by the same Johnson Drive design guidelines. So, they have

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flexibility built into them. It's not going to say it must be Southwest mission style -.

Mr. Babcock: I know it's not a must-be, but in encouraging-type language -?

Ms. Sitzman: It does encourage a certain color palette, certain materials that are long-lasting. I think it actually says we don't want it to be really stylized because then you get a not-quite-genuine development, where everything is the same, even though it's occurred a hundred years apart. So, the design guidelines have flexibility. They have standards in there for quality of materials, and visual interest, and things like that. But I don't think there's anything that says it has to be one way or the other in the downtown area, or in the east Gateway or west Gateway.

Ms. Dukelow: Just a clarification on Mr. Brown's comment about the open parking on the Martway side. We don't need that to be, like, glass or hard material, but perhaps some type of screen, maybe a perforated panel, or something like that --?

Mr. Brown: I'd be okay with glass or another hard material. I'm not trying to be obscure at all. I want it to look like the rest of the building.

Ms. Dukelow: To be able to, I guess, you know, provide a gap, so you can count on the air flow, and the water flow through there, too.

[Overlapping dialog.]

Unidentified: The water wouldn't flood through at that point. That's out of the 100-year flood plain, that corner of the building, and they only need airflow on two sides of the building and still call it a parking structure.

Mr. Arnold: I like the idea of something that's a little softer, landscaped, or screened, or something along those lines. We have seen glass used on parking, and it does present [inaudible] very hard surface. It's a little artificial because people know that there's not a business behind it, or a built space. We're trying to keep it as authentic as possible. We're open to some ideas. We can maybe propose some things. I don't know. One of the reasons we like this location is because Rock Creek Trail is right there, so we want this to be as nice as possible. And I like the idea of the landscaping lining that area, softening that. So, I was hoping we could find something that incorporates those, perhaps.

Ms. Dukelow: The other thing I was going to mention was Rock Creek, and my question has to do with the railing on each side of that creek, or a fence, or -? Because the one that is there isn't in very good condition.

[Overlapping dialog.]

Ms. Dukelow: So, I don't know where that falls under this.

Ms. Sitzman: That will be under the final site plan review. I know we've already had it come up in discussions.

Mr. Arnold: Yeah, I think we all want a safe solution there, one that looks better than it does currently.

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Mr. Troppito: Could you address the external lighting? The parking, and on the building, generating from the building.

Mr. Arnold: The project will adhere to all lighting codes, which generally does not allow any light bleed up into the sky. Just downward facing. Fortunately, a good portion of the parking is covered, so the lighting fixture will be recessed up into the bottom of the building.

The Chairman opened the public hearing.

Bill Nichols, 6019 West 61st Street, appeared before the Planning Commission and made the following comments:

Mr. Nichols: I understand emotions should not get involved with this. What I'm hearing from Danielle and your questions – and some of the answers – my biggest concern is parking. I figured [0:51:44] per bid is not enough. It looks like you all have figured that out, and hopefully will address it.

The other thing is our lights. You're told that it's 300 feet from their property to my house. Okay? The headlights of cars goes further than 300 feet. So, I'm hoping it will be screened in some way, but I have no idea how. I was over there today and I cannot figure out how to do it, particularly to keep the water flowing.

The other thing is the metal construction. This is the first time I've been involved with that, so I had to look up some information on the internet. According to Stockholm report on metal construction, it is cheaper, faster, but it takes more technical people to fasten everything together. So, please keep that in mind.

That's really about all I have, other than the lights, the noise, and what-not. Thank you.

Adam Dearing, 5711 West 61st Street, appeared before the Planning Commission and made the following comments:

Mr. Dearing: The first thing I'd like to say is the staff's statement about a public meeting lacks the sentiment of the attendees. Most people had considerably more concerns, including but not limited to the overall aesthetics, height, smell of trash, use of amenities, noise pollution, increased traffic, potential impact of increased flooding downstream, and overhead power supply. I think that was very understated.

Also, we have gone to the south side of the Aquatics Center, and we feel that the pictures of the proposed building are not to scale, and there are huge misrepresentations of the approximation of the size of this building. We took that from two or three of the pictures that they showed, and we saw those exact spots, compared the trees, and can see where a two-level building, which the top of their current building is, versus a five-level building. Those are not a good representation, whatsoever.

I just wanted to reiterate what I mentioned at the last public meeting, which is the fact that we have limited amenities in such a small neighborhood, right next to the Countryside area and the Rock Creek area. We feel that if there are another 150 to 200 people living there, especially with the use of a footbridge going across the creek, that what we are able to enjoy right now as a

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small community is going to be lost. Just the simple use of the park, the Aquatics Center, and the tennis courts would no longer be available for a small community with the population that we currently have. Thank you.

Dan Aldrich, 6001 West 61st Street, appeared before the Planning Commission and made the following comments:

Mr. Aldrich: I live in the gray house across from the park. We've been there 26 years. Love that pool. Put up with all that reconstruction. Understand the importance of light and noise pollution, and building barriers. That retaining wall to stop the headlights from getting into my front living room. Right? It's kind of shocking to hear that we're considering building something that goes beyond, literally 40 spaces of parking beyond our guidelines. If we build a foot bridge to the pool, I'll bet any money in the room what's going to happen. Folks are going to park at the pool at night when there are no cars there, and they're going to walk across that foot bridge to this place. So, there are going to be probably 40 cars parked across from my house, and everybody that lives on 61st Street. I can bet that that's going to happen because we've seen that impact in the past through our experience of 27 years here. Number one.

Number two. This guideline of reducing the feet, between the square feet or the footage between property lines, or what-not. Consolidating the space is going to do nothing for us, from a light perspective, a pollution perspective, a sound perspective. You guys realize, when you build an object shaped like a megaphone, or like any reflective disc for communications, the shape of this complex – a U-shape, if you will – with the park on the other side. Do you know where all that noise is going to go? It's going to go into the park where people go with their kids to have quiet time. So, that pretty much ticks off everybody in the neighborhood. Right? Sorry, but it just does.

The down-lighting. We worked with the pool folks on downlighting. The problem is, even with down-lighting, if we put these on posts and they down-light, you still get the light in your house. So, going five stories in this area, two stories beyond our guidelines, for this particular situation -. Yes, it's 300 feet to Bill's house, and my house, and other folks. But, the folks that, that's their back yard, how would you like it if I told you I was going to build a five-story building next to your house and shine lights in your bedroom windows? I mean, you've got to realize. We're depending on you guys to defend the rights of the people that live on 61st Street, that have been there for 20-some years.

Christian, great plan, great design. Not a hater. Just think we're going too big, too much here, for this little neighborhood. Does everyone in this room who lives in this area agree with what I'm saying?

[Several voices responded affirmatively.]

Mr. Aldrich: It would absolutely destroy the neighborhood we worked 27 years to create. Do you guys get that? I hope you do. And, you know, less all the folks with a pool, because we've dealt with that. And, you know, we did what was right for the kids. We've dealt with this. And the thought of this size and scope, out of character with our community, is unacceptable.

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By the way, let me just close. I propose that you guys consider reducing this thing down to three stories. At least one story off. That would give you 40 less units. There's your car-parking problem. It's going to reduce the light pollution, the sound pollution. If we do make access to the park available, I'd move it as far to the east as I can. Don't make it easy to park and walk across Everett. Don't do that. For us. For the people that you're representing. We're utterly paralyzed. We [inaudible] said, "You can't plant trees there." I'm walking my dogs, various dogs there over the years, every morning. There's power lines there. So, we need to plan, if you're going to do that, to bury these power lines. I'd bury them on the north side. If possible. For these guys. Thank you for the opportunity to talk to you guys.

Mary Ann Martens, 6200 West 61st Street, appeared before the Planning Commission and made the following comments:

Ms. Martens: I live on the north side, and have for 24 years. Reading through the 90-some pages of proposal tonight. In addition, starting within one-half mile radius, we already have 520 apartments, from my house. And then you add Mission Trails, which has just been approved. That brings us to 714 apartments in a half-mile radius. And then, if you add the Martway, or perhaps go out seven-tenths of a mile with Mission Gateway, we're over 1,090. Within two or three miles, with Overland Park and Westwood, we have another 849. When we go out to Lamar and Foxridge, we've got 1,693. How many do we need in Mission?

Getting back to the codes, standards of development, 405.090: *The granting of the deviation will not adversely affect the rights of adjacent property owners. Neighborhoods to the north and south. To impact public health, safety, morals, order, convenience, prosperity or general welfare.* That includes the power lines that would run behind my house, and all the trees that would have to be cut to put into this park. I think Mission needs to grow, and I agree with that. But, wouldn't it be wonderful if we could put this in a different place, rather than adjacent to a residential area that is going to detract from our property value. Thank you.

Kathleen VanBecelaere, 6101 Martway, appeared before the Planning Commission and made the following comments:

Ms. VanBecelaere: I am the property owner to the west of this proposed development. My concerns are mostly in the amount of deviations they're asking for. We purchased the property about three years ago, and we did purchase it because we like the neighborhood. And the massing. And we're concerned with the deviation of the massing. We're not concerned so much with development because change is good. But, the zero setback and the proposed screening on our west end is not something we feel is a good thing for our property directly, but also, it is already in your code [inaudible]. Also, the parking requirement. With an influx of that many units, I would see it growing further than what they are proposing. And addressing the height and the parking deviation, the parking setbacks, I think all of this needs to be addressed. That's really all I have to say.

Chuck Malachek, 5539 Barkley Street, appeared before the Planning Commission and made the following comments:

Mr. Malachek: I'm not family, ex-employee, or anything like that. I enjoy my retirement. I took

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care of the Martway Office Buildings from May 1977 until January of this year. I was the building manager, the maintenance man, engineer, painter, plumber, electrician – you name it, I did it. Yes, they do need to be demolished, and something needs to be put there, better than what's there. But, there's a lot of changes that have to be made in order to make it work.

First, the way it stands, with what's going into the sewer lines over there, you've only got 53 waste lines. With the new additions, you're hooking up 600-and-some. And a 24-inch line will not take that. That's what it is underneath Martway. I was there when they rebuilt it.

Your electrical needs. Basically, they're figuring a 100-amp panel per unit. That's 15,600 amps. The way the building stands, you've got 7,200. There are three three-faced lines with 300 amps each, going into each building. So, you have 2,400 amps going into each building. The amount of cable you'd have to run for that is going to be incredible, plus it's already a load on it the way it is now. I don't know how many times over the years the power went off. Not all of it. Like, one levy. Because it was just too much of a load on it. That's another reason they need to be replaced. But, the wiring is going to be a problem because you can't go under the creek.

Also, the creek wall needs to be replaced. I don't know how many tons of concrete and stone and everything else I poured in between the slope and that, the dirt, to keep it in one spot. That will have to be replaced, because the way it is right now, they're already moving during a good flood.

Secondly, none of the buildings in the 39 years, eight months I was there ever got flooded. The crawlspaces did, but it never got up to where the tenants are. It came within six inches of the floors. So, your going up astronomical heights is ridiculous. There's no reason for that. They could lower that height a bunch. I was there during the Plaza flood, and I know what flood waters look like. But, the creek has been done a few times since then, and a lot of it's taken care of.

The only other thing I've got is the boxed culvert that runs the entire west side of 6005. That drains all the parking lots north into that area. I've seen it where it comes out so massive water, it actually stops the creek until it breaks free and overtakes it. That's when you get the flooding. Like I said, it's never made it in where the tenants were. It just came across the parking lots, basically. That's it. So, going up astronomically is not going to help. It's a waste of time. That's all I have to say.

Jim Caulet, 5921 West 61st Street, appeared before the Planning Commission and made the following comments:

Mr. Caulet: I live on 61st Street, right across here. I don't think I have anything to add. I think everyone has pretty much said it. I just have a question. I mean, I look at the Wal-Mart thing, too. If we're going to have all these rules but not follow any of them, from now on, maybe we should just have the architects come in and say, "Well, there are 15 different rules, and we're probably only going to follow one of them. So, why don't we just deviate and say it's all right?" I mean, why bother with all this? If you're going to have the rules - You want to change them. Let's have a community discussion on what we think really ought to be there. As far as I know, none of you live on 61st Street. I haven't seen anyone on 61st Street that's going to have to look

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at this monstrosity, who thinks this is a good deal. So, I hope there will be at least a couple people on this Planning Commission that will say, "I don't think this is a good deal."

Melanie Monson, 6056 Juniper Drive, appeared before the Planning Commission and made the following comments:

Ms. Monson: I do not live in this direct area; I live on the other side of Shawnee Mission Parkway. This would not directly impact the value of my home, but I go to the community center all the time. I grew up in Overland Park, moved away for 30 years, and came back. And I can't believe the changes that have happened in Mission. Some are good; some are not. The location of this development I think will be detrimental to Countryside. I don't live there, but I know it's going to impact the area. We lived in Albuquerque, NM, for 35 years, and we watched the things that are going to be happening now if you pass this big, tall building next to all of these little ones, change the area. That's exactly what they did in the Southwest. They had residential areas next to commercial, and so on, and there's no continuity. So, this won't affect my property value directly, but I think it's going to change the traffic on Martway. You're going to have 156 units, each will have one or two cars, in and out, twice a day. That's 600 cars up and down Martway. It's going to turn into a boulevard. So, they're going to have to change that. At least slow the traffic down. Maybe with speed bumps. Anyway, I don't want to see it go five stories. I think it will change the area too much. Thank you.

Vickie Aldrich, 6001 West 61st Street, appeared before the Planning Commission and made the following comments:

Ms. Aldrich: I live across the street. All of our concerns that were addressed at the original meeting have been pretty much repeated here, except for one. It's not only that the landscaping [inaudible] to soften and camouflage the building and help with sound and noise pollution, but also, we completely lose the horizon of trees that we've enjoyed for so many years. Because as someone else said, the 61st Street houses do sit up considerably higher than the park, and we've always enjoyed, when we look out on the horizon where the sky meets the earth, there's a nice row of trees that soften and camouflage the commercial to the north. With five stories, we would lose that.

There being no one else who wished to speak, the Chairman closed the public hearing.

Mr. Braden: Would there be any consideration of maybe going to four stories? Or is that not feasible at all?

Mr. Arnold: Reducing the project by 25 percent is a huge impact to revenue. The maintenance, the operations – all the things that go into it. That's why you're seeing a lot of five-story buildings because at a certain threshold, it makes it financially viable.

Mr. Babcock: You know, my inclination is, personally, I don't mind five-story buildings on the main Johnson Drive. There is a precedence for that. I think rules are set for a reason, and I have a hard time, as we go away from that core, going with five stories. I think it needs to be graduated down to what our guideline is. It's actually more than that. I don't like all these deviations from the code. I don't like the idea that we're, I believe the way I read this, they are

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looking at wanting to cut the amount of trees in the parking lot. To me, it's too much. But, the main thing to me is the design and parking.

Ms. Buford: As someone who manages an apartment complex in Mission, and is also part of a company that manages over 1,200 apartments in downtown Kansas City, those are standard numbers we use. We don't have a problem at any of our properties. I own 224 units in the city of Mission. One bedroom, two bedroom. It's kind of standard. One car, one bedroom; two cars, two bedrooms.

[Overlapping dialog.]

Unidentified: That's the guideline, one car –

[Overlapping dialog.]

Ms. Buford: But that many residents aren't going to bring that many cars.

Unidentified: What's the workable ratio for a two bedroom? Because I know a lot of people who have a two-bedroom apartment and they have one person. Or there is a person and a child.

Ms. Buford: Two-bedroom apartments often have [inaudible].

[Overlapping dialog.]

Unidentified: I don't necessarily have a problem with [inaudible]. If I see someone [inaudible]... empty parking lot with [inaudible]. So, I don't have a problem with that.

Ms. Buford: The ones that are worried about the parking at the pool, you can get signs for the hours. I know we've done that.

Ms. Dukelow: I guess I have to share that I am also concerned about height, which is why I mentioned the idea of landscaping, recognizing that we can't do it right next to a building, so it could be in a park. But, I do sympathize, and I know that it's going to take a very long time before a tree is 67 feet high.

[Laughter.]

Chairman Lee: Well, if there are no more comments, I will ask for a motion.

Mr. Brown: I move for disapproval of Case No. 17-08. I just disagree with the height of the project.

Mr. Babcock: Second.

The vote on the motion was taken, (8-0). **The motion to deny this application carried.**

Ms. Sitzman: So, the motion to deny the application has passed. The applicant will likely come back to you, and based on the comments that I've heard, the height was the principal concern. So, if that adequately represents what you think you just did, we will proceed on to the next case.

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Case # 17-09 Final Site Plan-Mission Trails-EPC Real Estate

Ms. Sitzman: This application is a final site plan for Mission Trails, located at 6201 Johnson Drive. As you'll recall, this was reviewed for its preliminary site plan in June 2017, with four conditions. Two were to grant deviations to maximum height in ground floor uses in accordance with the Planned District regulations. You put stipulations on submission of final traffic and stormwater studies. Also, City Council approved the TIF project plan and redevelopment agreement for this development at their September meeting. That development agreement does stipulate reservation of 50 parking stalls for public use in the parking structure, and that construction must be completed by November 30th of 2020.

As you recall, this is a five-story mixed-use building containing apartments, retail space and offices on a larger site than the last one you considered. It's 2.8 acres of infill in the downtown, near the southwest corner of Johnson Drive and Beverly Avenue. Ground floor uses fronting Johnson Drive would include a restaurant and several small retail/service spaces, as well as leasing offices for the apartments. Two hundred apartments wrapping around an internal courtyard would be located on floors two through five, as well as behind the Johnson Drive frontage on the ground floor. A four-level parking garage would be located adjacent to the building to the southeast.

Included in the staff report is a table comparing what square feet you saw at the time of preliminary, and then this one. There's not a great deal of change. There has been some refinement of the number of parking stalls in the structure and on-street, how they deviate, and things like that. There's a slight change in the ground floor retail, which may be partly due to redesign, reconfiguration, or it may be better accounting for the true amount of space. In either case, the stipulation or the deviation that was placed had to do with the percentage of frontage, which has been met. As this is a final site plan, this is primarily the design review portion of the site plan process, and you do have the authority to conduct that design review.

Included in the staff report is an overview of the various components of the Johnson Drive Design Guidelines, which identifies topic areas, giving a recap of what the design guidelines say about those topic areas, and including some staff notes. I'm only going to go over the staff notes portion of that, looking for the relevant components.

The first and primary aspect is building site orientation. In this site plan, buildings are shown parallel to the public streets and extending the width of the property, with parking behind the primary façade. The building is located along the sidewalk with parking behind or to the side, and façade treatments were similar and appropriate to what you saw during the preliminary site plan time. We feel the building is appropriately sited.

Regarding parking, as I said, they are providing structured parking and minimizing the amount of surface parking in their development. They do this through a combination of surface, on-street and structured parking spaces. Access to that parking garage is both from Johnson Drive and Beverly Avenue.

We also had Traffic provide a full traffic impact analysis to follow up on the trip generation assessment they submitted previously. Obviously, this redevelopment will generate more trips,

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and the direction and flow of those changed as an office building, existing office building. We anticipate folks will be leaving the site in the morning, whereas with offices, they would have been arriving. We required an analysis to address all the immediately-adjacent intersections and to comment on their ability to operate, and what level of service would be provided in those areas with the additional traffic. Olsson Associates helped us with that review and are satisfied with the methodology of the analysis and the results. Therefore, no roadway improvements are recommended with the final site plan.

Regarding site access, we feel that adequate room has been reserved for the streetscape elements along Johnson Drive. We're looking for those to be designed to match the existing Johnson Drive streetscape that the City installed several years ago. So, the plantings and the street trees and the design of the on-street parking would be similar to the pattern that's already established in the downtown area. There are service and delivery areas located inside the building. Those would be accessed interior to the site. There are other features of the streetscape such as bike racks and street lights. We reviewed them with this plan and will continue to make sure that those are shown on the construction drawings to match that existing Johnson Drive streetscape. They've also provided a pedestrian connection to the community center from the south side of the site. That's something we were encouraging. Additional street right-of-way may be dedicated with the final plat as necessary to accomplish all of this.

Regarding screening, they have provided details for the trash enclosures, onsite transformers and utility cabinets. As I said, the service and delivery areas are interior to the building. The loading dock area will be contained inside the building and will have an overhead door that rolls down. It is screened when not in use. Also, the surface parking lot on the east side of the building has a retaining wall and some landscaping that helps to screen it from view, and it is the appropriate distance back from the public ways.

Regarding landscaping, there is adequate public landscaping, and they have provided private landscaping of equal or greater quality to that along the project portions of their site. That includes foundation landscaping around the entire perimeter and planting a western patio area and an internal courtyard.

Walls and fences are detailed. On the south side of the site, there will be some black iron fencing. Any retaining walls that are required would be as proposed, a segmental block wall with a matching color. At this time, they are anticipating a retaining wall along the east parking lot. They have made provision for possible replacement of the west edge of their property. They are not certain if they will need to replace that until they get further along in their analysis of the integrity of that wall and the impacts of construction. However, if it does need to be replaced, it will be of a similar design as the other retaining wall.

Regarding building façades, the applicant provided a description of their façade treatments. In general, wall faces are broken into solid and open spaces both horizontally and vertically using decks and tower features to accomplish that, as well as varied materials. There is a concentration of ground level features such as doors, storefronts, canopies, architectural lighting, decorative tile installations, and textured materials. Similar facade treatments are being implemented around all sides of the building. The face of the parking structure should be

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slightly altered, which I will discuss in a moment.

Regarding building proportion and scale, again, they provided a description of proportion and scale. In general, they are varying building heights and massing to accommodate the topography of the site. They have a sheet that shows the cut-throughs of the building in relationship to the different areas of the building, the different streetscapes. They do incorporate building backs at various levels of the building, and in different ways. The building represents an acceptable level of detail and design and is in compliance with the intent of the design guidelines.

Regarding building materials, there is a materials board here tonight that you all can view, with a general Spanish Revival or Mission Revival architectural theme represented in their elevations, consisting of cast stone bases, stone veneer, pre-cast panels, stucco, clear glass, tile roofs and synthetic wood timber canopy elements. It also has a mosaic Spanish tile accent. These are generally natural color tones that offer low reflectance. They are intended to be low maintenance. There is a quantification of the proportion of stucco used, but it's not itemized in the same way that our design guidelines call out, so I'm not able to tell you if they did or didn't meet that. I think they will discuss that with you tonight. Staff feels that stucco is a common material for this architectural style, and are taking into account that the amount they are providing may be appropriate in a greater proportion.

Regarding roofs, this is a flat roof behind a parapet, which is an acceptable design. Rooftop units would be screened by that parapet. Display windows would be included on the ground floor. Again, the quantification that they provided for the amount of ground floor glazing is slightly less, 43 to 57 percent, certainly less than the design guidelines request. They can discuss this for you, but as the entire ground floor is not retail, it has a slightly less amount of glazing, but is still appropriate, and certainly something you are able to review.

Regarding entrances, we feel those are appropriate. There are various entrances around all sides of the buildings. They follow the hierarchy that the design guidelines request in making primary entrances more obvious than surface entrances. There are canopies proposed along the ground floor to add interest to the façade, and they are appropriately designed. They have provided private sign criteria for your review. As we see with most private sign criteria, the applicant generally imposes more restrictions on the design aesthetic of signs than City code requires. They have done a good job of explaining what they would like to have on the building.

Regarding lighting, there are a variety of lighting techniques proposed for the site, including street lighting along Johnson Drive, which will match the corridor standard, as well as wall-mounted sconces, egress and pathway lighting, landscape accent lights, and parking lot site lighting. They provided the required photometric sheets, and we did check to make sure that adequate lighting levels are provided in pedestrian areas. They indicated the color temperature of the LEDs will be in the warm white color spectrum of 3,000 K, which is well below the level of the blue light LEDs that are oftentimes of concern for nighttime viewing. We would ask that they provide staff additional detail about specific luminaries that they are proposing. They picked a company that makes several different kinds and we were not able to verify that they were full

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cut-off before the staff report was published. We will check that before they are installed.

As I said, there is a parking structure included in this development, located behind the main structure and fronting Beverly Avenue. It's buffered from surrounding properties by a public street to the east and parking lots on City property to the south. Inside the parking garage there is planned covered bicycle storage. We will work with the applicant to make sure they picked the appropriate rack or locker system. The façade of the parking structure reflects similar design features of the main building, including stone and arched windows. There is pedestrian access inside the parking garage via separated walkways, which are connected to the Community Center and the surrounding street network. So, if you were to park in the parking garage, you could get to Beverly without having to traipse all the way through the parking field. You could also exit to the north and visit the development itself. An option metal garage canopy is shown on the top level. The structure provides walls that partially screen cars that are parked on various floors. There is an exhibit included that shows an outline of those vehicles and how much they would be visible in various areas. Staff suggested that the color and texture of some materials could be improved. That was our intent in the comments regarding use of stone veneer and/or similar treatments. I have discussed with the architect about other ways to meet that intent besides the specific stone veneer SV-1 and SV-2 applications. Staff feels that that would be an acceptable conversation to continue to have. I'm certainly not going to design the building for them, so if they tell me those are not appropriate for that construction type and they would be able to provide similar color and texture, that's what staff's comment was oriented towards.

We reviewed stormwater on site and Olsson Associates has reviewed the final stormwater summary and found it to be satisfactory. This developer appeared before the Sustainability Commission and received a favorable opinion. We reviewed to make sure the applicant met the deviations as far as the height of the building, and they have. A majority of the street frontage along Johnson Drive is shown as retail or service uses.

Included in your staff report is the findings of fact for a final site plan. Staff does recommend approval of the final site plan for #17-09 – Mission Trails, with stipulations. First, that prior to the issuance of any buildings permits, a final plat be approved by the City. This is for the dedication of right-of-way for all of those on-street improvements. Second, prior to the approval of construction drawings by staff, they accomplish several minor details. Those include providing acceptable bike racks or lockers in the parking structure; ensuring all Johnson Drive streetscape elements match the Johnson Drive project as-built drawings; and providing full cut-off information for the pole-mounted site lighting in the parking lot.

Finally, the third condition is one that has to do with the parking structure. We'd like them to submit a revised final site plan for staff review only, basically accomplishing extension of the stone material or similar treatment along the base of the parking structure to match the main structure, and that they integrate into the tower-like walls on the south and north elevations an additional color or texture treatment.

Included in your packet was their project narrative, our Olsson Associates' opinion letter; the full traffic impact study and stormwater study; their previous preliminary site plan and sign criteria;

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and the site plans. Also, the sign criteria should be approved with final site plan. That concludes staff's report.

Chairman Lee: Thank you. Would the applicant like to step forward?

Steve Coon, EPC Real Estate, appeared before the Planning Commission and made the following comments:

Mr. Coon: As Danielle said, we have been working hard to work through all these items and provide all the information she needs to evaluate the project. I think the first time I talked about this project, I said one of our goals was to create a building that withstands the test of time, one that fits in the community architecturally, something that is significant and blends with what's going in along Johnson Drive. We feel like we have accomplished that.

I want to turn this over to Henry Klover, our architect, who is going to walk you through some of the things we did. I'll be available for any questions you might have after. Thank you.

Henry Klover, Klover Architects, appeared before the Planning Commission and made the following comments:

Mr. Klover: It's my pleasure to be here tonight. Danielle did a magnificent job; she was very thorough. A lot of what I want to talk about are the massing of the building and how that came about. The site drops dramatically from right to left, west to east, so it gave us the opportunity to create a higher element at the corner of the project. What you see on the east side is the 2,500-square foot covered area that is going to have the ability to enclose in the wintertime. So, when it gets colder, we'll be able to extend the timeframe, and be a gathering area for the community. Wrapped around it is 5,000 square feet of retail that we're hoping is going to be either a one-user or a combination of users, coffee shops, which would make this a place to be and a place to gather. The retail on the west side is shorter, around 12 feet. As you get to the east side, those doors are about 16 feet in height.

A lot of discussion was on massing. We paid attention to how to mass the building, and the character elements. The first element that you see when you're coming from the east is a two-story element, which are set back so the units above it have 5-foot [inaudible], above it. So, the wall above is setback. The walls are articulated. Everything on the lower level is the natural stone that you see on the material board. We don't get into any of the stucco until above the second level. The arches are cast stone. We created canopy elements to provide for signage opportunities for each of those, as well, because right now, we don't know if it's going to be one; it might be two or three. But, it wraps that corner. So, the presence is not just the street presence on Johnson Drive, it's also the presence that faces north, as well. It's the same on both sides.

As you go down, obviously there's a main entrance to the residential, which is the arched element. We did a dusk view of the sunlight, you might say, so it's a little hard to see in this dusk view. We wanted to accentuate the character elements and features. The center of the building steps back, as well. What you're seeing is a combination of, the patio is stepped back five feet, and the center steps back 10 feet, and then, we pull the patios back out again. So, you have

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center column areas that are going up in the center, as well as a similar type thing on the other corner, where we wrapped the corner with patios again to create that back-and-forth. Retail on the first level is flush with the outside of the patio and the patio step back. So, the corners of the building articulate.

You can see where we've done the retail, and the restaurant area, you can see that it's got its own trash enclosure right below that. You can see the loading dock and trash. Those are behind the doors. They are very well hidden. We also took pains to create, instead of just a flat wall, on the west side, the building depths are only about 60 feet, and then they step back further. So, we're creating these environments. We thought that would be another nice environment, too, because that's where that wall she's talking about is today, the little stone wall that we don't know if it protects the foundation. It shouldn't. Our property line goes all the way to the face of the Salvation Army building. But, we were concerned when we started working with stuff, whether – It's a really nice wall, so we want to try and save it. But, the engineers are not sure because you have to put stormwater and all kinds of things in there. We're thinking of planting that wall, creating a nice area for pocket parking there. We take great pains. Everyone has dogs, so we have places for people to walk their dogs, too.

As Danielle said, there's interconnectivity between buildings. You can see the walkway connection. The disadvantage we have is the grade. We're stepping down and [inaudible] the building a few feet. If you've looked at that site, it drops almost 10 feet from one side to the other. It should work out nicely because you drive in off of Johnson Drive, you enter the parking garage. If you take a left, you're going through a controlled gate that goes to residents' parking. If you continue and go down, you will be in the public parking area, and you exit onto Beverly.

Part of the discussion was on the percentage of material. We're not used to calculating things on building elevations based on per floor because you don't look at things per floor. But, needless to say, the entire first level across Johnson Drive is all stone, or glass, or store front. We don't have a predominant massive quantity of glass, you might say, because it's keeping with the character. When you're building things that are supposed to look like natural stone, there are certain things they want to have. If you make them too narrow, they look spindly, and they look odd. We want to create something that is classic and timeless and would last, and 20 years from now, we would be proud of. Not kitschy or cute.

The site lighting. The light fixtures are LED fixtures, and they are completely viable. I've talked to the manufacturers and we will be able to satisfy any concerns you have. We are an exception to the conditions. The clarification for us was the material. The garage is built by Coreslab, or is intended to be built by Coreslab, so it's precast slab. There's all kinds of things they can do. I've seen them build something that looks like the wall behind you. It wasn't wood, but I've seen them build it. So, we've got the capability of doing anything, but you need to build it in the material as opposed to adding something after the fact. And that was my reservation. These will show up, they'll stand up, we're done. The base is easy. Obviously, we can do whatever we need to with the base, but something that's 40 feet in the air, we want it to be part of the building. And we've done projects like that garage where it's all acid wash, so it looks like cast stone, limestone. There's a lot of fun things we can do, and we'll work with staff on that. I just

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want to make sure that the statement of the SB-2, we don't intent to put tile up higher, and we wouldn't do that anyway.

I think I've covered all of the concerns. We're here to answer any questions you may have.

Unidentified: What couldn't be done because of construction type?

Mr. Klover: The precast of the garage. It's all concrete. The comment she had, she said she wanted something similar to the material in front. We can get it look similar to that, but it's not a tile added to the building, it's integral to the building. The construction of the building.

Mr. Brown: Are these your materials?

Mr. Klover: Yes, sir.

Mr. Brown: Okay. It's hard to –

[Overlapping dialog.]

Mr. Klover: I had to carry it in here. This is an actual product that we've used before. It's a natural stone that's actually thin, that we'll put on the outside of the building. It's similar to the material we put onto Oak Park Mall.

Mr. Brown: You can make it stay on?

Mr. Klover: That's not a problem with the material. That's a problem with installation. We're also going to fully embed this, too. We're planning to do real clay, not concrete. This is the material that she was talking about trying to get closer to. The colors are very subtle. When you hear the word "faux" – and this one is actually on foam. The wood is a VPython (?) material. We do that because if something is basically a synthetic that would rot, would disintegrate, [inaudible]. I've got [inaudible] buildings where wood is disintegrating and falling apart. And I've got this in projects now. The fence, the aluminum, this is an example of the mosaic tile that we're talking about doing on the corner. And then, there is the precast. This is a sample of the precast, by the way. It's an acid wash. You would not know that this is not cast stone. The wall that Danielle mentioned is this image over here. It's very rugged-looking, looks like natural stone. We use this on projects because most of the time, you can't tell a difference.

Mr. Troppito: What kind of security is going to be provided to the east end public space you were describing?

Mr. Klover: It will have garage doors that come down and you can close it off. I imagine in the summer time it won't be because it will be internal. That will be dependent on the tenants that we get and what type of operation they are. But there will also be cameras. These communities, there's about \$60,000 in cameras and security equipment. And the doors are all electronically locked. The gates are fast-opening for residents, as well. So, it's all about security and safety.

Mr. Brown: One of our discussion points in the preliminary was the amount of retail space on the lower level. That seems to have taken a 1,000-square foot reduction. What was the reason for that?

Mr. Klover: It's not actually a reduction. It's just that we didn't count some of the stuff that we

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counted before. What we're counting now is the restaurant. We're not even counting the 2,500 square feet of the patio space. So, you could say that's 7,500 square feet. The goal is to make it more usable in the wintertime. What you see all the way to the west side of the screen, that's the same retail. The only place the square footage went down was we didn't count the leasing office. We basically got it approved without counting the leasing office. So, that's what we did. We didn't reduce any of it.

Mr. Coon: It's important to point out that the storefront along Johnson Drive is all retail storefront. So, from a visual standpoint, I mean, all the retail, but the leasing office is also in the storefront. So, if you're driving on Johnson Drive, it looks like retail.

Mr. Klover: You wouldn't know that it's a broker's office.

Mr. Brown: Good. That's what we were asking for.

Mr. Braden: You mentioned a coffee shop. Did you say it's not going to be a restaurant, or an addition to the restaurant?

Mr. Coon: It's 5,000 square feet. How it gets used or broken up is still to be determined. You could do it with a single tenant, and they're talking to people in that respect. It could also be a sub shop and a coffee shop, too.

Mr. Klover: We'd be happy if it was a sit-down restaurant. Maybe a single sit-down restaurant, or maybe two. There's already a lot of retail along Johnson Drive, so we don't know what we're going to end up with until we get it filled.

Mr. Coon: And they don't make that decision when you're twenty-some-odd months out.

Mr. Klover: But we feel like with the open space and the arches, the lighting, the visibility to the street, it's got to be primo restaurant space. It just has to be.

Chairman Lee: Any other comments or questions?

Ms. Dukelow: I have a question, and this may be for staff. Are bike racks included -? We talked about it being in the parking structure, but I mean at the street level, as part of the streetscape.

Mr. Klover: Yeah, they're over here. When she's talking about the bike racks, we've got them internally here, but they're also over here in these areas. The reason she mentioned this style and type is because we picked something that the style was pretty close to what we saw there today. Basically, she'll get us exactly what you used, and we'll match it. We already tried to match it. We picked something that was close.

Ms. Dukelow: So, four tower-like walls on the south and north elevations. This is with regards to the parking structure, or are we talking about an extension of the stone veneer, SV-1, on the entire basement parking structure? You're telling me that the parking structure is only precast?

Mr. Klover: Yes. The stone at the base is added. Anything above that, we would like to make sure it's a [inaudible] material and color. And referring to the towers, there are arched elements that, for example –

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Unidentified: Correct me if I'm wrong, but I think she is referring to these elements here.

Ms. Sitzman: Actually, it's to the left –

Unidentified: These guys.

Ms. Sitzman: Yes.

Unidentified: I didn't understand before. I thought you miscounted.

Ms. Dukelow: Those elements -?

Ms. Sitzman: The one here, the one here, two, and on the east side there are a couple more.

Ms. Dukelow: And those are precast elements?

Mr. Klover: Yes. Everything you see here is precast. Except for the base. The stone base is added on after the fact. But they do wonderful work. Texture, character. If you go to the actual plan, if you want stepping stones, there's probably hundreds on the side of the building, all the different samples.

Chairman Lee: I'll entertain a motion.

Ms. Sitzman: Mr. Chair, the motion, if you want to amend the fourth condition to read "or similar," I think that would take care of the difference in the precast construction and what I called out in the original wording.

Ms. Dukelow: Was that the third?

Ms. Sitzman: Yes, I'm sorry, the third one. The one that says, "Submit a revised final site plan for staff review and approval, showing the extension of stone veneer" or similar. It's 3-a and 3-b. Instead of calling out specific material, SB-1 and SB-2, it would be "or similar." I think that's the intent in what is being discussed tonight.

Ms. Dukelow: All right. I'll make a motion. I'd like to make a motion that we approve Case No. 17-09, final site development plan for Mission Trails, with staff's recommended conditions:

1. Prior to the issuance of any building permits, a revised final plat must be approved by the City. Right-of-way should be dedicated including all on-street parking areas, sidewalks, and public infrastructure.
2. Prior to the approval of construction drawings by staff:
 - a. Provide an acceptable bike racks/locker in the parking structure
 - b. Ensure all Johnson Drive streetscape elements match the recent Johnson Drive project As-Built drawings
 - c. Provide full cut-off of parking lot/structure pole mounted site lighting.
3. Submit a revised final site plan for staff review and approval showing:
 - a. The extension of the stone veneer (SV-1), or similar, along the entire base of the parking structure to match the main structure.

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b. The four tower like walls on the south and north elevations should receive a treatment (SV-2) similar to the main north facade.

4. Approval of the private sign criteria as presented.

Chairman Lee: I'll second that.

The vote on the motion was taken, (8-0). **The motion carried.**

Planning Commission Comments/CIP Updates

Ms. Sitzman: I don't think there have been any CIP meetings since the last time you met. There is a bus tour that folks are going on this Wednesday evening, so there will be things to report back the next time you meet.

Ms. Dukelow: I have a couple questions. One is the issue of parking spaces. I don't know if we need to do something about that or not, but I understand that the two bedrooms with two cars is not reasonable. Again, I'm not sure what that ratio is, or what it should be. I feel like we have information, but we may need to formalize it.

Mr. Babcock: I agree.

Ms. Dukelow: That way, we don't feel like we're creating it at the time.

Mr. Babcock: I agree. The reason I agree is, I mean, like his desire to take a recommended parking criteria and drop it by 21 percent. You're saying that's okay. And I get what you're saying because that's your experience. Twenty-one percent is a big percentage. So, my point is, if our guideline's off, it shouldn't be our guideline. You know what I'm saying? I mean, we're supposed to be following these guidelines. If the guidelines aren't appropriate, maybe we should tweak them.

Ms. Dukelow: I was thinking that, as well. My next comment has to do with irrigation. I'm not sure that we should require irrigation because it's kind of wasteful. I would rather us encourage indigenous plants and no irrigation after the establishment period. And I'm not sure why we require irrigation. That's one of my questions. My follow-up question is whether the Johnson Drive corridor has irrigation.

Ms. Sitzman: To the first point, we have seen severe difficulty in establishing street trees, so we were making use of the ability to require irrigation so we weren't having to replant them once we made the improvements. Street trees are kind of in a constrained, hostile environment. So, we can give them a head start. That's our interest in irrigation, is that so elements of streetscapes thrive and survive after we accept them. I don't know that we would require irrigation of their on-site landscape. So, yes, things that are hardy and can thrive without extra inputs are the desire. But the requirements for irrigation have to do with streetscape.

Ms. Dukelow: Okay. Thank you.

Staff Update

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Staff provided an update on current and upcoming projects and events.

ADJOURNMENT

With no other agenda items, a motion was made and seconded to adjourn. (Vote was unanimous). The **motion carried**. The meeting was adjourned

Mike Lee, Chair

ATTEST:

Ashley Elmore, Secretary

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December 18, 2017

The regular meeting of the Mission Planning Commission was called to order by Chairman Mike Lee at 7:00 PM Monday, December 18, 2017. Members also present: Jim Brown, Scott Babcock, Stuart Braden, Brad Davidson, Robin Dukelow, Charlie Troppito and Frank Bruce. Also in attendance: Danielle Sitzman, City Planner; Brian Scott, Assistant City Administrator, Pete Heaven, Land Use Counsel for the City of Mission, and Ashley Elmore, Secretary to the Planning Commission.

Approval of Minutes from the September 25, 2017, Meeting

Mr. Braden moved and Ms. Dukelow seconded a motion to approve the minutes of the September 25, 2017, Planning Commission meeting.

The vote was taken (8-0). The **motion carried**.

Case # 17-08 Preliminary Site Plan – Martway Mixed Use-Clockwork Architecture + Design-Public Hearing

Ms. Sitzman: This is a preliminary site development plan for the Martway Mixed Use development. It's at 6005-6045 Martway Street. This application came before you back in September, at which time you made a recommendation of denial to City Council, based on concerns over height. The applicant chose at that time to rework their design and bring it back before you this evening. This is another public hearing since we advertised that second submittal. We also provided notice to neighbors within 700 feet of the development, which is not required by ordinance, but we notified those people of the first application when a neighborhood meeting was conducted, so we thought it was appropriate to re-notify them of the meeting tonight.

The subject property is currently occupied by three small offices. I will highlight the changes in their application tonight. As you can see in the plans, the applicant has removed one floor of the building, reducing its height. That has an impact on the parking requirements, so some of the requested deviations that they have made are no longer relevant. This is going to be Main Street District 2 zone and is also subject to the Downtown Johnson Drive Design Guidelines. The Comprehensive Plan calls for a pedestrian-friendly mix of mostly housing and limited office and retail uses in this area of town. It is intended to serve as a transition zone between truly low-density single-family homes and more moderate or higher-density, intense commercial activity along Johnson Drive.

As I said, there were a number of deviations requested with the original proposal. The MS-2 zoning district is a planned district and does allow for deviations to be granted. As I said, they reduced the height of the building, so I will go through those deviations and the impact on the proposed changes. Primarily, it is now a proposed four-story building with 117 dwelling units, which is a reduction of 39 units. All other elements of the site plan remain the same.

The first deviation they requested had to do with parking. That deviation is no longer needed as the required number of on-site parking stalls will be provided. That deviation no longer needs to be considered there because they provide all the required on-site parking.

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There are some outstanding comments that were made by our consulting engineer about the traffic study, which really has to do with determining whether the ground floor will be retail or office uses. That is not a major concern or consideration because it's such a small square footage of the building that's contributing to those traffic generation counts. That can be addressed at final site plan.

The remaining deviations – there are seven of them – have to do with elements of the site plan that you would be familiar with from the previous presentation. They have to do with rear yard setbacks; there's no change to the requested deviation there. They are still asking for the rear yard setback to be waived because the site is adjacent to the city park and essentially has the required setback built into the land and the park.

The next deviation has to do with building height. They are requesting an allowance of four stories and/or 56 feet 3 inches. This is one less story, and 10 feet 9 inches shorter than previously proposed. The maximum allowed height in this district in the underlying zoning is three stories and/or 45 feet. They are primarily requesting this additional height because the ground floor of the site is impacted by a flood plain, such that they can't have residential or office uses on that ground floor. They must leave that space clear for potential flood waters to move through. So, they've designed the building so that the ground floor is parking, which is an allowable use in or near the flood zone. It essentially boosts the ground floor height above what a normal habitable space would normally be to allow clearances for those vehicles. And then, additional stories of height that they're asking for to accommodate the dwelling units that would otherwise be on the ground floor. Essentially, they are offsetting the loss of the ground floor development due to the floodplain.

The next deviation has to do with minimum lot area per dwelling unit. This has been reduced somewhat because of the change in unit count, essentially changing the count on density. There are also several deviations - deviations 5 to 8 – from the original staff report that have to do with permissions to pursue an alternative design, buffering and screening on the site. Basically, they all have to do with parking lot setbacks, parking lot buffering, site trees, or interior open space. Those were all elements that could be designed to still accomplish the intent of the code, but to do it in a way slightly different than what our ordinances would customarily lay out for number of feet between things, etc. Generally, staff is amenable to an alternative design. We feel that the design of the parking underneath the building is a desirable feature, a slightly better design product in the first place. So, staff would recommend that the alternate design be allowed to be pursued as part of the final site plan to accommodate those alternative site planning elements.

I think that covers the deviations. Like I said, parking is no longer relevant, and we've adjusted the density and height considerations accordingly. As you know, because of this planned district, you can consider those deviations. There are findings that need to be made in order to allow them, so you can consider those as part of your deliberations this evening. Eventually, the application will be followed up by a final site plan, which is the point at which we would do a more in-depth review of the actual architecture of the building, evaluating it against the Johnson Drive Design Guidelines, for instance. Also, following up with those open questions that still remain about traffic impacts and stormwater design. Olsson & Associates is our on-call engineer and they have reviewed the preliminary studies for both of those elements. They are generally

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satisfied with the preliminary design, and just ask that we reserve the right to make future comments on some of the elements of those studies that are still to be determined.

Just to note that there would be some off-site improvements required in the streetscape. That is a requirement of the developer, and we would review their design for things like the impact to the Rock Creek trail in this area. There would also need to be a private sign criteria established. Both of those things can be taken care of at a final site plan review. Included in the staff report are findings for consideration for a final site plan. We do feel that the requirements have been met for a preliminary site plan, not final, excuse me.

So, to update the staff recommendation, we removed the deviation for parking, but staff does feel that the proposed plan conforms with the Comprehensive Plan, meets the overall intent of the MS-2 zoning district, and complies with the required findings for the planned zoning districts and preliminary site plans. Therefore, we are again recommending approval of the plan to the next City Council meeting, with the seven requested deviations. There are two additions included in staff's recommendations that have to do with the final traffic studies, and provide additional comment on ADA, storm drainage, and flood plain-related concerns. And then, a final condition having to do with requiring adequate right-of-way for the required streetscape elements.

That concludes staff's report. With me this evening is our land use attorney, Pete Heaven. He is available to answer questions.

Mr. Brown: Mr. Chairman, sorry to interrupt. At six o'clock this evening, Commissioner Babcock called and asked if I would meet him at the site to satisfy some concerns he had regarding some comments that were made at the last meeting regarding lights on the houses across the creek. So, we didn't discuss the item at all, we just rotated back and forth between somebody's headlights shining across the creek and the other person standing on the other side of the park. So, for full disclosure, we did that on our way to this meeting.

Mr. Babcock: Yeah, that's accurate.

Christian Arnold, Clockwork Architects, appeared before the Planning Commission and made the following comments:

Mr. Arnold: Good evening. We can quickly go through these. There aren't significant revisions, other than the major height reduction. When we visited in September, there were a number of concerns that were voiced, largely from the residents on 61st Street. So, we are hopeful that this evening, with the revision of the reduction in height by one full floor, that is reasonable, and that we are trying to be as aware and accommodating as possible.

Some of these are the same as before. For anyone that wasn't able to attend last time, we do feel like there is a change in housing preferences. We feel the site is still nicely positioned in a walkable community. To meet the growing demands, I think a lot of the people who are here, the residents that are here, tend to be single-family structures, homeowners. There is a changing demographic, and creating housing in this neighborhood will allow this area to continue to grow. With what we're seeing in most cities around the metro area, that you don't lose those residents as they get to a certain population, or younger residents that desire this type of housing. That housing does require more density, as you might imagine. As Danielle mentioned, we are

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requesting less density than was previously approved on the Mission Trails project, so hopefully that is a good thing for everyone here.

Also, it was brought to our attention that, unbeknownst to us, the *Shawnee Mission Post* put together a survey a couple of days before the last planning commission hearing, and it was refreshing to hear that, you know, in their informal survey, that a majority of people felt like this project was a great fit, or an okay fit. What ends up happening, unfortunately, is that people who are happy with the project don't really show up and support it. We kind of hear from people who are struggling with change.

All of the other attributes of the project are still intact – the amenities of the park, the pool, city hall, community center, Rock Creek trail. Buildings continue to lose tenants. 6045 is about 80 percent vacant now. The other buildings are at 33, 65 and 75 percent vacancy across the board. So, it's only a matter of time before the buildings become mothballed. They are not commercially viable in their current state.

As Danielle mentioned, we're kind of stuck in this tricky position where we've got to get the building out of the flood zone. It doesn't necessarily have to be as high as it's shown right now, but when we met with the fire department to get access to the back of the site, they established where that line of the first floor would be. So, we're looking at a three-story, but pushed up to get out of the flood zone and out of the way of fire department equipment. That's how the building height, scale and mass is working out.

As you might imagine, taking a floor off a building is significant. We're still getting our arms around that. We're getting updated cost estimates. General numbers, the lost gross revenue for the project is about \$600,000 per year. Obviously, that has a very significant impact on appraisal and future taxes. I don't think that with new construction, that anything less is going to be financially viable, so we're right at that threshold. We're optimistic, again, because of the location and the positive feedback we have received in the past, and in working closely with staff, that this still is a project worth pursuing.

All of the makeup is very similar to what we discussed before. Because of the lower density, all parking is on site now. Height and parking were two big concerns that came up last time. So, now that we have 39 less units, all parking is on site, which makes it pretty easy. One deviation that is requested is on the west parcel. We're working with our civil engineer to see if that can be pushed over. We think it can. Right now, what's driving that infringement on the west property line is really the turning radiuses of the fire trucks underneath the podium. So, to miss the columns and get around, it's pushed that last row of parking over. There's a section right here. This is the adjacent parking lot. When we met with Kathleen earlier, we talked about, you know, there's a little strip of landscaping, or grass, or gravel – I think gravel? Grass? - between the two. Since it's not very generous, we talked about getting rid of it. I think Kathleen was concerned about that. We do have our parking right up to that. So, we have parking, a four-foot strip of grass, and another parking lot. These columns right here are what are preventing us from pulling this parking over. We're hopeful that we can take it right out of this connection here and actually pull that back. If we're able to do that between now and the final site plan, those two deviation requests would go away. But, right now, that column is driven by where the fire truck turns are. We're still trying to refine that. That's kind of where we stand right now.

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One thing we wanted to do is pull the building away from the property line. So, rather than pushing it right up to the property line, we have it pulled back between 20 and 40 feet, just so that a lot of light will get into the building through the windows along that edge.

Here is the revised elevation, similar to where we were at before, but with one floor less. Right now, we're in line with the Mission Square project, where it's three stories on top of their parking structure. We're significantly lower by a whole floor from the Mission Trails project. Here are the views that we updated, looking from the south. Here's the updated view looking north. Light poles look like they're taller than the building. Here's the view from the flood way, and here's the views in comparison. So, as you can see, we're about the size of Mission Trails' parking structure in terms of scale. Here is how the elevations look on the site. Here's Martway, here's Mission Square, and here's Mission Trails, in relation to each other.

Here are the revised views from the street, showing the floor removed. Here is another view. A majority of the residents along 61st Street are not able to see the property. It's really just the ones that are right across from the park. So, as you can see from those views across from the park.

Once again, we are pleased that we have staff's approval. We hope that the changes that we've made and addressed, kind of a major concern in trying to reduce the height of the building. As we mentioned, we're going through all the other updates, the costs, operating performance, and things like that. Any questions?

Mr. Babcock: Go back to the one that showed the building footprint. I think you said 40 feet from, you pulled it back from the parking line.

Mr. Arnold: Well, we did that before. We already –

Mr. Babcock: I'm just looking at it right here.

Mr. Arnold: So, that's 40 feet from the property line, and this is 20 feet.

Mr. Babcock: And how far is it from the street?

Mr. Arnold: This is probably 15 feet. Rock Creek has to be preserved, so that kind of sets that dimension. And then, we're pulling it back a little further for doors of the retail and office spaces.

Mr. Babcock: Okay. You said it's a full floor less. As far as altitude, what's the difference between Mission Trails and the EPC Building? In height?

Mr. Arnold: I think it's about 12 or 18 inches taller than Mission Square, and it's a full floor, so, 11 feet shorter than Mission Ridge.

Mr. Babcock: But the ground drops off also.

Mr. Arnold: Oh, yeah, well ---

Mr. Babcock: What I'm getting at is, what's the actual altitude?

[Overlapping comments.]

Mr. Arnold: *[Inaudible]* It's 40 feet shorter, or something -? It's hard to see on that scale. You can kind of see proportionally where about the second or third level is.

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Mr. Babcock: Yeah, I just wanted to hear it.

Mr. Arnold: I would guess, ballpark, 30 feet lower.

Mr. Babcock: Than the EPC Building?

Mr. Arnold: Yeah.

Mr. Babcock: And then, how much from the Mission Square building?

[Multiple overlapping comments.]

Mr. Arnold: It's right, off to the left there. They're all in there. It's hard to read. There we go. So, the Martway site, the Mission Square site, Mission Trails site. So, we're 24 feet lower than Mission Trails, and then, the top of the building would be 31 feet shorter than Mission Trails.

Mr. Babcock: Okay. And when you're looking at the parking lots, when I drive over there currently, I drive into the existing parking lot. Is that the same level that your parking lot would be, or is that going to change?

Mr. Arnold: It's going to change. It will be pretty close to where it's at. We have to be very-

Mr. Babcock: What's your definition of "pretty close?"

Mr. Arnold: There are some low spots, there probably won't be much low spots.

Mr. Babcock: I mean, are we talking a foot, or five feet, or -?

Mr. Arnold: Oh, no. Within a foot. We're not able to adjust the grade too much because of the flood zone. If we adjust the grade in the flood zone, that triggers engineer work.

Mr. Babcock: When I read this, the way I read it, I don't see any landscaping. Can you talk about landscaping around this building? Because, like, for instance, you're looking for a deviation of the one-tree-per-20-parking spaces.

Mr. Arnold: Yeah. That's typically for, like, a surface parking lot. We have a building over the parking lot, so it would be less practical to put trees under that. I know that you know that, but I just want you to know that some of those deviations are not for mixed-use development that have parking.

Mr. Babcock: When I read this, it...

[Multiple overlapping conversations.]

Mr. Babcock: I get the feeling when I read this, I picture glass and concrete. I don't picture any green space. So, I want you to tell me what you're going to do about it.

Mr. Arnold: Okay. One of the things that we decided, that we like about this site, is because of the landscaping around it. We're keeping the landscaping and supporting everything that's along Rock Creek. So, we will have trees and grass along the front of the building. Everything on all sides of the building, there is landscaping, as well. We're just not showing any landscaping inside the parking lot, under the building. We have a landscaping plan that kind of shows all of that stuff that wraps all around the site, like any normal project would.

Mr. Babcock: Do you have something that you can -?

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Mr. Arnold: Yeah. So, all of the notes are going to follow the landscaping along all sides of the project. So, about 10 to 15-foot buffers in areas, parts of the back. There is a section right here where we're right up to the property line, so we're not putting any landscaping there. But we do have the creek buffer that's right behind it. It's about 25 or 30 feet. And then, we have landscaping and plants along the front. Over here is a sidewalk that runs along for egress from that stair, so people come out of the stair and go over here, and walk along that edge of the sidewalk. Currently, we're not showing anything here, but as I mentioned, if we're able to get our civil engineers to tighten this up at all, we might be able to get that buffer back. So, instead of being a four foot, it will be more of an eight foot.

Mr. Babcock: If I remember right, we actually have extra parking places. Is there any possibility, where there is a bump-out, to stick some in there?

Mr. Arnold: Yeah, and that's one of the things where, worse-case scenario, we slice out a row of parking all the way through here – one, two, three, four – and then this whole thing pulls in, in addition, you know, similar to this. We could pop something in here or here, as well. Create a little buffer along there.

[Multiple overlapping conversations.]

Mr. Babcock: The last thing I have for you is, what would you be willing to do on the City side to offer screening, or talk about screening between the residents to the south and what you plan to do between your parking.

Mr. Arnold: Last time we met, it was a lot easier to be generous because we had an extra \$600,000 a year in gross revenues coming into the project to kind of subsidize some of that. Now, I think the conversation is still open. We'd be open to what that would be. I just don't know what it is at this point. The project pro forma is in a very different place, as you might imagine.

Mr. Babcock: The last question I have is for Pete. Pete, I tend to be a black-and-white guy. I mean, we've got rules. For instance, the rules say max allowed stories is three stories. But, we all know there's a gray area here. The thing is, usually it's one or two deviations. In this case, we were asking for nine, and now we're asking for seven. Can you discuss how you handle grayness?

Pete Heaven appeared before the Planning Commission and made the following comments:

Mr. Heaven: The deviations that we have in our code are to give you flexibility, to prevent the black-and-white decision. In large part, it depends on the complexity of the project. And then, you couple with that whether it's an infill or a redevelopment project where you have much more challenges than a green field development. In a green field development, we would basically say, "Here's the code. Follow it." But, we're dealing with what may be an attribute to the community, something that the community wants to see. In an infill basis, deviations are always going to be asked for. The nice part about our deviations – there are four criteria you must find for each deviation – is they are really stated in the negative and not the positive. That's where your black and white comes in. If you find that a deviation does not affect property owners, then you can grant the deviation. If it doesn't adversely affect the public health. So, it's sort of stated in the negative to make it easier for you to make that decision. But, I've seen some projects where you've had 20 deviations, and each of them must stand on their own, and each are

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separate. In this case, yeah, there are seven, but they're not really cumulative. They're all separate areas that can be easily divided.

So, I know that doesn't answer your question. I wish I could give you a simple one. But, it's based on the complexity of the project.

Ms. Dukelow: I have a question relating to the landscaping. So, while we have that up, I think that would be, while we're still on this particular image. My question has to do with deviation #5:

5. Approval of the requested deviation to waive the 6' parking lot setbacks along the west property line. Alternative screening of the area should be provided for consideration with the final site plan.

Ms. Dukelow: It seems to me that we're not really waiving all of the buffers because there is a considerable amount of landscaping both to the south and the north. I just want to clarify that, because I don't want to request a deviation to waive parking lot buffers for the entire site, and then, lose what landscaping is able to fit on the site, which is, in fact, helping to buffer the parking. So, I'm looking for a clarification.

Mr. Arnold: We're able to accommodate it everywhere except that western parcel, the western property line. We looked at it, and there is a parking lot for about half of it. So, as far as screening, the buffer is between two certain spots. That's kind of why we're trying to push it over there, if there was something else of greater value or a different use there. But, that four-foot buffer would screen a parking lot from a parking lot. So, I think it's kind of like, you know, how do you weigh that out?

Now, like I said, if we're able to pull that over and tighten up that turning radius for the fire engine, we wouldn't need that deviation at all. But, that's the only side that we weren't able to get that buffer.

Ms. Dukelow: So, would it be appropriate to – this is a question for staff – to wordsmith that? Or should we just leave it for simplicity at this point?

Mr. Heaven: I would recommend wordsmithing it. If you wish to grant the deviation from the west boundary only, the setback or the buffer, you can do that. You could also grant a deviation to the extent necessary to accommodate this landscape plan. So, in the area where there is more than four feet, there would be no deviations. And if there are other smaller areas where there wouldn't be four feet, you could grant him that. So, for simplicity sake, I think based on what the applicant just said, I would deviate the west line only.

Mr. Troppito: To Pete's point about public health, the last time I asked you about the chromium content that was shown on your material boards, and whether or not you have an opinion by professional environmental engineers, or an industrial hygienist, who could speak to that, whether or not it is a health hazard. Have you gotten that?

Mr. Arnold: The manufacturer has confirmed that it meets all building material requirements, and that the materials that they use in their products, both notes, um, concerns or, or safety issues to the public.

Mr. Troppito: So, in other words, no.

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Mr. Arnold: No, the main factory does stand behind their product.

Mr. Troppito: The question was, do you have -?

Mr. Arnold: An independent source? We have not engaged an independent source to look at that material. The actual material gets approved in the final plan. These are representational of what they could be. There's a very good chance that through the planning and budgeting process that those materials could change because we need to adhere to the Johnson Drive guidelines. All those materials come back in the final plan submission. We don't select actual materials just yet.

Mr. Troppito: Well, let me just restate – for the second time – that I would like to see a professional opinion, not just the manufacturer's opinion. I would like to see a professional opinion from a registered professional environmental engineer, or a certified industrial hygienist, as to whether or not there are any adverse health effects that can be expected from the chromium content of the Nichiha board product.

Mr. Arnold: Yep. There's a good chance that that material won't be in the project when we come back with the actual material samples. But, in the event that they are, we will get that covered.

Mr. Davidson: For the record, the City swimming pool is south of the facility. Through that new pool construction process, we talked about – and again, I just want to throw this out for conversation – we once talked about a pedestrian bridge over the creek, you know, from Sylvester Powell, which this property right here obviously is built right there. The possibility of, through this project, you know, maybe addressing a pedestrian bridge, if that is something that could be a part of the project. Obviously, it would have to be designed and the logistics would have to be worked out. That's just for the record.

Mr. Arnold: I think we're open to that idea. I was getting mixed signals at the last meeting, where some residents didn't want anyone else to use the park. They just wanted their access to it. So, I would look for guidance as to what to do there. We like the idea, but we also don't want to upset residents.

Mr. Davidson: I wasn't at that October meeting, so I wasn't there to hear about that.

Mr. Arnold: Okay. That was my take on it, and that's why I liked the idea of a connection. But, some of the residents on that street did not like that idea.

Mr. Davidson: I can just see, like, summer campers coming from Sylvester Powell, and they've got to walk all the way around the facility, all the way around the police station.

Mr. Arnold: Absolutely. Agreed. And there is an area that is on the southern corner here that used to be part of the parcels that was given to the City. So, there's kind of a natural point across there. But, there could be others, as well.

Mr. Davidson: That's all I have.

Mr. Brown: Could you put up the south elevation? At the parking level on the south façade, what do we have there that would be adjacent to what is now the tennis courts?

Mr. Arnold: It might be easier to see if you go back to the landscaping plan. Right now, there is a landscaping buffer, and then parking.

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Mr. Brown: There's going to be some [inaudible] in this neighborhood right here, where there is no landscaping because you're right on the property line. So, what, if any, is the building façade material is at the parking level?

Mr. Arnold: There is no, it's just parking level -.

Mr. Brown: And that was for the reason of free-flow for flooding? Or, what is the purpose of not shielding the headlamps of the cars that are parked in there?

Mr. Arnold: I think there is a [inaudible]. I think, largely, it was just given the way the engineers laid out the parking lot, to try to get spaces along that back side. Is there a potential to put a thin buffer there and do compact car spaces along that back side? I think that is possible --.

[*Overlapping comments.*]

Mr. Brown: -- buffer as much as a screen for headlamps.

Mr. Arnold: Yeah.

Mr. Brown: So, any material that would stop that, that would be compatible with the building, that would, in my opinion, be appropriate at that location.

Mr. Arnold: Yeah. In that area, we have to be careful because we can't put things in the flood zone that impede the flow of water. But, in the event that we could, I would be open to that. You could probably look down and see the headlights if you're above. Is that correct, or -? You mentioned you went over there?

Mr. Brown: Yeah. Well, it depends on which vehicle you're driving, right? My truck sits up much higher than Scott's car. In the case of his vehicle, you know, it was kind of pointing down, so, when the headlamps were on normal, you didn't see them so much, with the exception of the house that's immediately adjacent to the creek. That shines right on their back wall. But that's more southwesterly anyway, on the other side of the north/south tree count. But, just an attempt to shield those headlamps, whether that be a flow-through louver of some kind, etc. It doesn't have to be anything arduous.

Mr. Arnold: Yeah, I think we're open to that. If you look at the topography, it seems like it's about 20 feet lower than those houses, so you won't have headlights shining into things. But, you will be able to, if you're standing on top of the hill, of course you can see the lights over there. But, I think we're open to figuring out how to be good neighbors and screen that in an appropriate manner.

Chairman Lee: With that said, we will open the public hearing.

The Chairman opened the public hearing.

Adam Dearing, 5711 West 61st Street, appeared before the Planning Commission and made the following comments:

Mr. Dearing: I have two concerns. I brought up a few things in the last meeting. Right now, I still feel there is a real elimination of surrounding green space. I think it was mentioned earlier

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tonight that most the deviations take up a lot of that green space area, with no trees given the parking under the building. That's understandable, but the surrounding area, it is quite limited.

Secondly, with the majority of the deviations being for those setbacks that could be green space, I'm curious if those deviations are not allowed, how that reduces the parking, which would also then possibly be a deviation. I'm curious what that looks like, if those deviations were not allowed.

Mr. Arnold: I can answer those quickly. Just to clarify, the only reduction of green space is that western edge that we talked about, which is the four-foot buffer. And, as I mentioned before, we're committed to seeing if we can put that back into it, so there is no other elimination of green space. That would not affect the parking because we have more parking than we need.

Sarah Hinkle, 5711 West 61st Street, appeared before the Planning Commission and made the following comments:

Ms. Hinkle: My main concern is still the height of the building. I know it's about 25 percent taller than the code, but the way the area is described, it's a transitional area between low-density single-family homes and the commercial side on Johnson Drive. But, I don't consider transitional to be a project that's 25 percent taller than City code. That's my biggest concern.

Dan Aldrich, 6001 West 61st Street, appeared before the Planning Commission and made the following comments:

Mr. Aldrich: Good to see you guys again. Height is the deal. I still think that's the deal. It has been a deal, and it's a game-changer for residents. What is it now? About 50 feet tall? It was 60-some. So, it's 56 feet tall and it's 300 feet from somebody's house. I really don't need to say any more than that to point out how ridiculous that sounds. But, it's true. You're talking about 56 feet high, 300 feet from somebody's home. And I didn't move into Mission 28 ½ years ago to have somebody building something like that next to my house. So, before we start granting deviations to height like that, at that scale, that close to somebody's residence, I think there needs to be a lot more done, at least at a park board level, or something, before we consider anything that silly. To me, it sounds silly.

Light pollution is still a major deal to us that live on 61st Street because those porch lights of all those residences are going to be on and shining into homes. It was a good point on the headlights. I really like what we did with the pool because us folks that live here, you know, dealt with the whole pool thing, and supported that. That's great, building up a berm like that to protect the lights from hitting people's houses. That's a great idea. This thing being 50 feet high with lights and all, that close to people's houses, I still can't fathom you guys considering this in its current form. And if it wasn't feasible, you said it wasn't feasible to go anything less than five stories, right? Is that what we heard? And now, four is okay. I mean, I'm just flabbergasted that we're at this point. So, thank you for hearing me.

Bill Nichols, 6019 West 61st Street, appeared before the Planning Commission and made the following comments:

Mr. Nichols: First, I want to thank the four newest best friends forever for peeling me off the sidewalk this evening. I missed a step. It's hard for me to get up anymore. I'm curious about something. Tonight, I went and shot the elevations at the driveway entrances for AMC. There's

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an eight-foot difference in elevation. The west side is 910 feet. The “C” side is 892. That’s at dashboard level. Now, my understanding is the deviation is one foot, maybe two. Where is it? Is it nine feet? Ten feet? I don’t know, because I haven’t kept up with this.

Now, as far as the light pole issue. It’s more than just headlights. We’ve got all the lights on the building shining onto the parking lot. And last time I talked about flat lenses and what-not, that needed to be addressed, please, because it does affect us on 61st Street. I don’t remember seeing anyone in here that was in opposition to the five-story along Johnson Drive. There could be. I didn’t see them. Again, maybe there was.

Unidentified: Yeah, there was. There were eight of us here.

Mr. Nichols: I do remember now. [*Laughter.*] I think that had to do with the apartments next to Sylvester Powell Community Center. Yes?

Unidentified: And Mission Square.

Mr. Nichols: But had nothing to do with 10-story single-family residents. Just us. I don’t have much else on this. I just don’t think it’s a good fit. And on social media, we – or at least I did – asked for the developers to meet with us. I didn’t hear from them, but that’s their business. Thank you very much.

William Wilson, 6180 West 61st Street, appeared before the Planning Commission and made the following comments:

Mr. Wilson: I’d like to back what the other guy said, that it’s still too tall for the area. I didn’t buy a house on that street to be looking up into somebody’s bedroom window, balcony, or whatever. And porch lights, and security – Yes, I agree, it has to have security lights, but I don’t want them on the back of my house, or in my living room.

The other thing is, the paper I got in the mail said the waste receptacle for pick-up was going to be on the southwest corner where the houses are. Why would you put trash by somebody else’s neighborhood when they could put it down by City Hall? Or where the buffer is on the planting spaces for the swimming pool. The question is, can you move the trash cans someplace else if the building is actually going to go there?

Ms. Cuppage, 6220 Martway, appeared before the Planning Commission and made the following comments:

Ms. Cuppage:

I’m back. And I really feel for the people on 61st. We’re at Mission Square, and we have exactly the same concerns we did about the amount of parking on Johnson Drive. Same concerns. We didn’t like it then, and we don’t like it now. I looked over all the questions and answers, and I’m not going to go into all the questions I had, or the answers, which really changed. The deviations are worrisome. I also noticed that the architect said, I’m not an English teacher, but “it might be possible,” “I think,” “We could,” “I’m open to that,” “It might change, but -.” Do we really know what he’s going to do? Maybe we do, maybe we don’t. I think you should know exactly what he plans to do. Not maybe moving to the west, maybe moving to the east.

Many communities now have second thoughts about density projects. They’re supposed to enhance the income of the city. I still haven’t heard anything about that. We already have one

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such project from which no property taxes will be collected for many, many years. We have been told that this will have no impact on the taxes of the other residents. We are included in that at Mission Square. We've also been told that the sales tax generated by the new residents shopping on Johnson Drive will compensate for that money. Maybe it will; maybe it won't. I suggest we wait and see. In five years, if Mission Trails is fully occupied, if the City can prove that the sales tax generated by these new residents shopping on Johnson Drive is equal to the money that taxpayers are going to be losing, or we're going to be paying, extra, then maybe it's time to build another one of these density projects. But, if you've been reading your newspapers, I think you've seen they're not all as wonderful as they have been led to believe. I don't want to name them all, but, there you are.

It was also hinted at one time that we seniors wouldn't have to worry about it. The comment was that we wouldn't be here in five years anyway. Well, I'm here to tell you, we will be. I may not be here, but Mission Square will be filled with wonderful residents who shop right there on Johnson Drive. And we intend to do that. And we don't have cheap property over there. I think we had a tour of the Planning Commission. I don't know if you came, or if it was City Council, but we're concerned about what's going to happen with this. I read nothing in the information that was given to me. It said that if you rent for market price – I have no idea what "market price" means. How many of each? How many studios? How many bedrooms? How many two bedrooms? Size of the balconies? We have wonderful balconies at Mission Square. I think there are too many questions on here to say go ahead with this project. I think they need to come back again, and maybe again. If it's that important to them and this is such a marvelous project, then I think we need to hear more. And I definitely agree with the green space. It's gone. And maybe we can be an example in Mission, that we aren't going to do what everybody else does, and we're going to keep this a green space. Maybe find something else for that area on Martway.

Kathryn Koca, 6220 Martway, appeared before the Planning Commission and made the following comments:

Ms. Koca: I am also a Mission Square resident. I have been here before, and one of my main concerns is still my main concern, and it's about the traffic on Martway. We do not have an entrance to Martway except through the Sylvester Powell opening. I can see that once the Mission Trails project is built, we will have a tremendous problem getting out onto Martway, especially making a left turn onto Martway. So, my concern is when you do your traffic impact analysis, that you please include the impact of the Mission Trails 200-whatever cars that will be coming in that way.

Also, my other comment is in support of the residents on 61st Street. I heard tonight that they didn't want their park to be used by other people. I heard what they said. What they said was that it would be overused, and there would not be availability for all the residents of the city. Those are my concerns. I hope you consider them.

Brad Ware, 6009 Outlook, appeared before the Planning Commission and made the following comments:

Mr. Ware: I live up by Martway and Outlook. My first question is, it's my understanding that these people own the property now. Is that right?

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Mr. Arnold: Yes.

Mr. Ware: I was wondering if we could count on them maintaining the new property like they're maintaining that falling-down, overgrown fence line that butts up against the creek right now. It seems like they don't care about it right now.

Another thing is, when I was in grade school, I had to walk down 61st because Martway wasn't there. I've seen quite a few changes here. I did see a picture of this, and it does look like a behemoth. You know, I realize they're in this to make money, but, you know, it seems like sardines, packing them in as tight as they can. It just doesn't seem like a fit. We walk our dog in the park quite a bit, and if you've got this thing right across the street to the park, the park is going to be totally different. It's not going to be enjoyable at all.

As far as residents, we've kept quite a few apartments up where we live, and we've seen some strange things. We had one guy out there yelling at traffic. We watched another guy break into one of the sliding glass doors on Martway. So, I don't know what we're going to get. If we had a guarantee of normal people coming in, that's one thing. That's the unknown. I just don't think it's all that great of a fit. They originally asked for five stories. Well, it's the art of the deal. You ask for a lot more than you're really willing to settle for. So, maybe they're just happy as they can be with four stories. Anyway, I just hate to see us become a crowded [inaudible]. Thank you.

There being no one else who wished to speak, the Chairman closed the public hearing.

Mr. Arnold: I jotted down a few of the comments; hopefully I can address them. From the last meeting, we did the same. We jotted down all the comments and responded in writing, sending them back to everybody that was within that 700-foot radius. Also, the gentleman mentioned reaching out to us on social media. In the letter I sent out, I encouraged anyone that wanted to visit with us to reach out directly. I didn't see the social media; otherwise, I would have contacted that individual.

Obviously, there is still a height deviation request. Our goal was to try and be flexible. The woman that mentioned that, you know, that we're open to comments, and that we are trying to make this as good as we can, that's really just a demonstration of what we want to try to do to get a good project here, and that we are trying to be flexible and listen to the residents. We are not trying to stand up here and say this is the way this has to be. So, some of the comments that we're trying to be flexible, I'm sorry that they were not perceived in the right light.

The gentleman that had the question about the headlights, I think we already talked about it. We are open to trying to create a buffer along that back side. Hopefully the landscape, we'll do more of that. As a reminder, you know, there were probably some residents that showed up to protest the Mission Square project, given that it's about the same height as what we're proposing. If that project would not have been approved and moved forward, a lot of the people here would not have a place to live right now. So, sometimes change is hard, but hopefully development creates a community –

[Overlapping noise and comments from the public.]

Mr. Arnold: -- and hopefully, it's a way to recruit younger people to the community that are looking for Class A apartments.

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There was a question about the trash. Right now, it is located in an area that is not in the flood zone. We did look at a location that was more concealed, but it was in a flood zone. The engineers said we had to move it out of the flood zone.

Chairman Lee: [*Bangs gave!*] I would ask that we, if we're going to have conversations, that we step outside, and not be interrupting.

Mr. Arnold: We also have to be careful because the way the trash picks up, it has to be outside from under the building. So, it almost has to be in an open space so they can lift it up and dump the trash. So, there are some areas that we can put those.

There was a comment that there was not enough information on the plan about the sizes of the units. All of the units are shown on the plans, the square footage is shown, the bedroom mix is shown, which ones have balconies, which ones do not. All that information is there.

There was a comment about the green space being gone. There's more green space being proposed than is there now. Keep in mind, it's all buildings and surface lots right now, so we are creating a buffer. There was a comment about the maintenance of the property, being overgrown. A lot of that is actually in the floodway. We have tried to maintain the front of the property for nice street appeal, hoping that will attract tenants in the meantime. The property is currently losing money, so we are very limited on how many resources we can put into maintaining the maintenance and landscaping. But we try to mow, trim trees, put in new lights, and things like that, that cover the basics. But still just being more and more vacant, and losing more money, you know, as you might imagine, it's difficult to run a business that way.

We are planning to build Class A apartments, so hopefully we deliver a quality, affordable apartment project to the market that will attract the right kind of residents. I can't guarantee "normal" people, but we would sure hope we would get "normal" people. I think that's all I have. I'd be happy to answer questions.

Chairman Lee: Any questions?

Mr. Brown: I have a question. Would you address – and this is a term I'm unfamiliar with – Mr. Nichols, what did you mean when you talked about "flat lens," and does the architect understand what he's talking about? And, is there a way to redesign the windows, or -? I assume it has something to do with the windows.

Mr. Nichols: Basically, light pollution.

Mr. Brown: If you wouldn't mind coming up and defining what the term "flat lens" means, so I understand what you're talking about. And if there's a way to address it.

Mr. Nichols: A little background on that. Someone put a pole across the street from my house, and for about 35 years, we had a big night light. And I asked the electrician when he came over if there was some way to fix that. He said, yeah, they would put a flat lens in. Which means that's the light, and that doesn't shine in all directions, it shines straight down. With this project, yeah, it would be nice for the parking lots, but what I was referring to particularly are lights on the buildings themselves, shining into the parking lot. Which means shining over on 61st Street. Now, we've been there 47 years, and I know most everyone else has been there a long time. It's

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just a matter of, we don't think this is the right thing for us. But, it's up to you guys. And the Council.

Mr. Brown: Thank you, because I had it completely wrong in my head. I assumed you meant the windows were creating some sort of lens.

Mr. Arnold: So, to answer the question, yes, all of the lighting would be directed down to provide the necessary egress lighting. We're not going to have lights shining onto 61st Street. You will see, just like if you go up and down 61st, the windows of the residents. You'll see [inaudible].

Mr. Brown: So, like balcony lights on that side of the building shining down, and you're not going to have a spotlight.

[Overlapping comments.]

Mr. Arnold: There's none facing the balconies. The balconies are on the north side.

Mr. Davidson: I have a question for you, Danielle. I'm sure it's on that plat, but what is the, the type of foundation/elevation on the first two homes, let's just say, to the south of the tennis courts? And the elevation of the actual, to the southwest corner of the parking lot?

Ms. Sitzman: I'm sorry, Mr. Davidson, I don't know if I have those exact measurements. I had looked at some other ones that had to do with the bathhouse and the street elevations adjacent to the bathhouse. There definitely is an elevation shift. Let me see if I can pull up --.

Mr. Davidson: On that first plat that you had, is there not -?

Ms. Sitzman: I don't think --.

Unidentified: Are you talking across the street -

Mr. Davidson: No, I'm talking to the two homes directly to the south of the tennis court, behind the creek.

Ms. Sitzman: You mean west of the tennis court?

Mr. Davidson: I'm sorry, west of the tennis court.

Ms. Sitzman: I'm not sure I have that at my fingertips.

Mr. Davidson: My concern, I just wanted to know what the difference in elevations, on the first floor elevations of those homes versus the parking lot that Jim and Scott were talking about, as far as lighting is concerned. And, their relationship to those homes versus a 56-foot, 3-inch structure. If it's 20 feet, you know, a lower elevation, then it's actually, you know, a 36 foot tall building. So, I just wanted to know that, just to get a better feel for the height.

Ms. Sitzman: This is going to show you lines at 10-foot elevations. So, the tennis court and these first lots, there's not a lot of elevation change in this area. But, how that compares to this side, I don't know that I can tell you that off the top of my head.

Mr. Davidson: Okay. I just thought it was there --.

Ms. Sitzman: Probably the best exhibits are the views they provided. Those were taken from street view across the area and kind of give you --

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Mr. Davidson: Can we go to the slide that you have? Which were very helpful.

Ms. Sitzman: So, some of these views.

Mr. Davidson: The one where you – That one right there.

Ms. Sitzman: This is the tennis court, these are those lots you're asking about. So, those are the views from the sidewalk or the street level.

Mr. Davidson: That's what I'm talking about, if you go back – I can't tell from the photo.

Unidentified: While we're on the 56 foot dimension, if we were to go back, and let's say there was no deviation being asked, the maximum height total would still be -?

Ms. Sitzman: Forty-five feet.

Mr. Davidson: I have one last comment I want to add as far as no lighting, parking lot lighting. I'm so proud of our Johnson Drive project and the beautiful street lights that we have, and the LEDs. You look down Johnson Drive and it almost looks like it's black. Meaning, you don't have any light reflection up above. Everything is straight down, and you don't know that you have the lights until you're underneath, you know? The concern as far as lights shining spots here and there, the LED lighting is set and it's engineered to focus exactly where it's supposed to go. So, that light pollution, in my opinion, is not a big concern. That's all I have.

Mr. Braden: First of all, I was reading in the storm drainage report, it appears that we were adding less than 5,000 square foot of impermeable surface, and it didn't trigger any kind of remediation. What is that? I guess 5,000 is when you start doing --?

Ms. Sitzman: There is an exemption. The baseline is if you have more impermeable surface, you have to do something about slowing down that water. If you have some but not a lot, which is what that exemption says, if you have a small amount, you still don't have to do the remediation. That's probably a better question for Olsson to answer in more detail later, but this property being right on the floodway as it is, that infiltration, or holding it –

Mr. Braden: That's what I thought. Even though it met the 5,000 square foot --.

Ms. Sitzman: Right. Typically, you're able to look at the whole system and how it functions. So, there are some sites where it doesn't do your system any good to withhold water and put it in later. It's actually better to get the water in the main system before the big crest comes through. So, if you can get water in and flowing at the lower elevation, lower rate, that's obviously better. That would be a consideration in an overall flood study.

Mr. Braden: And then, two other questions. I think this came up in the first meeting. If we're in the floodplain, there can't be anything really developed on grade as far as occupied spaces?

Ms. Sitzman: Right, there are limitations because it's a flood plain.

Mr. Braden: So, anything that's going to be built there, the first floor can't be occupied space.

Ms. Sitzman: Right. It has to be flood-proof, so even if it does have water, it's not flooding items out into the creek, so cars can't move off-site. Things like that. So, yes, there are limitations because of the floodplain on habitable spaces.

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Mr. Braden: The last question might be of the applicant. I keep hearing that we're losing all this green space. I'm trying to figure out where all this green space is that we're losing.

Mr. Arnold: I'm as confused as you are. We're not losing green space. We're putting in more green space than there is now. The flood zone presents challenges. I mean, this is a tricky site, and we're trying to go through all the things that we can. It's far less expensive to sit this building on the ground and build a three-story building that looks like all the other three-story apartment buildings in the area. If there wasn't a flood zone or a deviation from that requirement that we could build in a flood zone, it would be a very different conversation. But, we basically have a three-story apartment building in the flood zone so that fire trucks can get underneath it.

Bruce: You're building that tall enough to drive a fire truck under?

Mr. Arnold: Yes. We had it lower, but we met with the fire department, and they requested that we raise it up. So, that pushes the building up to the height that it's at.

Mr. Brown: That was what? Three feet taller?

Mr. Arnold: Than it was before? Yeah.

Mr. Brown: So we would be talking about an eight-foot deviation if it hadn't been for the request to be able to drive a fire truck underneath there?

Mr. Arnold: Yeah. Or, we wouldn't have the first floor as high as it is and just be on pylons. Parking requires less, but we also have a mixed-use requirement that says the first floor needs to have some [inaudible]. So, for instance, we're about the same size as the parking structure for Mission Trails because the floor are taller height and the parking structures are so much less. So, in concept, if we weren't in a flood zone, you'd probably bring the whole thing down, excavate into the flood zone for parking, and without that requirement and the fire department requirement, we wouldn't have the need for the height deviation. Which is how we got a three-story building taller than you normally would.

Mr. Brown: If you know, was the property zoned and three-story put on it before or after FEMA put it in the floodplain?

Ms. Sitzman: That's a good question. I don't know the history of the floodplain, but it being right on the channel, I would imagine it's been in the floodplain for a long time. The rezoning happened in 2006 or 2007, so I would imagine the flood plain has been there longer than the current zoning. Seeing how the flooding events in Mission were in the early 90s, and a lot of the follow-up flood studies came from that.

Mr. Brown: The reason I ask the question is because we're constantly changing the flood maps, and we built in an impervious upstream place. So, that changes the maps. That's why I asked.

Mr. Bruce: Did the fire department explain why they needed access to the rear of the building?

Mr. Arnold: They requested it.

Mr. Bruce: The building is fully sprinkled, correct?

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Mr. Arnold: It is. I think it's because of the park and the floodway on the back. They wanted to make sure they could access all sides of the building. That was their request. Because originally, we didn't have that.

Mr. Bruce: It just seems to be a little strange. On East Gateway, there is a parking garage at the rear of a very similar apartment building. I asked the specific question: Will that support fire equipment? The answer was no, that the fire department did not seem to have a problem not having access to the rear of those apartment buildings. So, I think it might be a little bit overkill.

Mr. Arnold: I share that sentiment, and the Mission Trails project has some limited access, as well. We pointed out those things to them. We had three or four conversations and a meeting with the fire marshal. There were three or four people, and the codes administrator from the City of Mission. They all required it. So, we made that request.

Mr. Bruce: But they did not say an NFPA rule required it.

Mr. Arnold: I don't recall if it was NFPA, but they stated that – Do you remember?

Unidentified: Just clearance for a fire truck.

Mr. Arnold: Yeah. They referenced a requirement, but I don't recall if it was NFPA. It likely was. They brought their documentation, they presented that, and said it was the dimension required for clearance. So, we adhered to it.

Ms. Dukelow: I have a question. I'm noticing that first floor to the second floor is 20 feet and 9 inches.

Mr. Arnold: Yes, that sounds right.

Ms. Dukelow: The subsequent floors are 11 feet?

Mr. Arnold: Yes. That's correct.

Ms. Dukelow: You also mention the mixed use requirement on the first floor. Is that also triggering the additional floor height?

Mr. Arnold: The 20 feet is the fire truck. The fire department requirement is, I think it's 18 feet, and we added about two feet of infrastructure and building structure.

Ms. Dukelow: So, this is above the podium. This is the first floor to the second floor –.

Mr. Arnold: Those are all 11 feet. The floor-to-floor height is 10 feet, 9 inches and feet.

Ms. Dukelow: Oh, I'm sorry. That's my error. Thank you.

Unidentified: Mr. Chairman, am I allowed to ask a question?

Chairman Lee: The meeting is closed, ma'am. We will entertain a motion at this point.

Mr. Babcock: I'll take a shot at a motion, with amendments. I move to recommend to the City Council **approval** of Case #17-08 the Preliminary Site Development Plan for Martway Mixed Use development with the staff recommended conditions # 1 – 10, and with added conditions 11 and 12, as follows:

1. Approval of the requested deviation to rear yard setbacks to waive the requirement

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- for a 25' setback along adjacent "R-1" zoned city property.
2. Approval of the requested deviation to height to allow a maximum building height of four (4) stories and or 56' 3" feet.
 3. Approval of the requested deviation to waive the minimum lot area per dwelling unit to allow for the proposed design of 117 units or 116,931 square feet of residential development in a mixed-use building.
 4. Approval of the requested deviation to waive the 6' parking lot setbacks along the west property line. Alternative screening of the area should be provided for consideration with the final site plan.
 5. Approval of the requested deviation to waive the parking lot buffers for the entire site.
 6. Approval of the requested deviation to waive the site tree requirement based on parking spaces.
 7. Approval of the requested deviation to waive the parking lot open space standard.
 8. A revised final traffic study and final stormwater drainage designs must be submitted for review with the final site plan application. The appropriate data, text, maps, drawings and tables must be included per the Olsson Associates review comments dated September 20, 2017 and attached to this report.
 9. Staff reserves the right to provide additional comments or stipulations on development plans until all traffic, circulation, ADA, storm drainage, and floodplain related concerns have been addressed.
 10. Provide adequate right-of-way for the required streetscape elements. A minimum of 10' wide paved clear path is required for the Rock Creek Trail separated from the back of curb by a minimum 5' way planting zone.
 11. Trash receptacle needs to be moved or screened from residents to the southwest.
 12. Light pollution remediation needs to be maximized to the satisfaction of staff before construction begins through screening, landscape, and appropriate fixtures.

Ms. Dukelow: Second.

Chairman Lee: Call the roll, please.

Ms. Dukelow: Mr. Chairman, I would request a clarification to the motion. Were islands installed in the parking lot?

Mr. Arnold: On the west boundary.

Ms. Dukelow: West boundary. Thank you.

The vote on the motion was taken (7-1), with Mr. Bruce voting in opposition to the motion to approve. **The motion to approve this application carried.**

Planning Commission Comments/CIP Updates

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Ms. Sitzman: This is a chance for you to provide any comments. Several of you are on the CIP committee. This would be a great opportunity to update you on the CIP committee and what's happening, and what should be happening next. So, if you'd like to take that opportunity -?

Mr. Babcock: I'm the chair of the CIP committee. I think at this point, we are a committee that is learning and in transition because we've got several folks that are having to bail on us. So, we'll look for appointments from the new mayor, I guess, to fill those vacancies.

Ms. Sitzman: The CIP committee has representatives from various boards and commissions, and there is some turnover happening on the Parks and Recreation and Tree Board. One of the next items they're going to be working on is to hear updates about, I believe stormwater, first off. There is a meeting coming up to discuss what stormwater needs, and the programming in the next five years, would be for the city to meet its stormwater needs. They will have two meetings following that each month on streets and how you maintain and prepare street planning, etc. And then, two meetings on parks. At the last meeting, we did a short presentation on what a Comprehensive Plan is and what your role on the Planning Commission is. We also heard a little bit about the Parks master plan. So, we started with that, went through all the areas the CIP covers, and wrapped back up with those elements. So, this is the first year that there has been a CIP committee, so they're all getting up to speed on what it is and how it works. Basically hammering out the next five-year plan. The Planning Commission does have to make a recommendation on whether it meets the Comprehensive Plan and if it's in conformance with the Comprehensive Plan. Eventually, it will be back before you, as well. Any other Planning Commission comments to share?

Mr. Babcock: I did make a comment at the last meeting that I think the Comprehensive Plan needs to be updated. Any thoughts on when that would start?

Ms. Sitzman: No. We know that that needs to be done. We started a Comprehensive Plan updating process, so we intend to get back to that. I don't have a calendar to tell you when and how we would exactly wrap that up. We're pretty close to the finish line on that, but there's probably still some public engagement that needs to happen over the goals and objectives section. So, it's likely that we'll have to ask for some additional funding or some outside resources to do that, as well. We're trying to do a lot of that process in house, but there's probably a need to get some outside expertise to engage the wider community. Anybody else?

Mr. Brown: Mr. Chairman, I think we still need to encourage a new study on zoning and density, and in particular, the type issue. Personally, I'm not interested in addressing any more deviations regarding height. I think we need to readdress that with the public in general and get a new direction and approach on that, because we've had two in a row now. They're not easy discussions. The public doesn't like them. We need to put that back in their purvey and readdress it.

Mr. Bruce: Mr. Chairman, I agree with Jim. The reason I voted "no" is that specific thing. If you go to our neighboring cities, you'll find buildings that exceed three floors in height. So, I think we either need to revise our codes, or comply with them. Whatever the city building codes are, we should be meeting them on every single project that comes along, or revise to be more realistic.

Mr. Babcock: And to expand on that, like I was saying to Pete, I tend to be a black-and-white guy. However, what we've got currently is a building of three stories. However, if you were to

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look at where that building is going to go, it's going to be four stories. It's four stories, but it's still shorter than Mission Square. So, relative height, it's shorter. Not only that, if you look at the house on the southwest corner, it's pretty much four stories to that residence. However, the majority of these residents that were making comments, most of those residents actually are 20 feet higher than the base of the parking lot. So, relatively speaking, it's more like a two-story building to them, which is one of the reasons why I give you the benefit of the doubt, because as Pete was saying, it's an infill project, which makes it a little bit harder to put that property to use. The thing is, I think it would benefit the decision-making process if we talked, rather than just a standard story, we talked heights, relative heights. And it's a graduated rather than a strict three-story building height throughout the corridor. In my mind, I can see 5, 4, 3, 2, or something to that effect, as we go away from the corridor. That's kind of what we're doing, but that's not the way it's written in the Comprehensive Plan. Danielle, do you know if any other, any more projects on the board that might be coming up like this?

Ms. Sitzman: No, there's nothing in the pipeline. The last larger parcel that we know is being marketed for some sort of housing use would be on the northwest side of town, at 56th and Foxridge. It's the former JC Penney call center site. That might be the, kind of the last easily-accessible, without subletting a lot of other parcels. That's where the Dial Senior Living property proposal started to look at. But I haven't had any serious interest. And that would be a Form Based Code district, too. So, a little bit different ballgame.

Mr. Davidson: The other thing, I guess, is, you know, traffic study. The traffic study is a lot of concern to a lot of people. Here's these traffic engineers, doing these traffic studies. Well, wait a minute. Now we've got another project that's coming up, and it's like, you really can't do, you know, a proper study when, Oh, wait a minute, there's another project here that's going to bring 200 more cars in, you know, into the area. So, that is a bit concerning.

Ms. Sitzman: Staff obviously knows the projects that have started down that path, and we try to make our engineers aware of it. There's kind of a standard traffic analysis that's typically asked of an applicant. If we know of extenuating circumstances, we'll typically tell them to try and take that into account as much as possible.

Mr. Davidson: This project right there, you know, they've been, you know, that traffic engineer has been, obviously knows about what's been approved.

Ms. Sitzman: Yeah. And when we send it out, it's, we need you to look a little farther afield than what they'd normally look at, too. Consider this intersection at Martway, or this next one closer, because we think it would have more of an impact. So, yeah, we're trying to be sensitive about that.

Mr. Babcock: Do we still own the printing company property?

Ms. Sitzman: Yes, we do still own 7080 Martway.

Mr. Babcock: It completely muddies the water. Just thinking out of the box. The bundle of the buildings you have, and you have a [inaudible] exchange with the city to do that – [Laughter.]

[Overlapping comments and conversations.]

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Mr. Babcock: You need to look at it because you could go more than three stories there. You wouldn't have floodplain issues. And we get a park adjacent to a current park.

Mr. Arnold: You could have mentioned this, like months ago.

Mr. Babcock: I didn't know about it.

Ms. Sitzman: So, let me tell you what I do know that is coming your way. In January, there is a special use permit that will be before you for off-site parking for 5700 Broadmoor. That's going to be high-rise office buildings. A category for Broadmoor Park. That ownership recently purchased the parking lot directly north of Broadmoor Park and would like to continue to park cars on there. I became aware that they were doing that, and they really do need to have a special use permit first to make it legal. Because it's off-site, basically. So, they've got a use over here; they want to do parking over here. So, that will be before you. Kind of talked about lighting concerns. They'd like to improve the lighting in that parking lot for the security and safety for the folks that park there, but it's immediately adjacent to some single-family family homes. So, they're working through the design for how to avoid trespassing and light pollution there. There are also concerns that we had, that if you have people parking across the street, how are they going to cross the street in that area? So, we are starting to talk to them about off-site improvements to extend the sidewalk, put in a crosswalk, so that folks can get across the street safely, or at least not be darting across in various other locations. That will be before you in January.

As you may notice, there is an empty chair. Dana Buford has decided to not continue on the Planning Commission. She has resigned her spot. There will be a new appointment happening this week. Burton Taylor has applied for the position and City Council will be considering approving that. So, in January, we will probably have a new planning commissioner. Everyone knows Scott is eventually going to be moving out of Mission, so Scott will kind of take over as our non-resident on the board, which Dana had been filling. So, we will have equal representation from the wards again, and one non-resident. That concludes everything that I have to share.

Staff Update

Staff provided an update on current and upcoming projects and events.

ADJOURNMENT

With no other agenda items, **Mr. Lee moved and Mr. Davidson seconded a motion to adjourn.** (Vote was unanimous). The **motion carried.** The meeting adjourned at 8:39 P.M.

Mike Lee, Chair

ATTEST:

Ashley Elmore, Secretary

Multifamily Density by City District-Updated 12.18.17									
Map Key	Property Name	Site Address	Number of Units	Lot Area (SqFT)	Min Lot Area (Lot Area/Unit)	Year Built	Current Code Requirement (Lot Area/Unit)	Acres	Units/Acre
Downtown District									
Zone									
DND	Maple Hill	5946 Maple St	12	19,103	1,592	1984	SF-4,500sqft 9.68 du/ac, TH-1.742sqft 25 du/ac, MF-872sqft 50 du/ac	0.44	27
DND	Mission Woods- At Home	5920 Reeds Rd (4 buildings on 4 parcels)	48	67,199	1,400	1972	SF-4,500sqft 9.68 du/ac, TH-1.742sqft 25 du/ac, MF-872sqft 50 du/ac	1.54	31
DND	Mission Gardens	5905 W. 58th St	25	33,602	1,344	1960	SF-4,500sqft 9.68 du/ac, TH-1.742sqft 25 du/ac, MF-872sqft 50 du/ac	0.77	32
DND	Mission Terrace - At Home	5720 Martway St	11	14,712	1,337	1964	SF-4,500sqft 9.68 du/ac, TH-1.742sqft 25 du/ac, MF-872sqft 50 du/ac	0.34	33
DND	The Gables-At Home	5934 Outlook St (2 buildings on 2 parcels unevenly distributed)	43	56,050	1,303	1966	SF-4,500sqft 9.68 du/ac, TH-1.742sqft 25 du/ac, MF-872sqft 50 du/ac	1.29	33
R-4	Mission Point - At Home	5708 Outlook St (2 buildings on 3 parcels)	34	44,101	1,297	1973	3,500 sqft	1.01	34
MS2	Mission Hills - At Home	5954 Woodson St (4 buildings on 4 parcels)	120	137,427	1,145	1976	1,245 sqft, 35 du/ac	3.15	38
MS2	The Maples	5811 Maple St	16	16,800	1,050	1964	1,245 sqft, 35 du/ac	0.39	41
MS2	Mission 58	5601 W 58th St	16	16,800	1,050	1968	1,245 sqft, 35 du/ac	0.39	41
DND	Outlook Apts	5933 Outlook St #2	24	25,198	1,050	1985	SF-4,500sqft 9.68 du/ac, TH-1.742sqft 25 du/ac, MF-872sqft 50 du/ac	0.58	41
DND	Mission Ridge - At Home	5911 Reeds Rd	30	30,760	1,025	1973	SF-4,500sqft 9.68 du/ac, TH-1.742sqft 25 du/ac, MF-872sqft 50 du/ac	0.71	42
MS2	Proposed 12.18.17 Martway Mixed Use	6005-6045 Martway St (1 building spanning 3 parcels)	117	76,971	658	2018	1,245 sqft, 35 du/ac	1.77	66
MS1	Mission Trails	6201 Johnson Dr	200	122,669	613	2018	None	2.82	71
MS2	Proposed 9.25.17 Martway Mixed Use	6005-6045 Martway St (1 building spanning 3 parcels)	156	76,971	493	2018	1,245 sqft, 35 du/ac	1.77	88
R-4	Mission Heights	5717 Outlook St	40	17,501	438	1974	3,500 sqft	0.40	100
West Gateway District									
FBC	The Welstone at Mission Crossing*	6050 Broadmoor St	101	98,868	979	2014	NA	2.27	44
East Gateway District									
Other Areas-Mission									
RP-4	Hillsborough	5401 Foxridge Dr (Many buildings on 2 parcels unevenly distributed)	329	1,279,324	3,889	1984	NA	29.37	11
R-6	Wellington Club	6900 W 50th Ter	224	759,024	3,389	1972	1,200 sqft	17.42	13
R-4	Bridges At Foxridge	5250 Foxridge Dr (Many buildings on 4 parcels)	317	1,044,140	3,294	1966	3,500 sqft	23.97	13
R-4	The Retreat at Mission	6230 W 51st St	108	302,618	2,802	1971	3,500 sqft	6.95	16
R-6	Silverwood	5100 Foxridge Dr	280	648,063	2,315	1986	1,200 sqft	14.88	19
R-6	Foxfire Apartments	5020 Glenwood St	280	548,172	1,958	1984	1,200 sqft	12.58	22
R-6	The Falls	6565 Foxridge Dr	435	675,134	1,552	1972	1,200 sqft	15.50	28
Other Areas-Outside Mission									
	Brookridge	Antioch Rd & I-435, Overland Park (131 acre site with many features)	2,076	5,706,360	2,749	2020		131.00	16
	The Heights-Linden Square	N. Oak Trafficway & 69th St-Downtown Gladstone	222	240,000	1,081	2015		5.51	40
	Meadow Brook-The Kessler Apartments	95th Street & Nall Ave, Prairie Village (6.8 acres of mixed use and parkland 42 ac site)	282	296,208	1,050	2017		6.80	41
	District at City Center-EPC	Not yet built 87th St & Rnner Blvd, Lenexa (2 buildings on 2 parcels)	175	156,030	892	2019		3.58	49
	Woodside village	Rainbow Blvd & 47th Pl-Westwood (Apts and live work units on Lot 5 & 2 other grdn fl uses)	330	240,000	727	2016		5.51	60
	Domain at City Center-EPC	87th St & Renner Blvd, Lenexa	203	140,133	690	2016		3.22	63
	Avenue 80-EPC	Metcaif Ave & 80th Street, Overland Park	218	148,674	682	2017		3.41	64
	Interurban Lofts	79th St & Conser St-Downtown OP (bldg also has ground floor office)	41	24,352	594	2017		0.56	73
	51 Main-EPC	Plaza south area-KCMO	176	94,500	537	20??		2.17	81
	The Vue	Under construction 80th St and Santa Fe Dr/southside-Downtown OP	219	100,924	461	2017		2.32	95
	Market Lofts	Under construction 80th St and Santa Fe Dr/by Rio-Downtown OP (bldg also has grnd fl retail)	36	15,342	426	2017		0.35	102

Property Name	Site Address	Number of Units	Rent Range and Unit Types	Amenities (pool/clubhouse/covered parking)	Year Original Construction (AIMS)	Major Renovations (Year/description/value-BIM)	2017 Appraised Value (AIMS)	2016 Appraised Value (AIMS)	% Change Value 16-17
Mission Gardens	5905 W. 58th St	25			1960	Oct 2016/ reroof/\$18,000 June 2016/ reroof/\$5,600	\$1,012,000.00	\$945,000.00	7.09%
Mission Terrace - At Home	5720 Martway St	11	\$810 - \$850 1 Bedroom	Google Fiber	1964	2013/multi-family reroof/\$13,895	\$493,000.00	\$472,000.00	4.45%
The Maples	5811 Maple St	16	1 ,2 Bedrooms		1964	No permit information found	\$781,000.00	\$751,000.00	3.99%
Bridges At Foxridge	5250 Foxridge Dr	317	\$840 - \$1150 1, 2, 3 Bedrooms	Pool, Clubhouse, Covered Parking, Garages, Dog Park, Tennis Court	1966	2016/emerg damage repair to kitchen/\$16,542 2015/reroof 2 apts bldgs/1 carport/\$43,780 2012/HVAC replacement - eight permits/\$525 ea Oct 2012/ HVAC replacement - twelve permits/\$525 ea 2011/replace meter can /\$2,200 2007/no description/\$150,000	\$5,552,000.00	\$5,321,000.00	4.34%
The Gables-At Home	5934 Outlook St	43	\$800 - \$1050 1,2 Bedrooms	Google Fiber	1966	2014/ reroof/\$19,500 2013/ deck replacement/\$40,000	\$1,477,000.00	\$1,417,000.00	4.23%
Mission 58	5601 W 58th St	16	\$625 - \$725 1, 2 Bedrooms	On site laundry, downtown proximity	1968	Nov 2014/ replace water heater/\$3,900 Oct 2014/gas leak repairs/\$5,000 2012/reroof/\$35,000	\$727,000.00	\$699,000.00	4.01%
The Retreat at Mission	6230 W 51st St	108	\$650 - \$975 1, 2, 3 Bedrooms	Pool, Garages, Basketball Court	1971	2016/HVAC/\$3150 2015/water heater - four permits/\$3100 ea Dec 2015/furnace replacement - four permits/0 value (together with water heater?) June 2015/ HVAC/\$2600 emer repair demo of apts due to fire/ \$1200 2001/ no description/\$10,998	\$5,169,000.00	\$4,630,000.00	11.64%
Mission Woods- At Home	5920 Reeds Rd	48	\$725 - \$880 1, 2 Bedrooms	Google Fiber	1972	no permit information found	\$635,000.00	\$609,000.00	4.27%
The Falls	6565 Foxridge Dr	435	\$659 - \$900 Studio, 1, 2 Bedrooms	Cover Parking, Pool, Clubhouse, Garages	1972	see attached page	\$18,229,000.00	\$17,507,000.00	4.12%
Wellington Club	6900 W 50th Ter	224	\$625 - \$975 1, 2, 3 Bedrooms	Clubhouse, Pool, Basketball Court, Sand Volleyball Court	1972	2014/water heater/\$1,000 2013/ Remodel of fire damaged apts/\$250,000 Feb 2013/Temp elect for apts/\$2500 Feb 2013/demo of apart bldg/\$15,000 2012/water heater/\$500 Dec 2009/reroof/\$102,500 2009/ Remodel from fire damage/\$47,444	\$11,208,000.00	\$10,471,000.00	7.04%
Mission Point - At Home	5708 Outlook St	34	\$800 - \$900 1,2 Bedrooms	Google Fiber	1973	2015/replace deck/\$14,288 Apr 2013/HVAC/\$10,200 Mar 2013/reroof/\$14,500	\$901,000.00	\$866,000.00	4.04%
Mission Ridge - At Home	5911 Reeds Rd	30	\$695 - \$825 Studio, 1 Bedroom	Google Fiber	1973	2012/AC/\$7,000 2011/Exter Alteration/\$108,084	\$1,406,000.00	\$1,352,000.00	3.99%
Mission Heights	5717 Outlook St	40	\$719 - \$910 1,2 Bedrooms		1974	Mar 2016/ HVAC replacement /\$3,100 ea - three permits Dec 2015/HVAC replacement/\$3,100 ea - five permits Oct 2015/HVAC replacement/\$3,100 - one permit July 2015/HVAC replacement /\$3,100 ea-two permits June 2015/HVAC/\$3,100-one permit March 2004/new patio/deck/\$8,000	\$587,000.00	\$563,000.00	4.26%

Property Name	Site Address	Number of Units	Rent Range and Unit Types	Amenities (pool/clubhouse/covered parking)	Year Original Construction (AIMS)	Major Renovations (Year/description/value-BIM)	2017 Appraised Value (AIMS)	2016 Appraised Value (AIMS)	% Change Value 16-17
Mission Hills - At Home	5954 Woodson St	120	\$800 - \$880 1,2 Bedrooms	Covered Parking, Google Fiber	1976	2014/reroof/\$28,500	\$1,562,000.00	\$1,501,000.00	4.06%
Foxfire Apartments	5020 Glenwood St	280	\$585 - \$740 1, 2 Bedrooms	Pool, Clubhouse, Covered Parking, Tennis Court	1984	2012/reroof/\$553,927 2011/replace retaining wall/\$19,878 stall of iron fence/addition to existing / \$2,670 2003/HVAC replacement/\$400,000 2000/no description/\$30,000 1997/stairs/\$305,000	\$15,313,000.00	\$14,517,000.00	5.48%
Hillsborough	5401 Foxridge Dr	329	\$790 - \$1040 1, 2 Bedrooms	Pool, Clubhouse, Covered Parking, Garages, Tennis Court, Basketball Court	1984	2016/gas water heater- five permits/\$400 ea y 2014/garage carport replacement/\$30,000 April 2014 / Demo of fire damaged apt./\$20,000 Mar 2014/elect repair due to fire/\$1500 Oct 2013/Fire repair to 4 units/\$300,000 Mar 2013/ electrical demo and temp power/\$2,500 and mechanical reconnect gas/\$400 2000/no description/\$19,622 1995/no description/\$3,536,000	\$17,479,000.00	\$17,092,000.00	2.26%
Maple Hill	5946 Maple St	12			1984	2011/re-roof/\$12,000 2013/water heater replacement/\$500	\$427,000.00	\$409,000.00	4.40%
Outlook Apts	5933 Outlook St #2	24			1985	2014/ deck and stair replacement/\$30,000	\$989,000.00	\$951,000.00	4.00%
Silverwood	5100 Foxridge Dr	280	\$738 - \$1405 1, 2 Bedrooms	Covered Parking, Pool, Clubhouse,	1986	Oct 2015/Water heater/\$500 July 2015/ stair replacement/\$108,000 2012/Retaining wall/\$14,890 2007/install eng key stone wall system/\$30,000	\$19,391,000.00	\$18,898,000.00	2.61%
The Welstone at Mission Crossing	6050 Broadmoor St	101	1, 2 Bedrooms	Clubhouse, WiFi, Prepared Meals	2014	16/inter remodel/\$100,000 014/New construction/\$8,100,000 April 2014/temp elect serv/\$1,000	\$10,550,840.00	\$7,887,370.00	33.77%

September 20, 2017

City of Mission
Community Development
Attention: Danielle L. Sitzman, AICP
6090 Woodson St.
Mission, Kansas 66202

RE: Project Name: Martway Mixed Use – Preliminary Development Plan – Site Civil & Traffic Review

Dear Ms. Sitzman,

We have completed our review of the 2nd submittal for the above mentioned Preliminary Development Plan. If approved, we would recommend the following stipulations be applied:

Martway Multifamily

Olsson Review for Preliminary Plan 2nd Submittal – 9-20/17

Floodplain Stipulations:

1. All design and construction must meet the provisions Article IV, Chapter 460 of the City Code
2. Any enclosed building space including mechanical equipment areas (such as equipment in elevator sumps) must be 2' above FEMA floodplain or must be water proofed.
3. At time of Final Development Plan application, a variance from Article IV of city code must be obtained for any parking or building areas that encroach into the Floodway. This will require a flood study that shows that the project does not increase the 100-year water surface elevation.
4. Prior to building permit, a Floodplain Development Permit shall be obtained from the City, including a study or documentation showing the proposed project will not increase 100-yr water surface elevations.
5. Prior to issuance of a building permit, a Floodplain fills permit from the State of Kansas shall be obtained.
6. Prior to close out of the Floodplain Permit a LOMR-F and elevation certificate is required.

Drainage Memo Stipulations:

1. At time of Final Development Plan application, provide an exhibit or multiple exhibits that show the existing and proposed development, existing and proposed drainage boundaries and floodplain lines. Please provide drainage boundaries, CN values, and flow for each drainage area within the site and all off-site water entering the site for the existing and proposed condition.
2. At time of Final Development Plan application, show and explain how drainage from the site is being collected (within storm sewer or overland flow), routed and discharged at the stream to for adequate erosion control protection.

Preliminary Development Plan Stipulations

1. At time of Final Development Plan application please address the drive aisle width in the two areas near the center of the lot. The 25' dimension provided in two areas near the center of the lot is not adequate as the angle of turns within the lanes is severe and driving lanes are unclear. It appears the drive lane conflicts with pedestrian circulation areas near the elevators. Additional striping showing the lanes in these areas must be provided. A turning template showing cars within each lane must be provided. Stalls in these areas may need to be eliminated to resolve the problem.
2. At time of Final Development Plan application show revised ADA paths to not be within drive lanes parallel with traffic flow as shown in the west entrance. Where ADA paths cross drive lanes, pedestrian paths must be striped.

Traffic Study Stipulations

1. At time of Final Development Plan application, please submit a revised traffic study with corrected trip generation data. The retail land use has now changed to office therefore the am and pm peak trips will change. Provide a flash drive with all electronic files including Synchro. (See attached Martway Mixed-Use Development Traffic Impact Analysis Review Letter dated September 20, 2017 for additional comments)

If you have any questions or comments or need additional information, please do not hesitate to contact me at 913-381-1170 or bsonner@olssonassociates.com.

Sincerely,



Brad Sonner, PLA, LEED AP
Vice President

Martway Mixed-Use Development Traffic Impact Analysis Review

The following comments are in reference to the *Traffic Impact Analysis* (revised submittal) submitted by Cook, Flatt & Strobel Engineers, P.A., dated September 13, 2017, for the Martway Mixed Use Development Project.

A full review of the submitted traffic impact study cannot be completed due to inaccurate trip generation calculations which will impact trip distribution and capacity analysis for the site. Review will be conducted after submittal of a revised traffic impact study.

1. Trip Generation:
 - a. The traffic impact study has been revised for office space (previously retail). The site plan and parking demand analysis submitted to the City indicate retail land use. The traffic impact study should reflect the use proposed for the site and be consistent with the site plan.
 - b. Trip generation calculations are inaccurate. Specifically, the office space should be reviewed. The estimated number of trips are not correct. Additionally, office space does not have a 50% entering/exiting split for the AM and PM peak hour periods. Trip generation calculations should be updated and trip distribution and capacity analysis appropriately revised.
 - i. To ensure trip generation is accurate, updated calculations may be submitted to the City, prior to completion of the final traffic impact study, for review. This information must be submitted in a timely manner to allow for review and comments (if necessary) to be returned prior to the final submittal.
2. Provide a flash drive with all electronic files including Synchro. This allows for more efficient review.

It is recommended that the revised final traffic impact study be submitted a minimum two weeks prior to the City submittal deadline for the final development plan. Adequate time is necessary to conduct a thorough review of the study, allow for comments to be addressed by the submitter, and City staff to develop final comments.

November 20, 2017

christian@clockwork-ad.com

423 delaware \ suite 102 \ \ kansas city \ \ mo \ \ 64105

At the September 25th Planning Commission Meeting we listened to the residents along 61st street that voiced concerns regarding the development.

To address the concerns, the revised submission has removed one entire floor of the building. This reduction in height also reduces the need for any off-site parking.

Over the last 2 months, we've received encouragement from residents and business owners who are excited to see the continued improvements in the City of Mission.

If any additional concerns or questions arise, please reach out so that they may be answered.

Regards,



Christian Arnold
Principal

Questions raised at the 9/25 Meeting, with comments added related to the proposed revision.

1) Does the developer own the property?

Yes.

2) What are the size of the apartments? Are washers and dryers included? Does each unit have it's own AC/Furnace? Does each unit have its own balcony?

Studio units are 504sf, one bedrooms range from 644sf to 720sf and 2 bedrooms range from 1,104sf to 1,144sf. Yes, each unit has its own AC and furnace unit. Yes, each unit has its own balcony except for the studio units.

3) Will there be a maintenance man/property manager on site at all times?

The original development had 156 units and would have dedicated staff. The current proposal has 117 units so it would not financially support dedicated staff.

4) Is Rock Creek being altered? Will there be any additional flood impacts/concerns?

No, Rock Creek is not being altered. No, there will not be any additional flood impacts.

5) How much larger will power poles and utilities need to be to supply the building? Can the utilities be buried?

There are currently (3) incoming power locations to service each of the existing buildings. This will be reduced to (1) to provide power to the new building. The power poles will not be any larger than existing. Power service from the transformer to the building will be buried and concealed.

6) Where is the trash located at?

The trash dumpster enclosure is currently shown on the site plan in the southwest corner of the site. The trash dumpsters will be screened with a privacy walls.

7) Did the design team look at the feasibility of a shorter building? Is there compromise for the building height? Can the footprint be widened to reduce a story?

Due to the existing floodway limits and the requirements of the City of Mission, the buildings first floor must be lifted above the floodway. The fire department clearance requirements establish the first floor height. The current proposal has removed a floor from the proposed building design. The building is now 3 stories of residential construction on top of parking and commercial space.

8) Are there any amenities for the development? We are concerned that other people will use the tennis courts and park and it could get too busy.

Since it is a public park, residents will be able to enjoy it. Internal amenities are still being considered and developed. The building is programmed with multiple flex spaces that could be utilized with a fitness facility and multipurpose rooms for community gatherings.

9) Has the design team considered the building's aesthetics? Concerns that the building does not reflect mission style architecture.

Yes, the design team has considered the building's aesthetics. The City's guidelines have been adhered to and there is no requirement for mission style architecture. The building aesthetics appeal to the targeted demographic and is designed to relate to the adjacent vernacular established by the existing neighboring buildings along Martway that have a mid-century modern aesthetic that Mission is known for.

10) Where are the local jobs to provide for the new housing?

The demand for housing is gauged by Occupancy rates and Mission is a desirable place to live.

11) If additional parking is needed off site, why wouldn't the development team scale the project back?

The original proposal utilized the adjacent empty surface lots, the current proposal does not require off-site parking.

12) Will there be any public parking on site?

On site parking is for residence only. 14 parking spaces are provided to serve the grade level business.

13) How do the traffic engineers not see an increase in traffic?

Martway is engineered to handle more traffic than currently exists. The traffic study took traffic counts at the intersections of Beverly & Martway and at Dearborn & Martway on typical weekdays during June of this year, and then the anticipated traffic which would be generated by the proposed apartments and the small amount of general office space. Traffic modeling software was used to simulate the existing traffic conditions and the proposed conditions with the additional site-generated traffic superimposed onto the existing volumes. The current proposal has even less than traffic previously approved.

14) What is the construction time frame?

The building will take approximately a year to 15 months to construct after breaking ground.

15) There are no basements. Where do people go to seek shelter from a severe storm?

The building will be designed to meet all applicable building codes. Stairwell shafts will be constructed out of 8" thick concrete and will serve as an area of refuge for storms.

16) Do you envision any children living in this building?

Yes, families with children are welcome to live in this building.

17) Just to confirm, these are market rate apartments? There won't be any subsidized housing?

These are market rate apartments.



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cfse.com

Other Offices:
Kansas City, Kansas
Lawrence, Kansas
Holton, Kansas
Topeka, Kansas
Wichita, Kansas
Branson, Missouri
Springfield, Missouri
Jefferson City, Missouri

September 15th, 2017

Danielle L. Sitzman, AICP
City Planner
City of Mission
6090 Woodson St.
Mission, KS 66202
Ph. 913.676.8363
email:dsitzman@missionks.org

RE: Maryway Mixed Use/Mission Trails Final Preliminary Plan Case #17-08-Staff
Review Comments

Dear Danielle;

In regards to the cities August 8th and August 9th, 2017 review comments we are resubmitting revised plans, and we offer the following responses:

Engineering Review Comments

Floodplain Comments

- 1. Compensatory volume for any fill within the 100 year floodplain must be provided. Please show fill areas and location for compensatory volume.

An exhibit has been added to the Drainage Memo showing the fill areas and the locations of compensatory volume.

- 2. Provide the maximum 100 year floodplain depth in the parking lot. 7” depth is the maximum allowed.

The 100 year floodplain limits over parking lot have been shown on Sheet C-203, Proposed Floodplain Plan. The parking lot has been graded such that no parking stall would pond over 7” based on the FEMA floodplain depths.

- 3. Show that the 1st floor retail is 2’ above the 100 year floodplain.

The finished floor elevations have been shown, and are 2’ or more above the FEMA 100yr floodplain elevations.

Board of Directors:
Kenneth M. Blair, P.E.
Robert S. Chambers, P.E.
Kevin K. Holland, P.E.
Daniel W. Holloway, P.E.
Charles C. LePage, P.E.
Lance W. Scott, P.E.
Sabin A. Yañez, P.E.

Associates:
Aaron J. Gaspers, P.E.
Michael J. Morrissey, P.E.
Gene E. Petersen, P.E.
Todd R. Polk, P.E.
William J. Stafford, P.E.
Richard A. Walker, P.E.
Lucas W. Williams, P.E.

Drainage Memo Comments

- 1. State that the additional impervious area is under 5,000 square feet as required by APWA 5600.

The Drainage Memo has been updated to address the 3418 sqft of additional impervious area from the proposed development.

2. Provide an exhibit or multiple exhibits that show the existing and proposed development, existing and proposed drainage boundaries and floodplain lines.

An exhibit has been added to the drainage memo.

Preliminary Development Plans

All Sheets/General Comments

1. Delineate building footprint with a darker line type as it's difficult to discern from parking lot.

The building footprint line type has been revised.

2. Show and label all columns, elevators and general site features.

All columns, elevators and general site features have been added to the site plans.

3. Provide a turning template for service and emergency vehicles as required within the site.

Fire truck access requirements were coordinated with the Fire Marshal of CFD#2 and that their largest truck is 44' from bumper to bumper and 48' from front bumper to back of basket, with a turning radius of 42' wall-to-wall, outside diameter, which is reflected on the plans.

C100/101

1. State ADA van and regular stalls provided in parking chart.

The number of regular and van ADA spaces has been added to the parking chart.

2. Confirm with city that variances requested are acceptable

Noted.

3. Show striping or linework to indicate vehicle circulation within parking area. There is a concern with vehicle flow within parking lot.

Circulation arrows have been added to the site plan.

4. Match legend to linework for floodplain limits hatch.

The legend for the floodplain limits has been updated.

5. Show internal pedestrian path for ADA route.

The internal pedestrian path for the ADA route has been added to the site plans.

6. The parking spaces along the east side of the property are directly adjacent to the driveway. Provide an adequate throat length (50' min.) to allow for vehicle queuing and reduce potential conflict when vehicles enter/exit the property via that drive and enter/exit parking spaces.

The parking layout has been revised to provide a 40' throat length to match the island adjacent to the easterly parking spaces.

7. In SW quadrant of parking lot, it appears that the two parking spaces (one is oriented N/S, the other E/W) would be in conflict with one another, specifically when the N/S vehicle tries to exit. Please resolve.

The parking layout has been revised to alleviate this conflict.

C200

1. Confirm retaining walls are not needed. If needed, show and state height of walls.

No retaining walls are required for this project.

Traffic Study Comments

The following comments are in reference to the Traffic Impact Analysis submitted by Cook, Flatt & Strobel Engineers, P.A., dated July 6, 2017, for the Martway Mixed Use Development Project.

1. Page 4 of report: Confirm posted speed limit along Johnson Drive (30 mph or 25 mph).

Eastbound Johnson Drive west of Beverly posted at 30 mph. Report revised.

2. Neither proposed drive provides alignment with the existing street network or existing access points. Recommend alignment of new drives at intersections (Beverly Avenue and Dearborn Street) to limit offset intersections and decrease the introduction of new conflict points along this segment of roadway.

- a. West Drive – Recommend alignment of the proposed west drive with Beverly Avenue. Intersection analysis sheets provided in the report indicate drive is aligned at the intersection, but the site plan illustrates an offset drive.

The west entrance driveway was shifted east to align with Beverly Avenue.

- b. East Drive – Based on the submitted site plan, it appears that the east drive cannot be aligned with Dearborn Street due to existing property lines. Recommend alignment of the drive with an existing access along the north side of Martway Street. Current drive alignment presents an offset intersection from Dearborn Street as well as existing access points along the north side of Martway Street. The provided intersection analysis sheets illustrate that analysis was conducted with the drive aligned at the intersection.

The east drive lane will be constructed close to its current location, offsetting Dearborn Street by approximately 35 ft, as it has been for the past forty or so years. If the eastern entrance is required to be shifted slightly to the west, we would anticipate minimal changes to the traffic or safety characteristics of the intersections. Also, turning radius for emergency vehicles would be impacted and could result in the loss of parking spaces due to inefficiencies in the layout. The Synchro models of the existing and proposed site conditions were revisited to include offset-links for both of the driveways.

3. Trip Generation:

- a. Daily trip generation is not provided.

The traffic study scope received on May 23, 2017, only called for AM and PM peak hour traffic counts, however, the daily trip generation has been included.

- b. Trip generation for the retail portion of the site (3,530 sf of retail space) was conducted using a shopping center land use. Based on the size of retail proposed with this development, the specialty retail land use may be more appropriate for this site. Recommend conducting analysis and revising report as necessary.
 - i. Page 8 of the report references a retail square footage of 3,254 sf in the trip generation paragraph and 3,530 sf in the trip generation table. Revise report as necessary for correct building square footage.

The trip generation calculations and report were revised to the updated 3,491 sqft building area, and changed from Specialty Retail (ITE Code 826) to General Office (ITE Code 710).

- c. Trip generation calculations were conducted using the average rate. For the majority of the land uses there is an adequate sample size and the R^2 value is greater than 0.75, thus use of the provided trip generation equation should be considered.

Both trip generation equations and the average rates were examined and higher values used in the traffic models.

4. Operational Analysis:

- a. Unsignalized intersection analysis was conducted with the proposed drives aligning with Dearborn/Beverly. The site plan indicates that these drives are offset. See comment 2 regarding access location recommendations. However, analysis should be conducted to be consistent with the proposed site plan. Analysis indicates a southbound right-turn movement at the intersection of Martway and Beverly associated with proposed traffic. For analysis considering alignment of the intersections, as illustrated on the provided files in the appendix, there should be no additional trips assigned to the southbound right-turn movement.

The southbound right-turn movements have been eliminated.

- b. It appears that analysis was conducted adding the proposed development trips to the existing volumes. The analysis should take into account the removal of trips associated with the existing development (proposed to be removed). I.e: trips entering and exiting the site should match the trip generation conducted (AM: 27 enter/62 exit, PM: 70 enter, 47 exit).

The small amount of traffic from the existing site has been removed from the traffic volumes.

5. Parking:

- a. Report states a portion of the provided parking spaces may have 6-8" of overbank water with the 100-year flood, but does not state how many spaces may be impacted. Please address also in Drainage Memo above.

Both the traffic study and the drainage memo has been updated to address parking lot ponding. The 100 year floodplain limits over parking lot have been shown on Sheet C-203, Proposed Floodplain Plan. The parking lot has been graded such that no parking stall would pond over 7" based on the FEMA floodplain depths.

- b. Report indicates 210 parking spaces are required but the development only provides 175 spaces. Report indicates an additional 35 spots will be leased off site.

Parking on the revised site has been reduced to 166 spaces and an additional 44 spaces will need to be leased off of the site. The traffic report has been updated to reflect this change.

6. Update report to include intersection figures for traffic volumes (existing, proposed trips, and existing plus proposed), trip distribution and level of service. This will allow for a more expedient review and is consistent with industry standard.

Added schematic traffic volume figures including: Existing Traffic with incoming & outgoing directional percentages, Site-Generated Traffic, and Total Combined Traffic.

7. Provide a flash drive with all electronic files including Synchro.

Planning Review Comments

Plat Comments

1. Re-platting of the property will be required prior to the issuance of building permits. Right-of-way must be dedicated to include all of the Rock Creek Trail, public sidewalks, and public infrastructure including stormwater facilities. A final plat may be submitted with the final site plan.

Noted.

Site Comments

2. Please explain the purpose for each of the deviations requested and how they meet the objectives and standards of the planned district regulations (Section 405.070)

Deviation 1- On site parking requirements reduction- Residential Use and Office Use are highly compatible uses due to the peak demand being offset between day and night use. Large empty parking lots is not the highest and best in a vibrant walkable neighborhood of Mission, so are intent is not to continue this trend. We anticipate that the 14 parking spaces for the business use will easily be handled on site due to this peak day/night offset. An expected operational vacancy for the residential use is 5% which reduces the actual parking demand from 196 to 186 required spaces. This results in a likely scenario of leasing approximately 10 parking spaces off-site.

The existing adjacent privately owned parking lots totaling over 200 parking spaces, are highly underutilized during day use and largely vacant for night use. We have reached out to several of the property owners and they are agreeable to leasing their surplus spaces for residential use, if needed.

Deviation 2- Rear yard setback reduction- The proposed building and parking footprint have been designed to maximize the potential of the site. The Rock Creek channel that runs along the rear of the entire property provides a natural landscape buffer of over 30' that exceeds the setback requirement. Additionally the City Park provides an additional buffer of approximately 300'.

Deviation 3- Maximum building height increase- The Martway Mixed Use project has been designed to accommodate a total unit count that will make the project financially sustainable. As such, the proposed residential unit count, coupled with the site's unique shape have resulted in the proposed design's footprint and overall building height. As illustrated in the composite views, Architectural detailing at the podium level, and the existing tree canopy to the south, and the 300' naturally landscaped City Park will effectively reduce the buildings height. The floodplain has required the building to be built on a podium structure. The fire department access to the rear of the building has dictated the height of the first floor podium elevation. The proposed structure is consistent with the surrounding precedents, the Mission Square building is approximately 56' above grade at its high point and the recently approved Mission Trails project is approximately 63' above grade at its high point. Due to the sloping topography, these projects sit 10' -20' higher than the the Martway site effectively making the proposed structure the lowest of the three

developments in elevation.

Deviation 4- Minimum lot area per multi-family increase- The Martway Mixed Use project has been designed in response to current marketplace trends for increased density as well as developmental targets to make the project an economically sustainable project. To continue developing a vibrant walk-able neighborhood and support the existing business along Johnson Drive additional density is required.

Deviation 5- Parking lot setback reduction- The proposed parking lot is designed to maximize the on-site parking potential to accommodate the residential and commercial parking requirements. The standard 6' dimension is typically related to incompatible uses and we don't want to create an awkward condition between the 2 parking lots (existing and new) at the west property line. Also, the proposed building massing design exceeds the setback requirements to provide more openness between the adjacent property owners to the east and west. As the site design continues to develop, we will look for opportunities to create landscape buffers where feasible and we will submit a proposed solution with final development plan if required. We can also evaluate compact parking dimensions and site optimization as the planning process moves forward in an effort to reduce the overall parking lot width. Please note that the 6' setback at the east property line is compliant. We are seeking this deviation at the west property line only.

Deviation 6- Minimum green space buffer reduction- The proposed parking lot is designed to maximize the on-site parking potential to accommodate the residential and commercial parking requirements. See above response for deviation request #5.

Deviation 7- Interior parking lot tree requirement- In lieu of a large open surface parking lot or multi-level parking deck, the proposed parking has intentionally been placed under the building's footprint to reduce its visual impact to the surrounding areas. As such, tree growth will not be possible. The Rock Creek channel creates a natural landscape buffer that exceeds the requirement.

Deviation 8- Parking lot interior open space requirement- The proposed parking lot is designed to maximize the on-site parking potential to accommodate the residential and commercial parking requirements. The majority of the parking is covered by the building above (so this requirement is more applicable to open suburban surface lots). As the site design continues to develop, we will look for opportunities to create landscape buffers where feasible and we will submit a proposed solution with final development plan if required.

3. Please provide any additional studies or data regarding the anticipated parking demand for this use. These may be counts or observations made at other similar projects for the number of vehicles per dwelling unit. A deviation for the number of required parking stalls may be considered. Staff would prefer this to deviations in parking lot design especially along the west and east property boundaries.

Based on past experience with mixed use projects located cities of Olathe, Overland Park and KCMO, it is not recommended to deviate from the 210 space parking requirement. As mentioned in our deviation request #1 response for onsite parking reduction, we anticipate that the 14 parking spaces for the business use will easily be handled on site due to this peak day/night offset. An expected operational vacancy for the residential use is 5% which reduces the actual parking demand from 196 to

186 required spaces. This results in a likely scenario of leasing approximately 10 parking spaces off-site. We do not foresee any further reduction in the anticipated parking demand.

4. The tree species shown for shade trees must comply with the City's approved list of street trees per Section 240.070. Please substitute another compliant species.

The landscape plan has been updated to show compliant species.

5. Automatic irrigation of the streetscape trees is required.

A note has been added to the landscape plan.

6. Leave sufficient room for the required streetscape elements. A minimum of 15' feet from back of curb to building is suggested. Sidewalks along Martway Street are part of the Rock Creek Trail system and must maintain a 10' wide clear path. See the previous platting comment. A five foot tree planting zone is preferred.

The plan has been revised to show a 5' planting zone, and a 8' trail with a 10' wide clear path.

7. Please show the pedestrian crosswalk locations along Martway Street and how they relate to the proposed building. Details of pedestrian circulation/access to the building on the site will need to be shown with final site plan drawings.

Existing and proposed pedestrian crosswalks have been added to the site plan.

8. The establishment of a private sign criteria to serve as the adopted sign code for this development is suggested. The criteria must be approved by the Planning Commission as part of the final site plan (Section 430.120). Staff recommends organizing the sign criteria by building area or use and including an analysis of how the proposed criteria is similar to the City Sign Ordinance. Objective criteria for signs such as type, area, height, number, illumination should to be provided in a separate document at that time. Signs are not approved as part of the site plan review process and individual sign permits must be issued before installation.

Clockwork is handling this comment.

9. Stories beyond the second story must incorporate a minimum 8' step back from the front facade of lower stories to meet the Johnson Drive Design Guidelines. Please keep this in mind for final site plan review.

Clockwork is handling this comment.

10. The primary facades of the parking structure along Martway Street should reflect similar materials and building quality as the main building. The Johnson Drive Design Guidelines require first floor buildings along Martway Street to incorporate glazing into at least 75% of the facade.

Clockwork is handling this comment.

11. Vehicles inside the parking structure must be screened so as to be obscured from view from the street. Additional screening treatment may be required.

The landscape plan has been revised to show screening between the parking structure and Martway.

12. Please provide additional perspective views of the building from the surrounding neighborhoods to the north and south so the impact to public health, safety, morals, order, convenience, prosperity or general welfare can be evaluated as part of the height deviation review.

Clockwork is handling this comment.

13. Indicate which area of the parking field will be designated for resident use or business use.

Business use and resident use spaces have been indicated on the site plans.

14. Surface parking stalls along the Rock Creek Trail must be screening with hardscape and plantings or an equivalent evergreen landscape a minimum of 3' in height.

The landscape plan has been revised to show screening between the parking structure and Martway.

15. The Johnson Drive Design Guidelines encourage hard surfaced exterior materials that do not artificially simulate other materials. Please explain how Nichiha fiber cement board panels as proposed accomplish this.

Clockwork is handling this comment.

16. Windows along the ground floor along Martway Street should be elevated above the sidewalks by 18-24". Bulkheads should be constructed out of sturdy materials

Clockwork is handling this comment.

17. A floodplain development permit will be required per Section 460. Please explain how the proposed design will meet these standards.

A floodplain permit will be filed based on city standards.



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To: Danielle L. Sitzman, AICP
City Planner
City of Mission
6090 Woodson St.
Mission, KS 66202
Ph. 913.673.8363
Email: dsitzman@missionks.org

Date: September 15, 2017

From: Todd Howard
Clockwork Architecture & Design
423 Delaware, #102
Kansas City, MO 64133

Project: Martway Mixed Use

RE: Responses to Preliminary Planning Review Comments

Comment # & Response

Plat Comments:

- 1) Re-platting of the property will be required prior to the issuance of building permits. Right-of-way must be dedicated to include all of the Rock Creek Trail, public sidewalks, and public infrastructure including stormwater facilities. A final plat may be submitted with the final site plan.

Acknowledged.

Site Comments:

- 2) Please explain the purpose for each of the deviations requested and how they meet the objectives and standards of the planned district regulations (Section 405.070

Refer to attached responses prepared by CFS Engineers.

- 3) Please provide any additional studies or data regarding the anticipated parking demand for this use. These may be counts or observations made at other similar projects for the number of vehicles per dwelling unit. A deviation for the number of required parking stalls may be considered. Staff would prefer this to deviations in parking lot design especially along the west and east property boundaries.

Refer to attached responses prepared by CFS Engineers.

- 4) The tree species shown for shade trees must comply with the City's approved list of street trees per Section 240.070. Please substitute another compliant species.

Refer to attached responses prepared by CFS Engineers.

- 5) Automatic irrigation of the streetscape trees is required.

Refer to attached responses prepared by CFS Engineers.



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- 6) Leave sufficient room for the required streetscape elements. A minimum of 15' feet from back of curb to building is suggested. Sidewalks along Martway Street are part of the Rock Creek Trail system and must maintain a 10' wide clear path. See the previous platting comment. A five foot tree planting zone is preferred.

Refer to attached responses prepared by CFS Engineers.

- 7) Please show the pedestrian crosswalk locations along Martway Street and how they relate to the proposed building. Details of pedestrian circulation/access to the building on the site will need to be shown with final site plan drawings.

Refer to attached responses prepared by CFS Engineers.

- 8) The establishment of a private sign criteria to serve as the adopted sign code for this development is suggested. The criteria must be approved by the Planning Commission as part of the final site plan (Section 430.120). Staff recommends organizing the sign criteria by building area or use and including an analysis of how the proposed criteria is similar to the City Sign Ordinance. Objective criteria for signs such as type, area, height, number, illumination should to be provided in a separate document at that time. Signs are not approved as part of the site plan review process and individual sign permits must be issued before installation.

Refer to new Signage Details 11"x17" sheet. The signage criteria has been organized by building area and includes objective criteria for sign type, area, height, number and illumination. All building signage shall comply with Mission design guidelines and section 430.120 'Private Sign Criteria'.

- 9) Stories beyond the second story must incorporate a minimum 8' step back from the front facade of lower stories to meet the Johnson Drive Design Guidelines. Please keep this in mind for final site plan review.

Acknowledged. We understand that this guideline relates to the historical buildings along Johnson Drive to respect the scale of the existing single story buildings and provide setback relief from the street to simulate the vernacular of a historical downtown main street. Given that this project is not directly on Johnson Drive and the existing adjacent and surround buildings do not provide an 8' step back from their front façade of the lower stories, the current design aligns with the existing context and fabric on Martway Street. An 8' setback at the second floor and above would result in the loss of 14 units per floor or 56 total units for floors 2-5 parallel to Martway street. A redesign to push the building further back into the site would conflict with alleviating building massing concerns for the residents directly behind the project along 61st Street.



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- 10) The primary facades of the parking structure along Martway Street should reflect similar materials and building quality as the main building. The Johnson Drive Design Guidelines require first floor buildings along Martway Street to incorporate glazing into at least 75% of the facade.

The primary façade of the parking structure does reflect similar materials and building quality of the main building. The Nichiha wood wall panel is used as an accent on the main building at the back wall of all balcony insets and between windows. Per the Mission Design Guidelines, lower levels of buildings should be differentiated architecturally from upper levels, which is reflected in the current design.

Regarding glazing and openness area at the first floor, calculations have been provided on the elevations showing the overall area of the first floor façade (6,861 sf) and the area and percentage of glazing and openness (3,762 sf) (55%). Refer to A200.

Please note that glazing area was reduced 170 sf (5%) to provide an 18" bulkhead per planning comment #16.

- 11) Vehicles inside the parking structure must be screened so as to be obscured from view from the street. Additional screening treatment may be required.

Refer to attached responses prepared by CFS Engineers.

- 12) Please provide additional perspective views of the building from the surrounding neighborhoods to the north and south so the impact to public health, safety, morals, order, convenience, prosperity or general welfare can be evaluated as part of the height deviation review.

Three additional photomontage/composite views have been added. Refer to A202.

- 13) Indicate which area of the parking field will be designated for resident use or business use.

Refer to attached responses prepared by CFS Engineers.

- 14) Surface parking stalls along the Rock Creek Trail must be screening with hardscape and plantings or an equivalent evergreen landscape a minimum of 3' in height.

Refer to attached responses prepared by CFS Engineers.



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- 15) The Johnson Drive Design Guidelines encourage hard surfaced exterior materials that do not artificially simulate other materials. Please explain how Nichiha fiber cement board panels as proposed accomplish this.

Nichiha fiber cement board panels are a hard surface exterior material. Only one of the three panel types that have been specified simulate another material, wood. When compared to wood, the Nichiha fiber cement panel is more durable, requires less maintenance, has better color stability, is resistant to delamination, resists warping, rotting and pests, has a fire rating and is a higher end product when compared to the cost of wood. The Nichiha fiber cement panel carries a 15 year warranty, which cannot be provided with true wood. Please see attached Nichiha vs wood comparison chart.

- 16) Windows along the ground floor along Martway Street should be elevated above the sidewalks by 18-24". Bulkheads should be constructed out of sturdy materials.

An 18" tall bulkhead has been added to base of the ground floor windows along Martway. Refer to A200.


- 17) A floodplain development permit will be required per Section 460. Please explain how the proposed design will meet these standards.

Acknowledged. A floodplain permit will be filed based on city standards.

WOOD CLADDING COMPARISON CHART

See how Nichiha's Wood Series Architectural Wall Panels stack up against the competition...



	NATURAL WOOD	PARKLEX FACADE	 NICHIIHA fiber cement <i>the power of possibilities™</i>	LONGBOARD	RESYSTA
Wood Texture	✓		✓		✓
Color Stability		✓	✓	✓	✓
Exclusive manufacturer of wall cladding			✓	✓	
Integrated Rainscreen		✓	✓	✓	
Easy Installation	✓		✓	✓	✓
Fire Rating		✓	✓	✓	
Resistant to warping rotting and pests		✓	✓	✓	✓
50-year or more limited lifetime warranty			✓	✓	
Resistant to delamination			✓	✓	✓
Budget friendly	✓		✓		✓



Safety Data Sheet (SDS)

[1. PRODUCT AND COMPANY IDENTIFICATION]

PRODUCT NAME	Nichiha NichiProducts: NichiBoard, NichiPanel, NichiShake, NichiStaggered, NichiStraight, NichiSoffit, NichiTrim, NichiFrontier
MANUFACTURER	Nichiha USA, Inc.
ADDRESS	3150 Avondale Mill Road, Macon, GA 31216
HEADQUARTERS ADDRESS	6565 East Johns Crossing, Johns Creek, GA 30097
PHONE	866-424-4421
DATE PREPARED	June 2015

[2. SUMMARY OF HAZARDOUSNESS/HARMFULNESS]

GHS classification

Health harmfulness

- Skin corrosivity/irritation: Classification 1
- Serious eye damage/eye irritation: Classification 1
- Carcinogenicity: Classification 1A
- Specific target organ toxicity (single exposure): Classification 1 (respiratory system)
- Specific target organ toxicity (repeated exposures): Classification 1 (respiratory system, kidney)

GHS label element(s)

Symbols



Signal Word: DANGER

Hazard Statements

- Serious chemical damage to skin
- Serious eye damage
- Carcinogenicity
- May damage the respiratory system if inhaled.
- May damage the respiratory system or kidneys through long-term or repeated exposures.

Safety Measures

- Wash your hands and face thoroughly after handling the product.
- Wear protective gloves, clothes, goggles and mask.
- Do not inhale powder dust.
- Do not eat, drink or smoke while using this product.

First-aid Measures

- Inhalation: Move the victim to a place with fresh air and rest patient in the posture comfortable for breathing.
- Skin contact: Immediately take off/remove all contaminated clothes. Wash the skin under running water.
- Eye contact: Rinse the eye with water carefully for a few minutes. Next, if contact lenses are worn, remove them if easy to remove. Continue washing the eye with water. Immediately seek medical advice/attention.
- When ingested: Wash the mouth. Do not induce vomiting.
- When reusing the contaminated clothes: Wash them prior to use.
- Seek medical attention if you were exposed or feel sick.

Disposal

- Follow applicable local, state, and federal construction waste management requirements. Prevent potential dust exposure for others.

[3. COMPONENT/INFORMATION ON INGREDIENTS]

Classification of single product or mixture: Mixture

Ingredients: Cement, silicate material, organic fiber, additives

NAME	CAS#	%content
Crystalline silica	14808-60-7	0 ~ 10
Calcium silicate	1344-95-2	30 ~ 60
Cellulose	9004-34-6	5 ~ 10
Mica	12001-26-2	3 ~ 5

- The product does not contain asbestos.
- The product does not contain formaldehydes.

[4. FIRST AID]

Eye contact: Immediately wash the eye for at least 15 minutes using clean water and then seek attention of a doctor.

Skin contact: Immediately wash the skin thoroughly with soap and water. Seek medical attention as needed if irritation develops or persists.

Inhalation: Immediately move to a place with fresh air away from dust, gargle with water, and seek medical attention as needed.

Ingestion: Wash the inside of the mouth thoroughly with water and seek medical attention. If the victim is groggy or unconscious, do not induce vomiting, but seek medical attention without delay.

When exposed or potentially exposed to silica dust: Seek medical attention/treatment as necessary.

[5. MEASURES TAKEN IN CASE OF FIRE]

Flammability of the product: Non-combustible when tested under ASTM E136.

Extinguishing method: Cut off the combustion path to the source of fire and extinguish the fire using water and fire-extinguishing medium. Fight the fire from the upwind side and wear respiratory protection gear if necessary.

Fire-extinguishing media: Water, powder, carbonic acid gas, foam

[6. MEASURES TAKEN IN CASE OF LEAK]

The product is normally in a solid sheet-shaped state, so no special measures are needed.

[7. HANDLING AND STORAGE PRECAUTIONS]

Handling:

- Wear protective gloves (work gloves, etc.) when handling the product.
- Provide local exhaust measures when cutting the material and use cutting equipment with anti-dust function. Also wear proper protective equipment (anti-dust mask, protective goggles, etc.) so as not to inhale powder dust or let it enter the eyes.
- Clean dust with HEPA filter equipped vacuum. Do not dry sweep or use compressed air.
- Do not wet the product.
- Rinse face, hands, mouth, etc., with water after handling the product.

Storage: Store the product away from water.

[8. MEASURES FOR PREVENTION OF EXPOSURE]

See below if powder or dust is generated from cutting or otherwise processing the product.

Japan Society for Occupational Health (2014)

Inhalant crystalline silica	0.03 mg/m ³ (TWA)
Inhalant powder dust	1 mg/m ³ (TWA)
Total powder dust	4 mg/m ³ (TWA)

ACGIH TLV (2006):	
Crystalline silica	0.025 mg/m ³ (TWA)
Inhalant powder dust	3 mg/m ³ (TWA)
Total powder dust	10 mg/m ³ (TWA)

OSHA PEL (2015) (Refer to 29 CFR 1910 Table Z-3 regarding mineral dusts):

Crystalline silica (Quartz) (Action Level)	25 µg/m ³ (TWA)
(Permissible Exposure Limit [PEL])	50 µg/m ³ (TWA)
Calcium Silicate (Respirable Fraction)	5 mg/m ³ (TWA)
(Total)	15 mg/m ³ (TWA)
Cellulose (Respirable Fraction)	5 mg/m ³ (TWA)
(Total)	15 mg/m ³ (TWA)
NIOSH REL (2015)	
Mica (Respirable Fraction)	3 mg/m ³ (TWA)

Facility/Engineering Measures: Cut the product outdoors or in a well-ventilated place using a saw with fiber cement saw blades and dust-collecting function. When handling the product indoors, provide a ventilation system, etc., to keep the concentration of airborne dust to the controlled level or below or cut using fiber cement shears.

Personal Protective Equipment:

Eyes:	Anti-dust goggles compliant with ANSI Z87.1.
Hands:	Protective work gloves, regularly washed.
Respiratory:	Use a properly-fitted N, O, or P 100 respirator when cutting or otherwise abrading product.
Skin:	Select personal protective equipment for the body based on the task being performed. Pants, long-sleeve shirts recommended to prevent skin from dust exposure.

[9. PHYSICAL AND CHEMICAL PROPERTIES]

Appearance:	Sheet shaped
Bulk specific gravity:	1.2 ± 0.2
Solubility:	Insoluble in water

[10. STABILITY AND REACTIVITY INFORMATION]

Stability/Reactivity:	Stable
Hazardous/harmful reaction potential:	Not applicable
Hazardous/harmful decomposition products:	Not applicable

[11. INFORMATION ON TOXICOLOGY/HARMFULNESS]

Acute toxicity: No data is available.

Skin corrosivity/irritation and serious damage/irritation to eye:

- If product comes into contact with water, it may exhibit strong alkalinity (pH12 to 13) and cause irritation to the eye, nose and skin as well as inflammation to the cornea, tissues inside the nose, and skin.

Respiratory organ sensitization or skin sensitization:

- The cement contains a trace amount of chromium compound and may cause allergic reaction in people sensitive to hexavalent chromium.

Carcinogenicity: No data is available.

- The product is classified under carcinogenicity classification 1A because it contains crystalline silica.

Reproductive cell mutagenicity: No data is available.

Reproductive toxicity: No data is available.

Specific target toxicity (single exposure): No data is available.

- The product is classified as specific target toxicity (single exposure) classification 1 (respiratory system) because it contains crystalline silica that is classified as having specific target toxicity (single exposure).

Specific target toxicity (repeated exposures): The product may cause pneumoconiosis if inhaled in large quantities over a long period of time.

- The product is classified as specific target toxicity (repeated exposures) classification 1 (respiratory system) because it contains crystalline silica that is classified as having specific target toxicity (repeated exposures).

[12. INFORMATION ON ENVIRONMENTAL IMPACT]

Environmental impact/bio-toxicity

- Exercise caution to prevent negative environmental impact, water may exhibit strong alkalinity (pH12 to 13) with prolonged exposure.

[13. PRECAUTIONS ON DISPOSAL]

Follow all local, state, and federal regulations with respect to construction waste material disposal. When cleaning up dust, never dry sweep. Wet the dust prior to sweeping or use a HEPA vacuum. Take measures to prevent potential dust exposure to others.

[14. PRECAUTIONS ON TRANSPORT]

Information on codes and classifications under international regulations: Not applicable

Specific safety measures and conditions for transport:

- Prevent collapse of cargo, etc., without fail.
- Pay attention to prevent wetting.

[15. REGULATORY INFORMATION]

United States inventory (TSCA) listed items: Quartz – Crystalline Silica (14808-60-7), Calcium Silicate (1344-95-2).

SARA 302/303: No Extremely Hazardous Substances.

SARA 311/312:

	<u>Acute</u>	<u>Chronic</u>	<u>Fire</u>	<u>Pressure</u>	<u>Reactive</u>
Crystalline Silica (Quartz)	yes	yes	no	no	no

[16. OTHER INFORMATION]

Cited Literatures

- JIS Z 7253: 2012 (Japan)
- Health, Labour and Welfare Ministry's Workplace Safety Site, Information on GHS-compliant Model Labeling/Model SDS (Japan)

This data sheet has been prepared based on documents, information and data currently available, but the contents, physical/chemical properties, hazardousness information and other values are not guaranteed. Also note that the cautionary instructions assume normal handling, and if the product will be handled in any special manner, implement safety measures appropriate for the specific application/method of use.

MARTWAY MIXED USE – STORMWATER DRAINAGE MEMORANDUM

PAGE 1 OF 3

DATE: September 13th, 2017
RE: Martway Mixed Use Apartments
6045 Martway
Mission, Kansas 66202
CFS Project No. 17-5085

On behalf of the owners of the Martway Mixed Use development, CFS Engineers, P.A., requests a waiver from stormwater management based on the minimal change in surface runoff characteristics between the pre and post-development site conditions. The site is bounded on the north by Martway Street, on the south by Rock Creek, and along the east and west by low-rise commercial/office buildings. Johnson Drive and Mission's downtown shopping strip is located less than a quarter mile to the north. The Sylvester Powell Jr. Community Center is located to the northwest. The Mission Aquatic Center is located across Rock Creek to the southeast.



**Site Location Map, Mission, Kansas
Proposed Martway Mixed Use Development**

The proposed 1.767 acre site calls for the removal of three existing single story office buildings and parking lots along the southern side of Martway Street between Beverly Avenue and Dearborn Street and replacing them with a multi-story apartment complex building with business space and parking on the lower level. The apartment building would be elevated to provide

MARTWAY MIXED USE – STORMWATER DRAINAGE MEMORANDUM

PAGE 2 OF 3

parking at ground level. The changes between the pre and post-development impervious surface area was less than 5,000 sq ft per the APWA 5600, and was measured as follows:

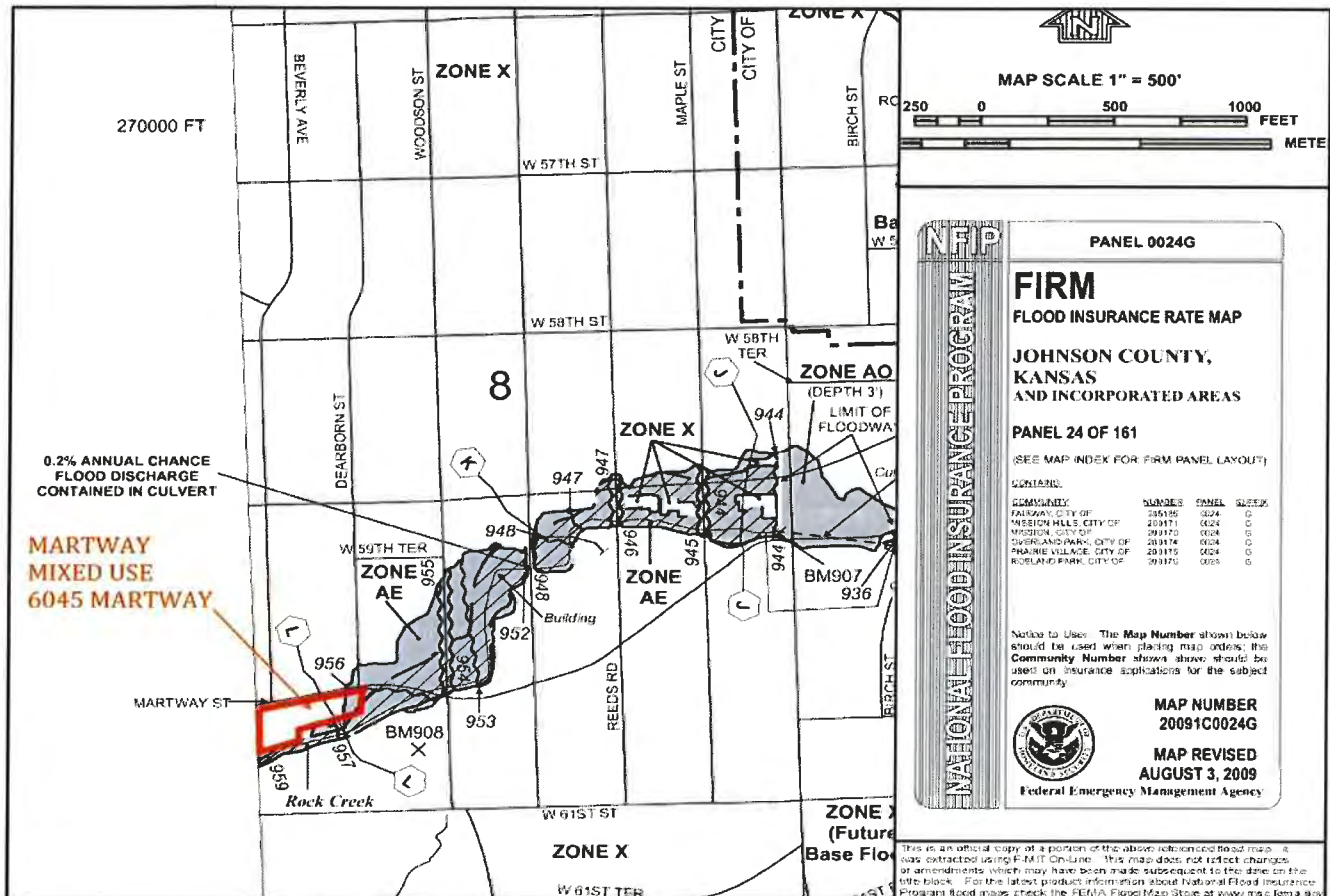
Pre-Development Conditions:

- Total Site Area = 1.767 acres
- Impervious Surface = 1.415 acres/61,653 sqft
- 80.1% Impervious

Post-Development Conditions:

- Total Site Area = 1.767 acres
- Impervious Surface = 1.494 acres/65,071 sqft (3,418 sqft increase)
- 84.6% Impervious

Under the APWA Section 5601.3.A.3, “Remodeling, repair, replacement or other improvements to any existing structure or facility and appurtenances on sites smaller than two acres that does not cause an increased area of impervious surface on the site in excess of 10 percent of that previously existing.” The 1.767 acre site is smaller than two acres and the 3,418 sqft increase in impervious surface from 80.1% to 84.6% does not exceed the allowable 5,000 sqft increase limit allowed by the APWA.

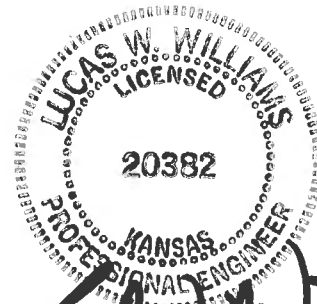


MARTWAY MIXED USE – STORMWATER DRAINAGE MEMORANDUM

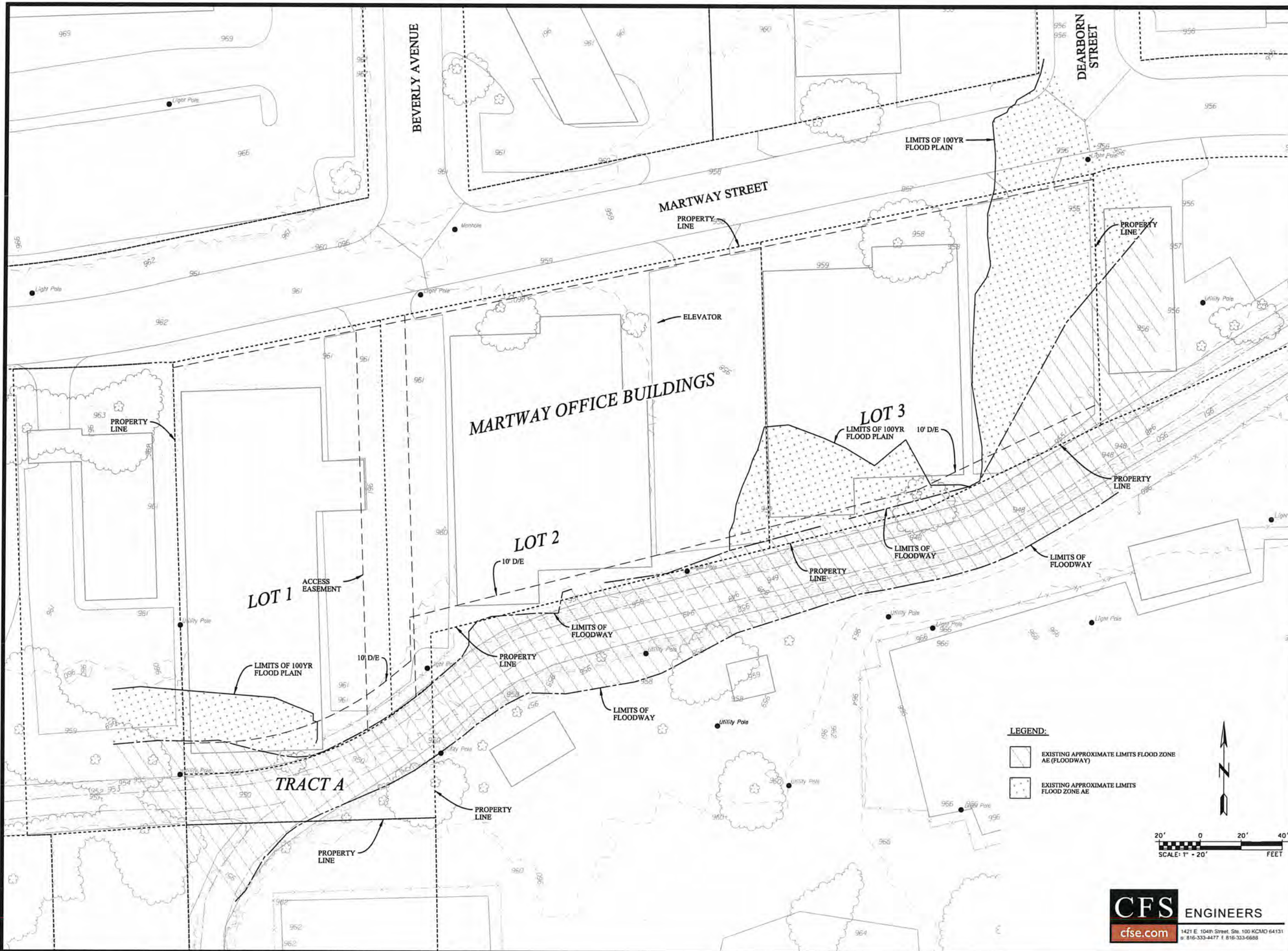
PAGE 3 OF 3

Stormwater runoff from the site's proposed parking lot and building roofs would be collected and drained directly into Rock Creek flowing eastwards along the rear of the property. The FEMA FIRM Panel 20091C0024G indicates that a small portion of the site is designated within 100-year flood zone AE from flooding during the 100-year storm event. The 3,491 sqft of business space on the ground floor would be set along the northern side of the site fronting Martway Street, and would be out of the FEMA 100-year flood zone. The upper floor apartments would be constructed on raised piers above the ground floor parking lot and would be one story above the FEMA 100-year flood zone.

The proposed parking lot plan has a total of 166 spaces (including five ADA accessible spaces and one ADA van-accessible space), so the developer would need to lease an additional 44 off-site parking spaces to meet the City's total 210 space requirement for the proposed apartments and business space. Superimposing the 100-year FEMA floodplain elevations from Rock Creek onto the proposed parking lot grading indicated that 40 spaces would be within the floodplain limits, however, no space would have more than the allowable 7 inches of water during the 100-year event.



Lucas W. Williams
9/13/17





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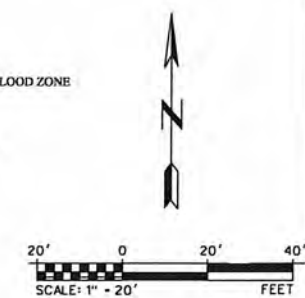
LUCAS W. WILLIAMS - ENGINEER
 KS PE # 20382

Martway Mixed Use

6009 - 6045 Martway
 Mission, KS 64202

LEGEND:

-  EXISTING APPROXIMATE LIMITS FLOOD ZONE AE (FLOODWAY)
-  EXISTING APPROXIMATE LIMITS FLOOD ZONE AE



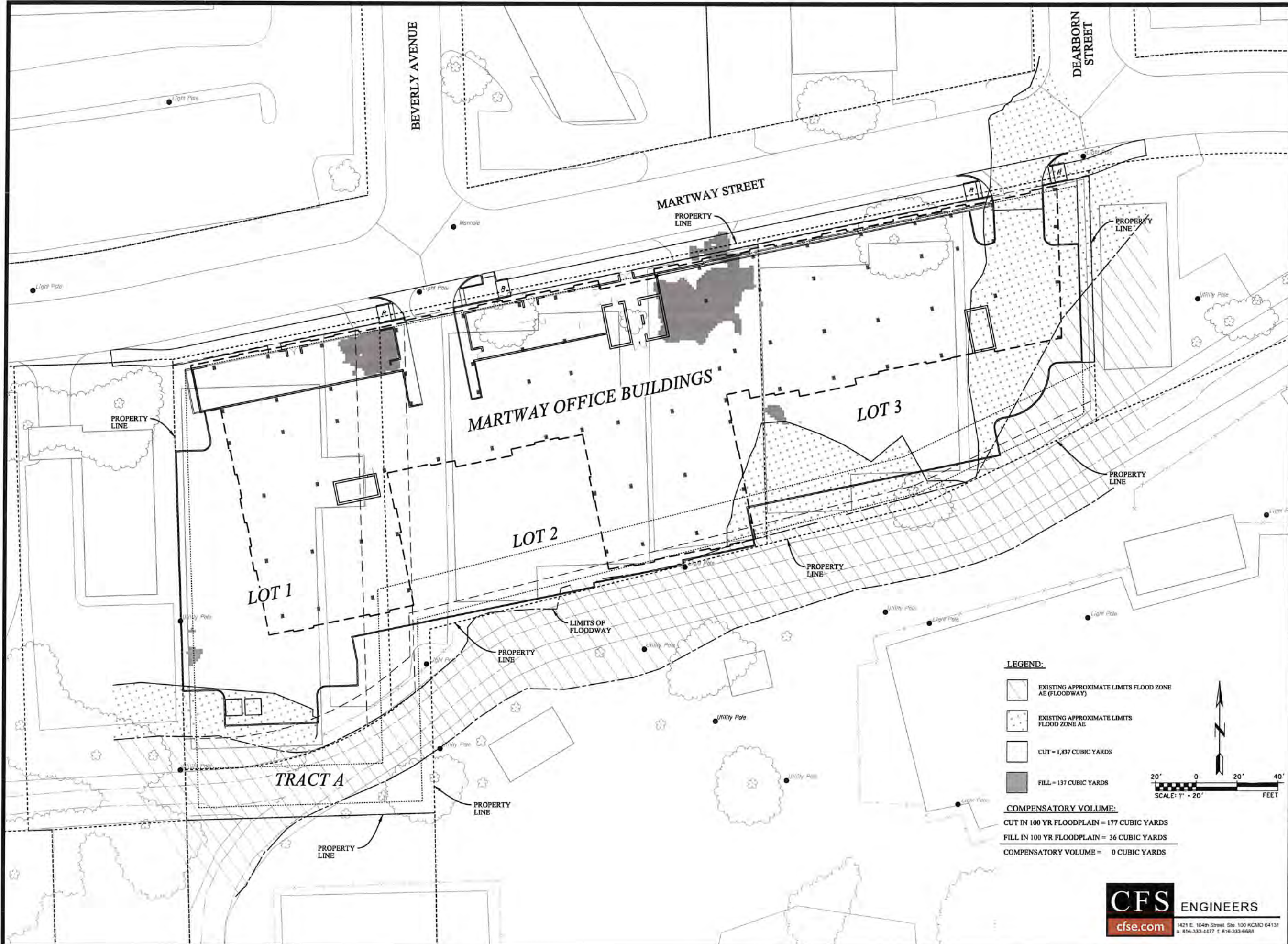
REV	ISSUE	DATE
	PRELIM. DEV. PLAN SUBMITTAL	07.12.2017

16.05.Martway

C202

EXISTING FLOOD PLAIN PLAN

CFS ENGINEERS
 cfse.com
 1421 E. 104th Street, Ste. 100 KCMO 64131
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LUCAS W. WILLIAMS - ENGINEER
 KS PE # 20382

Martway Mixed Use

6009 - 6045 Martway
 Mission, KS 64202

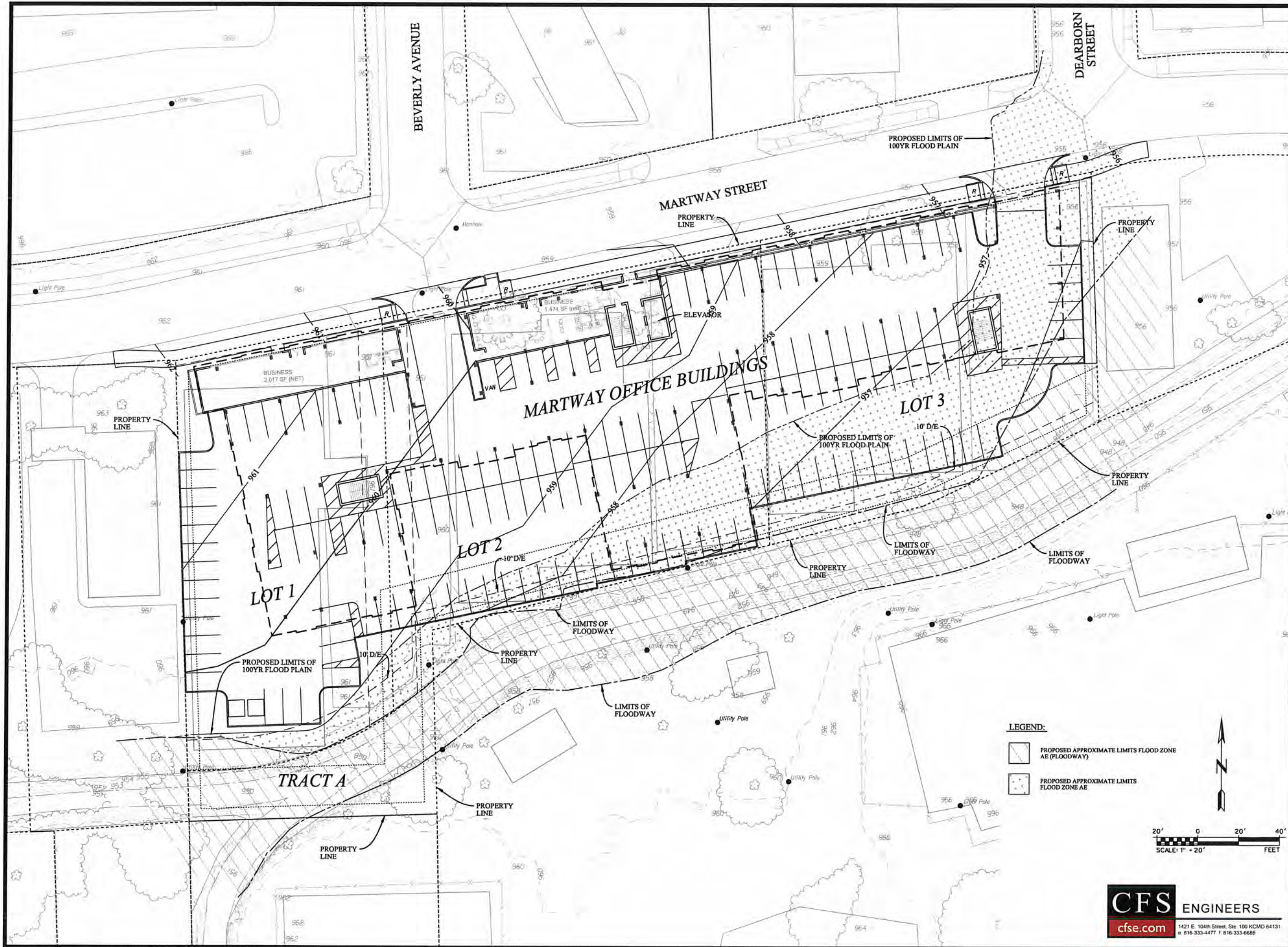
REV	ISSUE	DATE
	PRELIM DEV. PLAN SUBMITTAL	07.12.2017

16.05.Martway

C201

CUT & FILL PLAN

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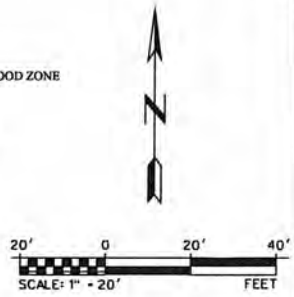
LUCAS W. WILLIAMS - ENGINEER
 KS PE # 20382

Martway Mixed Use

6009 - 6045 Martway
 Mission, KS 64202

LEGEND:

- PROPOSED APPROXIMATE LIMITS FLOOD ZONE AE (FLOODWAY)
- PROPOSED APPROXIMATE LIMITS FLOOD ZONE AE



REV	ISSUE	DATE
	PRELIM. DEV. PLAN SUBMITTAL	07.12.2017

16.05.Martway

C203

PROPOSED FLOOD PLAIN PLAN

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MARTWAY MIXED USE DEVELOPMENT
Proposed Building Redevelopment and Parking
6045 Martway
Mission, Kansas 66202
CFS Project No. 17-5085

Traffic Impact Analysis

September 13, 2017

Prepared for:
Clockwork Architecture & Design
423 Delaware, Suite 102
Kansas City, Missouri 64105



Prepared by:
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Review of Existing Site Conditions

This Traffic Impact Analysis for the proposed Martway Mixed Use building and parking lot improvements at 6045 Martway in Mission, Kansas, has been prepared in accordance with the City of Mission's Street Design Criteria. The proposed 1.767 acre site calls for the removal of three existing single-story office buildings along the southern side of Martway Street between Beverly Avenue and Dearborn Street and replacing them with a multi-story apartment building elevated on piers to provide street-level parking.

The site is bounded on the north by Martway Street, on the south by Rock Creek, and along the east and west by low-rise commercial/office buildings. Johnson Drive and Mission's downtown shopping area is located less than a quarter mile to the north. The Sylvester Powell Jr. Community Center is located to the northwest. The Mission Aquatic Center is located across Rock Creek to the southeast.



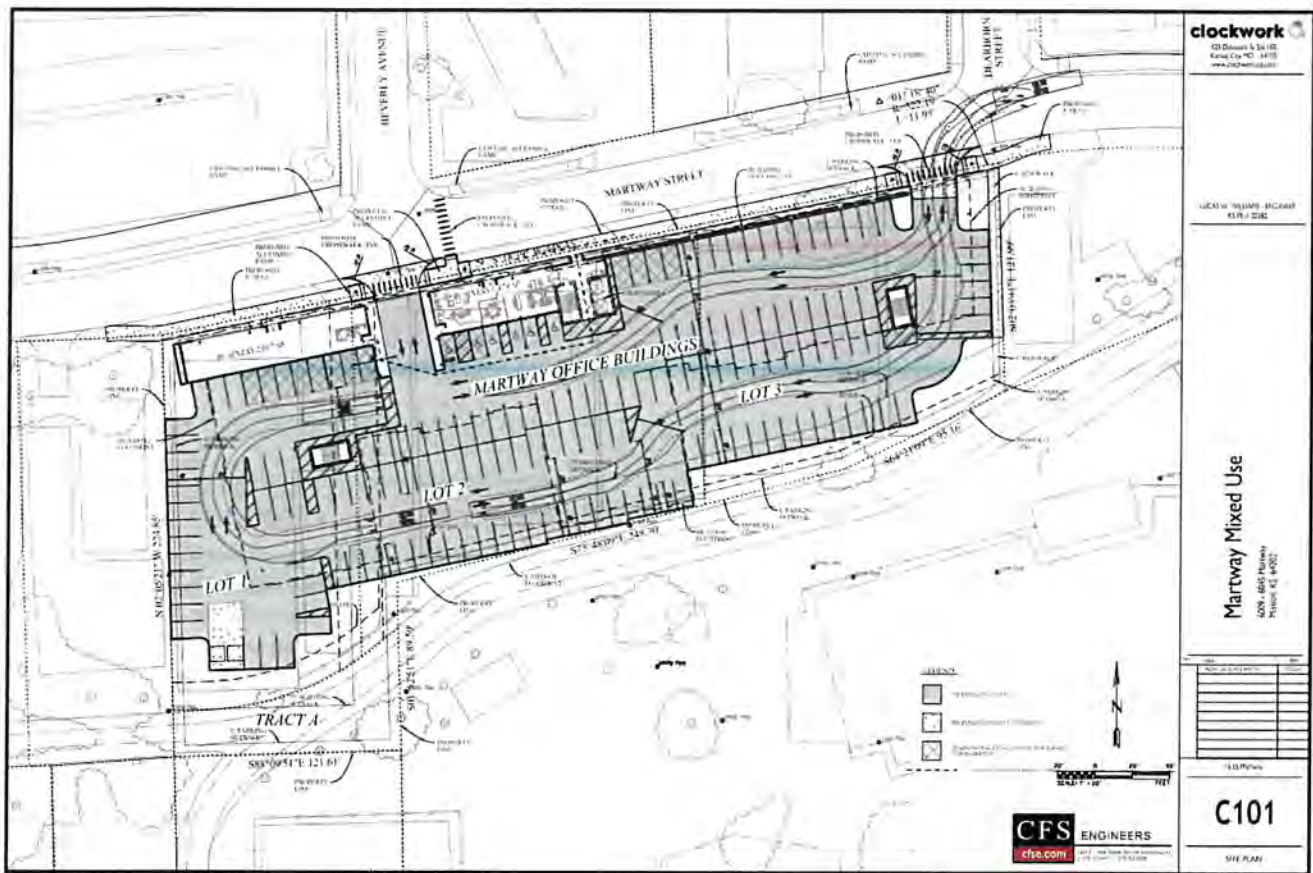
**Site Location Map, Mission, Kansas
Proposed Martway Mixed Use Development**

Area Street and Highway Network: The existing streets around the Martway Mixed Use Development site include:

- Martway Street – Two-lane collector.
 - Posted speed limit of 25 mph.
- Lamar Avenue – Two-lane collector.
 - Posted speed limit of 30 mph.
- Johnson Drive – Four-lane thoroughfare.
 - Posted speed limit of 30 mph.
- Beverly Avenue – Two-lane local.
 - Posted speed limit of 25 mph.
- Dearborn Street – Two-lane local.
 - Posted speed limit of 25 mph.
- Woodson Road – Two-lane local.
 - Posted speed limit of 25 mph.
- W. 61st Street – Two-lane local.
 - Posted speed limit of 25 mph.

The existing intersection of Martway & Beverly Avenue is a tee intersection with a 25 ft+/- offset driveway for the existing office building parking lot. For the proposed improvements, the existing driveway would be shifted to the east to align with Beverly Avenue. Both Martway Street and Beverly Avenue are two lane, 28 ft wide (back of curb to back of curb). The intersection corner radii are 25 ft. There are sidewalks along the north and south sides of Martway and along the west on Beverly. There is a painted crosswalk across the northern leg of the intersection. The intersection is stop controlled with free movement for the east and westbound traffic on Martway and a stop sign for southbound traffic on Beverly.

Grades along Martway are less than 2% and the intersection sight distance from the proposed western driveway entrance to the Martway Mixed Use Development was estimated at approximately 500 ft looking east and approximately 450 ft looking west. Martway has a posted speed limit of 25 mph. A realistic design speed for regular traffic was estimated at 35 mph. AASHTO's Exhibit 9-55, Design Intersection Sight Distance-Case B1- Left Turn from Stop, requires a design intersection sight distance of 390 ft at a design speed of 35 mph. AASHTO's Exhibit 9-58, Design Intersection Sight Distance-Case B2- Right Turn from Stop, requires a design intersection sight distance of 335 ft at a design speed of 35 mph. The proposed western driveway entrance to the Martway Mixed Use Development appears to have adequate intersection sight distance.



Proposed Martway Mixed Use Site Plan and Street-Level Parking Layout

The existing intersection of Martway & Dearborn Street is a tee intersection with a 35 ft +/- offset driveway for the existing office building parking lot. For the proposed improvements, the existing driveway would be held in the same location. Both Martway and Dearborn Street are two lane, 28 ft wide (back of curb to back of curb). The intersection corner radii are 25 ft. There are sidewalks along the north and south sides of Martway and along the west on Dearborn. There is a painted crosswalk across the northern leg of the intersection. The intersection is stop controlled with free movement for the east and westbound traffic on Martway and a stop sign for southbound traffic on Dearborn.

Grades along Martway are less than 2% and the intersection sight distance from the proposed eastern driveway entrance to the Martway Mixed Use Development was estimated at approximately 400 ft looking east and approximately 700 ft looking west. Martway east of the driveway entrance curves to the south, so motorists would have to turn their heads further to observe oncoming traffic, but there are no physical obstructions within the right-of-way to obscure the view. Martway has a posted speed limit of 25 mph. A realistic design speed for regular traffic was estimated at 35 mph. AASHTO's Exhibit 9-55, Design Intersection Sight Distance-Case B1- Left Turn from Stop, requires a design intersection sight distance of 390 ft at a design speed of 35 mph. AASHTO's Exhibit 9-58, Design Intersection Sight Distance-Case B2- Right Turn from Stop, requires a design intersection sight distance of 335 ft at a design speed of 35 mph. The proposed eastern driveway entrance to the Martway Mixed Use Development appears to have adequate intersection sight distance.

There are no known programmed improvements or future planned improvements for any of the roadways listed above in the region surrounding the Martway Mixed Use site.

Land Uses and Proposed Density: For the proposed 1.767 acre site, three lots would be combined into one (parcels KP20600000 0001, 0002 & 0003). The site has a current land use of offices with three existing single-story office buildings with a total footprint of approximately 34,465 sqft. For the proposed mixed use apartments and general office space, the existing buildings would be demolished and replaced with a multi-story apartment building with 156 units and 3,491 sqft of general office space. The main building would be raised on support piers to allow for parking beneath the structure. The general office space would be on the ground floor/parking level, comprised of two enclosed building sections flanking the sides of the entrance drive even with Beverly Avenue.

Water Conflicts: The FEMA FIRM Panel 20091C0024G indicates that a portion of the site is designated within 100-year flood zone AE from flooding from the adjacent Rock Creek which flows along the rear property line. In a 100-year flooding event, approximately 40 parking spaces along the creek could have up to seven inches of overbank water. A portion of the existing parking lot has been in the floodplain fringe area, and the reconfigured parking would closely match the existing parking limits in this area.

Existing Alternative Transportation Mode Choices: There are sidewalks along both sides of Martway Street, along the western side of Beverly Avenue, and along the western side of Dearborn Street. Portions of the Rock Creek Walking Trail coincide with the widened sidewalk section along the southern side of Martway Street fronting the proposed site. There are no designated bicycle lanes on any of the surrounding streets. There are Johnson County bus transit service stops on both sides of Martway Street approximately 300 ft west of Beverly Avenue.

Anticipated Phasing and Time-line: Construction is anticipated to begin in the spring of 2018 and would take approximately 18 months.

Existing and Projected Traffic Volumes

Existing Traffic Volumes: Weekday AM and PM Peak Hour traffic counts were taken at the intersections of Martway & Beverly Avenue and at Martway & Dearborn Street. Traffic volumes were recorded in 15 minute intervals on Wednesday June 14, 2017 and on Thursday June 15, 2017 during the AM Peak Hour from 7AM to 9AM and during the PM Peak Hour from 4PM to 6PM. Bad weather conditions or national holiday traffic did not impact traffic counts. The following tables summarize the traffic volumes measured for a typical AM and PM Peak Hour on a weekday:

Martway & Beverly Avenue, AM Peak Hour Traffic Movements (Wednesday, 06-14-17)

PHF	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
0.94	56	160	2	3	124	35	1	0	1	19	0	41

Martway & Beverly Avenue, PM Peak Hour Traffic Movements (Wednesday, 06-14-17)

PHF	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
0.91	36	253	0	0	274	36	1	0	3	42	0	76

Martway & Dearborn Street, AM Peak Hour Traffic Movements (Thursday, 06-15-17)

PHF	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
0.92	16	131	7	3	137	5	1	0	1	5	1	24

Martway & Dearborn Street, PM Peak Hour Traffic Movements (Thursday, 06-15-17)

PHF	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
0.92	32	268	0	1	244	16	6	0	3	12	0	26

The PM peak hour traffic was notably heavier than the AM. Directional east-west distribution along Martway Street was roughly even during both AM and PM peak hours. The following tables show the measured traffic volumes and directional distribution percentages used to develop the trip distribution of the additional trip generation volumes:

Directional Distribution (AM Incoming)

Intersection	Direction	Volume	Percentage
Beverly & Martway	EB	218	48.3%
	SB	60	13.4%
Dearborn & Martway	WB	145	31.8%
	SB	30	6.5%
Total		457	100.0%

Directional Distribution (AM Outgoing)

Intersection	Direction	Volume	Percentage
Beverly & Martway	WB	166	40.0%
	NB	91	22.0%
Dearborn & Martway	EB	137	32.9%
	NB	21	5.1%
Total		431	100.0%

Directional Distribution (PM Incoming)

Intersection	Direction	Volume	Percentage
Beverly & Martway	EB	289	41.0%
	SB	118	16.7%
Dearborn & Martway	WB	261	36.9%
	SB	38	5.4%
Total		719	100.0%

Directional Distribution (PM Outgoing)

Intersection	Direction	Volume	Percentage
Beverly & Martway	WB	351	46.7%
	NB	72	9.6%
Dearborn & Martway	EB	283	37.3%
	NB	48	6.4%
Total		755	100.0%

Site's Trip Generation and Design Hour Volume Data

Trip Generation and Design Hour Volume Data: Trip generation calculations utilized the land use types categorized by the Institute of Transportation Engineer's Trip Generation Guidelines, 9th Edition. The ITE Land Use categories used to estimate the traffic volumes anticipated to be generated by the site were Apartments (ITE Code 220) and General Office (ITE Code 710). The estimated number of trips generated by the buildings were calculated based on the total 156 dwelling units (DU) in the apartments and 3,491 sqft of floor area for the general office space. Both the ITE's trip generation equations and the average rates were used to calculate the site-generated traffic, and the higher/more conservative figures were used to model the proposed traffic characteristics of the development. The following table shows the parameters for measurement units, total trip generation volumes for the weekday AM and PM peak hour traffic, and the corresponding total vehicles for AM and PM peak hour traffic and the total weekday traffic at the site:

ITE Traffic Generation Volumes for the Proposed Site Improvements (vph)

Description / ITE Code	Units	AM Total	AM Enter	AM Exit	PM Total	PM Enter	PM Exit	Weekday Total
Apartments (220)	156 DU	87	25	62	109	66	43	1069
General Office (710)	3.49 KSF	16	8	8	6	3	3	156
Total		103	33	70	115	69	46	1225

Reductions for Pass-By and Diverted-Link Trips: Not applicable for apartments, and the amount of general office space was relatively small compared to the entire development, so pass-by and diverted-link trips were not included in the proposed trip distribution and traffic assignment.

Trip Distribution and Traffic Assignment

Trip Distribution and Traffic Assignment: Trip distribution patterns were determined based on a gravity model based on the peak hour counts around the development's surrounding origins and destinations. Directional percentages were applied along incoming and outgoing paths so that site-generated trips could be distributed proportionally. Appendix III includes trip generation calculations and traffic distribution diagrams for the existing traffic volumes, the site-generated traffic and the existing plus site-generated traffic for the AM and PM peak hour conditions.

Capacity Analysis

Creating Synchro Scenarios: Using the traffic counts and the ITE trip generation volumes, four Synchro models were created for the traffic conditions surrounding the site.

- Scenario 1 - Existing street/pre-development conditions
(Pre-development AM Peak Traffic 2017)
- Scenario 2 - Proposed site with trip-generated conditions
(Post-development AM Peak Traffic 2017)
- Scenario 3 - Existing street/pre-development conditions
(Pre-development PM Peak Traffic 2017)
- Scenario 4 - Proposed site with trip-generated conditions
(Post-development PM Peak Traffic 2017)

Capacity and Level of Service Analysis: Three performance measures commonly used for Traffic Impact Studies are vehicle delay, level-of-service (LOS), and queue length. Vehicle delay is the average delay, in seconds, experienced by one vehicle passing through the intersection. The quality of traffic operation at an intersection is defined through level-of-service (LOS) which consists of assignments of 'A' for free-flowing conditions through 'F' for congested conditions. The procedures and methodology for determining the LOS are outlined in the Highway Capacity Manual (HCM 2010), produced by the Transportation Research Board. LOS 'A' through 'C' is considered acceptable. For intersections, no individual lane should be below LOS D. 95th percentile queue length is the overall length of a string of stopped vehicles. Note that for stop control intersections, the queue length is measured in terms of accumulated number of vehicles which would be lined up waiting to proceed. The "-" symbol represents shared lane or non-existent movement, thus no queue length given. The results of the Synchro models for the left-turn movements at the intersections of Martway & Beverly Avenue are summarized in the table below (Delays are in seconds and Queues are in vehicle lengths set at a nominal 25 ft for the actual length of the design vehicle plus the buffer spacing between vehicles):

Martway & Beverly Avenue (Two-Way Stop Controlled)

Scenario	Intersection Delay (sec)	NBL D-LOS-Q	EBL D-LOS-Q	WBL D-LOS-Q	SBL D-LOS-Q
1-AM-Pre	2.4	0/A/0	7.7/A/0.1	0/A/0	10.5/B/0.3
2-AM-Post	3.2	12.4/B/0.2	7.7/A/0.1	7.6/A/0	11.2/B/0.4
3-PM-Pre	2.7	0/A/0	8/A/0.1	0/A/0	13.9/B/0.9
4-PM-Post	3.5	15.3/C/0.2	8.1/A/0.1	7.9/A/0	16.1/C/1.3

Martway & Beverly Avenue (Two-Way Stop Controlled): At the Beverly Avenue intersection, the intersection delay was 2.4 sec (LOS A) in the AM and 2.7 sec (LOS A) in the PM for the pre-development scenarios. The post-development scenarios intersection delays increased marginally to 3.2 sec (LOS A) in the AM and 3.5 sec (LOS A) in the PM. Eastbound and westbound movements were free except for the left-turns which had to yield to oncoming traffic. EB and WB average delay for left-

turns ranged from 7.6 sec (LOS A) to 8.1 sec (LOS A) throughout all scenarios. Northbound delays increased to 12.4 sec in the AM and 15.3 sec in the PM for the post-development scenario. Southbound delays increases to 11.2 sec in the AM and 16.1 sec in the PM for the post-development scenario. The longest 95th percentile queue length of any of the scenarios was 1.3 vehicle lengths.

Martway & Dearborn Street Avenue (Two-Way Stop Controlled)

Scenario	Intersection Delay (sec)	NBL D-LOS-Q	EBL D-LOS-Q	WBL D-LOS-Q	SBL D-LOS-Q
1-AM-Pre	1.2	0/A/0	7.6/A/0	0/A/0	9.5/A/0.1
2-AM-Post	2.3	11.1/B/0.2	7.6/A/0	7.6/A/0	9.7/A/0.1
3-PM-Pre	1.1	0/A/0	7.9/A/0.1	0/A/0	11.7/B/0.2
4-PM-Post	1.8	14.6/B/0.2	7.9/A/0.1	7.9/A/0	12.5/B/0.3

Martway & Dearborn Street (Two-Way Stop Controlled): At the Dearborn Street intersection, the intersection delay was 1.4 sec (LOS A) in the AM and 1.3 sec (LOS A) in the PM for the pre-development scenarios. The post-development scenarios intersection delays increased marginally to 2.3 sec (LOS A) in the AM and 1.8 sec (LOS A) in the PM. Eastbound and westbound movements were free except for the left-turns which had to yield to oncoming traffic. EB and WB average delay for left-turns ranged from 7.6 sec (LOS A) to 7.9 sec (LOS A) throughout all scenarios. Northbound delays increased to 11.1 sec in the AM and 14.6 sec in the PM for the post-development scenario. Southbound delays increases to 9.7 sec in the AM and 12.5 sec in the PM for the post-development scenario. The longest 95th percentile queue length of any of the scenarios was 0.3 vehicle lengths.

Traffic Accident History

Traffic Accident History: No accident report were reviewed in the preparation of this study.

Internal Circulation and Parking

Proposed Site Access: The proposed Martway Mixed Use parking area would have two entranced drives coinciding with the existing entrances to the office buildings at 6009 and 6045 Martway. The entrances would be open without any security gating. The parking configuration would include head-in parking spaces around the outer perimeter with an inside tier of head-to-head parking spaces which would allow the service drive to loop around the central spaces and connect to both the east and west access driveway back to Martway Street. The proposed apartment building would be perched above supported by piers.

The proposed building would consist of 3,491 sqft of lower-floor general office space with 156 apartment units on the upper floors. Per the City of Mission's MS-2 Parking Regulations, Chapter 410.250, the proposed development would require the following number of parking spaces:

Parking Requirements

Building Use	Space Requirements	Parking Required
General Office	4 per 1000 sqft * 3,491 sqft	14 spaces
Apartments (156 Total Units)		
Studio Apartments (24 Units)	1 space per unit * 24 units	24 spaces
One Bedroom (92 Units)	1 space per unit * 92 units	92 spaces
Two Bedroom (40 Units)	2 spaces per unit * 40 units	80 spaces
		210 spaces

The proposed parking lot plan has a total of 166 spaces (including five ADA accessible spaces and one ADA van-accessible space), so the developer would need to lease an additional 44 off-site parking spaces to meet the City's total 210 space requirement for the proposed apartments and general office space. Superimposing the 100-year FEMA floodplain elevations from Rock Creek onto the proposed parking lot grading indicated that 40 spaces would be within the floodplain limits, however, no space would have more than the allowable 7 inches of water during the 100-year event.

Traffic Operations and Geometric Improvements

Driveways: The proposed west driveway entrance would be re-aligned to match Beverly Avenue and the proposed east driveway would remain close to its existing location. The east and west driveways are spaced approximately 300 ft apart and both entrances would be two-lane, full-access connections. Security gating would not be installed at either driveway entrance.

Right-Turn Lane for eastbound Martway Street at East or West Entrance Driveway: A review of KDOT's Access Management Policy indicated that neither driveway entrance would warrant the addition of a right-turn lane. The design speed for Martway Street coupled with the relatively low traffic volumes would not meet the minimum threshold levels on the KDOT Access Management Policy's Table 4-25, Right-Turn Treatment Guidelines for Two-Lane Highways.

Left-Turn Lane for westbound Martway Street at East or West Entrance Driveway: A review of KDOT's Access Management Policy indicated that neither driveway entrance would warrant the addition of a left-turn lane. The design speed for Martway Street coupled with the relatively low traffic volumes would not meet the minimum threshold levels on the KDOT Access Management Policy's Table 4-27, Recommended Left-Turn Lane Warrants for Two-Lane Highways.

Signalization: With the relatively low volume of traffic on Martway Street and the trips that would be generated from the proposed Martway Mixed Use development, signals are not warranted on Martway Street at either of the intersections with Beverly Avenue or with Dearborn Street.

Summary and Recommendations

Summary: This study addressed the street access and potential traffic congestion for the proposed Martway Mixed Use Development at 6045 Martway Street in Mission, Kansas. The site would call for the demolition of three existing single-story office buildings and replacing them with a multi-story apartment building perched above a ground-floor parking lot. The apartment building would have 156 units comprised of 24 studio, 92 single and 40 double bedroom apartments. The development would need a total of 210 spaces and the proposed below-building parking lot would provide 166 spaces, with the developer obtaining off-site parking agreements to make-up the additional 44 spaces required.

An assessment of the proposed trip generation traffic and the traffic volumes on Martway Street at the intersections with Beverly Avenue and with Dearborn Street indicated that no right or left-turn auxiliary lanes would be warranted for the proposed driveway entrances to the site. Level-of-service ratings at the intersections would remain at the LOS-A or B levels, and queued traffic would be kept at low levels.

Recommendations: The following recommendations are made for the Martway Mixed Use Development and the surrounding area:

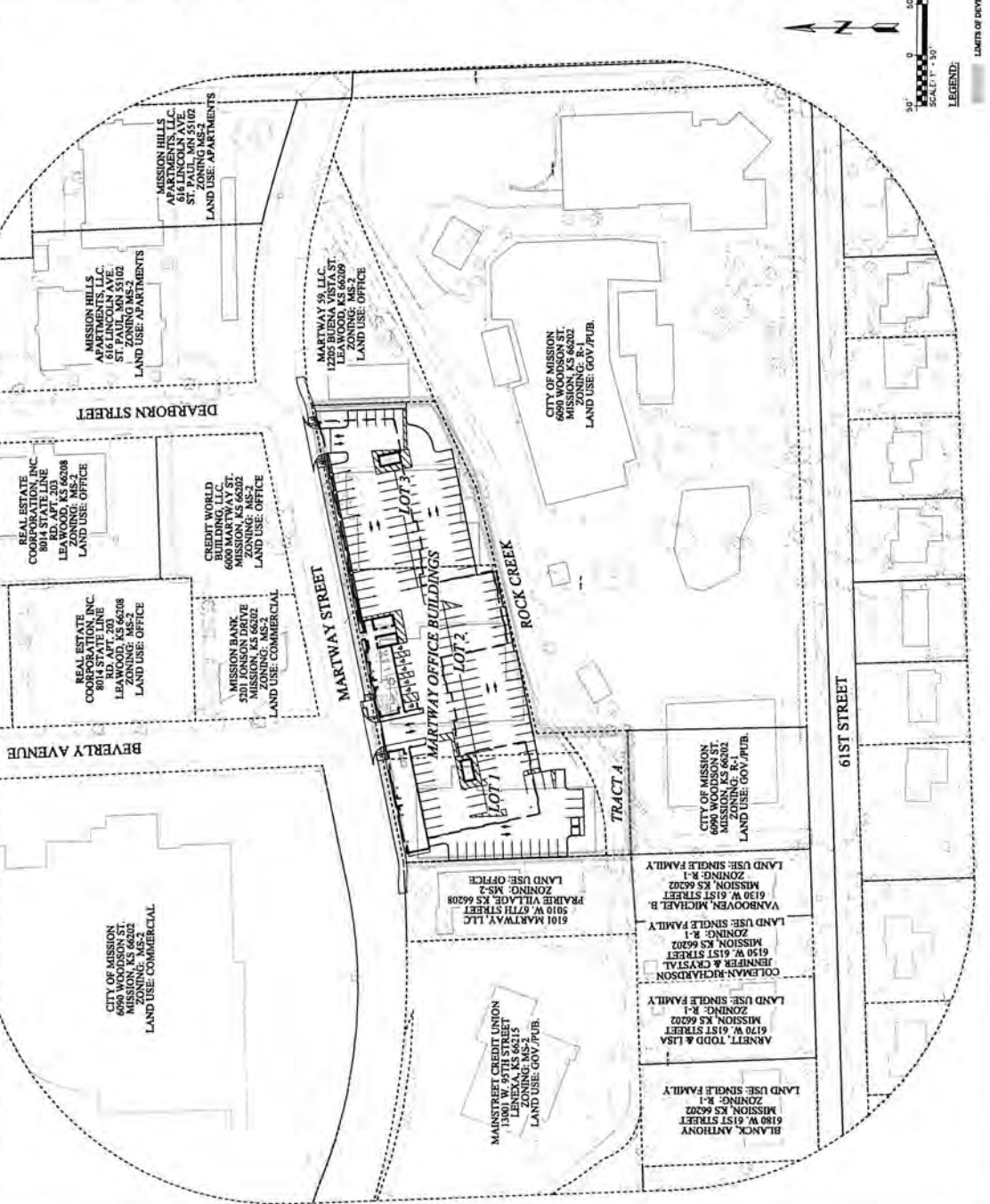
- The proposed west driveway entrance would be re-aligned to match Beverly Avenue and the east driveway entrance should remain at the present locations for the existing buildings at 6009 and 6045 Martway. Both driveways should be full-access entrances.
- The proposed development requires 210 parking spaces, and the proposed parking lot provides 166 spaces. The developer would need to lease an additional 44 off-site parking spaces to meet the City's total 210 space requirement.
- The existing Rock Creek walking trail running along the front side of the proposed building would remain unaltered by the development. During construction, the developer must make provisions to close the trail and divert pedestrian traffic to the northern side of Martway Street. The developer shall make all reasonable efforts to re-open the trail as quickly as possible once construction has been substantially completed and there would be no hazards to pedestrians.

Appendix I - Exhibit Maps (Site Plan, FEMA FIRM Map)

MARTWAY MIXED USE

PRELIMINARY DEVELOPMENT PLAN
6009-6025 MARTWAY, MISSION, KS 66200

PROJECT LOCATION



LEGAL DESCRIPTION

LOTS 1, 2 AND 3 OF MARTWAY OFFICE BUILDING A SUBDIVISION OF LAND IN THE NORTHWEST QUARTER OF SECTION 16, TOWNSHIP 21 NORTH, RANGE 13 EAST OF THE SIXTH PRINCIPAL MERIDIAN IN THE CITY OF MISSION, JOHNSON COUNTY, KANSAS

SITE DEVELOPMENT SUMMARY TABLE

EXISTING ZONING	LAND AREA	1.757 ACRES OR 76,191 SQUARE FEET MORE OR LESS
MS-2		

EXISTING BUILDINGS

OFFICE

PROPOSED: A MIXED USE DEVELOPMENT WITH A FACTORY ANNEALMENT COMPLEX WITH COMMERCIAL SPACE AND PARKING DECK ON THE FIRST FLOOR

BUILDING HEIGHT

MAXIMUM BUILDING HEIGHT: 4-STORY 45'-0"

BUILDING FLOOR AREA

1ST FLOOR	137,777 S.F.
2ND FLOOR	137,777 S.F.
3RD FLOOR	137,777 S.F.
4TH FLOOR	137,777 S.F.
TOTAL FLOOR AREA	551,110 S.F.

FLOOR AREA RATIO

2.00

PARKING DEMAND

PROPOSED PARKING: 1,875 SPACES

PERMITTED PARKING BY USE

PERMITTED BY	PERMITTED PARKING BY USE	OFFICE	RETAIL	RESIDENTIAL	INDUSTRIAL	OTHER
MS-2	1,875	1,875	0	0	0	0

OWNER

MARTWAY 59, LLC
12205 BUESA VISTA ST.
LEAWOOD, KS 66009

DEVELOPER

ENGINEER/LANDSCAPE ARCHITECT/SURVEYOR/PLANNER
141 E. 10TH STREET, SUITE 100
KANSAS CITY, MO 64101

REQUESTED DEVIATIONS

- THE OFFICE PARKING SPACE REQUIREMENT BE REDUCED FROM 235 SPACES TO 146 SPACES.
- THE REAR YARD SETBACK REQUIREMENT BE REDUCED FROM 25 TO A 5' SETBACK.
- THE MAXIMUM BUILDING HEIGHT REQUIREMENT BE REDUCED FROM A 4-STORY 45 FOOT TO A 3-STORY 40 FOOT MAXIMUM BUILDING HEIGHT.
- ADJUTANT GENERAL BUILDING BE REDUCED FROM 33 UNITS PER LOT TO 10 UNITS PER LOT.
- THE PARKING LOT SETBACK BE REDUCED FROM 4 TO 2.
- THE MINIMUM GREEN SPACE BETWEEN PARKING AND OUTDOOR LOT LINES BE REDUCED FROM 7 TO 5.
- THE INTERIOR PARKING LOT TREE REQUIREMENTS.
- THE PARKING LOT INTERIOR OPEN SPACE REQUIREMENT.

ARCHITECT

ENGINEER/LANDSCAPE ARCHITECT/SURVEYOR/PLANNER
141 E. 10TH STREET, SUITE 100
KANSAS CITY, MO 64101

OWNER

CITY OF MISSION
6090 WOODSON ST.
MISSION, KS 66202

DEVELOPER

ENGINEER/LANDSCAPE ARCHITECT/SURVEYOR/PLANNER
141 E. 10TH STREET, SUITE 100
KANSAS CITY, MO 64101

REQUESTED DEVIATIONS

- THE OFFICE PARKING SPACE REQUIREMENT BE REDUCED FROM 235 SPACES TO 146 SPACES.
- THE REAR YARD SETBACK REQUIREMENT BE REDUCED FROM 25 TO A 5' SETBACK.
- THE MAXIMUM BUILDING HEIGHT REQUIREMENT BE REDUCED FROM A 4-STORY 45 FOOT TO A 3-STORY 40 FOOT MAXIMUM BUILDING HEIGHT.
- ADJUTANT GENERAL BUILDING BE REDUCED FROM 33 UNITS PER LOT TO 10 UNITS PER LOT.
- THE PARKING LOT SETBACK BE REDUCED FROM 4 TO 2.
- THE MINIMUM GREEN SPACE BETWEEN PARKING AND OUTDOOR LOT LINES BE REDUCED FROM 7 TO 5.
- THE INTERIOR PARKING LOT TREE REQUIREMENTS.
- THE PARKING LOT INTERIOR OPEN SPACE REQUIREMENT.

LEGEND

LIMITS OF DEVELOPMENT

SCALE: 1" = 30'

SCALE: 1" = 30'

SCALE: 1" = 30'

SCALE: 1" = 30'

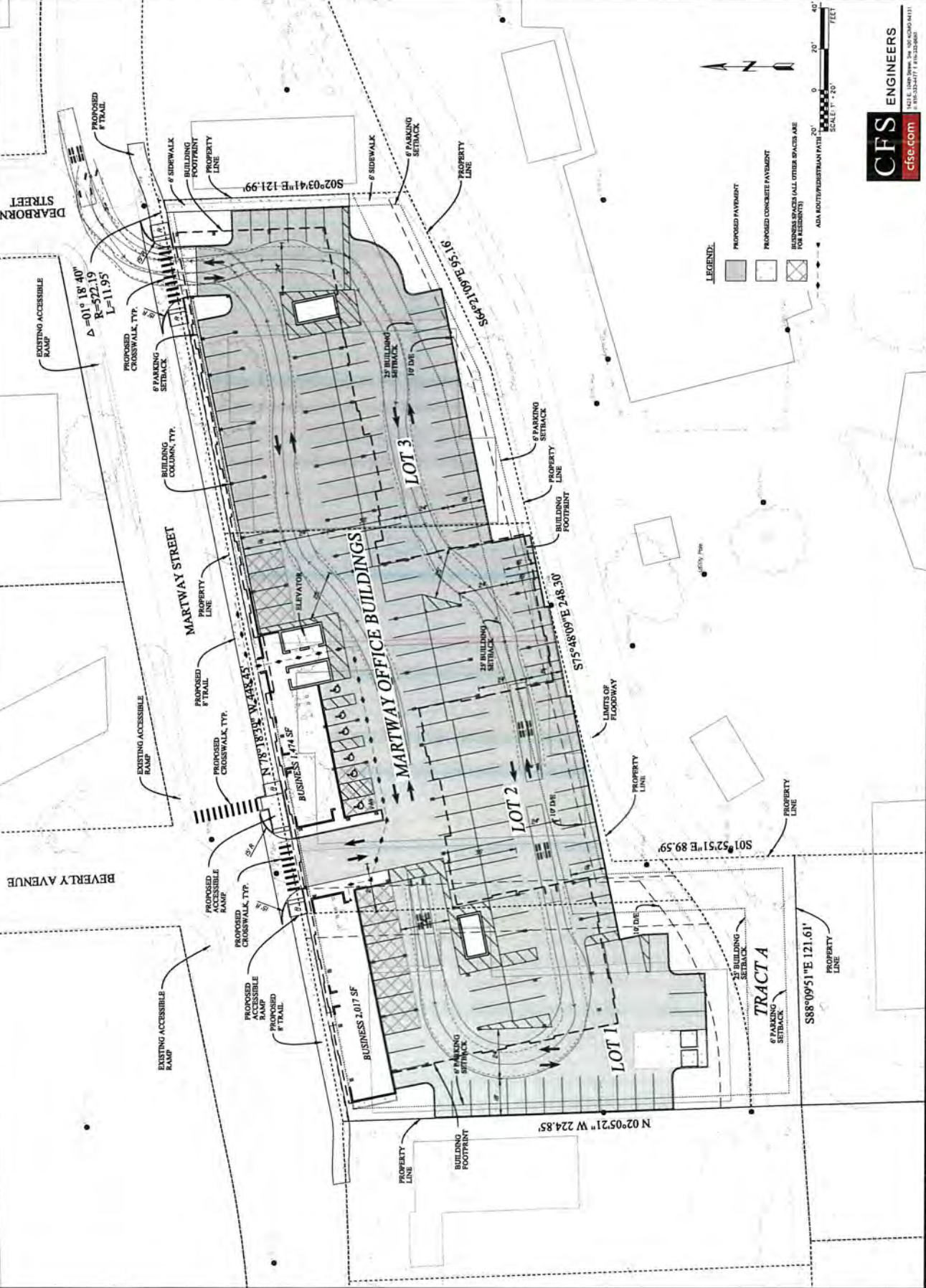
SCALE: 1" = 30'

NO.	DATE	DESCRIPTION

16.05.Murphy

C101

SITE PLAN

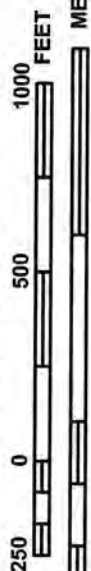


CFS ENGINEERS
cfe.com
1421 E. 104th Street, Suite 200, Shawnee, MO 64151
P: 913-233-2177 F: 913-233-8800

DATE PLOTTED: 04/23/2024 10:27:37 AM



MAP SCALE 1" = 500'



NATIONAL FLOOD INSURANCE PROGRAM

PANEL 0024G

FIRM FLOOD INSURANCE RATE MAP

JOHNSON COUNTY, KANSAS AND INCORPORATED AREAS

PANEL 24 OF 161
(SEE MAP INDEX FOR FIRM PANEL LAYOUT)

CONTAINS:

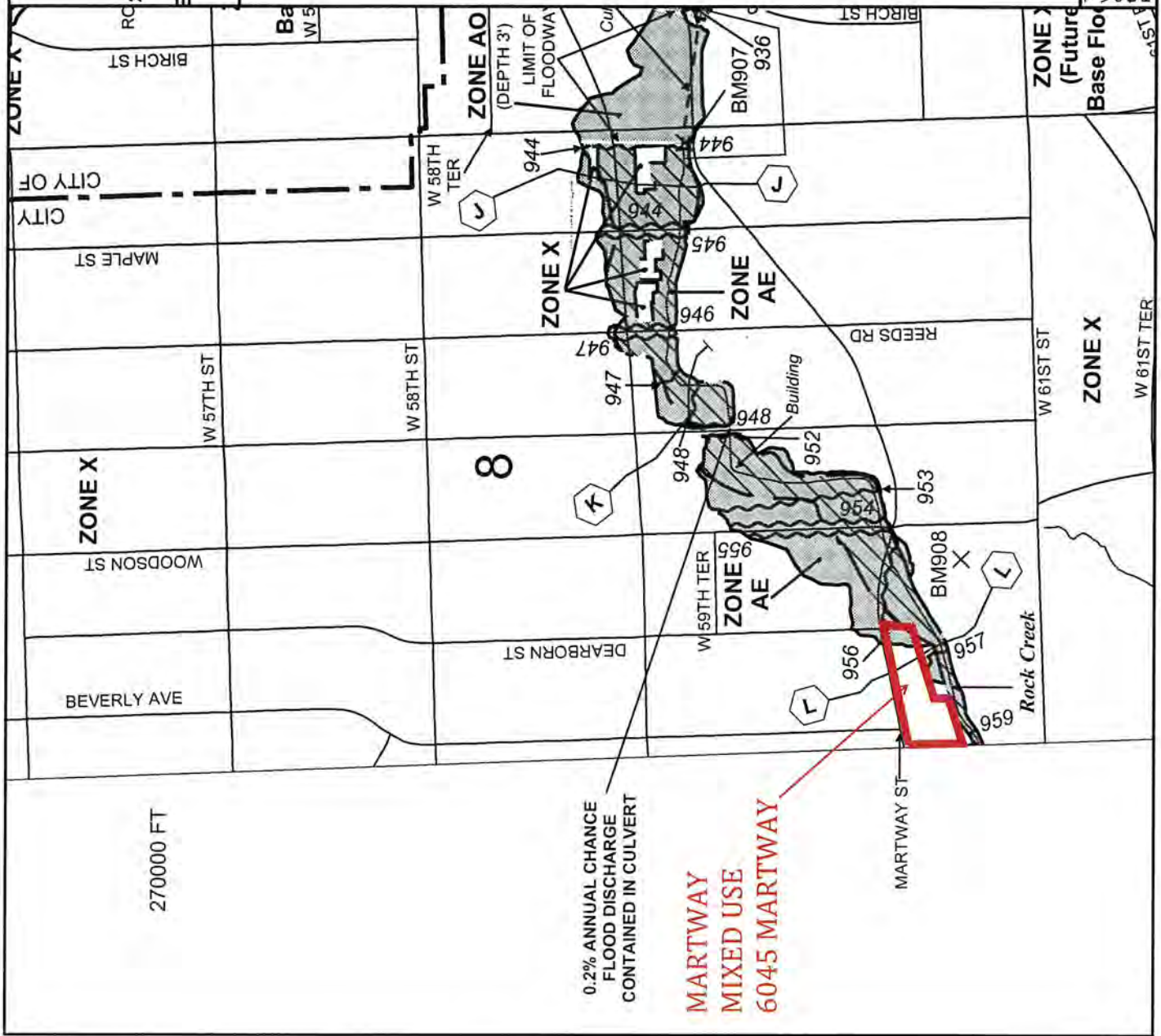
COMMUNITY	NUMBER	PANEL	SUFFIX
FARWAY, CITY OF	200185	0024	G
MISSION, CITY OF	200171	0024	G
MISSION, CITY OF	200171	0024	G
OVERLAND PARK, CITY OF	200174	0024	G
PRAIRIE VILLAGE, CITY OF	200175	0024	G
ROELAND PARK, CITY OF	200176	0024	G

Notice to User: The Map Number shown below should be used when placing map orders; the Community Number shown above should be used on insurance applications for the subject community.



MAP NUMBER
20091C0024G
MAP REVISED
AUGUST 3, 2009
Federal Emergency Management Agency

This is an official copy of a portion of the above referenced flood map. It was extracted using F-MIT On-Line. This map does not reflect changes or amendments which may have been made subsequent to the date on the title block. For the latest product information about National Flood Insurance Program flood maps check the FEMA Flood Map Store at www.msc.fema.gov



Appendix II – Traffic Counts

Martway St, Mission, Kansas - 2017 Traffic Counts

Wed 6-14-2017	Martway St & Beverly Ave Turning Movement Counts												Total Sum
Time	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
7:00 AM	3	17	1	0	20	1	0	0	0	4	1	0	47
7:15 AM	7	20	0	1	24	3	0	0	0	2	0	6	63
7:30 AM	10	34	2	0	30	9	0	0	0	4	0	15	104
7:45 AM	12	46	0	0	38	13	0	0	0	3	0	6	118
8:00 AM	15	37	0	0	23	5	0	0	0	8	0	11	99
8:15 AM	16	37	1	2	31	11	0	0	0	5	0	7	110
8:30 AM	13	40	1	1	32	6	1	0	1	3	0	17	115
8:45 AM	5	30	0	1	24	7	0	0	1	2	1	17	88
PHF	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
0.94	56	160	2	3	124	35	1	0	1	19	0	41	

Max													118
Hourly Sum	32	117	3	1	112	26	0	0	0	13	1	27	332
Hourly Sum	44	137	2	1	115	30	0	0	0	17	0	38	384
Hourly Sum	53	154	3	2	122	38	0	0	0	20	0	39	431
Hourly Sum	56	160	2	3	124	35	1	0	1	19	0	41	442
Hourly Sum	49	144	2	4	110	29	1	0	2	18	1	52	412

Wed 6-14-2017	Martway St & Beverly Ave Turning Movement Counts												Total Sum
Time	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
4:00 PM	2	50	2	0	59	5	3	0	3	4	0	15	143
4:15 PM	9	37	0	0	45	8	0	0	1	5	0	14	119
4:30 PM	8	70	0	0	62	7	1	0	0	7	0	15	170
4:45 PM	9	61	0	0	70	9	0	0	0	10	0	16	175
5:00 PM	8	58	0	0	67	9	0	0	2	10	0	25	179
5:15 PM	11	64	0	0	75	11	0	0	1	15	0	20	197
5:30 PM	4	59	0	0	72	4	1	0	0	6	0	13	159
5:45 PM	4	68	0	0	45	9	0	0	1	5	0	8	140
PHF	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
0.91	36	253	0	0	274	36	1	0	3	42	0	76	

Max													197
Hourly Sum	28	218	2	0	236	29	4	0	4	26	0	60	607
Hourly Sum	34	226	0	0	244	33	1	0	3	32	0	70	643
Hourly Sum	36	253	0	0	274	36	1	0	3	42	0	76	721
Hourly Sum	32	242	0	0	284	33	1	0	3	41	0	74	710
Hourly Sum	27	249	0	0	259	33	1	0	4	36	0	66	675

Thur 6-15-2017	Martway St & Dearborn St Turning Movement Counts												Total Sum
Time	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
7:00 AM	2	15	2	0	23	0	0	0	0	0	0	2	44
7:15 AM	3	22	0	0	32	0	0	0	0	1	0	3	61
7:30 AM	4	19	0	0	34	0	0	0	0	1	0	7	65
7:45 AM	5	25	3	0	47	1	0	0	0	2	0	7	90
8:00 AM	4	38	2	1	32	1	1	0	1	1	1	7	89
8:15 AM	5	34	1	2	26	2	0	0	0	0	0	5	75
8:30 AM	2	34	1	0	32	1	0	0	0	2	0	5	77
8:45 AM	8	30	0	0	31	3	0	0	0	1	0	4	77
PHF	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
0.92	16	131	7	3	137	5	1	0	1	5	1	24	

Max													90
Hourly Sum	14	81	5	0	136	1	0	0	0	4	0	19	260
Hourly Sum	16	104	5	1	145	2	1	0	1	5	1	24	305
Hourly Sum	18	116	6	3	139	4	1	0	1	4	1	26	319
Hourly Sum	16	131	7	3	137	5	1	0	1	5	1	24	331
Hourly Sum	19	136	4	3	121	7	1	0	1	4	1	21	318

Thur 6-15-2017	Martway St & Dearborn St Turning Movement Counts												Total Sum
Time	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
4:00 PM	3	46	0	0	53	2	3	0	1	2	0	4	114
4:15 PM	5	63	1	0	49	2	0	0	1	0	0	3	124
4:30 PM	1	56	0	0	61	3	0	0	0	2	0	9	132
4:45 PM	6	72	0	0	58	2	1	0	1	2	0	7	149
5:00 PM	3	70	0	0	67	4	1	0	0	4	0	9	158
5:15 PM	8	72	0	1	67	6	4	0	1	3	0	3	165
5:30 PM	15	54	0	0	52	4	0	0	1	3	0	7	136
5:45 PM	14	50	0	0	61	1	3	0	0	1	0	5	135
PHF	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
0.92	32	268	0	1	244	16	6	0	3	12	0	26	

Max													165
Hourly Sum	15	237	1	0	221	9	4	0	3	6	0	23	519
Hourly Sum	15	261	1	0	235	11	2	0	2	8	0	28	563
Hourly Sum	18	270	0	1	253	15	6	0	2	11	0	28	604
Hourly Sum	32	268	0	1	244	16	6	0	3	12	0	26	608
Hourly Sum	40	246	0	1	247	15	8	0	2	11	0	24	594

Wed 6-14-2017	Martway St & Beverly Ave Turning Movement Counts											
PHF	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
0.94	56	160	2	3	124	35	1	0	1	19	0	41

Wed 6-14-2017	Martway St & Beverly Ave Turning Movement Counts											
PHF	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
0.91	36	253	0	0	274	36	1	0	3	42	0	76

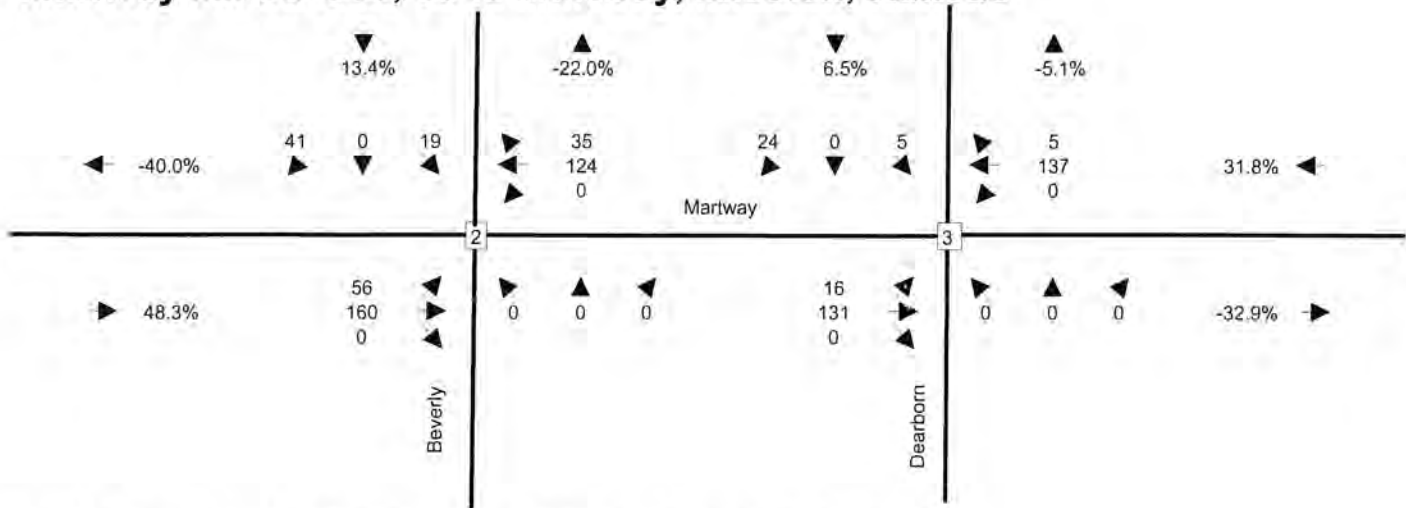
Thur 6-15-2017	Martway St & Dearborn St Turning Movement Counts											
PHF	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
0.92	16	131	7	3	137	5	1	0	1	5	1	24

Thur 6-15-2017	Martway St & Dearborn St Turning Movement Counts											
PHF	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
0.92	32	268	0	1	244	16	6	0	3	12	0	26

Appendix III – Trip Generation & Traffic Distribution

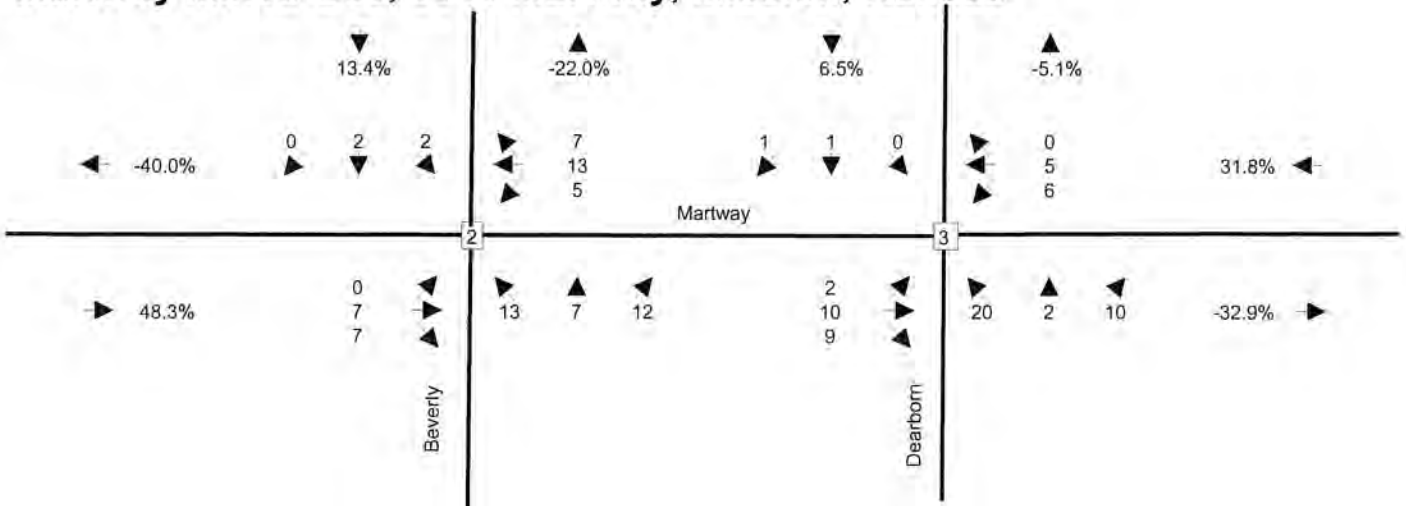
AM Peak Hour Existing Traffic

Martway Mixed-Use, 6045 Martway, Mission, Kansas



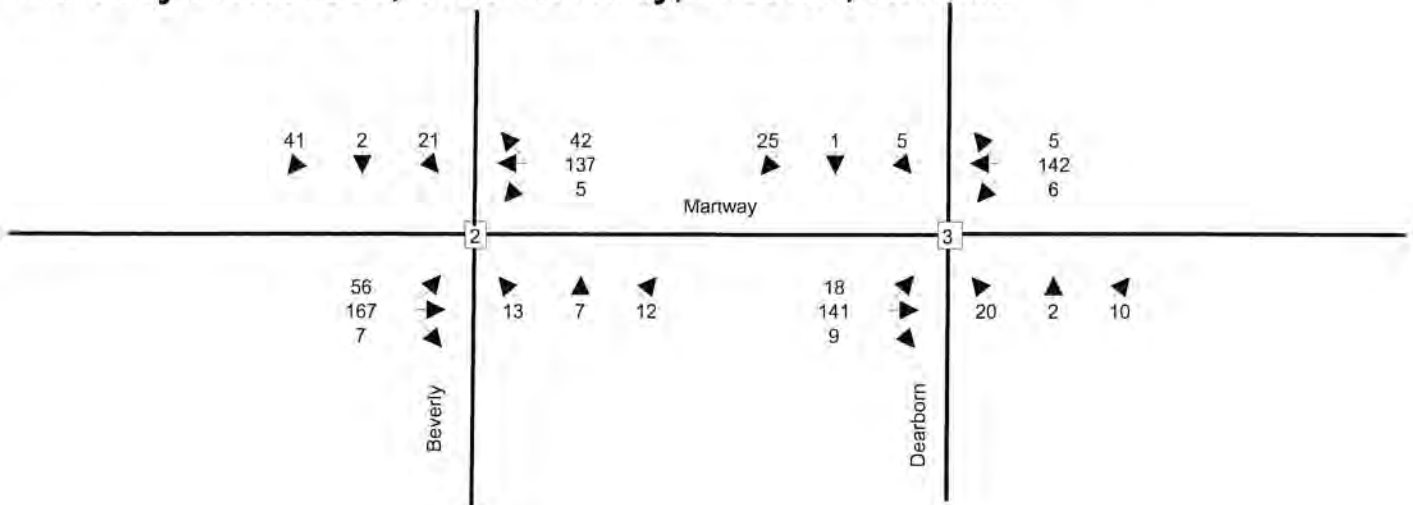
AM Peak Hour Site-Generated Traffic

Martway Mixed-Use, 6045 Martway, Mission, Kansas



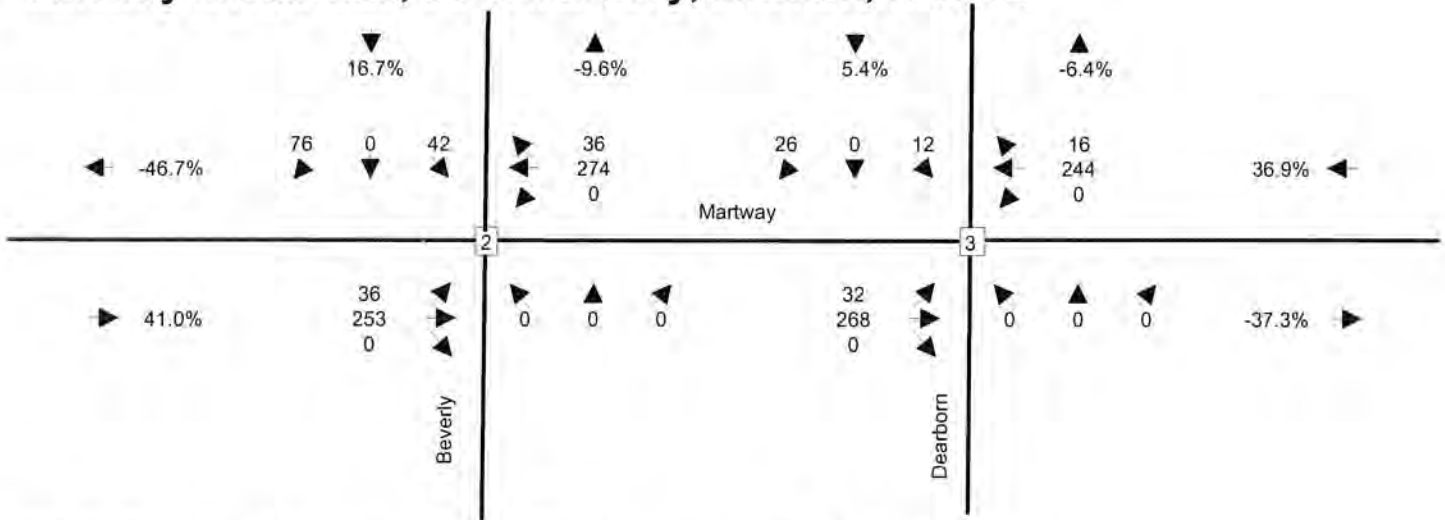
AM Peak Hour Existing Plus Site-Generated Traffic

Martway Mixed-Use, 6045 Martway, Mission, Kansas



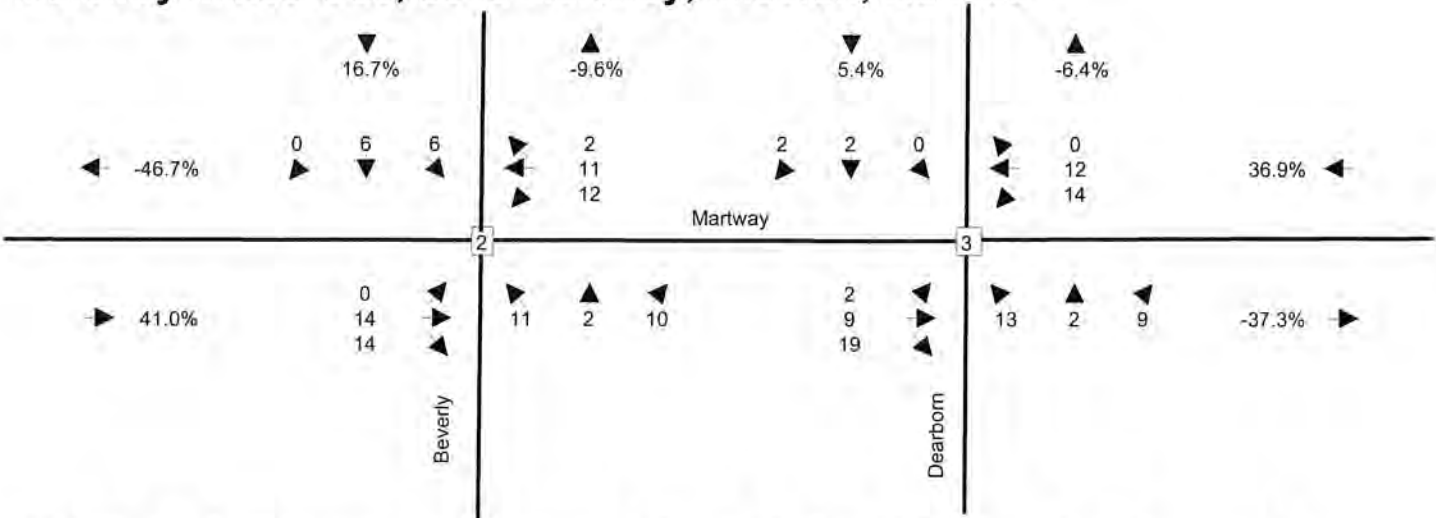
PM Peak Hour Existing Traffic

Martway Mixed-Use, 6045 Martway, Mission, Kansas



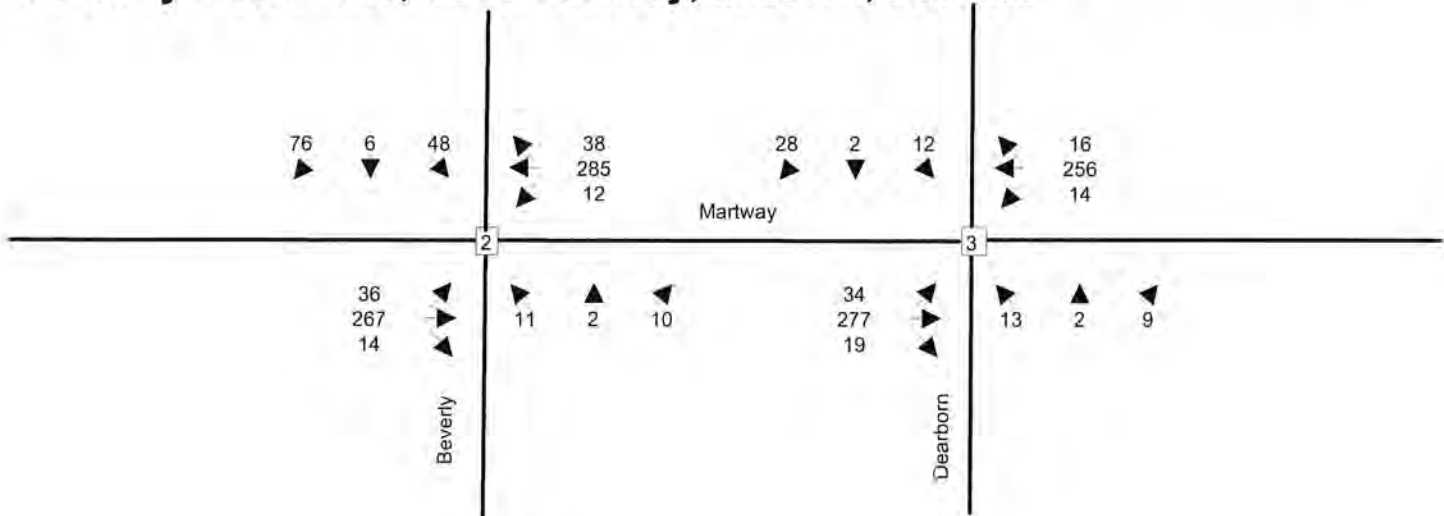
PM Peak Hour Site-Generated Traffic

Martway Mixed-Use, 6045 Martway, Mission, Kansas



PM Peak Hour Existing Plus Site-Generated Traffic

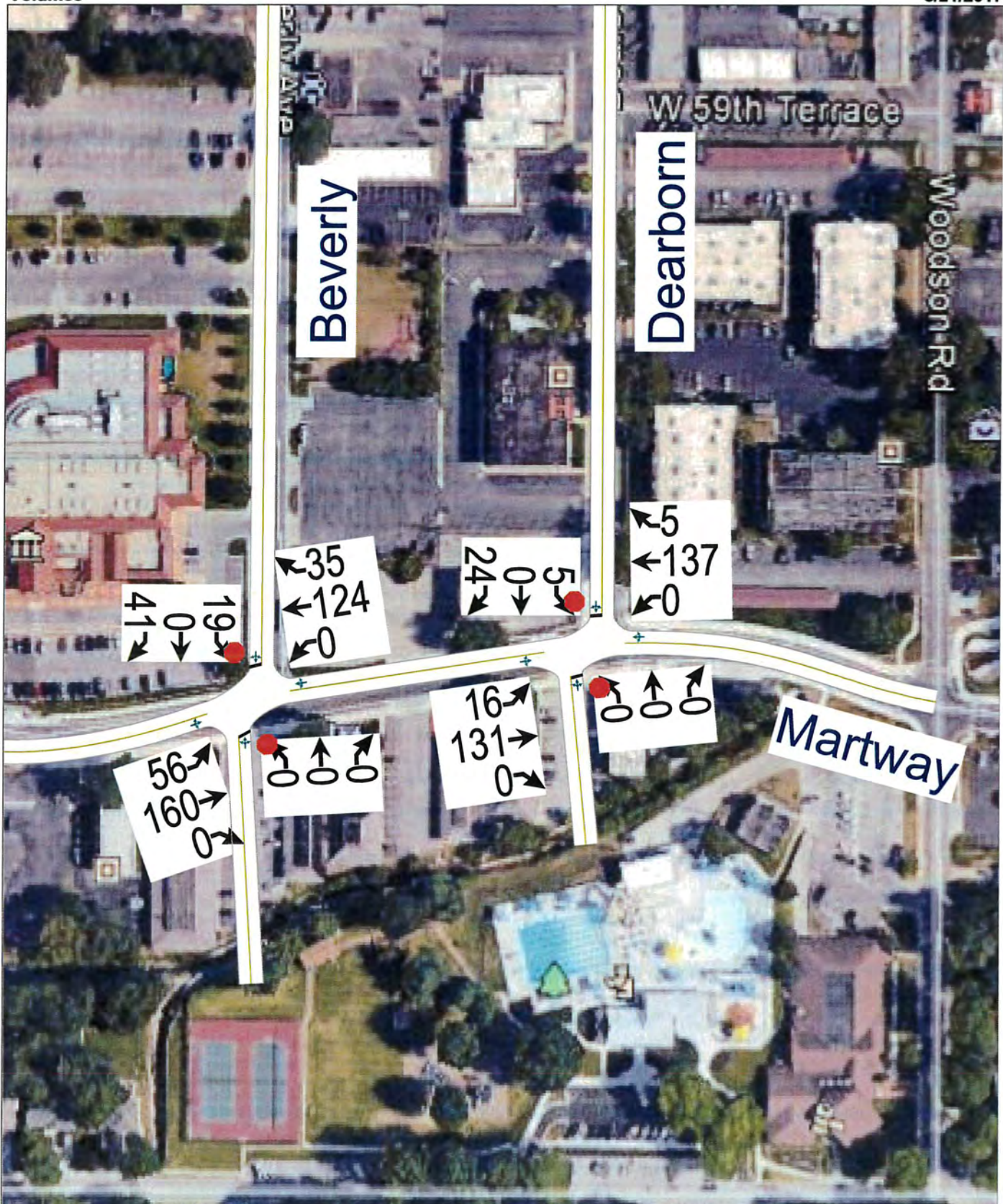
Martway Mixed-Use, 6045 Martway, Mission, Kansas



Appendix-IV - Synchro Results, AM Peak Traffic Conditions / Pre-Development

Map - Martway Mixed Use-AM Pre-Development
Volumes

8/21/2017



Martway Mixed Use-AM Pre-Development

C:\Users\Traffic User\Desktop\175085 - Martway\Synchro\Martway AM Pre.syn

Intersection

Int Delay, s/veh 2.4

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Vol, veh/h	56	160	0	0	124	35	0	0	0	19	0	41
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	61	174	0	0	135	38	0	0	0	21	0	45

Major/Minor	Major1	Major2	Minor1	Minor2								
Conflicting Flow All	173	0	0	174	0	0	472	469	174	450	450	154
Stage 1	-	-	-	-	-	-	296	296	-	154	154	-
Stage 2	-	-	-	-	-	-	176	173	-	296	296	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1404	-	-	1403	-	-	502	492	869	519	504	892
Stage 1	-	-	-	-	-	-	712	668	-	848	770	-
Stage 2	-	-	-	-	-	-	826	756	-	712	668	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1404	-	-	1403	-	-	459	468	869	500	480	892
Mov Cap-2 Maneuver	-	-	-	-	-	-	459	468	-	500	480	-
Stage 1	-	-	-	-	-	-	678	636	-	807	770	-
Stage 2	-	-	-	-	-	-	785	756	-	678	636	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	2	0	0	10.5
HCM LOS			A	B

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	-	1404	-	-	1403	-	-	715
HCM Lane V/C Ratio	-	0.043	-	-	-	-	-	0.091
HCM Control Delay (s)	0	7.7	0	-	0	-	-	10.5
HCM Lane LOS	A	A	A	-	A	-	-	B
HCM 95th %tile Q(veh)	-	0.1	-	-	0	-	-	0.3

HCM 2010 TWSC
3: Martway & Dearborn

8/21/2017

Intersection

Int Delay, s/veh 1.2

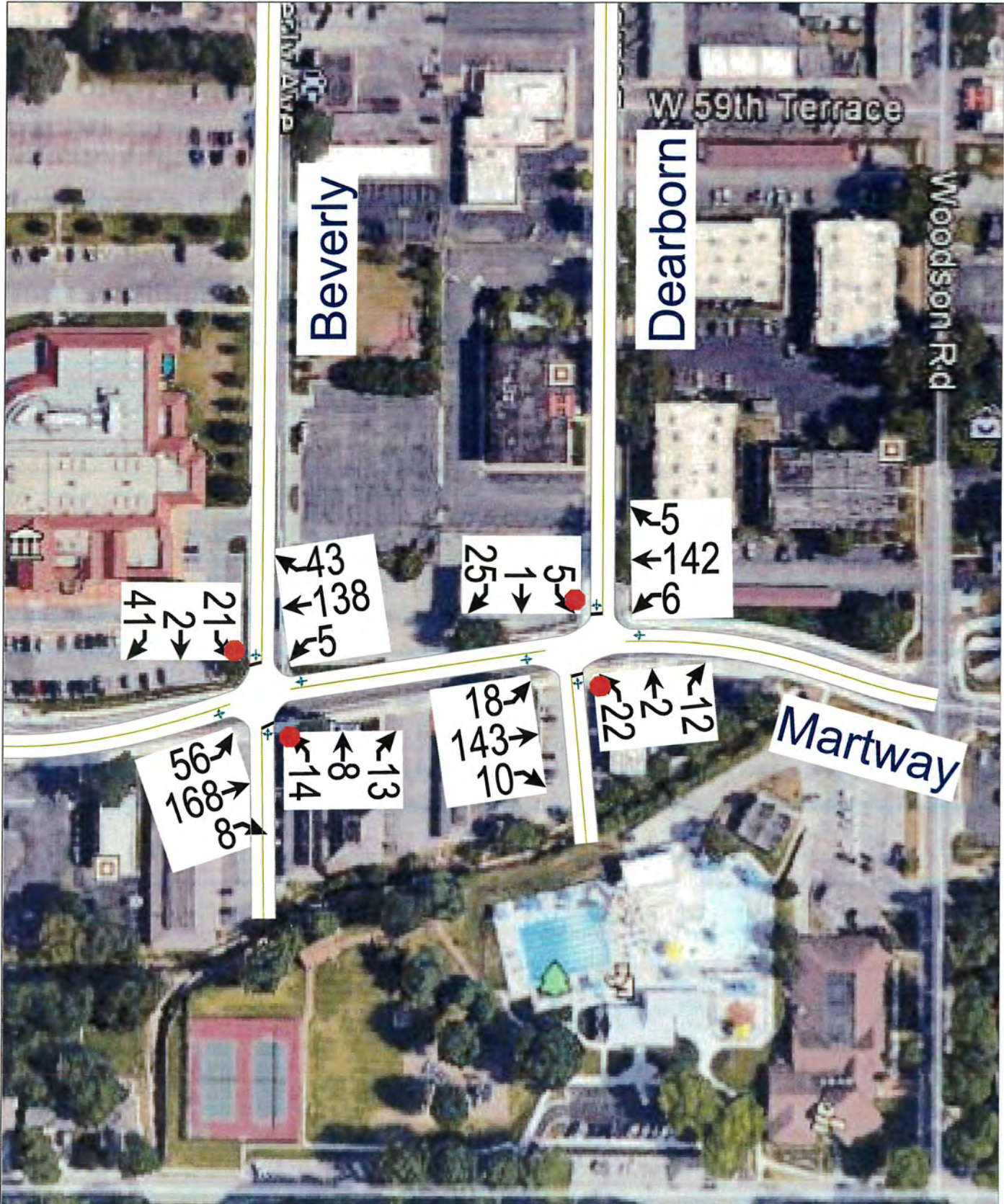
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Vol, veh/h	16	131	0	0	137	5	0	0	0	5	0	24
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	17	142	0	0	149	5	0	0	0	5	0	26

Major/Minor	Major1	Major2	Minor1	Minor2								
Conflicting Flow All	154	0	0	142	0	0	342	331	142	329	329	152
Stage 1	-	-	-	-	-	-	177	177	-	152	152	-
Stage 2	-	-	-	-	-	-	165	154	-	177	177	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1426	-	-	1441	-	-	612	588	906	624	590	894
Stage 1	-	-	-	-	-	-	825	753	-	850	772	-
Stage 2	-	-	-	-	-	-	837	770	-	825	753	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1426	-	-	1441	-	-	588	580	906	618	582	894
Mov Cap-2 Maneuver	-	-	-	-	-	-	588	580	-	618	582	-
Stage 1	-	-	-	-	-	-	814	743	-	839	772	-
Stage 2	-	-	-	-	-	-	813	770	-	814	743	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.8	0	0	9.5
HCM LOS			A	A

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	-	1426	-	-	1441	-	-	830
HCM Lane V/C Ratio	-	0.012	-	-	-	-	-	0.038
HCM Control Delay (s)	0	7.6	0	-	0	-	-	9.5
HCM Lane LOS	A	A	A	-	A	-	-	A
HCM 95th %tile Q(veh)	-	0	-	-	0	-	-	0.1

Appendix V - Synchro Results, AM Peak Traffic Conditions / Post-Development



Intersection

Int Delay, s/veh 3.2

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Vol, veh/h	56	168	8	5	138	43	14	8	13	21	2	41
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	61	183	9	5	150	47	15	9	14	23	2	45

Major/Minor	Major1	Major2	Minor1	Minor2								
Conflicting Flow All	197	0	0	191	0	0	517	517	187	504	497	173
Stage 1	-	-	-	-	-	-	309	309	-	184	184	-
Stage 2	-	-	-	-	-	-	208	208	-	320	313	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1376	-	-	1383	-	-	469	462	855	478	475	871
Stage 1	-	-	-	-	-	-	701	660	-	818	747	-
Stage 2	-	-	-	-	-	-	794	730	-	692	657	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1376	-	-	1383	-	-	425	437	855	444	449	871
Mov Cap-2 Maneuver	-	-	-	-	-	-	425	437	-	444	449	-
Stage 1	-	-	-	-	-	-	666	627	-	777	744	-
Stage 2	-	-	-	-	-	-	748	727	-	638	624	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	1.9	0.2	12.4	11.2
HCM LOS			B	B

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	527	1376	-	-	1383	-	-	648
HCM Lane V/C Ratio	0.072	0.044	-	-	0.004	-	-	0.107
HCM Control Delay (s)	12.4	7.7	0	-	7.6	0	-	11.2
HCM Lane LOS	B	A	A	-	A	A	-	B
HCM 95th %tile Q(veh)	0.2	0.1	-	-	0	-	-	0.4

Intersection												
Int Delay, s/veh	2.3											

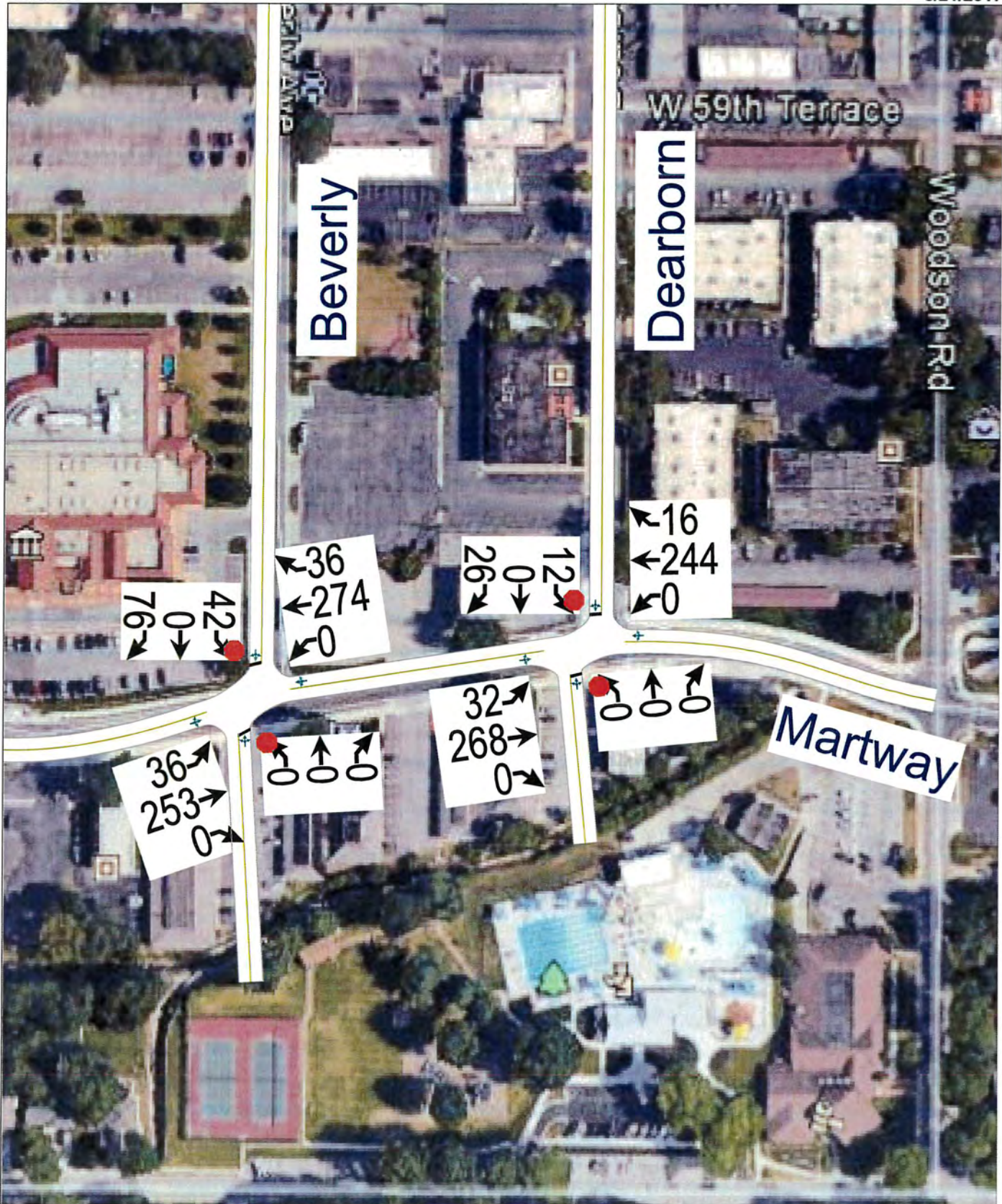
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Vol, veh/h	18	143	10	6	142	5	22	2	12	5	1	25
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	20	155	11	7	154	5	24	2	13	5	1	27

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	160	0	0	166	0	0	384	373	161	378	375	157
Stage 1	-	-	-	-	-	-	200	200	-	170	170	-
Stage 2	-	-	-	-	-	-	184	173	-	208	205	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1419	-	-	1412	-	-	574	557	884	580	556	889
Stage 1	-	-	-	-	-	-	802	736	-	832	758	-
Stage 2	-	-	-	-	-	-	818	756	-	794	732	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1419	-	-	1412	-	-	547	545	884	561	544	889
Mov Cap-2 Maneuver	-	-	-	-	-	-	547	545	-	561	544	-
Stage 1	-	-	-	-	-	-	789	724	-	819	754	-
Stage 2	-	-	-	-	-	-	788	752	-	767	720	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.8	0.3	11.1	9.7
HCM LOS			B	A

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	626	1419	-	-	1412	-	-	797
HCM Lane V/C Ratio	0.063	0.014	-	-	0.005	-	-	0.042
HCM Control Delay (s)	11.1	7.6	0	-	7.6	0	-	9.7
HCM Lane LOS	B	A	A	-	A	A	-	A
HCM 95th %tile Q(veh)	0.2	0	-	-	0	-	-	0.1

Appendix VI - Synchro Results, PM Peak Traffic Conditions / Pre-Development



HCM 2010 TWSC
2: Martway & Beverly

8/21/2017

Intersection

Int Delay, s/veh 2.7

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Vol, veh/h	36	253	0	0	274	36	0	0	0	42	0	76
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	39	275	0	0	298	39	0	0	0	46	0	83

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	337	0	0	275	0	0	712	690	275	670	670	317
Stage 1	-	-	-	-	-	-	353	353	-	317	317	-
Stage 2	-	-	-	-	-	-	359	337	-	353	353	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1222	-	-	1288	-	-	347	368	764	371	378	724
Stage 1	-	-	-	-	-	-	664	631	-	694	654	-
Stage 2	-	-	-	-	-	-	659	641	-	664	631	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1222	-	-	1288	-	-	298	354	764	360	364	724
Mov Cap-2 Maneuver	-	-	-	-	-	-	298	354	-	360	364	-
Stage 1	-	-	-	-	-	-	639	607	-	668	654	-
Stage 2	-	-	-	-	-	-	584	641	-	639	607	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	1	0	0	13.9
HCM LOS			A	B

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	-	1222	-	-	1288	-	-	532
HCM Lane V/C Ratio	-	0.032	-	-	-	-	-	0.241
HCM Control Delay (s)	0	8	0	-	0	-	-	13.9
HCM Lane LOS	A	A	A	-	A	-	-	B
HCM 95th %tile Q(veh)	-	0.1	-	-	0	-	-	0.9

HCM 2010 TWSC
3: Martway & Dearborn

8/21/2017

Intersection

Int Delay, s/veh 1.1

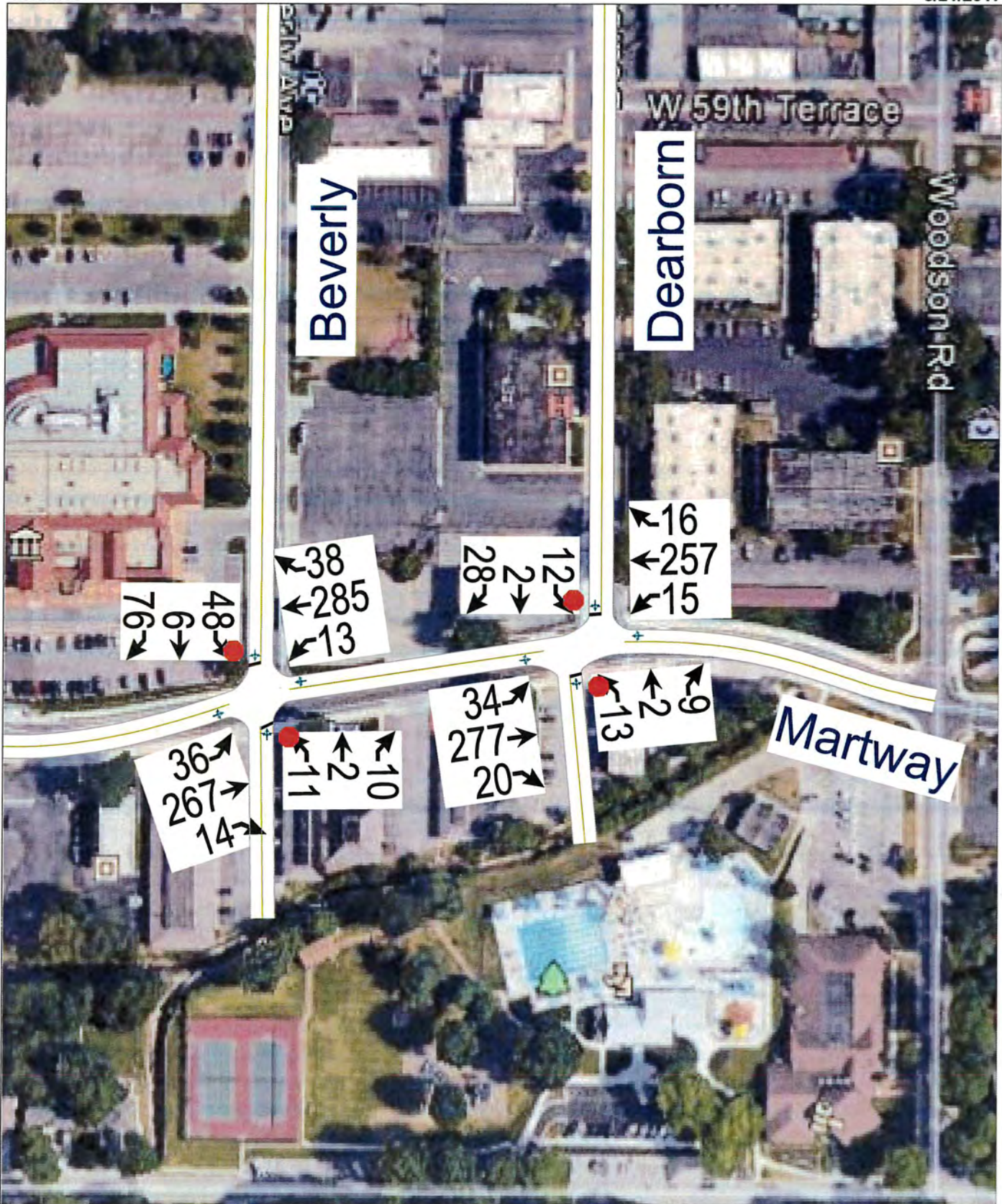
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Vol, veh/h	32	268	0	0	244	16	0	0	0	12	0	26
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	35	291	0	0	265	17	0	0	0	13	0	28

Major/Minor	Major1	Major2	Minor1	Minor2								
Conflicting Flow All	283	0	0	291	0	0	649	644	291	635	635	274
Stage 1	-	-	-	-	-	-	361	361	-	274	274	-
Stage 2	-	-	-	-	-	-	288	283	-	361	361	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1279	-	-	1271	-	-	383	391	748	391	396	765
Stage 1	-	-	-	-	-	-	657	626	-	732	683	-
Stage 2	-	-	-	-	-	-	720	677	-	657	626	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1279	-	-	1271	-	-	360	378	748	381	383	765
Mov Cap-2 Maneuver	-	-	-	-	-	-	360	378	-	381	383	-
Stage 1	-	-	-	-	-	-	635	605	-	708	683	-
Stage 2	-	-	-	-	-	-	693	677	-	635	605	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.8	0	0	11.7
HCM LOS			A	B

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	-	1279	-	-	1271	-	-	580
HCM Lane V/C Ratio	-	0.027	-	-	-	-	-	0.071
HCM Control Delay (s)	0	7.9	0	-	0	-	-	11.7
HCM Lane LOS	A	A	A	-	A	-	-	B
HCM 95th %tile Q(veh)	-	0.1	-	-	0	-	-	0.2

Appendix VII - Synchro Results, PM Peak Traffic Conditions / Post-Development



Martway Mixed Use-PM Post-Development

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Intersection												
Int Delay, s/veh	3.5											

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Vol, veh/h	36	267	14	13	285	38	11	2	10	48	6	76
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	39	290	15	14	310	41	12	2	11	52	7	83

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	351	0	0	305	0	0	779	755	298	742	743	330
Stage 1	-	-	-	-	-	-	376	376	-	359	359	-
Stage 2	-	-	-	-	-	-	403	379	-	383	384	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1208	-	-	1256	-	-	313	338	741	332	343	712
Stage 1	-	-	-	-	-	-	645	616	-	659	627	-
Stage 2	-	-	-	-	-	-	624	615	-	640	611	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1208	-	-	1256	-	-	262	320	741	312	325	712
Mov Cap-2 Maneuver	-	-	-	-	-	-	262	320	-	312	325	-
Stage 1	-	-	-	-	-	-	620	592	-	633	618	-
Stage 2	-	-	-	-	-	-	538	606	-	604	587	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.9	0.3	15.3	16.1
HCM LOS			C	C

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	373	1208	-	-	1256	-	-	466
HCM Lane V/C Ratio	0.067	0.032	-	-	0.011	-	-	0.303
HCM Control Delay (s)	15.3	8.1	0	-	7.9	0	-	16.1
HCM Lane LOS	C	A	A	-	A	A	-	C
HCM 95th %tile Q(veh)	0.2	0.1	-	-	0	-	-	1.3

HCM 2010 TWSC
3: Martway & Dearborn

8/21/2017

Intersection

Int Delay, s/veh 1.8

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Vol, veh/h	34	277	20	15	257	16	13	2	9	12	2	28
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	37	301	22	16	279	17	14	2	10	13	2	30

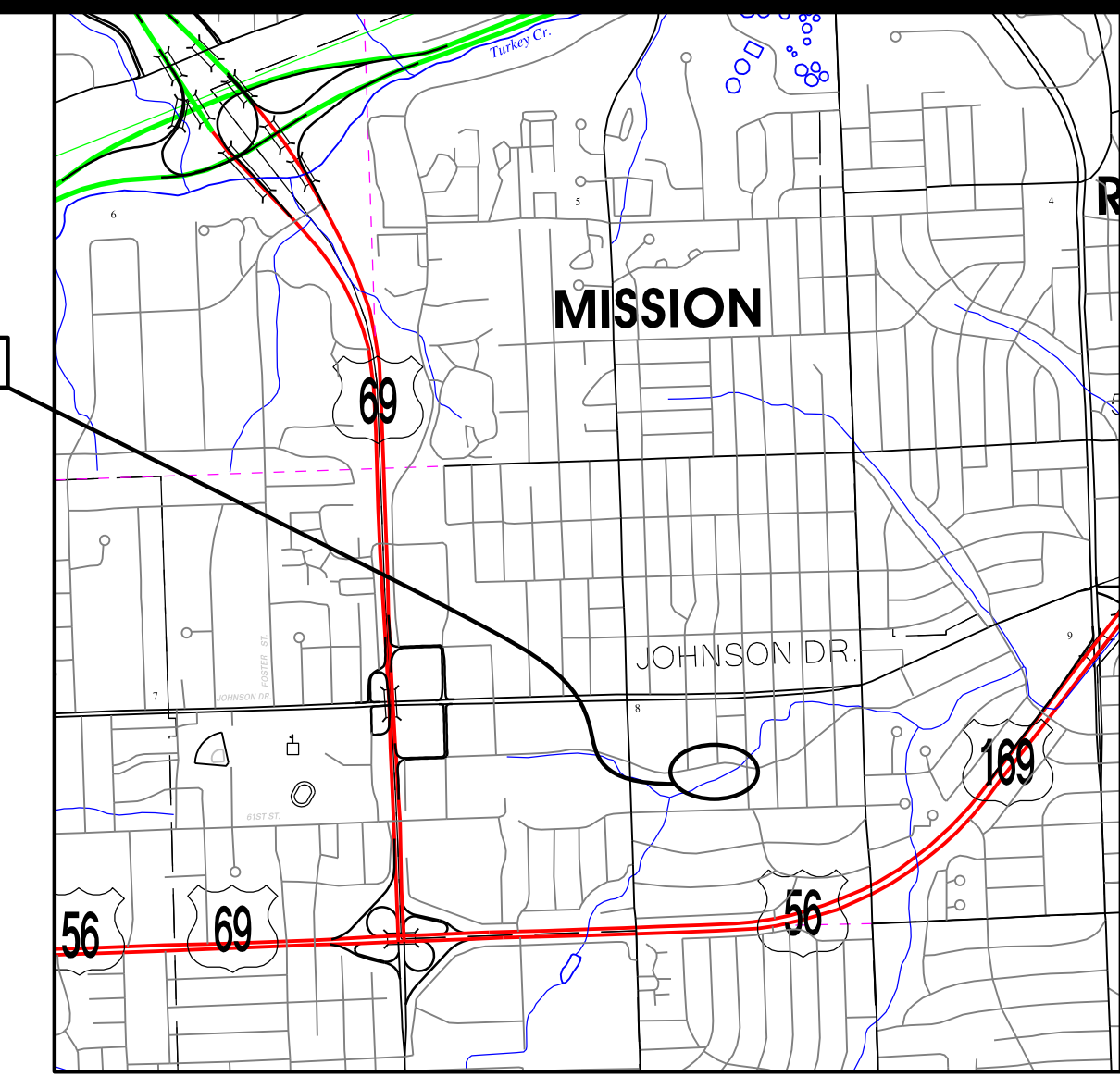
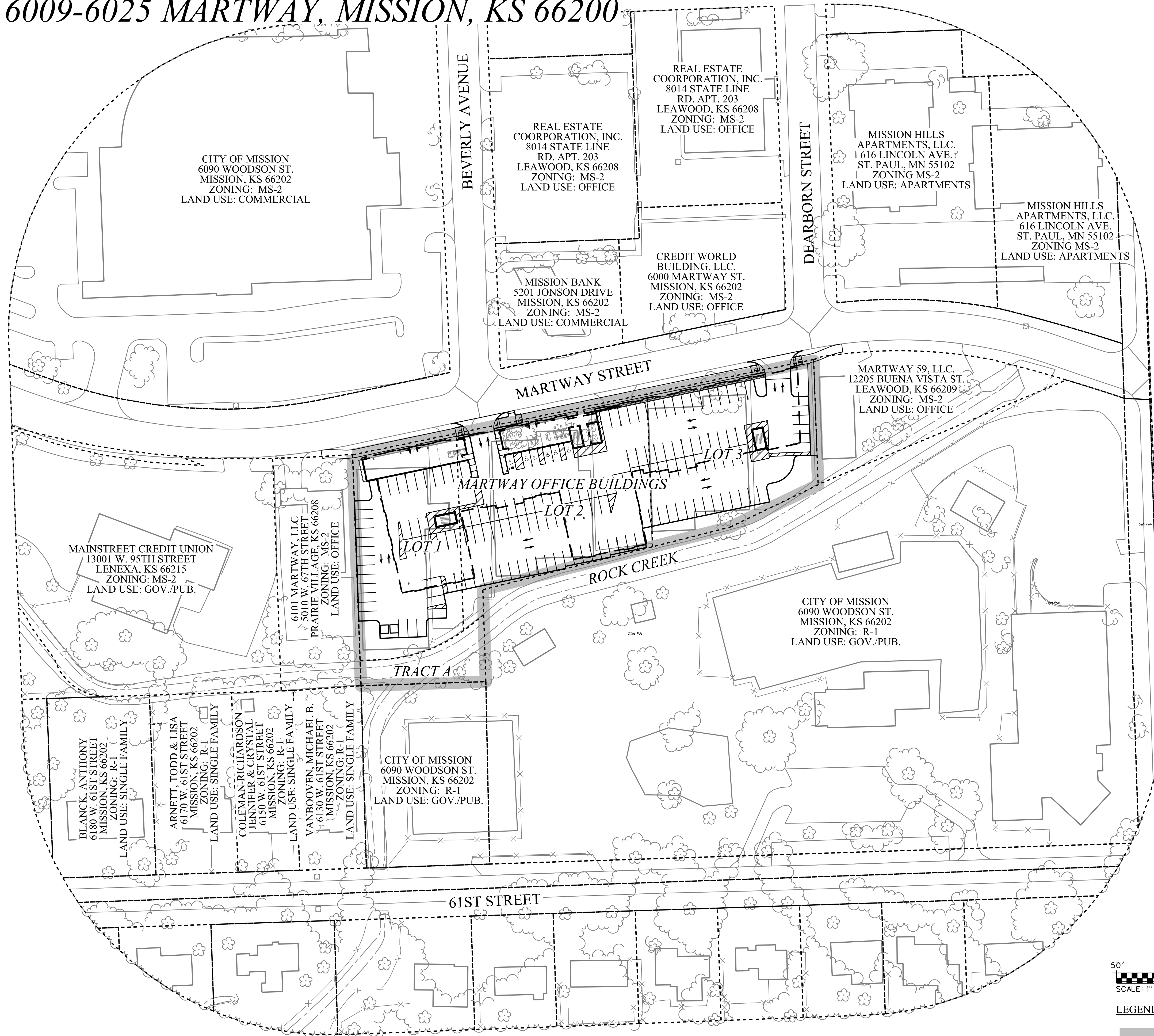
Major/Minor	Major1	Major2	Minor1	Minor2								
Conflicting Flow All	297	0	0	323	0	0	723	715	312	713	718	288
Stage 1	-	-	-	-	-	-	386	386	-	321	321	-
Stage 2	-	-	-	-	-	-	337	329	-	392	397	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1264	-	-	1237	-	-	342	356	728	347	355	751
Stage 1	-	-	-	-	-	-	637	610	-	691	652	-
Stage 2	-	-	-	-	-	-	677	646	-	633	603	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1264	-	-	1237	-	-	314	338	728	327	337	751
Mov Cap-2 Maneuver	-	-	-	-	-	-	314	338	-	327	337	-
Stage 1	-	-	-	-	-	-	614	588	-	666	642	-
Stage 2	-	-	-	-	-	-	637	636	-	600	581	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.8	0.4	14.6	12.5
HCM LOS			B	B

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	402	1264	-	-	1237	-	-	526
HCM Lane V/C Ratio	0.065	0.029	-	-	0.013	-	-	0.087
HCM Control Delay (s)	14.6	7.9	0	-	7.9	0	-	12.5
HCM Lane LOS	B	A	A	-	A	A	-	B
HCM 95th %tile Q(veh)	0.2	0.1	-	-	0	-	-	0.3

MARTWAY MIXED USE

PRELIMINARY DEVELOPMENT PLAN 6009-6025 MARTWAY, MISSION, KS 66200



VICINITY MAP:

LEGAL DESCRIPTION

LOTS 1, 2 AND 3 OF MARTWAY OFFICE BUILDINGS A SUBDIVISION OF LAND IN THE NORTHWEST QUARTER OF SECTION 8, TOWNSHIP 25 SOUTH, RANGE 25 EAST OF THE SIXTH PRINCIPLE MERIDIAN IN THE CITY OF MISSION, JOHNSON COUNTY, KANSAS.

SITE DEVELOPMENT SUMMARY TABLE

EXISTING ZONING: MS-2
 LAND AREA: 1.767 ACRES OR 76,971 SQUARE FEET MORE OR LESS
 LAND USE:
 EXISTING: OFFICE
 PROPOSED: A MIXED USE DEVELOPMENT WITH A 5-STORY APARTMENT COMPLEX WITH COMMERCIAL SPACE AND PARKING DECK ON THE FIRST FLOOR
 BUILDING HEIGHT:
 MAXIMUM BUILDING HEIGHT: 3-STORY/45'-0"
 PROPOSED BUILDING HEIGHT: 5-STORY/67'-0"
 BUILDING FLOOR AREA:
 FIRST FLOOR: 3,491 S.F.
 SECOND FLOOR: 38,977 S.F.
 THIRD FLOOR: 38,977 S.F.
 FOURTH FLOOR: 38,977 S.F.
 FIFTH FLOOR: 38,977 S.F.
 TOTAL FLOOR AREA 159,399 S.F.

FLOOR AREA RATIO

2.07

PARKING SUMMARY

PROPOSED PARKING RATIO: 0.79

BUILDING USE	SPECIFIC USE REQUIREMENT	SPACE REQUIREMENT
COMMERCIAL SPACE	4 SPACES PER 1000 S.F. 3,491 S.F.	14 SPACES
EFFICIENCY APARTMENTS	1 SPACE PER UNIT 24 UNITS	24 SPACES
ONE BEDROOM APARTMENTS	1 SPACE PER UNIT 92 UNITS	92 SPACES
TWO BEDROOM APARTMENTS	2 SPACES PER UNIT 40 UNITS	80 SPACES
TOTAL SPACES REQUIRED		210 SPACES
TOTAL SPACES PROVIDED		**166 SPACES

** 6 ACCESSIBLE SPACES WITH 1 VAN SPACE HAS BEEN PROVIDED

OWNER:

MARTWAY OFFICE WORKS, LLC
 423 DELAWARE STREET, STE. 102
 KANSAS CITY, MO 64105

ARCHITECT:

CLOCKWORK
 423 DELAWARE STREET STE. 102
 KANSAS CITY, MO 64105

DEVELOPER:

MARTWAY OFFICE WORKS, LLC
 423 DELAWARE STREET, STE. 102
 KANSAS CITY, MO 64105

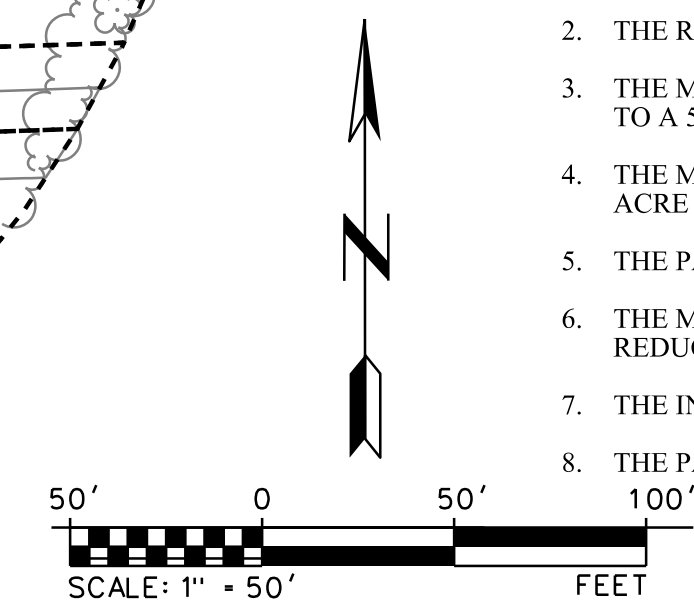
ENGINEER/LANDSCAPE ARCHITECT/SURVEYOR/PLANNER:

CFS ENGINEERS, P.A.
 1421 E. 104TH STREET, SUITE 100
 KANSAS CITY, MO 64131

REQUESTED DEVIATIONS

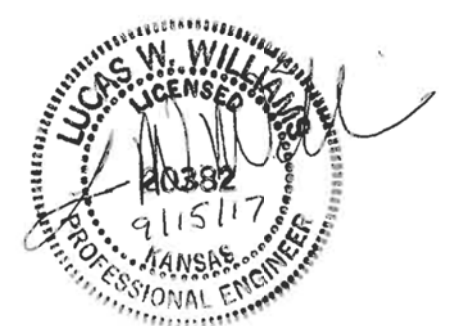
THE DEVELOPER REQUESTS THE FOLLOWING DEVIATIONS TO THE CITY OF MISSION'S MUNICIPAL CODE:

1. THE ONSITE PARKING SPACE REQUIREMENT BE REDUCED FROM 210 SPACES TO 166 SPACES AS LONG AS THE ADDITIONAL 44 SPACES ARE OBTAINED IN AN OFF-SITE PARKING AGREEMENT.
2. THE REAR YARD SETBACK REQUIREMENT BE REDUCED FROM 25' TO A 0' SETBACK.
3. THE MAXIMUM BUILDING HEIGHT REQUIREMENT BE RAISED FROM A 3-STORY 45 FOOT TO A 5-STORY 67 FOOT MAXIMUM BUILDING HEIGHT.
4. THE MINIMUM LOT AREA PER MULTI-FAMILY DWELLING BE RAISED FROM 35 UNITS PER ACRE TO 88.64 UNITS PER ACRE.
5. THE PARKING LOT SETBACK BE REDUCED FROM 6' TO 0'.
6. THE MINIMUM GREEN SPACE BUFFER BETWEEN PARKING AND INTERIOR LOT LINES BE REDUCED FROM 4' TO 0'.
7. THE INTERIOR PARKING LOT TREE REQUIREMENTS.
8. THE PARKING LOT INTERIOR OPEN SPACE REQUIREMENT.



LEGEND:
 [Symbol] LIMITS OF DEVELOPMENT

clockwork
 423 Delaware St Ste 102
 Kansas City MO 64105
 www.clockwork-ad.com



LUCAS W. WILLIAMS - ENGINEER
 KS PE # 20382

Martway Mixed Use

6009 - 6045 Martway
 Mission, KS 64202

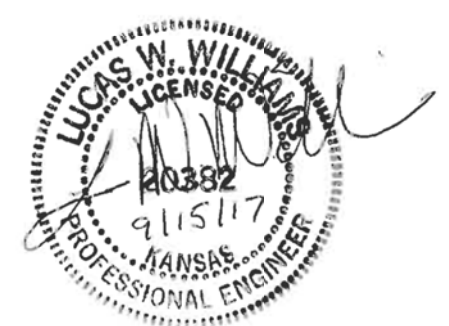
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	PRELIM. DEV. PLAN SUBMITTAL	07.12.2017
	FINAL DEV. PLAN SUBMITTAL	09.15.2017

16.05.Martway

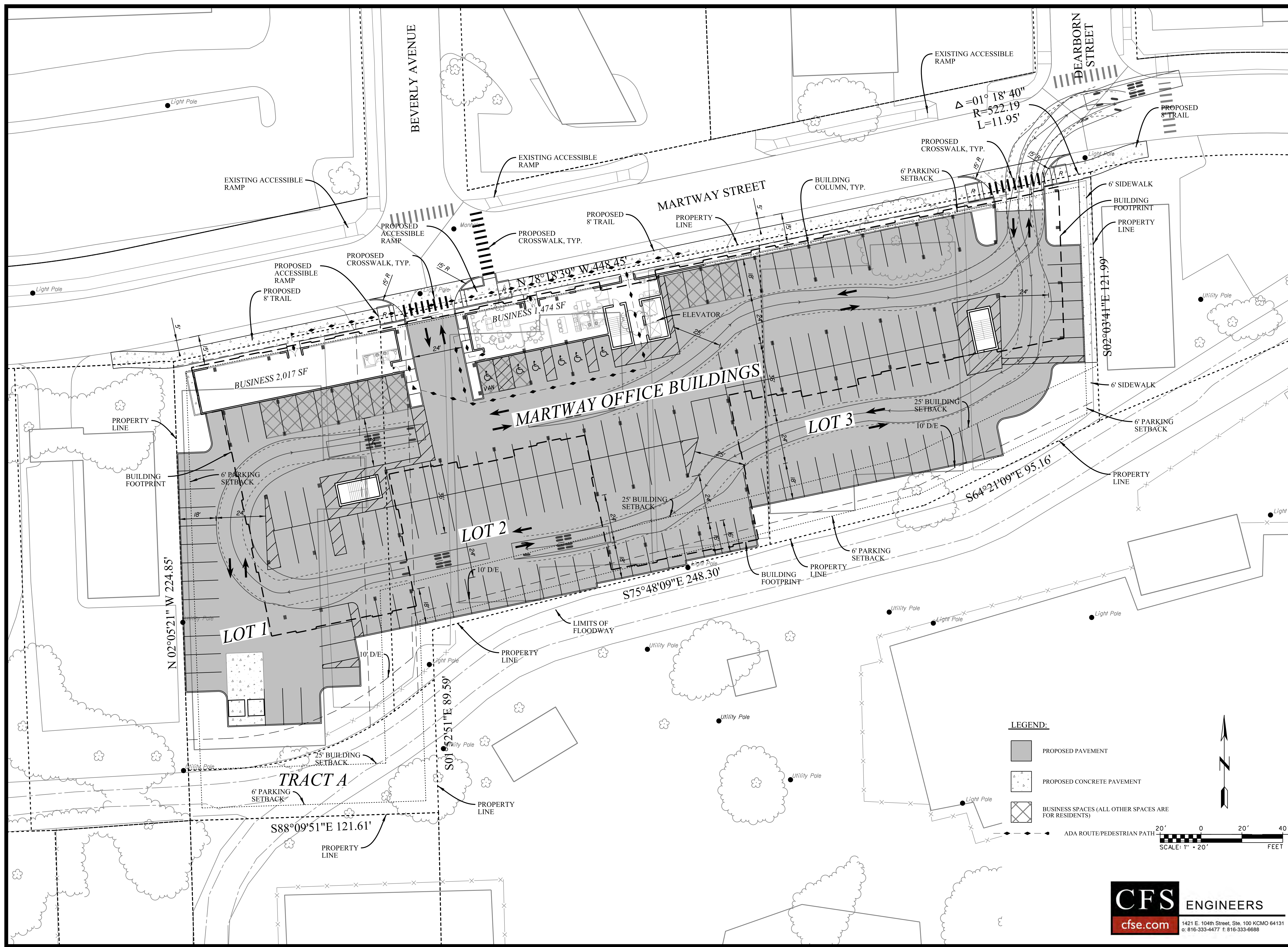
C100

OVERALL SITE PLAN

CFS ENGINEERS
 cfse.com
 1421 E. 104th Street, Ste. 100 KCMO 64131
 o: 816-333-4477 f: 816-333-6688



LUCAS W. WILLIAMS - ENGINEER
KS PE # 20382



Martway Mixed Use

6009 - 6045 Martway
Mission, KS 64202

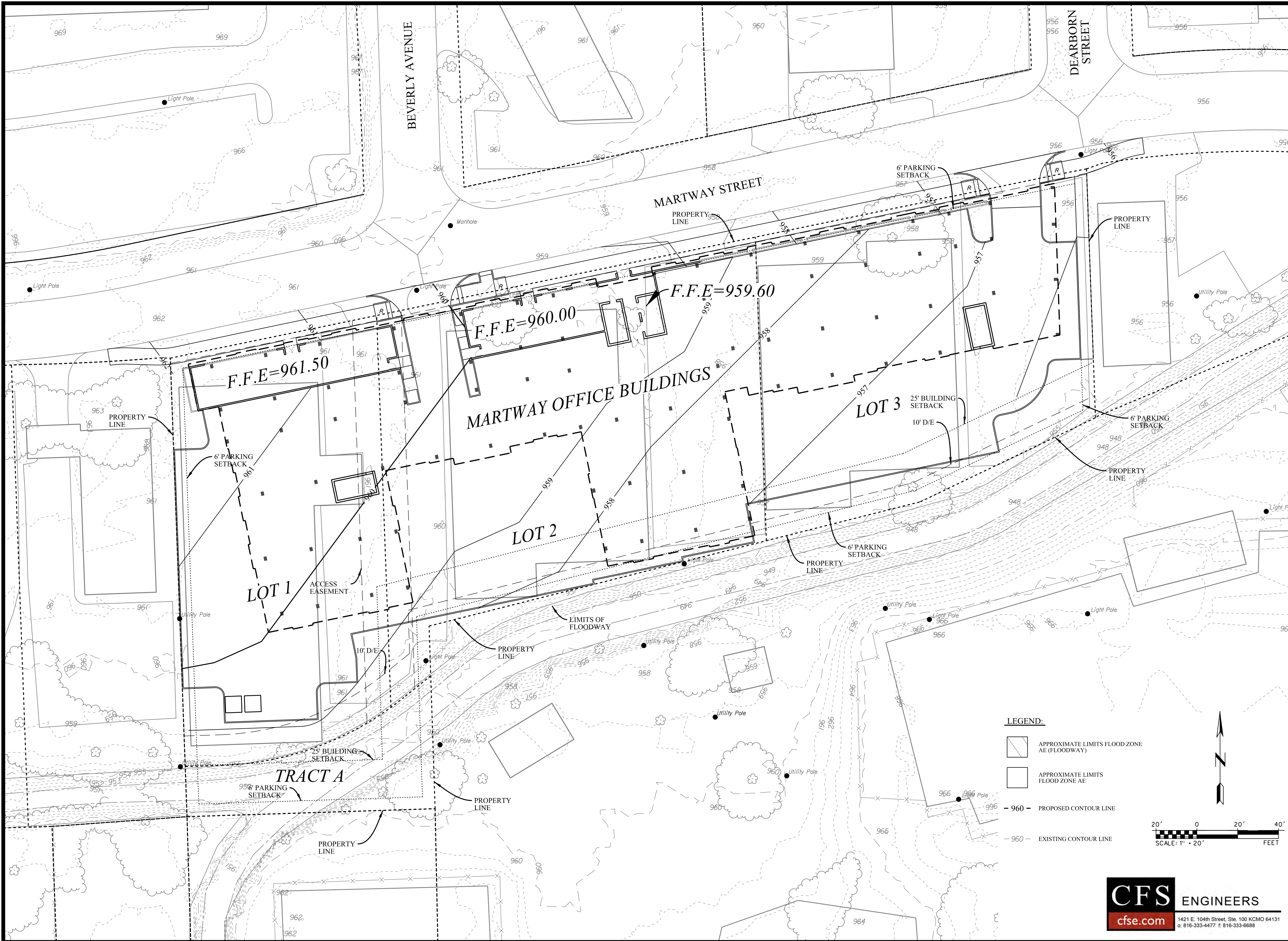
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	FINAL DEV. PLAN SUBMITTAL	09.15.2017

I6.05.Martway

C101

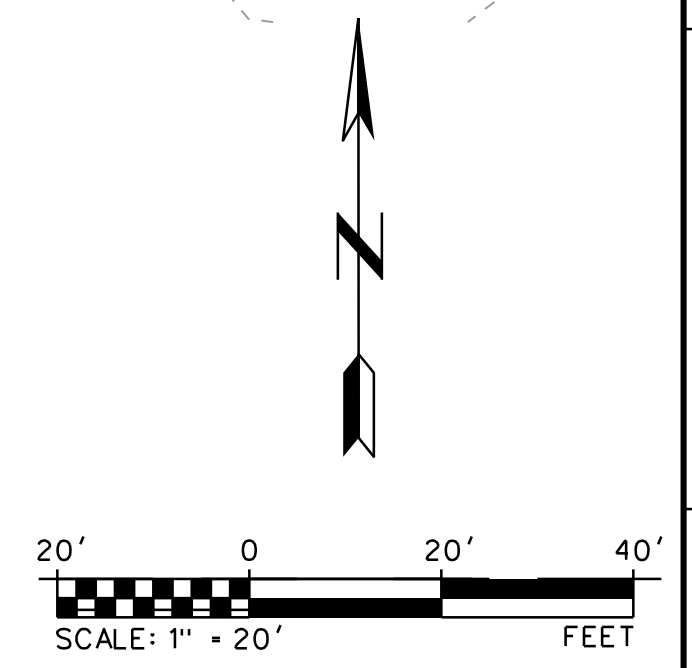
SITE PLAN

CFS ENGINEERS
cfe.com
1421 E. 104th Street, Ste. 100 KCMO 64131
o: 816-333-4477 f: 816-333-6688



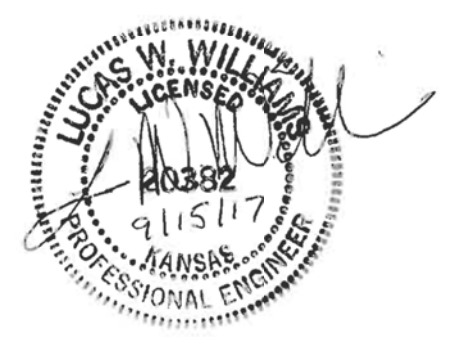
LEGEND:

- APPROXIMATE LIMITS FLOOD ZONE AE (FLOODWAY)
- APPROXIMATE LIMITS FLOOD ZONE AE
- 960 - PROPOSED CONTOUR LINE
- 960 - EXISTING CONTOUR LINE



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 o: 816-333-4477 f: 816-333-6688

clockwork
 423 Delaware St Ste 102
 Kansas City MO 64105
 www.clockwork-ad.com



LUCAS W. WILLIAMS - ENGINEER
 KS PE # 20382

Martway Mixed Use

6009 - 6045 Martway
 Mission, KS 64202

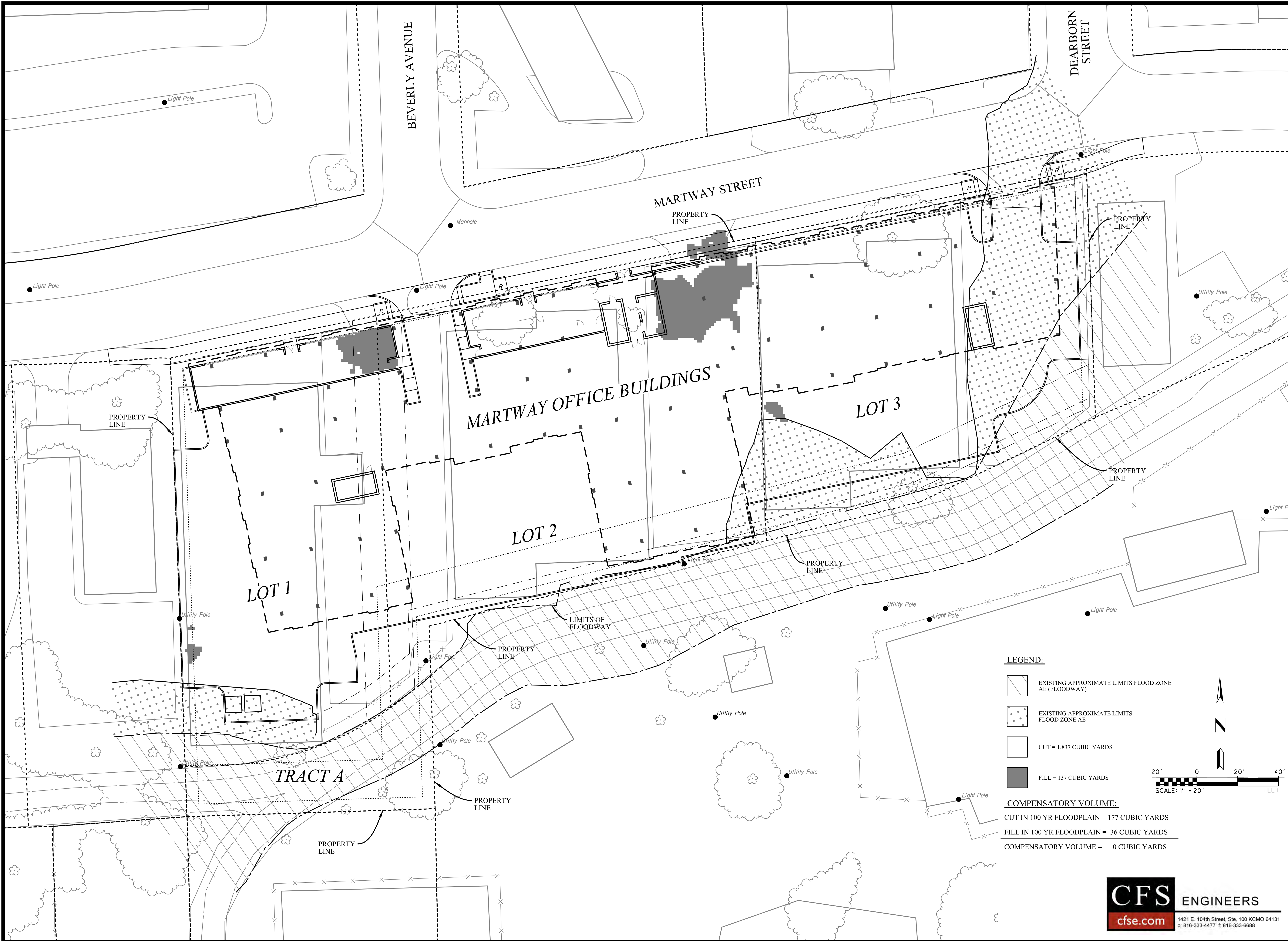
rev	issue	date
	PRELIM. DEV. PLAN SUBMITTAL	07.12.2017
	FINAL DEV. PLAN SUBMITTAL	09.15.2017

16.05.Martway

C200

GRADING PLAN

G:\My Drive\IT5085\CADD\IT5085-ST-5H-Grading-Plan.dgn



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 KS PE # 20382

Martway Mixed Use

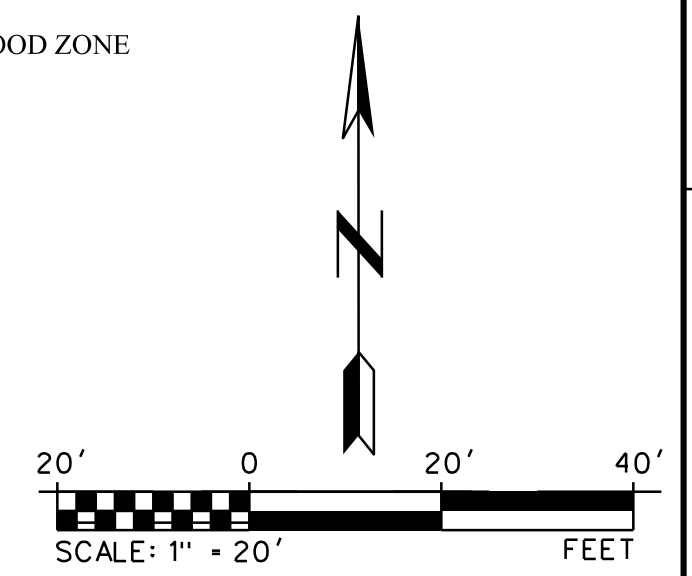
6009 - 6045 Martway
 Mission, KS 64202

LEGEND:

- EXISTING APPROXIMATE LIMITS FLOOD ZONE AE (FLOODWAY)
- EXISTING APPROXIMATE LIMITS FLOOD ZONE AE
- CUT = 1,837 CUBIC YARDS
- FILL = 137 CUBIC YARDS

COMPENSATORY VOLUME:

CUT IN 100 YR FLOODPLAIN = 177 CUBIC YARDS
 FILL IN 100 YR FLOODPLAIN = 36 CUBIC YARDS
 COMPENSATORY VOLUME = 0 CUBIC YARDS



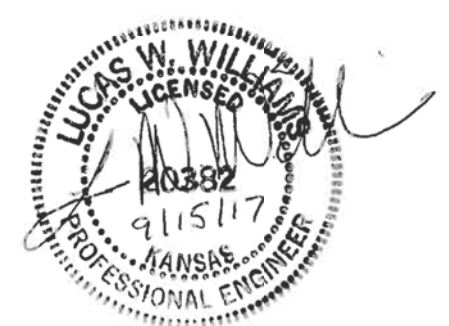
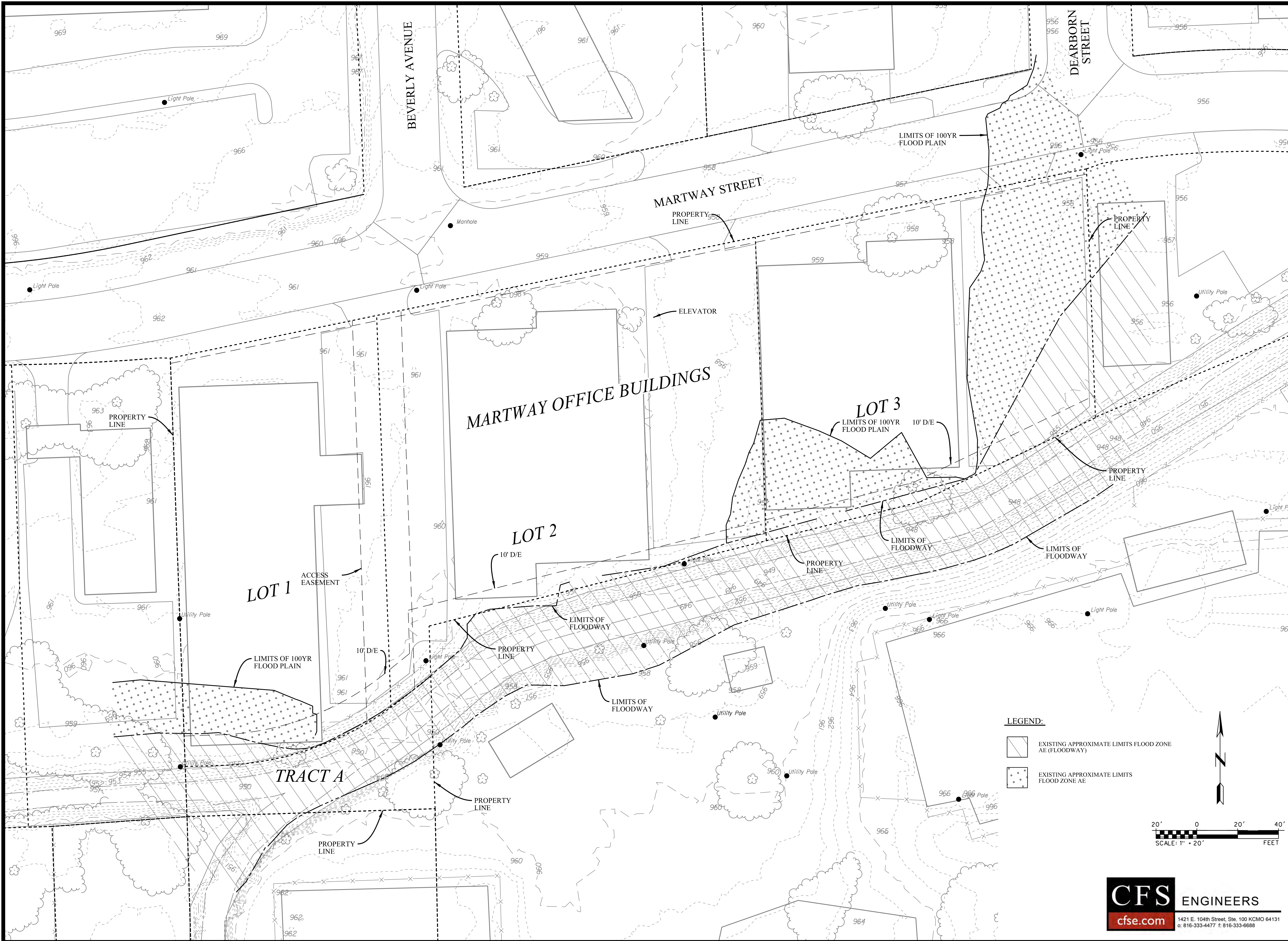
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16.05.Martway

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 cfse.com
 1421 E. 104th Street, Ste. 100 KCMO 64131
 o: 816-333-4477 f: 816-333-6688

C201

CUT & FILL PLAN



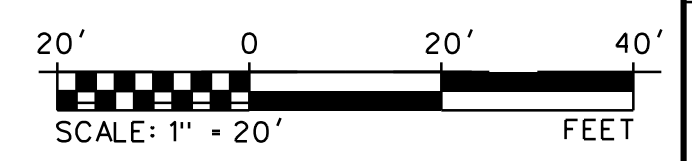
LUCAS W. WILLIAMS - ENGINEER
 KS PE # 20382

Martway Mixed Use

6009 - 6045 Martway
 Mission, KS 64202

LEGEND:

- EXISTING APPROXIMATE LIMITS FLOOD ZONE AE (FLOODWAY)
- EXISTING APPROXIMATE LIMITS FLOOD ZONE AE



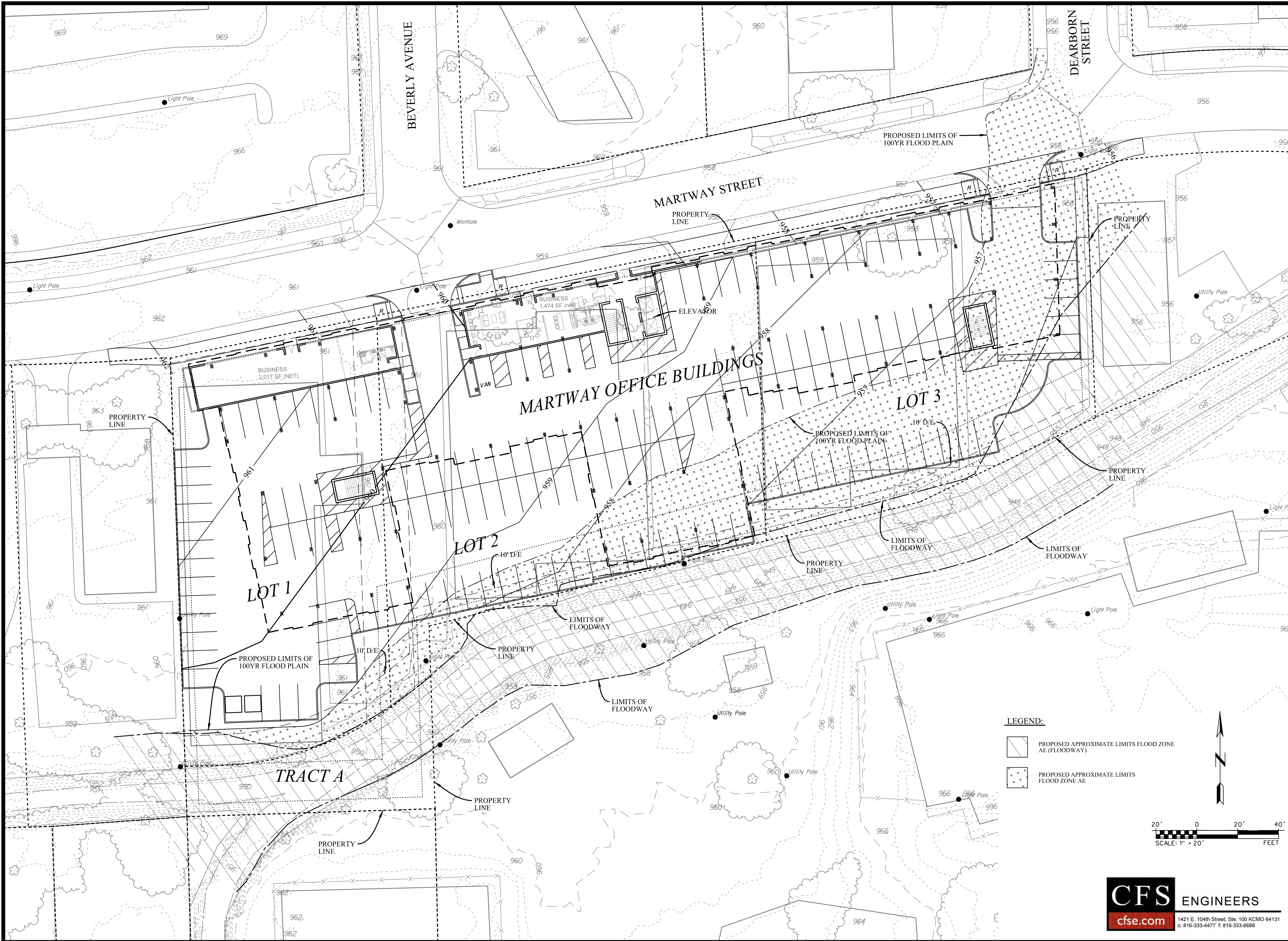
rev	issue	date
	PRELIM. DEV. PLAN SUBMITTAL	07.12.2017
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I6.05.Martway

CFS ENGINEERS
 cfse.com
 1421 E. 104th Street, Ste. 100 KCMO 64131
 o: 816-333-4477 f: 816-333-6688

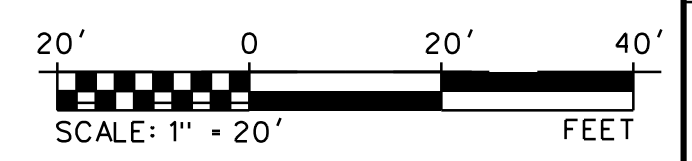
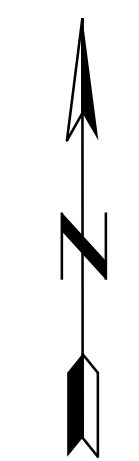
C202

EXISTING FLOOD PLAIN PLAN



LEGEND:

- PROPOSED APPROXIMATE LIMITS FLOOD ZONE AE (FLOODWAY)
- PROPOSED APPROXIMATE LIMITS FLOOD ZONE AE



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LUCAS W. WILLIAMS - ENGINEER
 KS PE # 20382

Martway Mixed Use

6009 - 6045 Martway
 Mission, KS 64202

rev	issue	date
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16.05.Martway

C203

PROPOSED FLOOD PLAIN PLAN

- EXTERIOR ELEVATIONS GENERAL NOTES:
1. WE ARE SEEKING A DEVIATION TO ALLOW THE MAXIMUM BUILDING HEIGHT REQUIREMENT BE RAISED FROM A THREE STORY 45' TO A FIVE STORY 65' MAXIMUM BUILDING HEIGHT. ROOF MOUNTED MECHANICAL EQUIPMENT SHALL BE HIDDEN BY THE USE OF PARAPETS.



04 | Elevation - South
Scale 1/16" = 1'-0"

GRAPHIC PATTERN	MATERIAL DESCRIPTION
	NICHIHA 'ILLUMINATION' WALL PANEL VERT. RUNNING BOND 18"x72"
	NICHIHA 'ARCH. BLOCK' WALL PANEL HORIZ. STACK BOND 18"x72"
	NICHIHA SIM. WOOD WALL PANEL (CEDAR) 6"x120"
	NICHIHA 'ILLUMINATION' WALL PANEL VERT. RUNNING BOND 18"x72"



03 | Elevation - West
Scale 1/16" = 1'-0"



02 | Elevation - East
Scale 1/16" = 1'-0"



GLAZING & OPENNESS AREA CALCULATION
TOTAL GLAZING & OPENNESS AREA @ FIRST FLOOR = 3,762 SF
TOTAL FACADE AREA @ FIRST FLOOR = 6,861 SF
TOTAL PERCENTAGE OF GLAZING AND OPENNESS = 55%

01 | Elevation - North
Scale 1/16" = 1'-0"

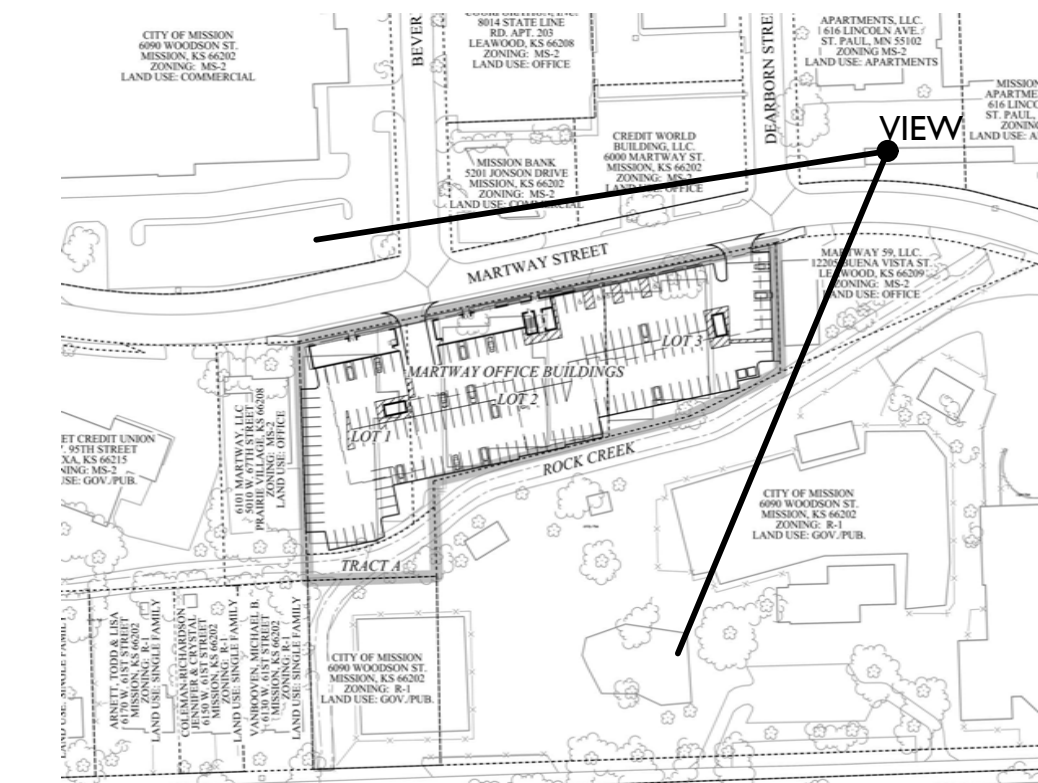
Martway Mixed Use
 6009 - 6045 Martway
 Mission, KS 64202

rev	issue	date
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	REVISED PRELIM. PLAN SUBMITTAL	09.15.2017

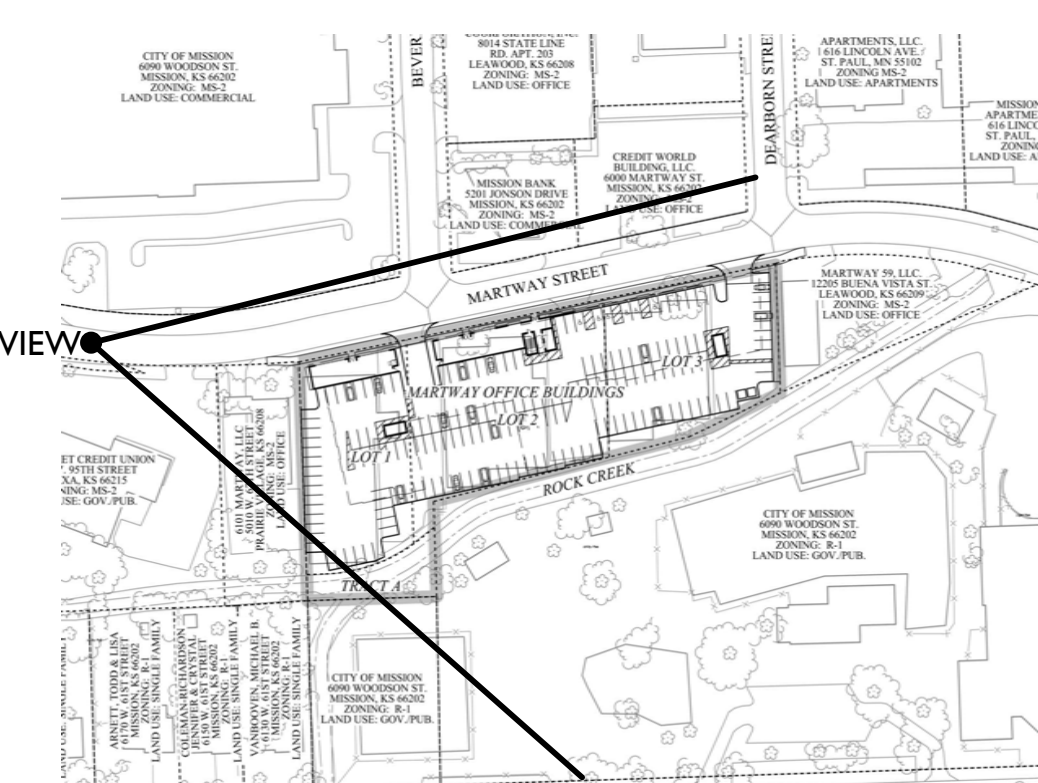
16.05.Martway

A200

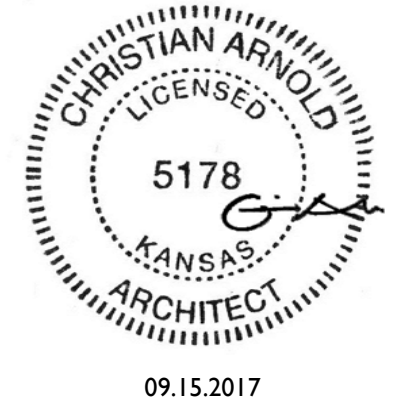
ELEVATIONS



02 | Perspective View Looking Southwest
Scale N.T.S.



01 | Perspective View Looking Southeast
Scale N.T.S.



09.15.2017

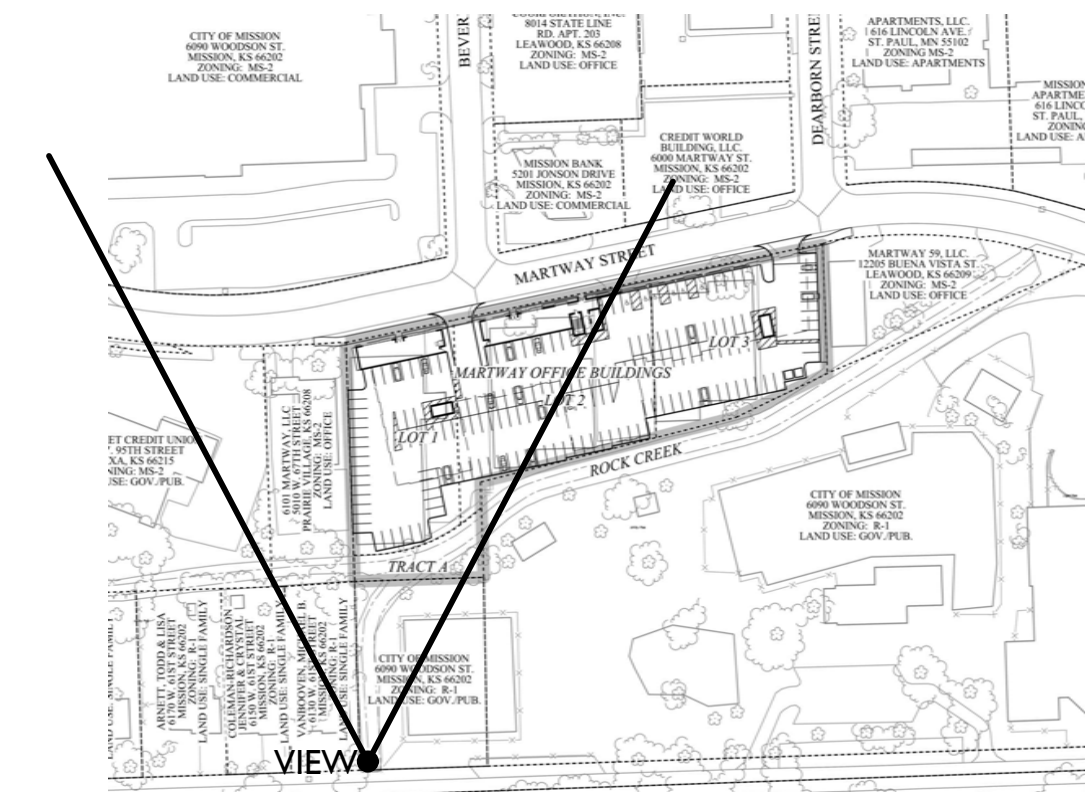
Martway Mixed Use

6009 - 6045 Martway
Mission, KS 64202

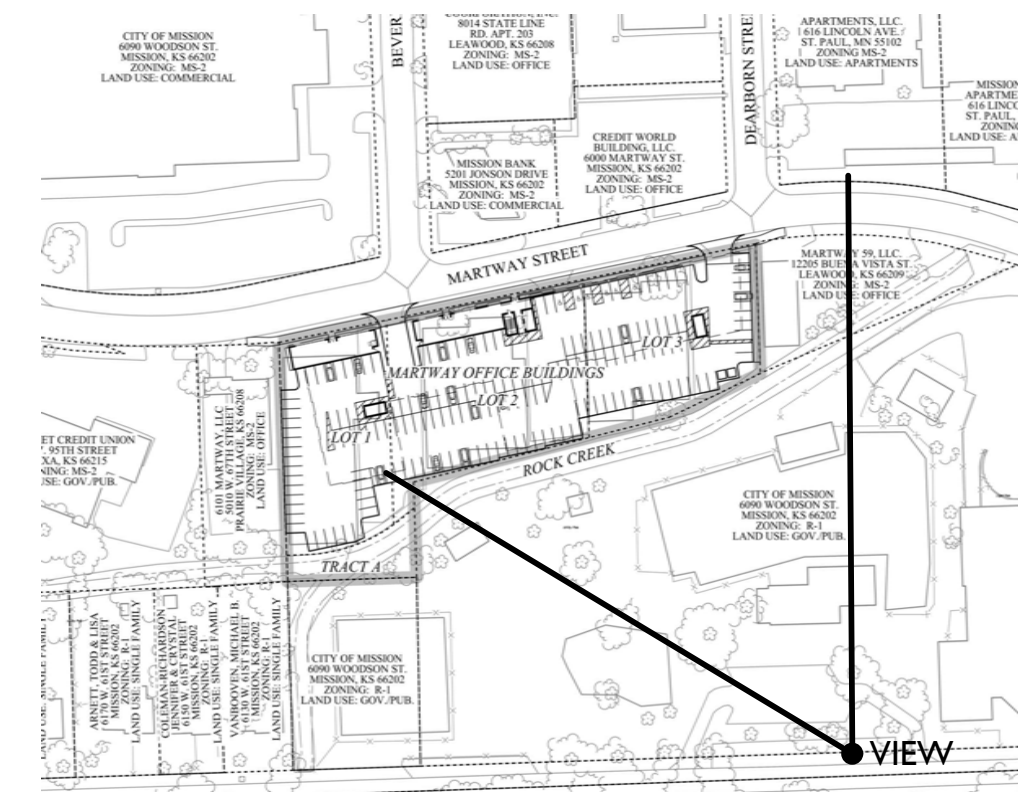
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	REVISED PRELIM. PLAN SUBMITTAL	09.15.2017

16.05.Martway

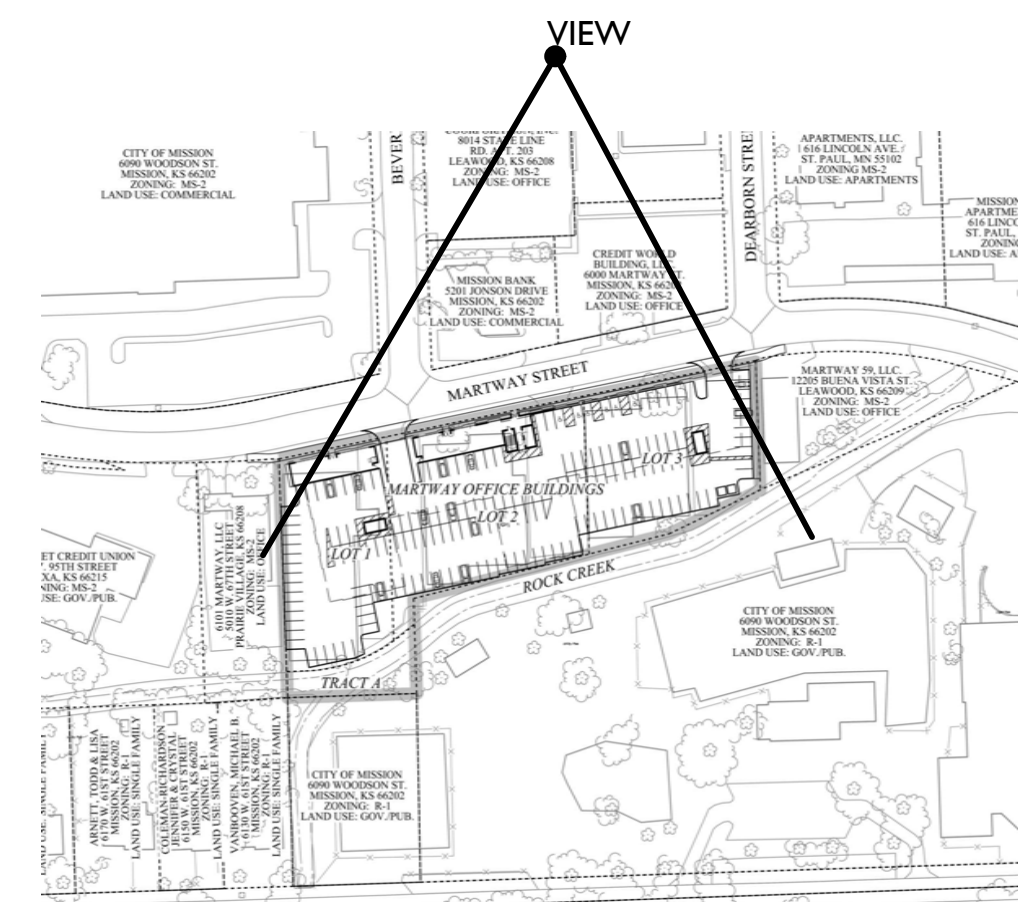
A201



03 | View Looking North
Scale N.T.S.



02 | View Looking Northwest
Scale N.T.S.



01 | View Looking South
Scale N.T.S.



Martway Mixed Use

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Mission, KS 64202

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	PRELIM. DEV. PLAN SUBMITTAL	07.12.2017
	REVISED PRELIM. PLAN SUBMITTAL	09.15.2017

16.05.Martway

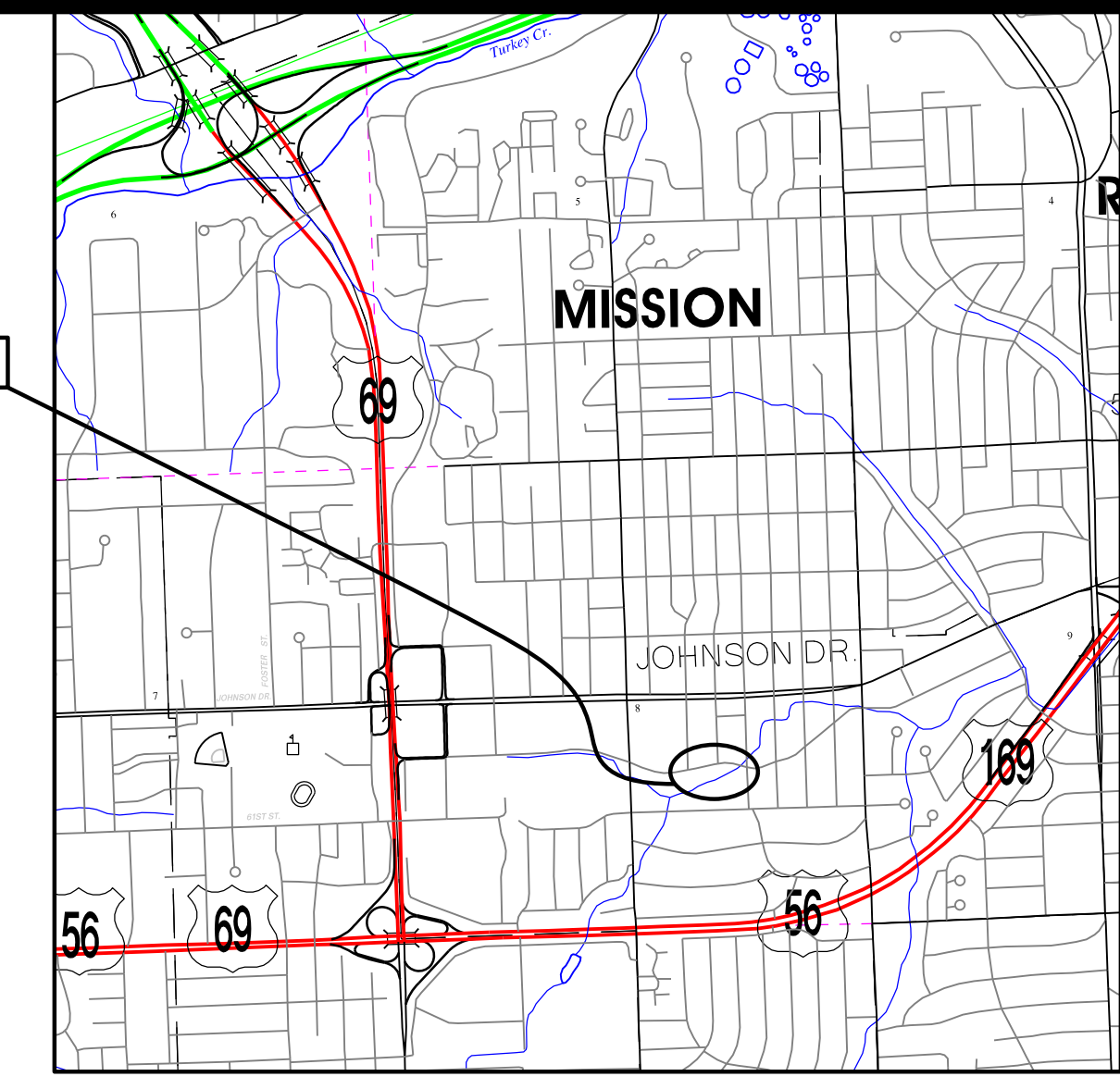
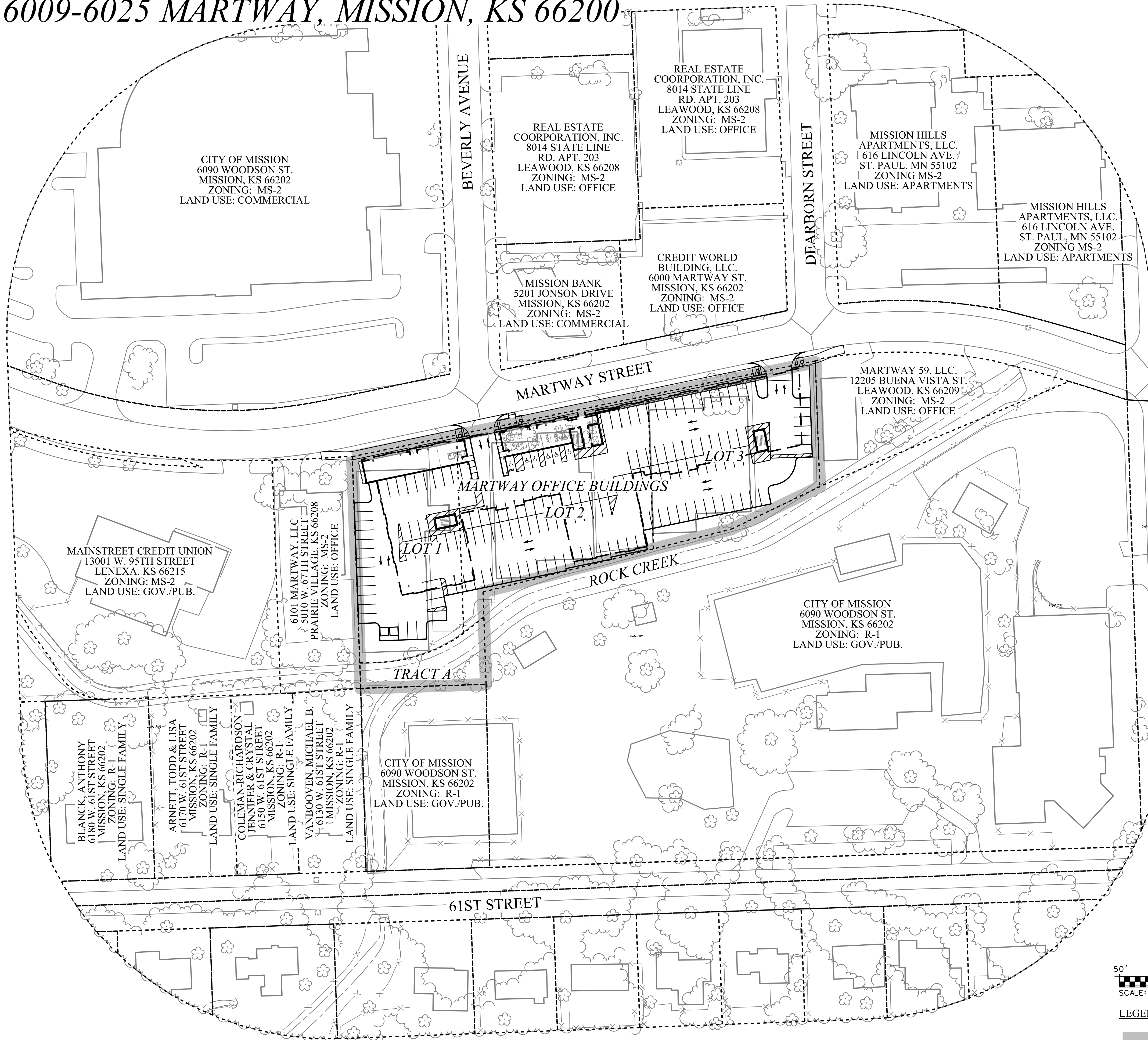
A202

EXTERIOR VIEWS

MARTWAY MIXED USE

PRELIMINARY DEVELOPMENT PLAN

6009-6025 MARTWAY, MISSION, KS 66200



VICINITY MAP:

LEGAL DESCRIPTION

LOTS 1, 2 AND 3 OF MARTWAY OFFICE BUILDINGS A SUBDIVISION OF LAND IN THE NORTHWEST QUARTER OF SECTION 8, TOWNSHIP 25 SOUTH, RANGE 25 EAST OF THE SIXTH PRINCIPLE MERIDIAN IN THE CITY OF MISSION, JOHNSON COUNTY, KANSAS.

SITE DEVELOPMENT SUMMARY TABLE

EXISTING ZONING: MS-2

LAND AREA: 1.767 ACRES OR 76,971 SQUARE FEET MORE OR LESS

LAND USE:

EXISTING: OFFICE

PROPOSED: A MIXED USE DEVELOPMENT WITH A 4-STORY APARTMENT COMPLEX WITH COMMERCIAL SPACE AND PARKING DECK ON THE FIRST FLOOR

BUILDING HEIGHT:

MAXIMUM BUILDING HEIGHT: 3-STORY/45'-0"
PROPOSED BUILDING HEIGHT: 4-STORY/56'-3"

BUILDING FLOOR AREA:

FIRST FLOOR:	3,491 S.F.
SECOND FLOOR:	38,977 S.F.
THIRD FLOOR:	38,977 S.F.
FOURTH FLOOR:	38,977 S.F.
TOTAL FLOOR AREA	120,422 S.F.

FLOOR AREA RATIO: 1.56

PARKING SUMMARY

PROPOSED PARKING RATIO: 1.03

PARKING SUMMARY TABLE		
BUILDING USE	SPECIFIC USE REQUIREMENT	SPACE REQUIREMENT
COMMERCIAL SPACE	4 SPACES PER 1000 S.F. x 3491 S.F.	14 SPACES
EFFICIENCY APARTMENTS	1 SPACE PER UNIT x 18 UNITS	18 SPACES
ONE BEDROOM APARTMENTS	1 SPACE PER UNIT x 69 UNITS	69 SPACES
TWO BEDROOM APARTMENTS	2 SPACES PER UNIT x 30 UNITS	60 SPACES
	TOTAL SPACES REQUIRED	161 SPACES
	TOTAL SPACES PROVIDED	**166 SPACES

** 6 ACCESSIBLE SPACES WITH 1 VAN SPACE HAS BEEN PROVIDED

OWNER:

MARTWAY OFFICE WORKS, LLC
423 DELAWARE STREET, STE. 102
KANSAS CITY, MO 64105

ARCHITECT:

CLOCKWORK
423 DELAWARE STREET STE. 102
KANSAS CITY, MO 64105

DEVELOPER:

MARTWAY OFFICE WORKS, LLC
423 DELAWARE STREET, STE. 102
KANSAS CITY, MO 64105

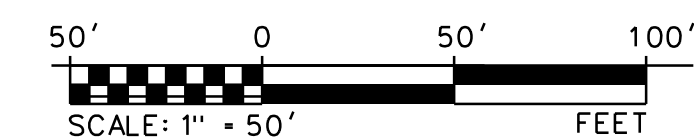
ENGINEER/LANDSCAPE ARCHITECT/SURVEYOR/PLANNER:

CFS ENGINEERS, P.A.
1421 E. 104TH STREET, SUITE 100
KANSAS CITY, MO 64131

REQUESTED DEVIATIONS

THE DEVELOPER REQUESTS THE FOLLOWING DEVIATIONS TO THE CITY OF MISSION'S MUNICIPAL CODE:

1. THE REAR YARD SETBACK REQUIREMENT BE REDUCED FROM 25' TO A 0' SETBACK.
2. THE MAXIMUM BUILDING HEIGHT REQUIREMENT BE RAISED FROM A 3-STORY 45' TO A 4-STORY 56'-3" MAXIMUM BUILDING HEIGHT.
3. THE MINIMUM LOT AREA PER MULTI-FAMILY DWELLING BE RAISED FROM 35 UNITS PER ACRE TO 66.21 UNITS PER ACRE.
4. THE PARKING LOT SETBACK BE REDUCED FROM 6' TO 0' AT WEST PROPERTY LINE ONLY.
5. THE MINIMUM GREEN SPACE BUFFER BETWEEN PARKING AND INTERIOR LOT LINES BE REDUCED FROM 4' TO 0' AT WEST PROPERTY LINE ONLY.
6. THE INTERIOR PARKING LOT TREE REQUIREMENTS.
7. THE PARKING LOT INTERIOR OPEN SPACE REQUIREMENT.



LEGEND:
LIMITS OF DEVELOPMENT

clockwork
423 Delaware St Ste 102
Kansas City MO 64105
www.clockwork-ad.com



LUCAS W. WILLIAMS - ENGINEER
KS PE # 20382

Martway Mixed Use

6009 - 6045 Martway
Mission, KS 64202

rev	issue	date
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	REV. PRELIM. DEV. PLAN SUBMITTAL	11.06.2017

16.05.Martway

C100

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cfs.com
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o: 816-333-4477 f: 816-333-6688

OVERALL SITE PLAN



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KS PE # 20382

Martway Mixed Use

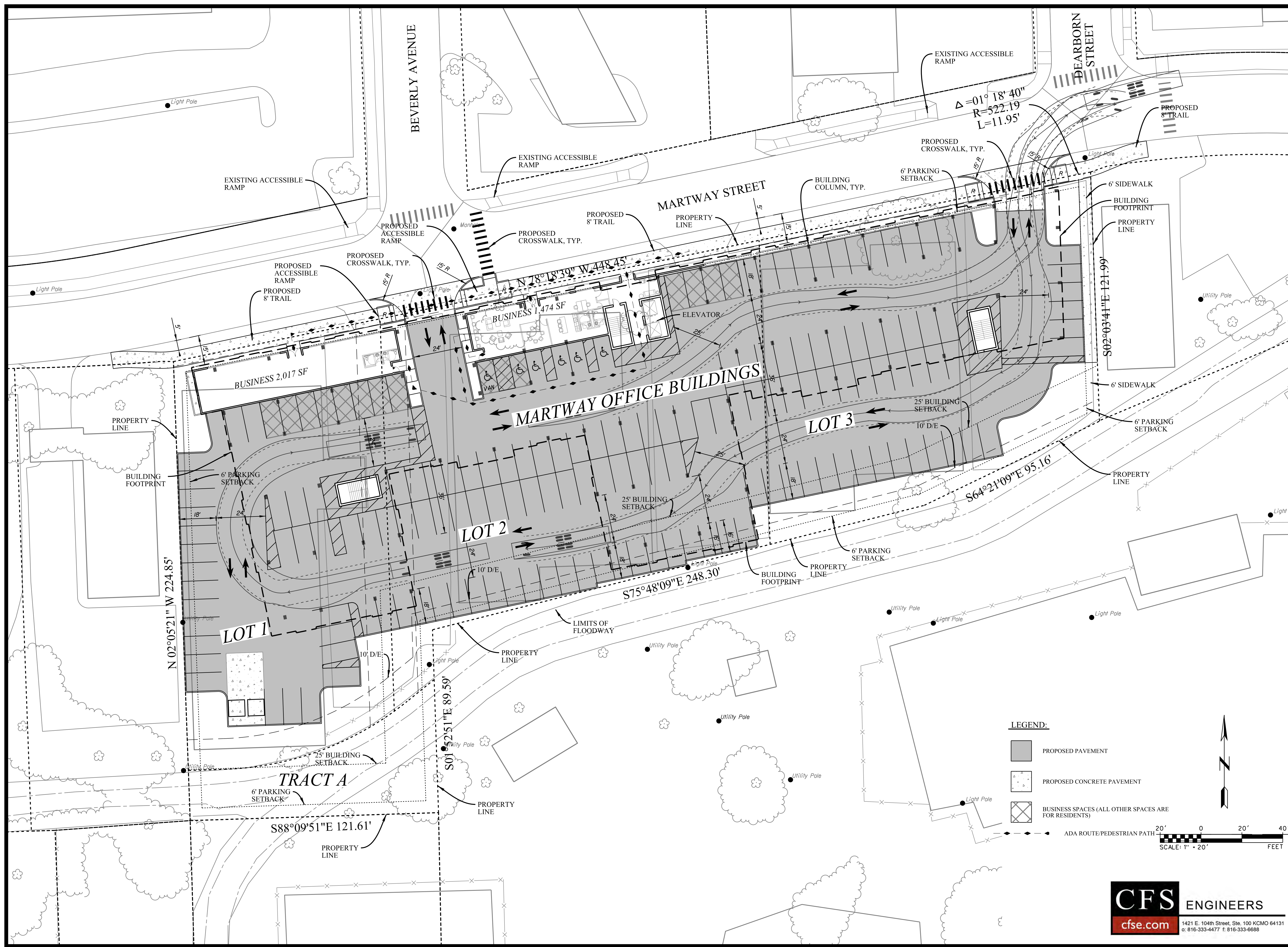
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16.05.Martway

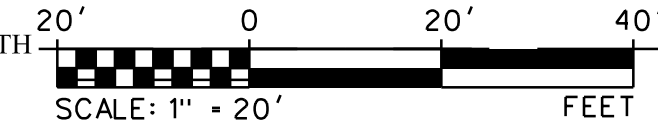
C101

SITE PLAN

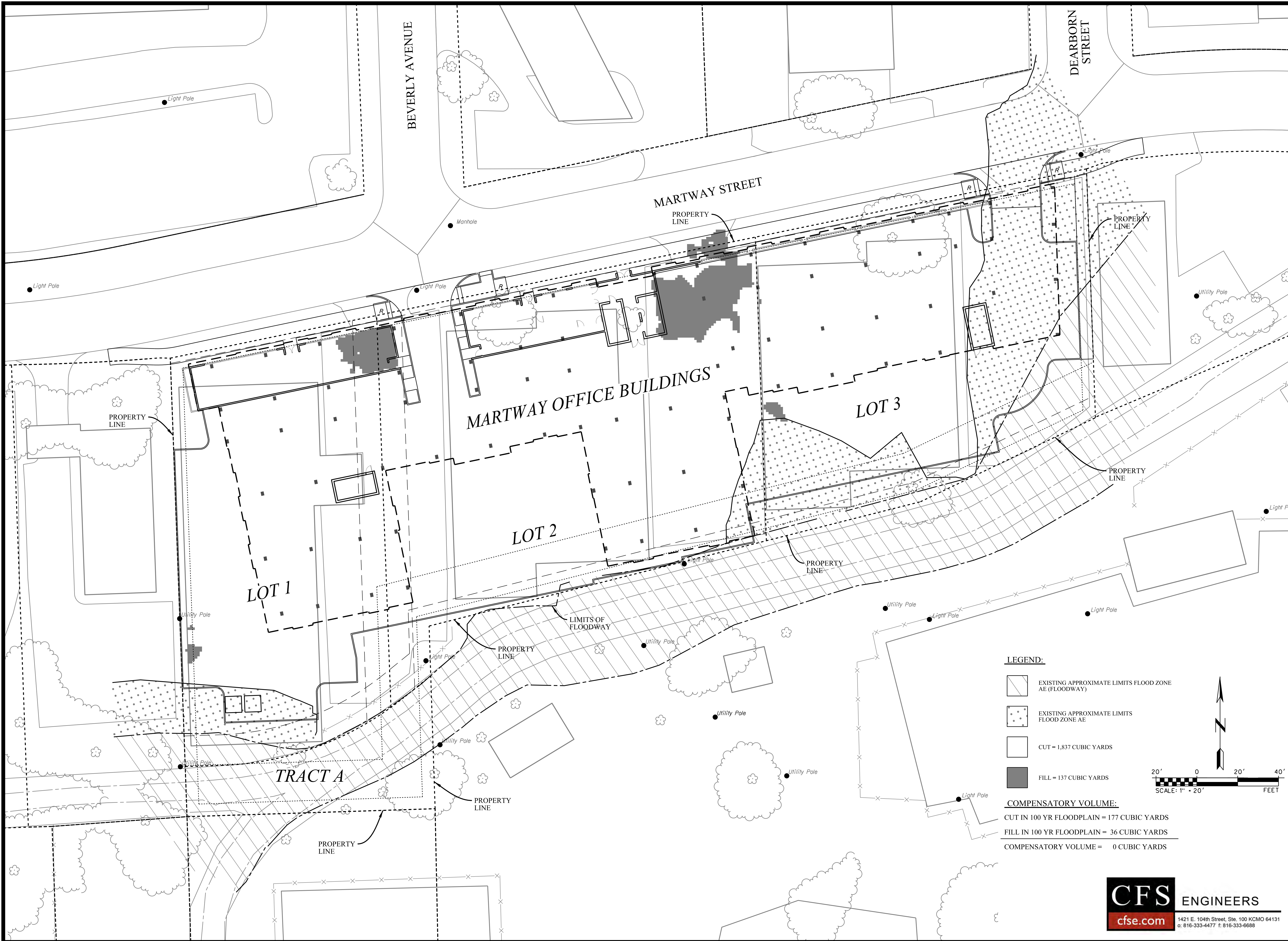


LEGEND:

- PROPOSED PAVEMENT
- PROPOSED CONCRETE PAVEMENT
- BUSINESS SPACES (ALL OTHER SPACES ARE FOR RESIDENTS)



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 KS PE # 20382

Martway Mixed Use

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 Mission, KS 64202

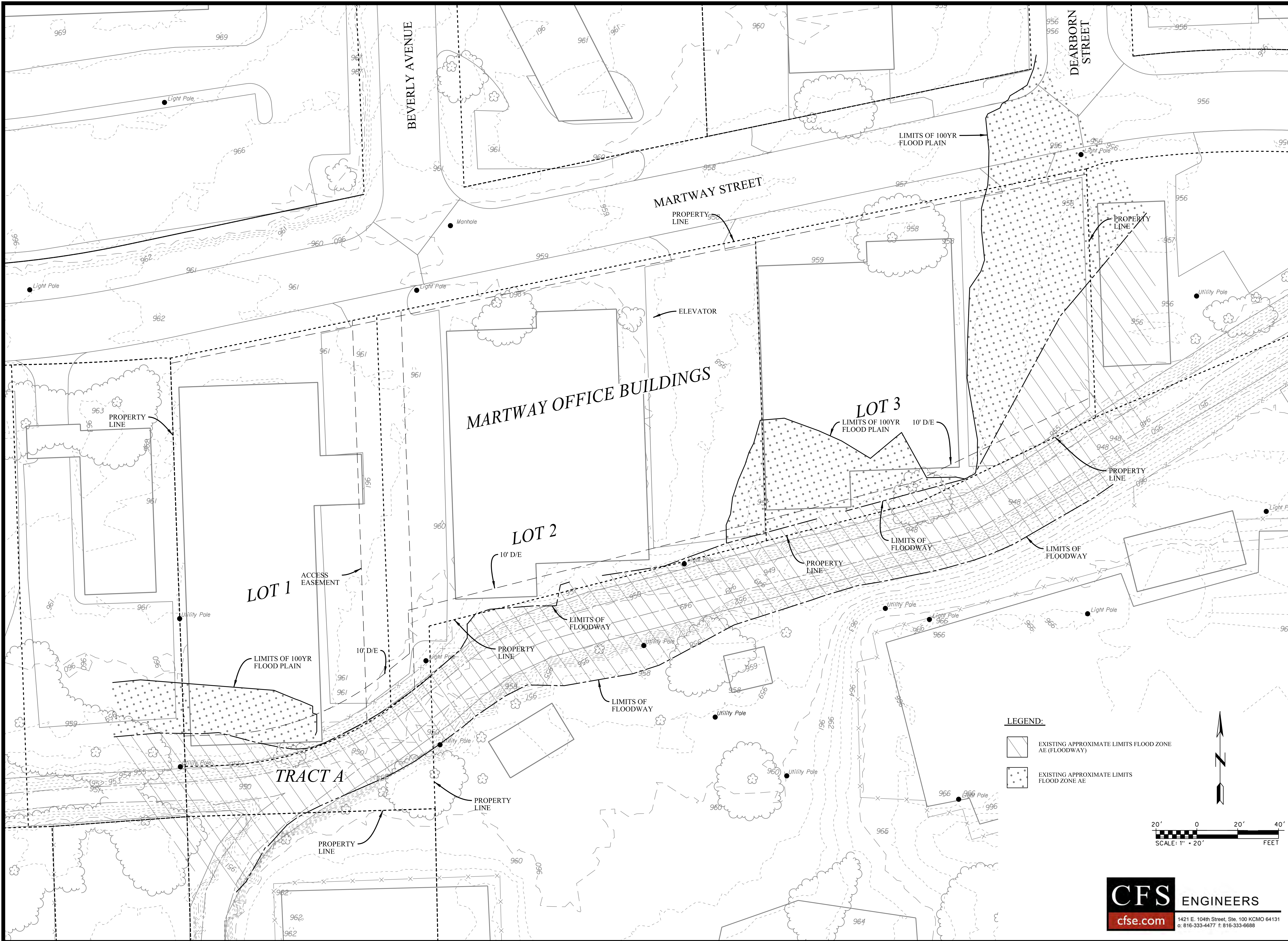
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	REV. PRELIM. DEV. PLAN SUBMITTAL	11.06.2017

16.05.Martway

C201

CUT & FILL PLAN

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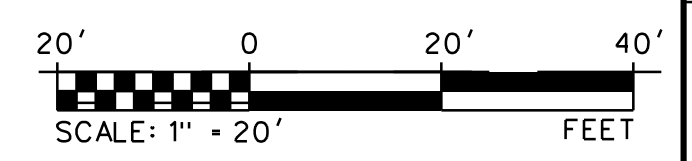
LUCAS W. WILLIAMS - ENGINEER
 KS PE # 20382

Martway Mixed Use

6009 - 6045 Martway
 Mission, KS 64202

LEGEND:

-  EXISTING APPROXIMATE LIMITS FLOOD ZONE AE (FLOODWAY)
-  EXISTING APPROXIMATE LIMITS FLOOD ZONE AE



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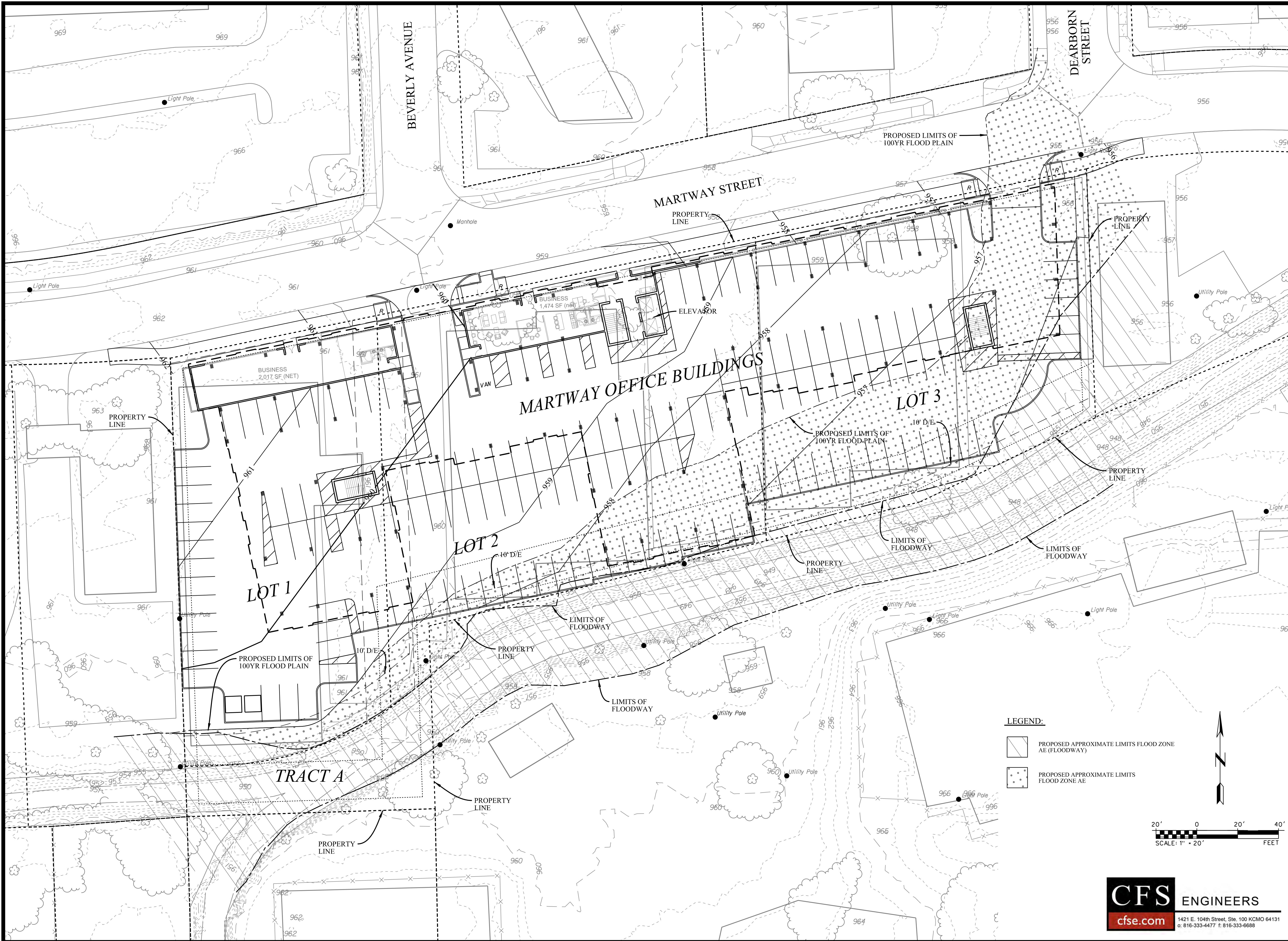
16.05.Martway



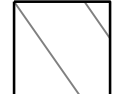
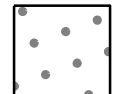
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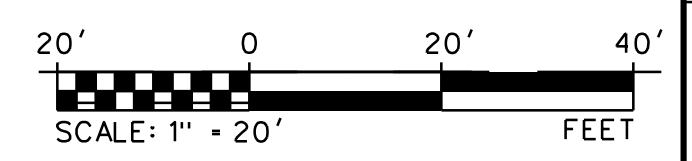
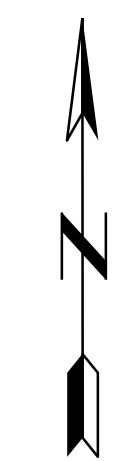
C202

EXISTING FLOOD PLAIN PLAN



LEGEND:

-  PROPOSED APPROXIMATE LIMITS FLOOD ZONE AE (FLOODWAY)
-  PROPOSED APPROXIMATE LIMITS FLOOD ZONE AE



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LUCAS W. WILLIAMS - ENGINEER
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Martway Mixed Use

6009 - 6045 Martway
 Mission, KS 64202

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16.05.Martway

C203

PROPOSED FLOOD PLAIN PLAN



Martway Mixed Use

6009 - 6045 Martway
Mission, KS 64202

rev	issue	date
	PRELIM. DEV. PLAN SUBMITTAL	07.12.2017
	REVISED PRELIM. PLAN SUBMITTAL	09.15.2017
	REV. PRELIM. DEV. PLAN SUBMITTAL	11.06.2017

MARTWAY UNIT MIX			
	SQ.FT.	UNITS PER FLOOR	TOTAL UNITS
1B/1B (MICRO)	504	4	12
1B/1B (ADA)	644	2	6
1B/1B	720	23	69
2B/2B (ADA)	1,104	3	9
2B/2B	1,112	5	15
2B/2B	1,144	2	6
TOTAL:		39	117
AVERAGE UNIT SIZE =			795.48
FLOORS 2-4 FOOTPRINT AREA =	38,977 SF	# OF BEDS =	147

01 | Typical Floor Plan
Scale 1/8" = 1'-0"



A100

FLOOR PLAN





EXTERIOR ELEVATIONS GENERAL NOTES:

1. WE ARE SEEKING A DEVIATION TO ALLOW THE MAXIMUM BUILDING HEIGHT REQUIREMENT BE RAISED FROM A THREE STORY 45' TO A FOUR STORY 56'-3" MAXIMUM BUILDING HEIGHT.
2. ROOF MOUNTED MECHANICAL EQUIPMENT SHALL BE HIDDEN BY THE USE OF PARAPETS.



04 | Elevation - South
Scale 1/16" = 1'-0"

GRAPHIC PATTERN	MATERIAL DESCRIPTION
	NICHIHA 'ILLUMINATION' WALL PANEL VERT. RUNNING BOND 18"x72"
	NICHIHA 'ARCH. BLOCK' WALL PANEL HORIZ. STACK BOND 18"x72"
	NICHIHA SIM. WOOD WALL PANEL (CEDAR) 6"x120"
	NICHIHA 'ILLUMINATION' WALL PANEL VERT. RUNNING BOND 18"x72"



03 | Elevation - West
Scale 1/16" = 1'-0"



02 | Elevation - East
Scale 1/16" = 1'-0"



GLAZING & OPENNESS AREA CALCULATION
TOTAL GLAZING & OPENNESS AREA @ FIRST FLOOR = 3,762 SF
TOTAL FACADE AREA @ FIRST FLOOR = 6,861 SF
TOTAL PERCENTAGE OF GLAZING AND OPENNESS = 55%

01 | Elevation - North
Scale 1/16" = 1'-0"

Martway Mixed Use

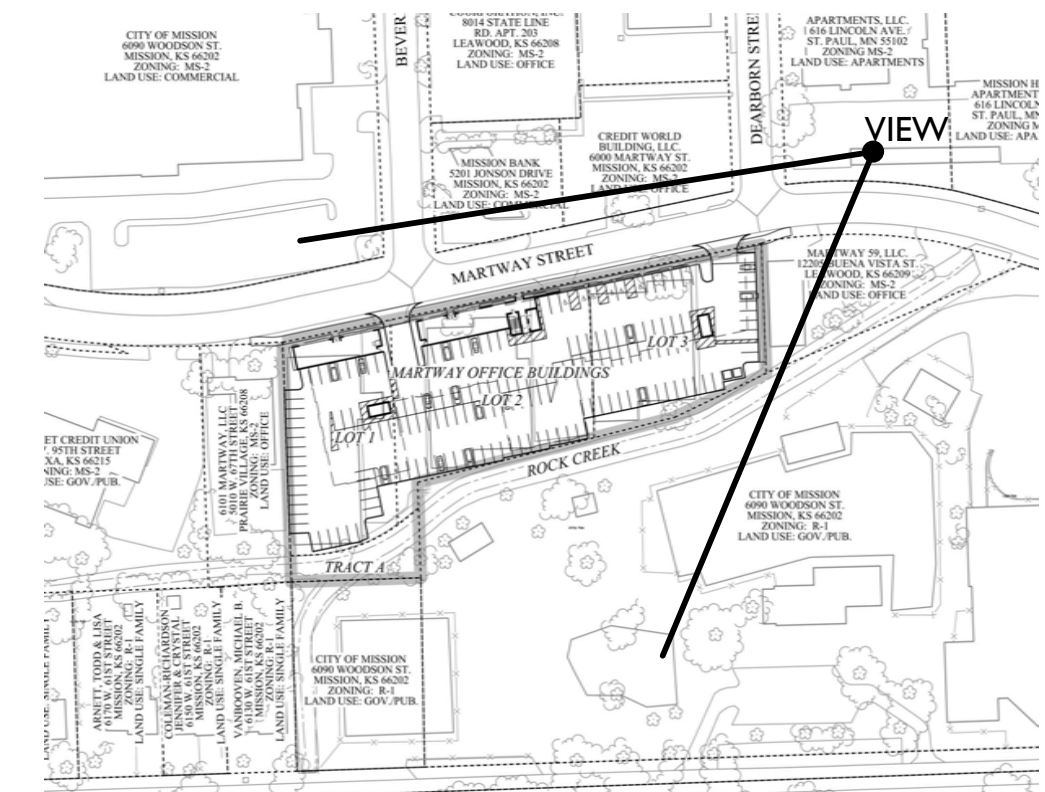
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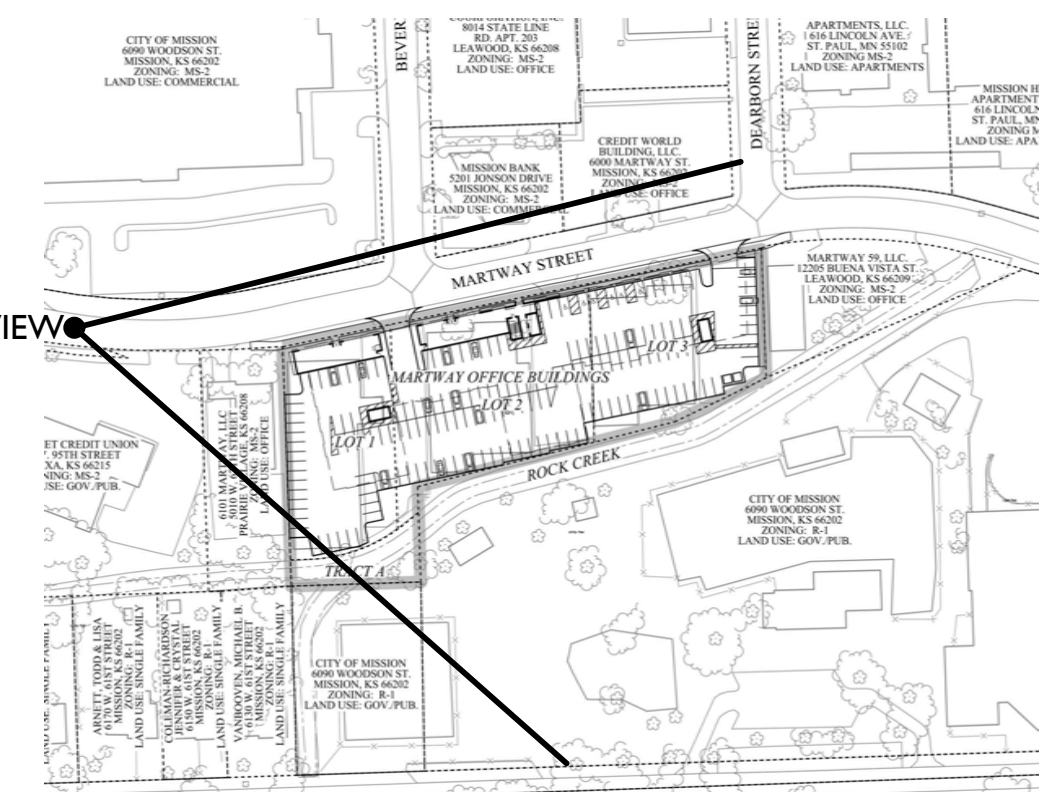
16.05.Martway

A200

ELEVATIONS



02 Perspective View Looking Southwest
 Scale N.T.S.



01 Perspective View Looking Southeast
 Scale N.T.S.

Martway Mixed Use

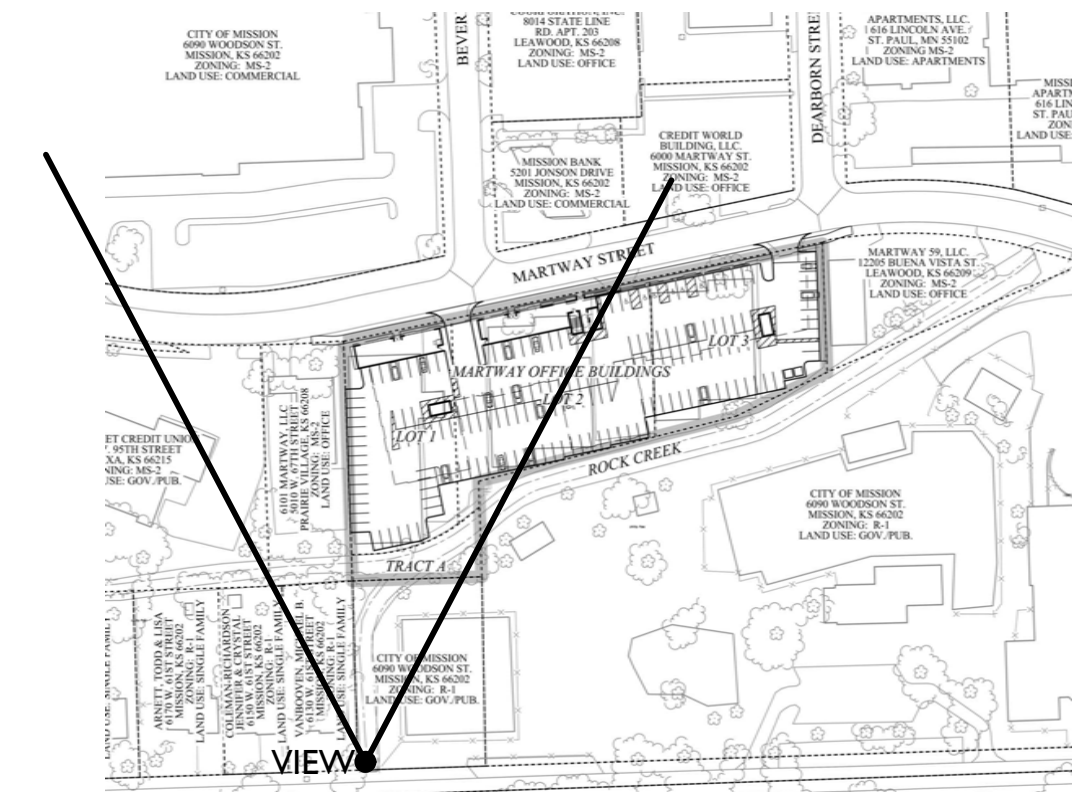
6009 - 6045 Martway
 Mission, KS 64202

rev	issue	date
	PRELIM. DEV. PLAN SUBMITTAL	07.12.2017
	FINAL PRELIM. PLAN SUBMITTAL	09.15.2017
	REV. PRELIM. DEV. PLAN SUBMITTAL	11.06.2017

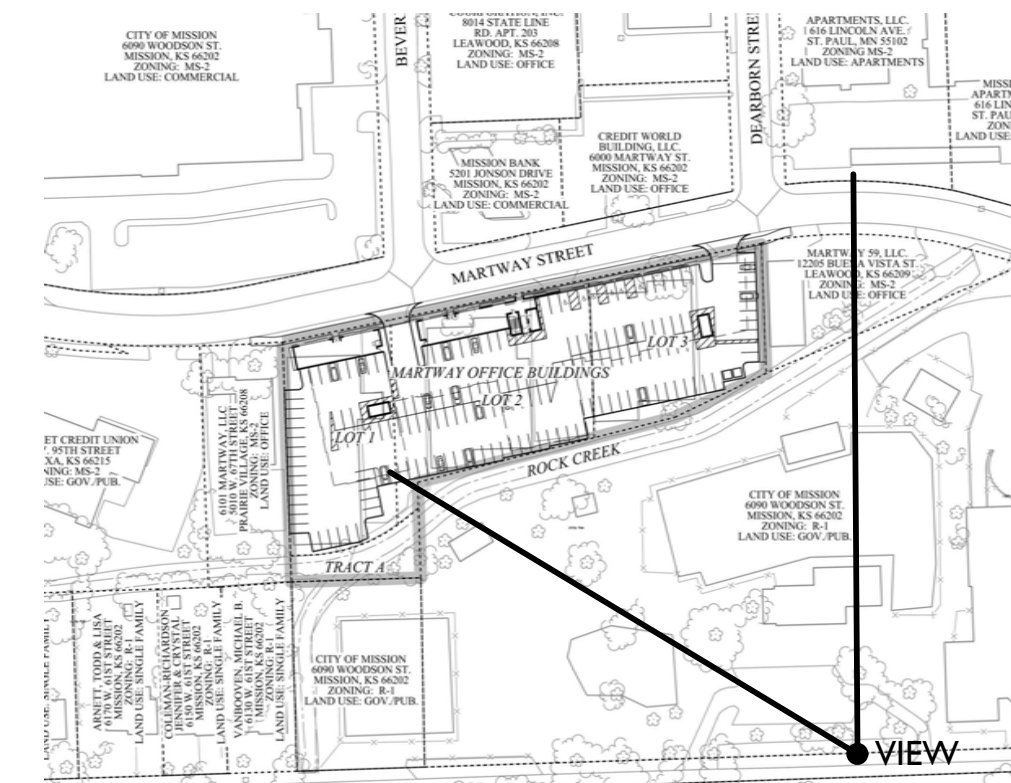
16.05.Martway

A201

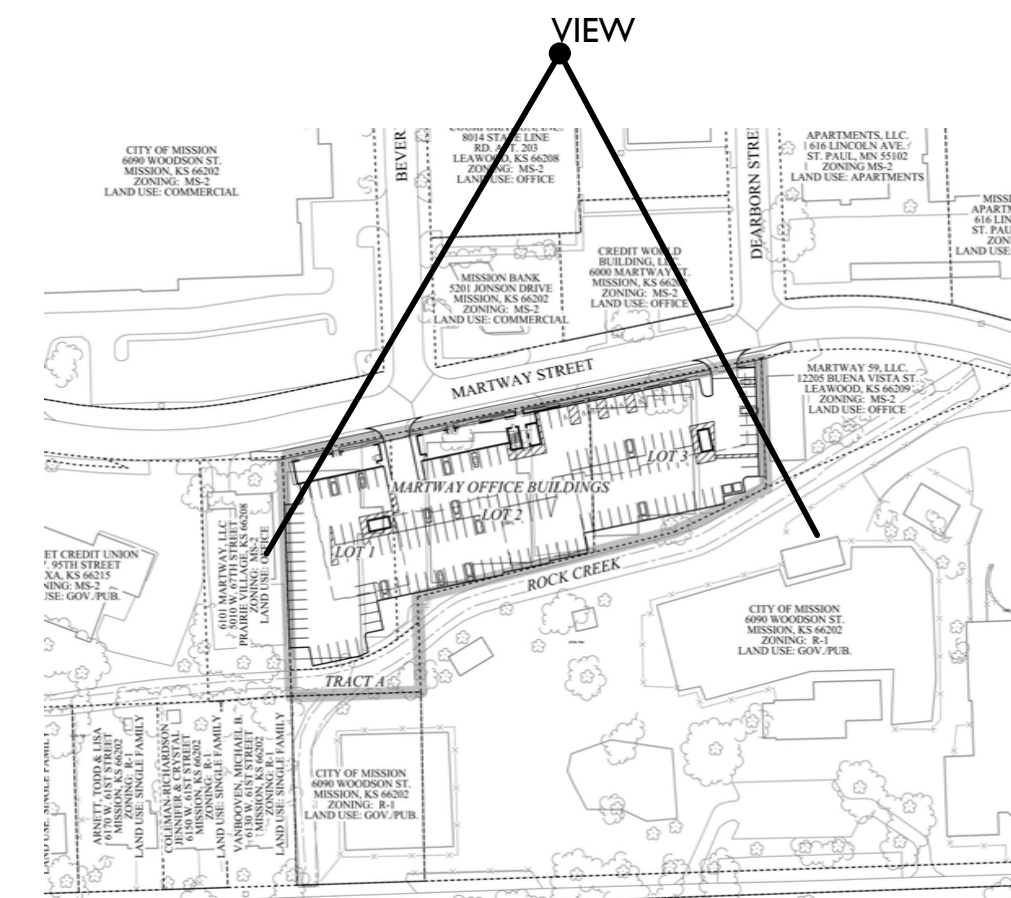
EXTERIOR RENDERINGS



03 | View Looking North
Scale N.T.S.



02 | View Looking Northwest
Scale N.T.S.



01 | View Looking South
Scale N.T.S.



Martway Mixed Use

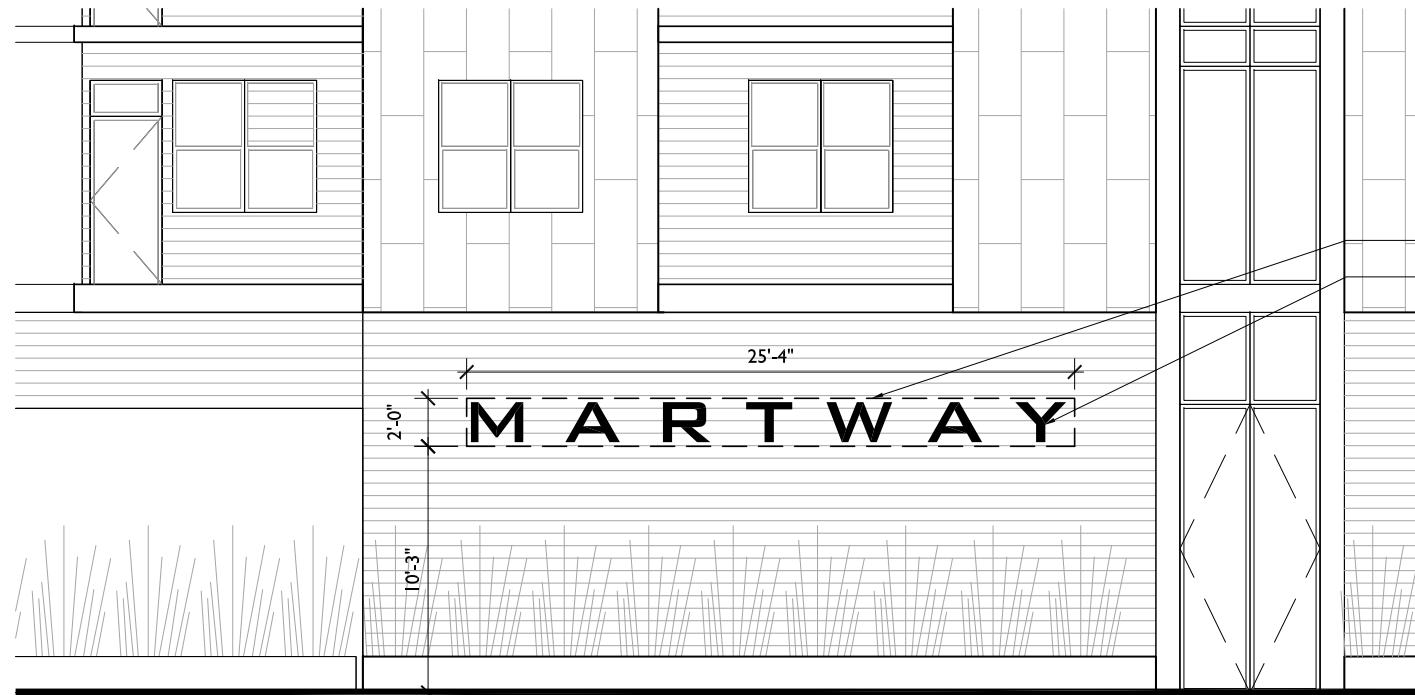
6009 - 6045 Martway
Mission, KS 64202

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	PRELIM. DEV. PLAN SUBMITTAL	07.12.2017
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16.05.Martway

A202

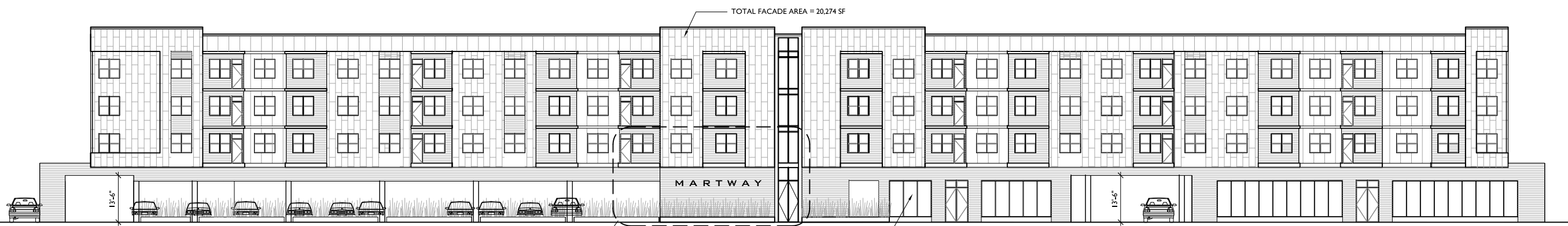
EXTERIOR VIEWS



TOTAL SIGN AREA = 50.66 SF (.0025% OF TOTAL FACADE AREA)
 PIN MOUNTED STAINLESS STEEL LETTERS
 BACKLIT WITH WHITE LED TAPE LIGHT

- SIGNAGE GENERAL NOTES:**
- SIGNAGE TO COMPLY WITH MISSION KS DESIGN GUIDELINES AND SIGNAGE CRITERIA - SECTION 430.090 'SIGNS PERMITTED IN COMMERCIAL AND INDUSTRIAL DISTRICTS' AND SECTION 430.120 'PRIVATE SIGN CRITERIA'.
 - FOR ANY ADDITIONAL SIGNAGE PROVIDED AT COMMERCIAL TENANT SPACES AS REQUIRED, PROVIDE COMPLIANT SIGNAGE IN ACCORDANCE WITH ABOVE SECTIONS, WITH TOTAL SIGNAGE AREA TO BE LESS THAN OR EQUAL TO 10% TOTAL FACADE AREA.

02 | Enlarged Signage Elevation
 Scale 1/8" = 1'-0"



GLAZING & OPENNESS AREA CALCULATION
 TOTAL GLAZING & OPENNESS AREA @ FIRST FLOOR = 3,762 SF
 TOTAL FACADE AREA @ FIRST FLOOR = 6, 861 SF
 TOTAL PERCENTAGE OF GLAZING AND OPENNESS = 55%

01 | North Elevation
 Scale 1/32" = 1'-0"

City of Mission	Item Number:	5b.
ACTION ITEM SUMMARY	Date:	February 21, 2018
COMMUNITY DEVELOPMENT	From:	Danielle Sitzman

Action items require a vote to recommend the item to full City Council for further action.

RE: Establishment of a Special Use Permit for off-site parking at 5655 Broadmoor Street (Parcel ID# KF251208-1033)

RECOMMENDATION: Uphold the recommendation of the Planning Commission for approval of the submitted Special Use Permit with the conditions noted.

DETAILS: The subject property is located in the West Gateway District and is adjacent to several office buildings. The property is currently developed as a surface parking lot with approximately 117 stalls. It has been a paved parking lot for many years, showing on Johnson County Land Records since the early 1990's.

The property was purchased in November of 2015 by CAPROCQ KC Mission LLC at the same time that they purchased the Mission Towers building at 5700 Broadmoor Street. Mission Towers is a 10-story, 245,000 square foot office building with multiple tenants. It was built in 1971. Purchase of the off-site parking lot came to staff's attention in the fall of 2016 when the property manager for the new property owner made inquiries about making improvements to the site lighting. Staff determined that a Special Use Permit would first need to be approved before a building permit could be issued. Off-site parking is not an allowable use in the zoning district where this property is located and is specifically called out in Section 445.180 of the Municipal Code as a designated use for a Special Use Permit. At this time, the property owner wishes to obtain a Special Use Permit to continue to use the parking lot for employee parking and to make improvements to the site for its safe operation. A site plan showing these improvement has been submitted.

The submitted site plan shows restriping of the parking lot to accommodate 86 parking stalls. Other improvements include installation of a sidewalk and crosswalk to accommodate pedestrians accessing the satellite lot from the Mission Towers building, street trees, landscaping and screening, and bollard-style lights. The property owner is proposing only to use 42" tall bollards for lighting. The basketball hoop would be removed. There is an existing City maintained wood privacy fence installed along the east side of the parking field. The fence would remain.

Staff has reviewed the proposed site plan for compliance with current zoning standards and future Form Based code compliant public improvements. The design of the parking lot and stormwater controls have also been reviewed by the City's on-call engineers at Olsson Associates. The exact location of the crosswalk is yet to be determined. The applicant has complied with all staff comments on the safety and design of the proposed use. The City's attorney has reviewed the attached ordinance as to form.

Related Statute/City Ordinance:	Including but not limited to 410.070, 445.180, 440.120-140
Line Item Code/Description:	NA
Available Budget:	NA

City of Mission	Item Number:	5b.
ACTION ITEM SUMMARY	Date:	February 21, 2018
COMMUNITY DEVELOPMENT	From:	Danielle Sitzman

Action items require a vote to recommend the item to full City Council for further action.

Planning Commission Recommendation

The Planning Commission, at their January 22, 2018 meeting, voted 6-0 to recommend approval of Case #17-13 to the City Council for a Special Use Permit for the use of the subject property as an off-site parking lot for tenants of 5700 Broadmoor Street. The permission would run with the use of the property with the following conditions:

1. Limit the use of the subject property to the parking of vehicles to support the daily employee parking needs of 5700 Broadmoor Street.
2. Require that the on-site and off-site improvements as detailed in the submitted site plans to be substantially completed no later than November 1, 2018.
3. Require the platting the property for the dedication of right-of-way be completed prior to the issuance of any permits for improvements.
4. The final location of the crosswalk and sidewalk is to be coordinated with City Staff.

Municipal Code

According to Section 440.120 of the Municipal Code, after the Planning Commission submits a recommendation, and the reasons therefore, the City Council may:

1. Approve and adopt such recommendation;
2. Override the Planning Commission recommendations by two-thirds (2/3) majority vote of the City Council; or
3. Return such recommendations to the Planning Commission with a statement specifying the basis for the City Council's failure to approve or disapprove.

The Governing Body shall not take action on application for a special use permit until fourteen (14) days have elapsed after the date of the conclusion of the Planning Commission public hearing in order to allow the filing of a protest petition. No protest petition was received and the deadline has expired.

CFAA CONSIDERATIONS/IMPACTS: Improvements to the street network surrounding this property will include adding sidewalks, shade trees, and a crosswalk. These features will help residents and visitors to Mission travel without a vehicle in a safe manner and improve access to Broadmoor Park.

Related Statute/City Ordinance:	Including but not limited to 410.070, 445.180, 440.120-140
Line Item Code/Description:	NA
Available Budget:	NA

STAFF REPORT
Planning Commission Meeting January 22, 2018

AGENDA ITEM NO.: 3
PROJECT NUMBER / TITLE: Application # 17-13

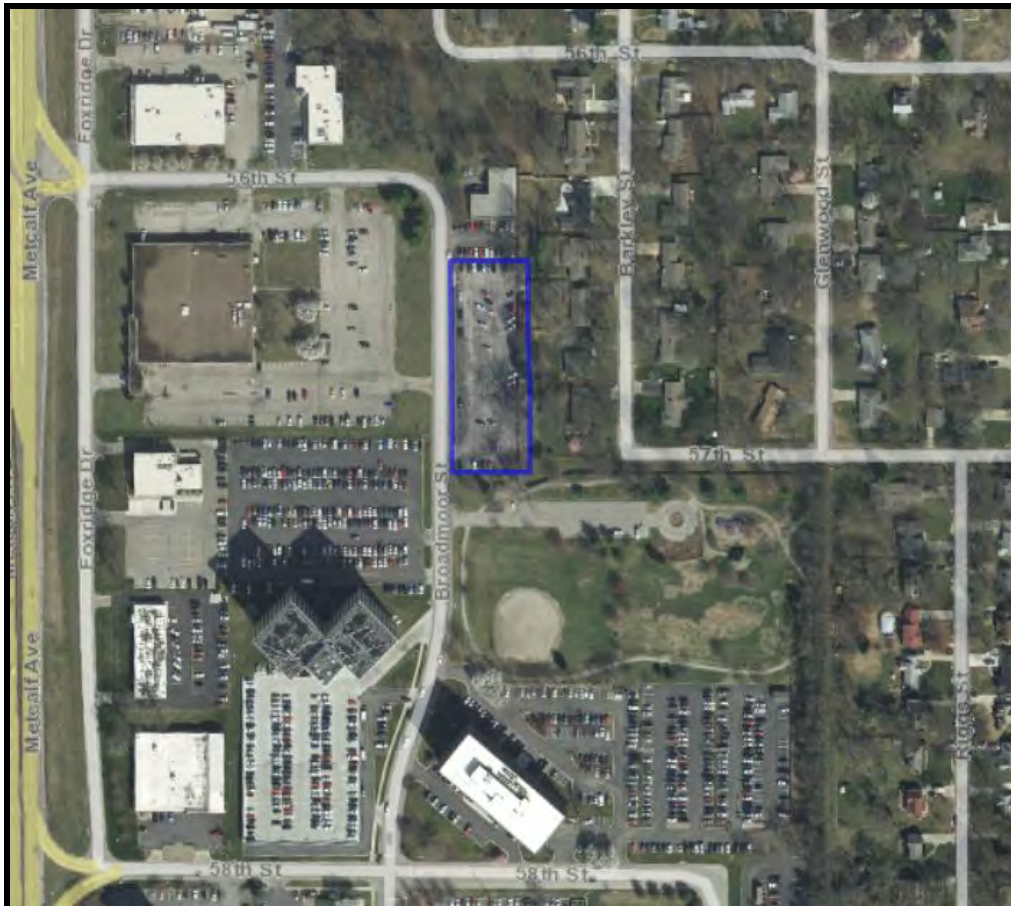
REQUEST: Special Use Permit (SUP) for satellite/off-site parking lot

LOCATION: 5655 Broadmoor St
Immediately north of the Broadmoor Park entrance on Broadmoor Street- Parcel KF251208-1033

APPLICANT: Mike Osbourn
Kaw Valley Engineering, Inc
14700 W 114th Ter
Lenexa, KS 66215

PROPERTY OWNER: CAPROCQ KC Mission LLC
1 Allied Dr, Ste 1500
Little Rock, AR, 72202

STAFF CONTACT: Danielle Sitzman
ADVERTISEMENT: 1/2/2018 - The Legal Record Newspaper
PUBLIC HEARING: 1/22/18 - Planning Commission



Property Information:

The subject property is located in the West Gateway District and is adjacent to several office buildings. The property is currently developed as a surface parking lot with approximately 117 stalls. It has been a paved parking lot for many years, showing on Johnson County Land Records since the early 1990's. Neither it nor the surrounding properties have ever been platted. Prior to its development it was located in an area of undeveloped farm/ranch lands adjacent to the Missouri and Kansas Interurban Railway otherwise known as the Strang Line. The Strange Line was an interurban trolley line running from Kansas City, Missouri through Downtown Overland Park, to Olathe, Kansas.



The Comprehensive Plan indicates this area is appropriate for medium density residential and parks or trails. The West Gateway Form Based Code also designates this area as an extension of Broadmoor Park to serve as a greenway for future residents and visitors.

Surrounding properties are zoned and developed as follows:

- North: "C-0" Office Building District, 1-story office building
- East: "R-1" Single-family Residential District, detached single-family homes
- West: "MP" Industrial Park District, unoccupied former warehouse/office building
- South: "CP-0" Planned Office, a public park

Background:

The subject property was purchased in November of 2015 by CAPROCQ KC Mission LLC at the same time that they purchased the Mission Towers building at 5700 Broadmoor Street. Mission Towers is a 10-story, 245,000 square foot office building with multiple tenants. It was built in 1971. The current zoning standard requires approximately 695 parking stalls for this use. A review of aerial imagery for the site indicates approximately 595 parking stalls are currently provided on site in both a 2-level parking structure and a surface parking lot. It is not known what the parking standard was at the time of the development of the property. Over the last five years, occupancy of the building has increased with new tenants on multiple floors.

Purchase of the off-site parking lot came to staff's attention in the fall of 2016 when the property manager for the new property owner made inquiries about making improvements to the site lighting. Staff determined that a Special Use Permit would first need to be approved before a building permit could be issued. Off-site parking is not an allowable use in the zoning district where this property is located and is specifically called out in Section 445.180 of the Municipal Code as a designated use for a Special Use Permit. At this time, the property owner wishes to obtain a Special Use Permit to continue to use the parking lot for employee parking and to make improvements to the

site for its safe operation. A site plan showing these improvement has been submitted. As stated, this would qualify as off-site or satellite parking, and a Special Use Permit is required.

The submitted site plan shows restriping of the parking lot to accommodate 86 parking stalls. Other improvements include installation of a sidewalk and crosswalk to accommodate pedestrians accessing the satellite lot from the Mission Towers building, street trees, landscaping and screening, and bollard-style lights. The property owner is proposing only to use 42" tall bollards for lighting. The basketball hoop would be removed. There is an existing City maintained wood privacy fence installed along the east side of the parking field. The fence would remain.

Staff has reviewed the proposed site plan for compliance with current zoning standards and future Form Based code compliant public improvements. The design of the parking lot and stormwater controls have also been reviewed by the City's on-call engineers at Olsson Associates. The exact location of the crosswalk is yet to be determined. The applicant has complied with all staff comments on the safety and design of the proposed use.

Code review:

Under Municipal Code Section 445.180.C The Planning Commission and City Council may designate such other uses as appropriate for a special use permit upon a finding that the use is appropriate in a certain location but is not listed as allowed in any district or is only allowed in a district which contains other uses inappropriate in this subject location.

According to Section 445.190 special uses may be approved by action of the City Council after recommendation from the Planning Commission. Special uses may be approved with conditions including, but not limited to, the following:

1. Requirements for special yards, open spaces, density, buffers, fences, walls and screening.
2. The installation of landscaping and maintenance.
3. Provisions for erosion control.
4. Limitations on ingress and egress movements into and out of the site and traffic circulation.
5. Limitation on signage.
6. Limitation on hours of operation and other characteristics of operation.
7. Conditions specifically listed under the individual special use.
8. Other conditions deemed necessary to ensure compatibility with surrounding land uses.

In addition, Section 445.220 of the Municipal Code states that special use permits generally run with the use of the property. Conditions placed must be clearly spelled out in the motion for approval. Section 445.230 of the City Code allows for the termination of a SUP at any time for several reasons. This includes non-compliance with any special conditions placed or if conditions in the neighborhood have changed to

the extent that approval of the permit would be clearly unwarranted if being applied for at the time of revocation.

Special Use Permit:

Section 440.140.E, Criteria for Considering (SUP) applications, lists the criteria to be used by the Planning Commission and City Council in the consideration of this application. An evaluation of these criteria is as follows:

1. The character of the neighborhood.

The subject property is located in the West Gateway District on Broadmoor Street directly north of Broadmoor Park. It is adjacent to several high-rise offices with large surface parking lots, small offices, a vacant industrial building, and a single-family residential neighborhood. There is no direct connection to the existing residential neighborhood and an existing privacy fence runs the length of the boundary between these uses. The property is located in Block E of the Form Based Code and intended to support the surrounding uses either as a greenway or medium density residential. The Form Based Code intends for adjacent properties to front onto Broadmoor Street and for future commercial and residential development. The property has been used as a small surface parking lot for many years.

The proposed use is an existing use and would not be out of character with the existing neighborhood. Future redevelopment would discourage new surface parking lots in favor of structured parking.

2. The zoning and uses of nearby properties, and the extent to which the proposed use would be in harmony with such zoning and uses.

Nearby properties are zoned for office or industrial use similarly to the subject property. The residential uses are separated by a fence. The proposed use would be an extension of an existing use and only involves minor improvements to the site. It does not preempt a future compliant use.

The proposed use is an extension of an existing use to a different owner. It would not generate substantial additional activity in the area.

3. The suitability of the property for the uses to which it has been restricted under the applicable zoning district regulations.

The property is suitable for the uses to which it has been restricted under the applicable zoning district regulations and changes to its zoning regulations are not appropriate at this time.

4. The extent to which approval of the application would detrimentally affect nearby properties.

An established pattern of traffic and foot traffic already exists in the neighborhood. Allowing the use would help ensure adequate parking for office tenants does not otherwise impact the surrounding neighborhood in the short term. The proposed

use would add 86 parking stalls to the approximate 595 parking stalls on site at Mission Towers.

Approval of the application is not expected to detrimentally affect nearby properties.

5. The length of time the property has remained vacant as zoned.
The property is not vacant and future redevelopment is still possible.
6. The relative benefit to the public health, safety and welfare by retaining applicable restrictions on the property as compared to the destruction of the value of the property or hardship to the owner association with denying its request.
Retaining the existing restrictions on the property would limit its use to a parking lot for office tenants at the office to the north or force development of the parcel. Continued use of the property as parking does not undermine the Form Based Code until such time as other parcels in the vicinity are available to assemble for redevelopment. Furthermore, the proposed improvements to the property are not so substantial that they could not be removed for future redevelopment. Also, stipulations proposed by staff include the dedication of right-of-way to ensure future plans for public improvements can proceed.

There is less relative benefit to the public in retaining the applicable restrictions on the property than allowing for the granting of the Special Use Permit.

7. The master plan or comprehensive plan.
The proposed use does not conform with the long term vision of the Future Land Use section of the Comprehensive Plan or the Sector Plan of the Form Based Code. However, the use requires only minor improvements to be made which in the long-term does not compromise the ability of the property to become compliant in the future.
8. The extent to which the proposed use would adversely affect the capacity or safety of that portion of the road network influenced by the use, or present parking problems in the vicinity of the property.
The proposed use will not generate substantially more traffic than the existing use already does as the number of parking stalls will be reduced from 117 to 86 and will serve the same or similar tenants.
9. The recommendation of the professional staff.
The site plan has been reviewed by staff and the City's on-call engineer for compliance with zoning, design, and engineering standards.

Staff recommends approval of the requested use with the stipulations listed below.

10. The extent to which utilities and services, including but not limited to, sewers, water service, police and fire protection, and parks and recreation facilities, are available and adequate to serve the proposed use.

All utilities and services are in place and are adequate to serve the proposed use. Public improvements to sidewalks, crosswalks and street trees will be made by the applicant as a condition of approval.

11. The extent to which the proposed use would create excessive stormwater runoff, air pollution, water pollution, noise pollution or other environmental harm.
Improvements will reduce the amount of impervious surface.

The proposed use will not result in any additional or excessive stormwater runoff or any form of pollution as the site is already developed.

12. The extent to which there is a need for the use in the community.
Over the past five years, occupancy rates in the surrounding office buildings have generally increased. In addition, illegal off-site parking on vacant properties has been enforced as those properties prepare for redevelopment. The applicant is not interested in making significant improvements to their existing on-site parking to accommodate additional vehicles.

The property owner has indicated that their tenants need additional employee parking.

13. The economic impact of the proposed use on the community.
There proposed use will make the leasing of tenant spaces in the adjacent Mission Towers building more desirable to tenants with employees who require parking thus potentially improving rents and property values. No city incentives are being requested by the applicant.

14. The ability of the applicant to satisfy any requirements applicable to the specific use imposed pursuant to the zoning district regulations.
Staff expects the applicant will be able to satisfy any applicable requirements.

Staff Recommendation

Staff recommends the Planning Commission recommend approval of Case #17-13 to the City Council for a Special Use Permit for the use of the subject property as an off-site parking lot for tenants of 5700 Broadmoor Street. The permission would run with the use of the property with the following conditions:

1. Limit the use of the subject property to the parking of vehicles to support the daily employee parking needs of 5700 Broadmoor Street.
2. Require that the on-site and off-site improvements as detailed in the submitted site plans to be substantially completed no later than November 1, 2018.
3. Require the platting the property for the dedication of right-of-way be completed prior to the issuance of any permits for improvements.

Staff Recommendation

The Planning Commission, at their January 22, 2018 meeting, voted 6-0 to recommend approval of Case #17-13 to the City Council for a Special Use Permit for the use of the subject property as an off-site parking lot for tenants of 5700 Broadmoor Street. The permission would run with the use of the property with the following conditions:

1. Limit the use of the subject property to the parking of vehicles to support the daily employee parking needs of 5700 Broadmoor Street.
2. Require that the on-site and off-site improvements as detailed in the submitted site plans to be substantially completed no later than November 1, 2018.
3. Require the platting the property for the dedication of right-of-way be completed prior to the issuance of any permits for improvements.
4. The final location of the crosswalk and sidewalk is to be coordinated with City Staff.

MINUTES OF THE PLANNING COMMISSION MEETING-DRAFT

January 22, 2018

The regular meeting of the Mission Planning Commission was called to order by Chairman Mike Lee at 7:00 PM Monday, January 22, 2018. Members also present: Scott Babcock, Stuart Braden, Brad Davidson, Robin Dukelow, and Frank Bruce. Absent were members Charlie Troppito and Burton Taylor. Also in attendance: Danielle Sitzman, Planning & Development Services Manager, and Ashley Elmore, Secretary to the Planning Commission.

Election of Officers for 2018

Mr. Lee called the meeting to order at 7:00 P.M. The meeting began with election of officers for 2018. Mr. Lee was re-elected as Planning Commission Chair. (Vote was 5-0). Mr. Braden was re-elected as Planning Commission Vice-Chair. (Vote was 5-0). Ashley Elmore was re-elected as Planning Commission Chair. (Vote was 6-0).

Approval of Minutes from the December 18, 2017, Meeting

Ms. Dukelow moved and Mr Babcock seconded to approve the minutes of the December 18, 2017, Planning Commission meeting with corrections, as noted by Mr. Bruce. The vote was taken (6-0) **The motion carried.**

Case # 17-13 Special Use Permit-CAPROCQ Satellite Parking Lot-Public Hearing

Ms. Sitzman, Mission City Planner, appeared before the Planning Commission and provided background information on the parking lot north of Broadmoor Park. She explained that staff was alerted to the use of off-site parking at the site in the fall of 2016 when the property manager inquired about making improvements to the property. Staff then informed them a Special Use Permit would need to be approved before a building permit could be issued, since off-site parking is not an allowable use in the zoning district where this property is located and is specifically called out in Section 445.180 of the Municipal Code.

The property owner wishes to obtain a Special Use Permit to continue to use the parking lot for employee parking and to make improvements to the site for its safe operation. A site plan showing these improvement has been submitted.

Staff recommended the Planning Commission recommend approval of Case #17-13 to the City Council for a Special Use Permit for the use of the subject property as an off-site parking lot for tenants of 5700 Broadmoor Street with the following conditions:

1. Limit the use of the subject property to the parking of vehicles to support the daily employee parking needs of 5700 Broadmoor Street.
2. Require that the on-site and off-site improvements as detailed in the submitted site plans to be substantially completed no later than November 1, 2018.
3. Require the platting the property for the dedication of right-of-way be completed prior to the issuance of any permits for improvements.

MINUTES OF THE PLANNING COMMISSION MEETING-DRAFT

January 22, 2018

Michael Osbourn, Kaw Valley Engineering, appeared before the Planning Commission to give a brief overview of the project and address any questions. He explained that they chose pedestrian-style lighting because the parking lot is directly adjacent to single family homes.

Mr. Davidson inquired whether or not the curblin would be in the city-required easement.

Mr. Osbourn explained that while the curblin is not currently in the right-of-way, once they make all the required improvements, it will be.

The Chairman opened the public hearing, but no one wished to speak. The Chairman closed the public hearing.

Ms. Dukelow made a motion to recommend approval to the City Council with staff recommendations 1-3, as well as an additional recommendation:

4) The final location of the crosswalk and associated pedestrian paths be coordinated with City Staff.

Mr. Babcock seconded the motion. The vote was taken (6-0). **The motion carried.**

PC Comments/CIP Committee Update

Ms. Sitzman gave an overview of the what the CIP Committee was working on. The last CIP meeting covered stormwater improvements, funding, and debt service, projecting out over the next five years. Ms. Sitzman explained the committee will move on to streets for the two next meetings, then move on to parks.

Ms. Dukelow inquired as to whether or not the stormwater channel behind Andersen Park was in the five year plan. Ms. Sitzman explained that there were more pressing erosion concerns farther east in the channel that would take precedence.

Mr. Davidson inquired about the light being out in the parking lot at Starbucks. Ms. Sitzman suggested that if any of the Planning Commission members see anything after hours to report it staff the next day.

Ms. Sitzman also reminded the Planning Commission the National Planning Conference is coming up and the City has budget for two Planning Commissioners to attend.

ADJOURNMENT

With no other agenda items, **Ms. Dukelow moved and Mr. Davidson seconded a motion to adjourn.** (Vote was 6-0). The meeting adjourned at 7:35 P.M.

Mike Lee, Chair

ATTEST:

MINUTES OF THE PLANNING COMMISSION MEETING-DRAFT

January 22, 2018

Ashley Elmore, Secretary

Memo

To: City of Mission
From: Olsson Associates
cc: CAPROCQ KS Mission, LLC
Date: January 10, 2018
Re: Review for CAPROCQ KC Mission Parking Lot at 5700 Broadmoor Street

Olsson Associates comments and recommendations are listed below for the revised planning documents dated January 4, 2018 for the CAPROCQ KS Mission, LLC parking lot renovation project at 5700 Broadmoor Street in Mission. We have reviewed the submittal documents for Stormwater and Traffic concerns.

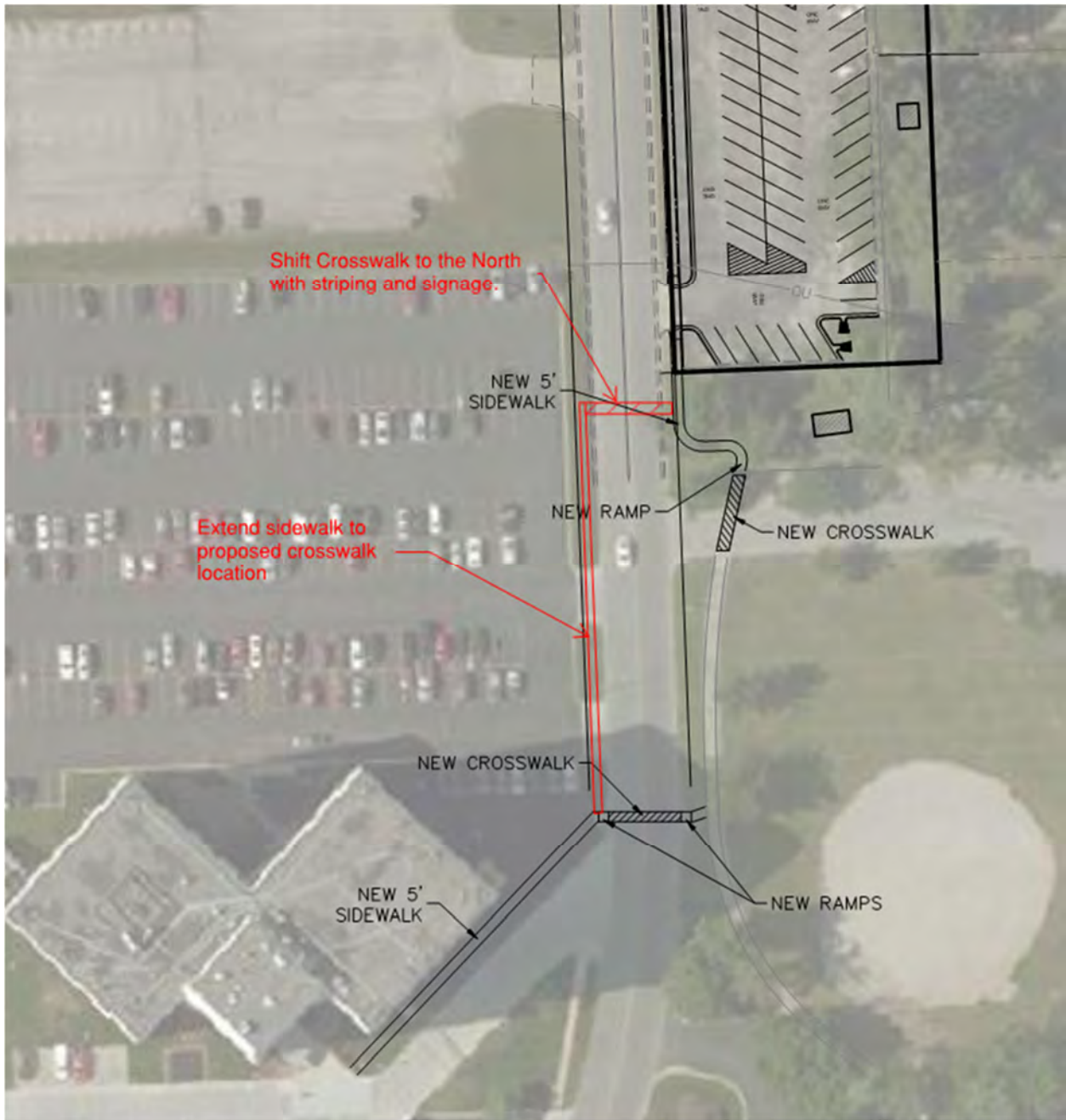
Sheet C100

- The current crosswalk location (crossing Broadmoor St) is close to multiple drives which may conflict with crossing pedestrians.
 - Applicant should consider shifting north to be further from these driveways and be closer to the new parking lot and park entrance. (see attachment for possible schematic)
- Crosswalk location shall have Pedestrian Crossing Warning Signs with flashing beacons installed for both directions of travel. Signs shall have pushbuttons to activate beacons. Advanced warning signage in both directions further upstream may also be required depending on final crosswalk location.
- Crosswalk striping shall be installed per APWA standards.

Sheet SL200 - SL400

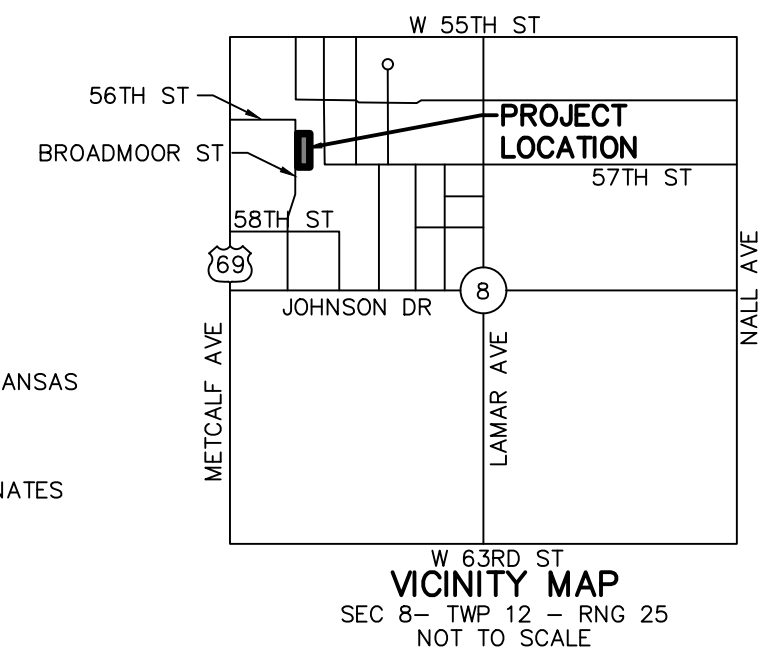
- Lamp output correlated color temperature (CCT) is specified as 4200K. Recommend 3200K or lower CCT adjacent to residences to minimize perceived glare associated with lamps that contain more blue light.

- Site Lighting – The existing parking lot is currently not lighted. Per discussions with the project engineer, the proposed parking lot lighting design is aimed at providing a level of security for the users at night that the ownership is acceptable with while maintaining sensitivity to the residential use to the east. The proposed lighting design does not fully meet industry standards for parking lots in regards to light levels. The owner and project engineer acknowledge this. Should safety or functionality of the parking lot become an issue or concern this can be addressed with the renewal of the Special Use permit.



BOUNDARY/TOPOGRAPHIC SURVEY

SECTION 8, TOWNSHIP 12, RANGE 25 EAST
MISSION, JOHNSON COUNTY, KANSAS



PROPERTY OWNER/ADDRESS

CAPROCC KC MISSION, LLC
5700 BROADMOOR STREET
MISSION, KANSAS 66202

REFERENCE DEEDS:

- SPECIAL WARRANTY DEED, BOOK 201511, PAGE 4756.
- KANSAS WARRANTY DEED, BOOK 200601, PAGE 1284.
- SHERIFF'S DEED, BOOK 4780, PAGE 458.

REFERENCE MAPS:

- WATERONE MAP #057-067

REFERENCE PLATS:

- ALTA VISTA HEIGHTS, BOOK 15, PAGE 41.

NOTES:

- NO TITLE WORK WAS PROVIDED BY THE CLIENT AT THE TIME OF THIS SURVEY AND NO EASEMENT INVESTIGATION WAS PERFORMED.
- CONTAINS 46,509 SQUARE FEET/1.068 ACRES MORE OR LESS.

DESCRIPTION (BOOK 201511, PAGE 4756)

ALL THAT PART OF THE WEST HALF OF THE NORTHWEST QUARTER OF SECTION 8, TOWNSHIP 12, RANGE 25, NOW IN THE CITY OF MISSION, JOHNSON COUNTY, KANSAS, MORE PARTICULARLY DESCRIBED AS FOLLOWS:
BEGINNING AT A POINT ON THE SOUTH RIGHT OF WAY LINE OF 57TH STREET, AS NOW ESTABLISHED, SAID POINT BEING ON THE SOUTHERLY EXTENSION OF THE WEST LINE OF ALTA VISTA HEIGHTS NO. 3, A SUBDIVISION OF LAND NOW IN THE CITY OF MISSION, JOHNSON COUNTY, KANSAS, AND 50 FEET SOUTH OF THE SOUTH LINE OF LOT 14; THENCE NORTHERLY ALONG THE WEST LINE AND ITS EXTENSION OF LOTS 14 THRU 11 INCLUSIVE OF SAID ALTA VISTA HEIGHTS NO. 3, A DISTANCE OF 355 FEET; THENCE WESTERLY, ALONG A LINE 335 FEET NORTH OF AND PARALLEL TO THE WESTERLY EXTENSION OF THE SOUTH RIGHT OF WAY LINE OF SAID 57TH STREET, A DISTANCE OF 131 FEET, TO A POINT ON THE EASTERLY RIGHT OF WAY LINE OF BROADMOOR STREET, AS NOW ESTABLISHED; THENCE SOUTHERLY, ALONG THE EASTERLY RIGHT OF WAY LINE OF SAID BROADMOOR, A DISTANCE OF 355 FEET, TO ITS INTERSECTION WITH THE WESTERLY EXTENSION OF THE SOUTH RIGHT OF WAY LINE OF SAID 57TH STREET; THENCE EASTERLY, ALONG THE WESTERLY EXTENSION OF THE SOUTH RIGHT OF WAY LINE OF SAID 57TH STREET, A DISTANCE OF 131 FEET, TO THE POINT OF BEGINNING, ALL SUBJECT TO THE PART THEREOF DEDICATED FOR STREET PURPOSES.

FLOOD STATEMENT:

SUBJECT PROPERTY IS SHOWN TO BE LOCATED IN ZONE "X" (AREAS DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE FLOODPLAIN) AND SUBJECT PROPERTY IS SHOWN TO BE LOCATED IN ZONE "X" OTHER FLOOD AREAS (AREAS OF 0.2% ANNUAL CHANCE FLOOD; AREAS OF 1% ANNUAL CHANCE FLOOD WITH AVERAGE DEPTHS OF LESS THAN 1 FOOT OR WITH DRAINAGE AREAS LESS THAN 1 SQUARE MILE; AND AREAS PROTECTED BY LEVEES FROM 1% ANNUAL CHANCE FLOOD) AS DEPICTED ON THE FLOOD INSURANCE RATE MAP COMMUNITY PANEL NO. 20091C00236, MAP REVISED AUGUST 3, 2009, CITY OF OVERLAND PARK, JOHNSON COUNTY, KANSAS. LOCATION DETERMINED BY A SCALED GRAPHICAL PLOT OF THE FLOOD INSURANCE RATE MAP.

UTILITY STATEMENT:

THE UNDERGROUND UTILITIES SHOWN HEREON ARE FROM FIELD SURVEY INFORMATION OF ONE-CALL LOCATED UTILITIES, FIELD SURVEY INFORMATION OF ABOVE GROUND OBSERVABLE EVIDENCE, AND/OR THE SCALING AND PLOTTING OF EXISTING UTILITY MAPS AND DRAWINGS AVAILABLE TO THE SURVEYOR AT THE TIME OF SURVEY. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. FURTHERMORE, THE SURVEYOR DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES BY EXCAVATION UNLESS OTHERWISE NOTED ON THIS SURVEY. KANSAS ONE CALL TICKET #17360255 & #17360260 WATERONE TICKET #13183999

HORIZONTAL AND VERTICAL DATUM:

THE COORDINATES SHOWN HEREON ARE GRID COORDINATES BASED ON THE KANSAS STATE PLANE NORTH ZONE (NAD 83-HARN) (NAVD 88)
CAF: 0.999922619
1 METER = 3.28083333 U.S. SURVEY FEET
GROUND COORDINATES X COMBINED ADJUSTMENT FACTOR (CAF) = GRID COORDINATES SCALED AROUND 0,0

SHAWNEE 2/BM #1
NORTHING: 263968.29 (GRID)
EASTING: 2251815.46 (GRID)
ELEV = 1006.06

DATUM BENCH MARK:

SHAWNEE 2
NCS MONUMENT SET IN CONCRETE
N: 263968.29
E: 2251815.46
ELEV = 1006.06

NCS WY50

N: 276062.45
E: 2253225.89
ALUMINUM CAP IN MONUMENT BOX IN CENTER OF INTERSECTION.
ELEV = 969.09

PROJECT BENCH MARK:

BM-1
CHISELED SQUARE CUT ON TOP OF CONCRETE CURB & GUTTER, WEST SIDE OF BROADMOOR, APPROXIMATE 42' WEST OF NORTHWEST CORNER OF PROPERTY.
ELEV = 1049.25

BM-2
CHISELED SQUARE CUT ON TOP OF CONCRETE CURB & GUTTER, WEST SIDE OF BROADMOOR, APPROXIMATE 42' WEST OF SOUTHWEST CORNER OF PROPERTY.
ELEV = 1046.55

PROJECT CONTROL: (GROUND)

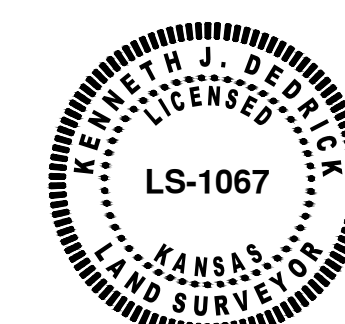
CP #200
1/2"x24" REBAR W/ CONTROL CAP
NORTHING: 270016.743
EASTING: 2259622.577
ELEV = 1049.30

CP #201
1/2"x24" REBAR W/ CONTROL CAP SET
NORTHING: 269864.853
EASTING: 2259627.536
ELEV = 1048.85

CP #202
1/2"x24" REBAR W/ CONTROL CAP SET
NORTHING: 269870.144
EASTING: 2259635.618
ELEV = 1047.69

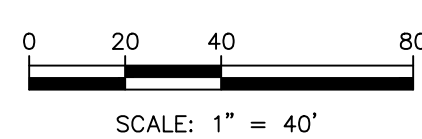
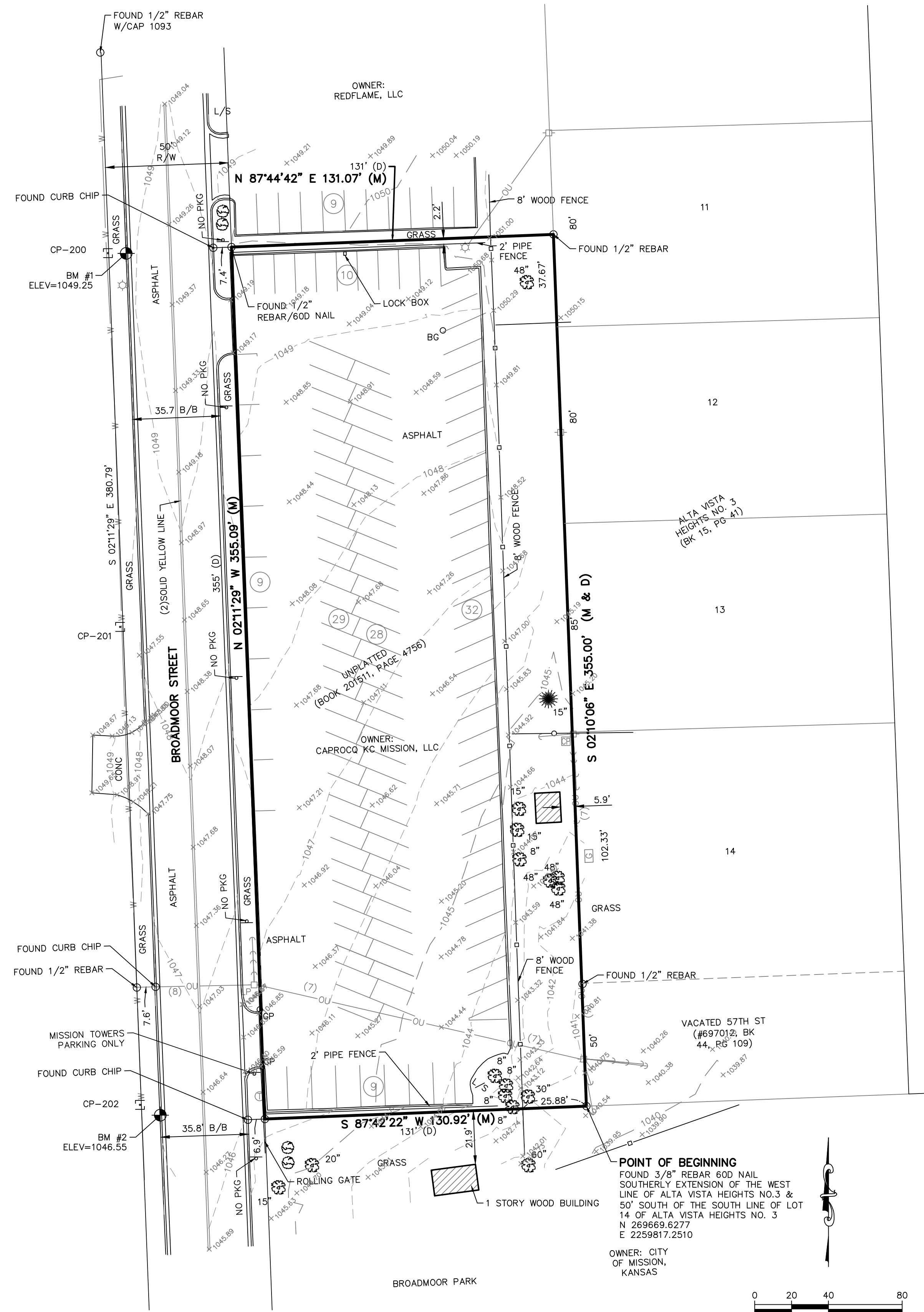
SURVEYOR'S CERTIFICATION:

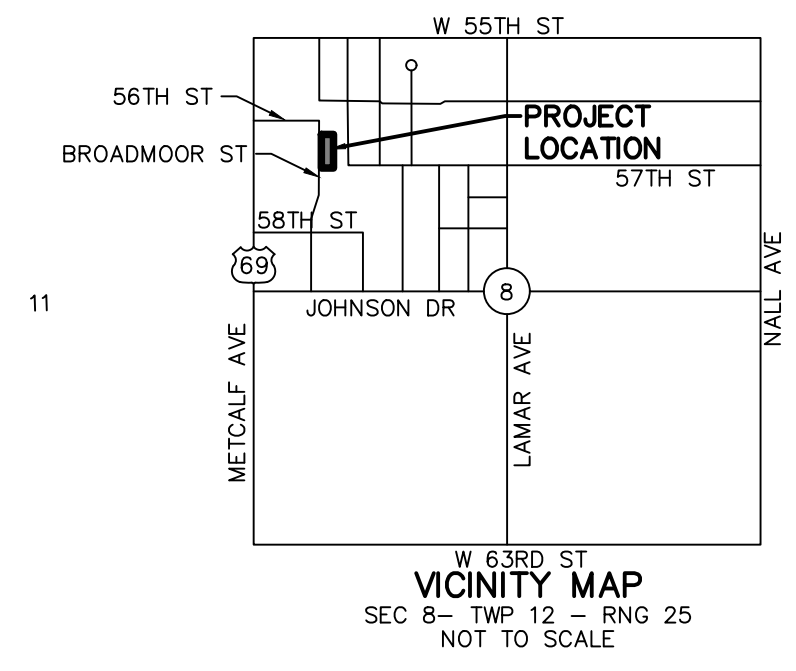
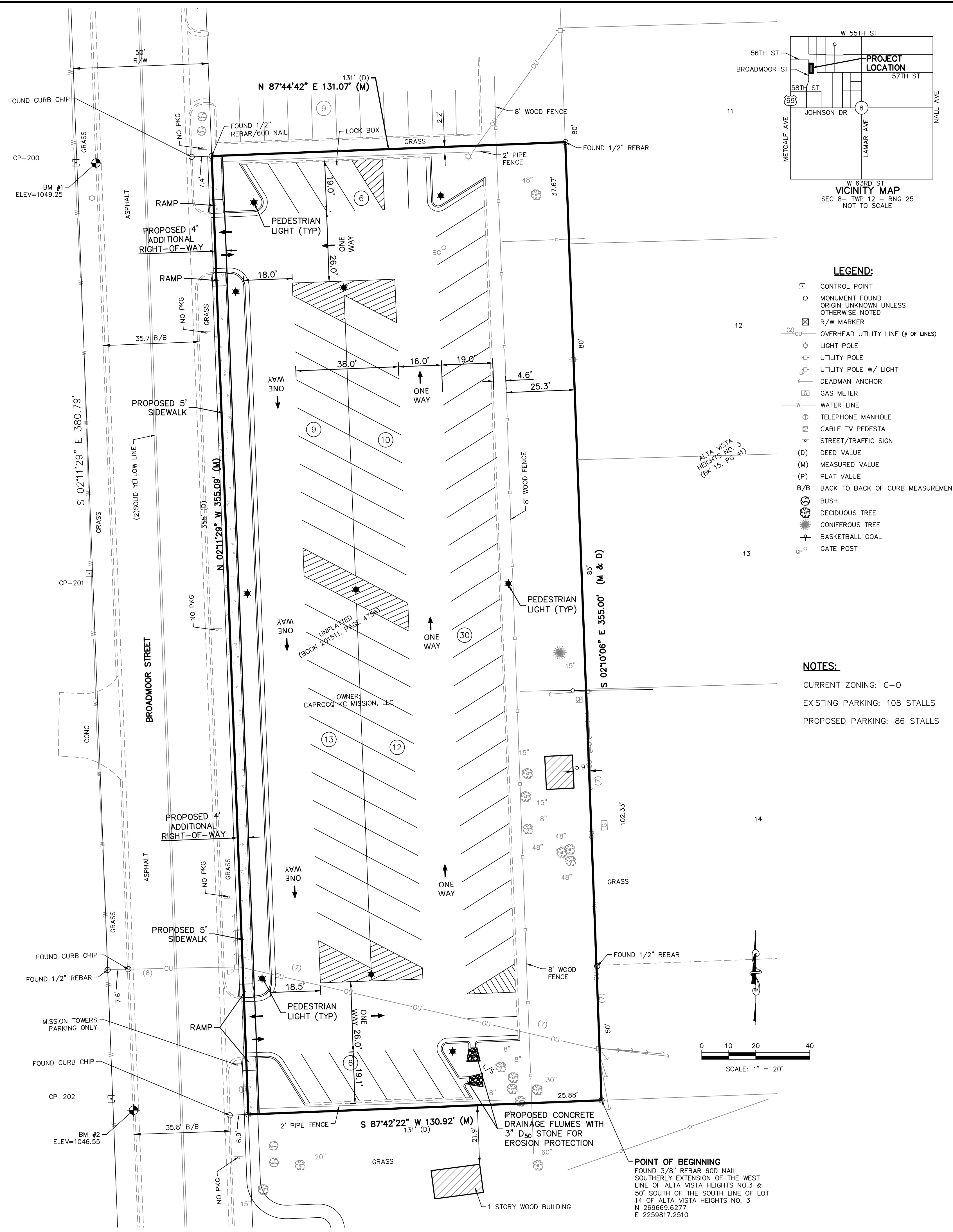
I, KENNETH J. DEDRICK, BEING A DULY REGISTERED AND LICENSED LAND SURVEYOR IN THE STATE OF KANSAS, HEREBY CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT WAS BASED WAS MADE BY ME OR UNDER MY DIRECT SUPERVISION, AND MEETS OR EXCEEDS THE CURRENT KANSAS MINIMUM STANDARDS FOR BOUNDARY SURVEYS. THAT THE LINES OF POSSESSION ARE DEPICTED HEREON, THE COURSES AND DISTANCES SHOWN HEREON ARE THOSE MEASURED ON THE DATE OF THE SURVEY AND THE TOPOGRAPHIC SURVEY WAS PERFORMED TO MEET OR EXCEED THE STANDARDS OF PRACTICE ADOPTED BY THE NATIONAL SOCIETY OF PROFESSIONAL SURVEYORS FOR TOPOGRAPHIC SURVEYS AND THAT THE SURVEY WAS COMPLETED IN THE FIELD AND ON THE GROUND AND MAY BE RELIED UPON BY THE PARTIES CERTIFIED AS TO BEING CORRECT TO THE BEST OF MY BELIEF AND KNOWLEDGE.
THE FIELD WORK WAS COMPLETED ON AUGUST 16, 2017.
DATE OF PLAT OR MAP: SEPTEMBER 1, 2017



KENNETH J. DEDRICK
KANSAS PS NO. 1067
dedrick@kveng.com

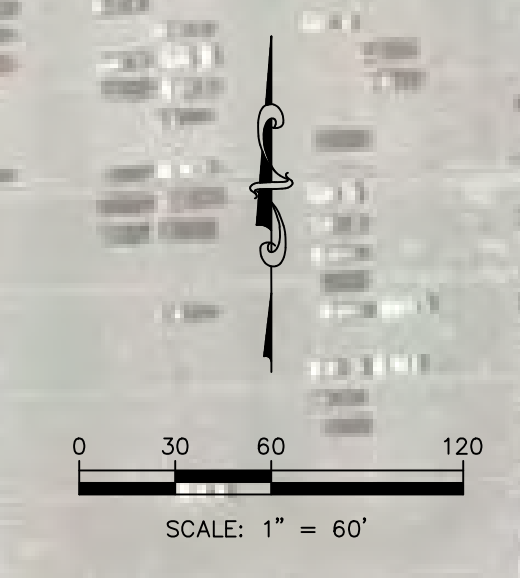
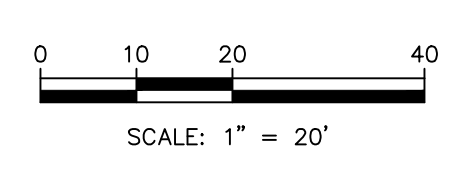
	14700 WEST 114TH TERRACE LENEXA, KANSAS 66215 PH. (913) 894-5150 FAX (913) 894-5977 lx@kveng.com www.kveng.com	PROJECT NO. C17_9526 DRAWN BY EDV CHECKED BY KJD CFN 9526TOPO SHEET 1 OF 1
	PREPARED FOR: COLLIERS INTERNATIONAL 1 ALLIED DRIVE, SUITE #1500 LITTLE ROCK, ARKANSAS 72202	
KAW VALLEY ENGINEERING, INC., IS AUTHORIZED TO OFFER SURVEYING SERVICES BY KANSAS STATE CERTIFICATE OF AUTHORIZATION NO. LS-20. EXPIRES 12/31/18		





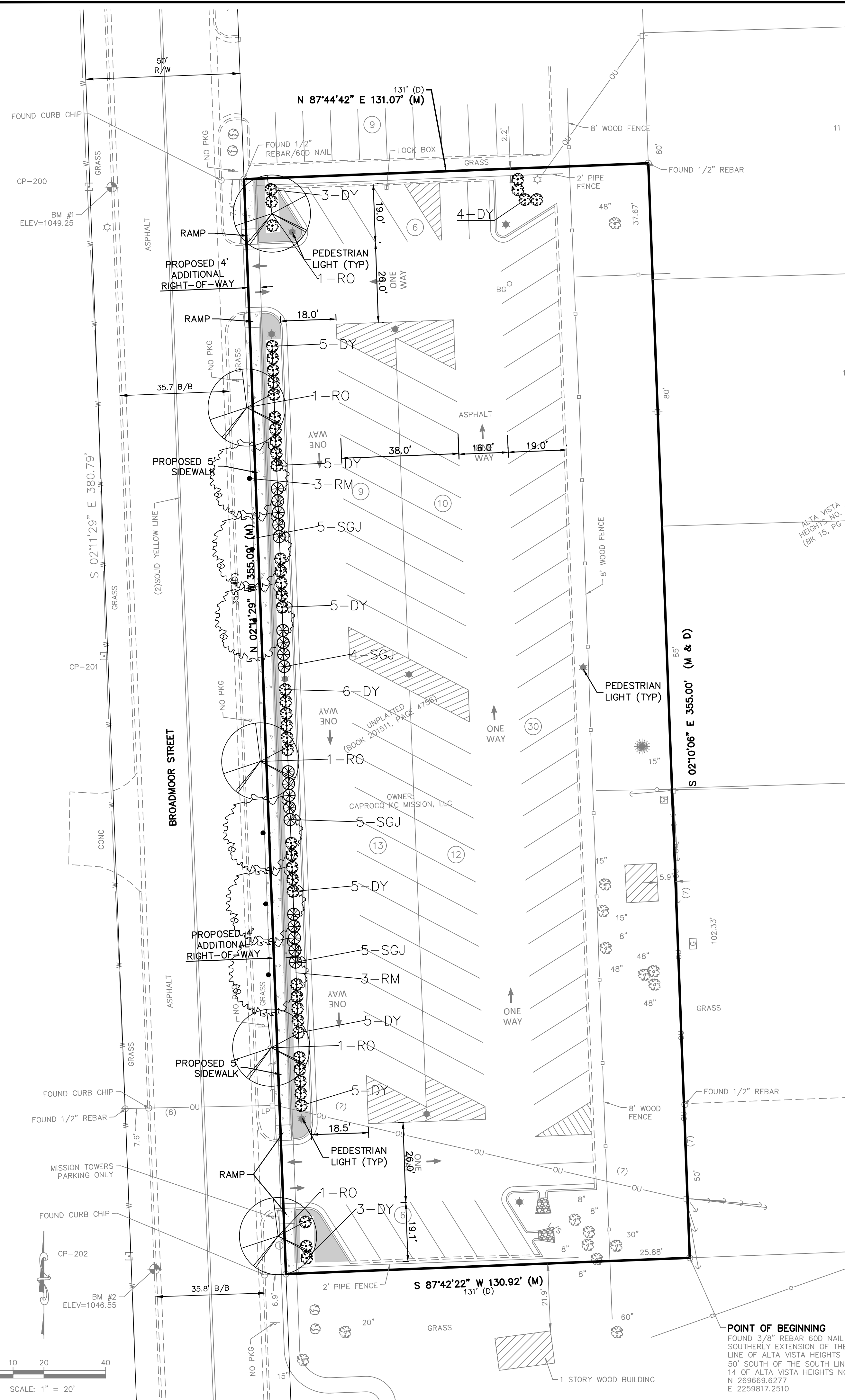
- LEGEND:**
- CONTROL POINT
 - MONUMENT FOUND ORIGIN UNKNOWN UNLESS OTHERWISE NOTED
 - ⊗ R/W MARKER
 - (2)— OVERHEAD UTILITY LINE (# OF LINES)
 - ☆ LIGHT POLE
 - ⊕ UTILITY POLE
 - ⊕ UTILITY POLE W/ LIGHT
 - ⊕ DEADMAN ANCHOR
 - ⊕ GAS METER
 - WATER LINE
 - ⊕ TELEPHONE MANHOLE
 - ⊕ CABLE TV PEDESTAL
 - ⊕ STREET/TRAFFIC SIGN
 - (D) DEED VALUE
 - (M) MEASURED VALUE
 - (P) PLAT VALUE
 - B/B BACK TO BACK OF CURB MEASUREMENT
 - ⊕ BUSH
 - ⊕ DECIDUOUS TREE
 - ⊕ CONIFEROUS TREE
 - ⊕ BASKETBALL GOAL
 - ⊕ GATE POST

- NOTES:**
- CURRENT ZONING: C-0
 - EXISTING PARKING: 108 STALLS
 - PROPOSED PARKING: 86 STALLS

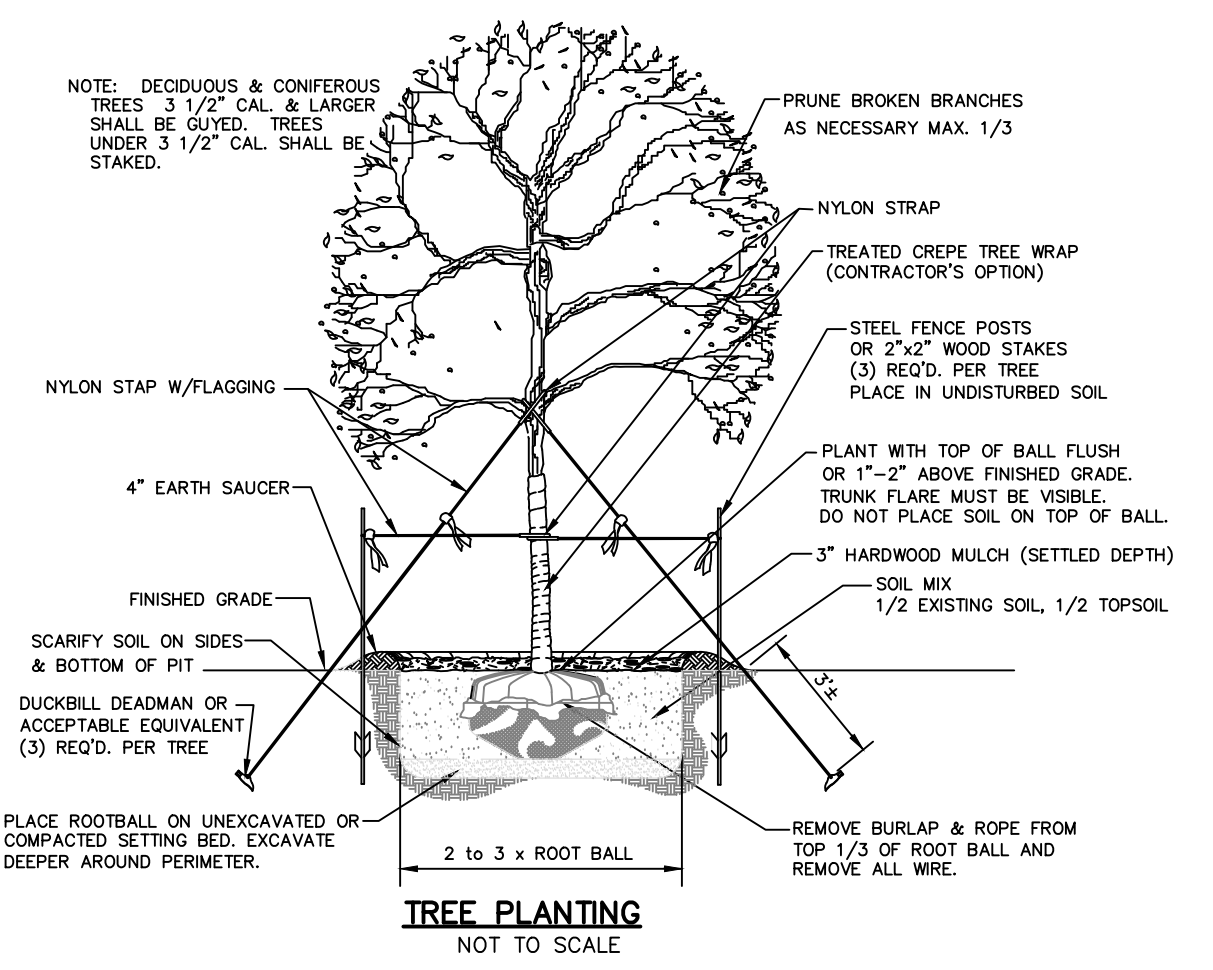
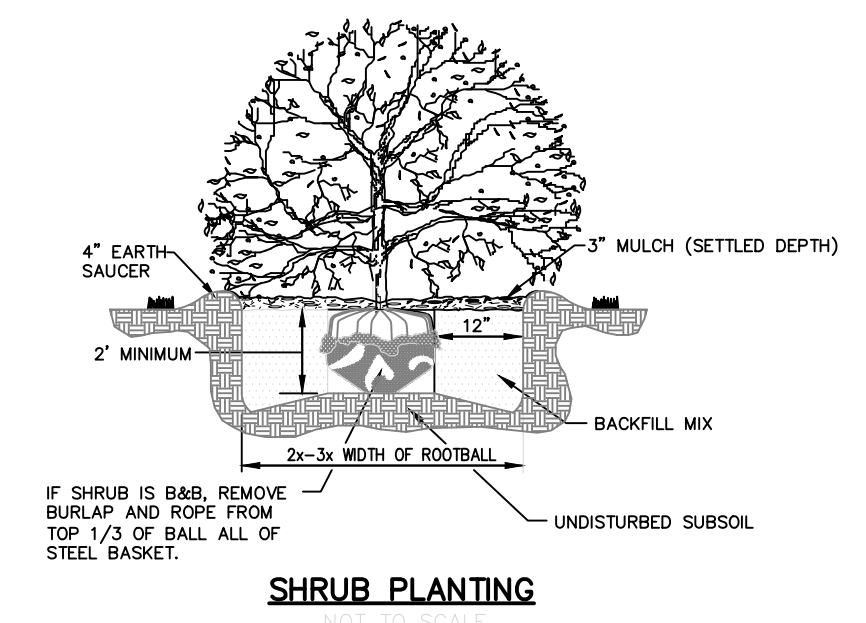
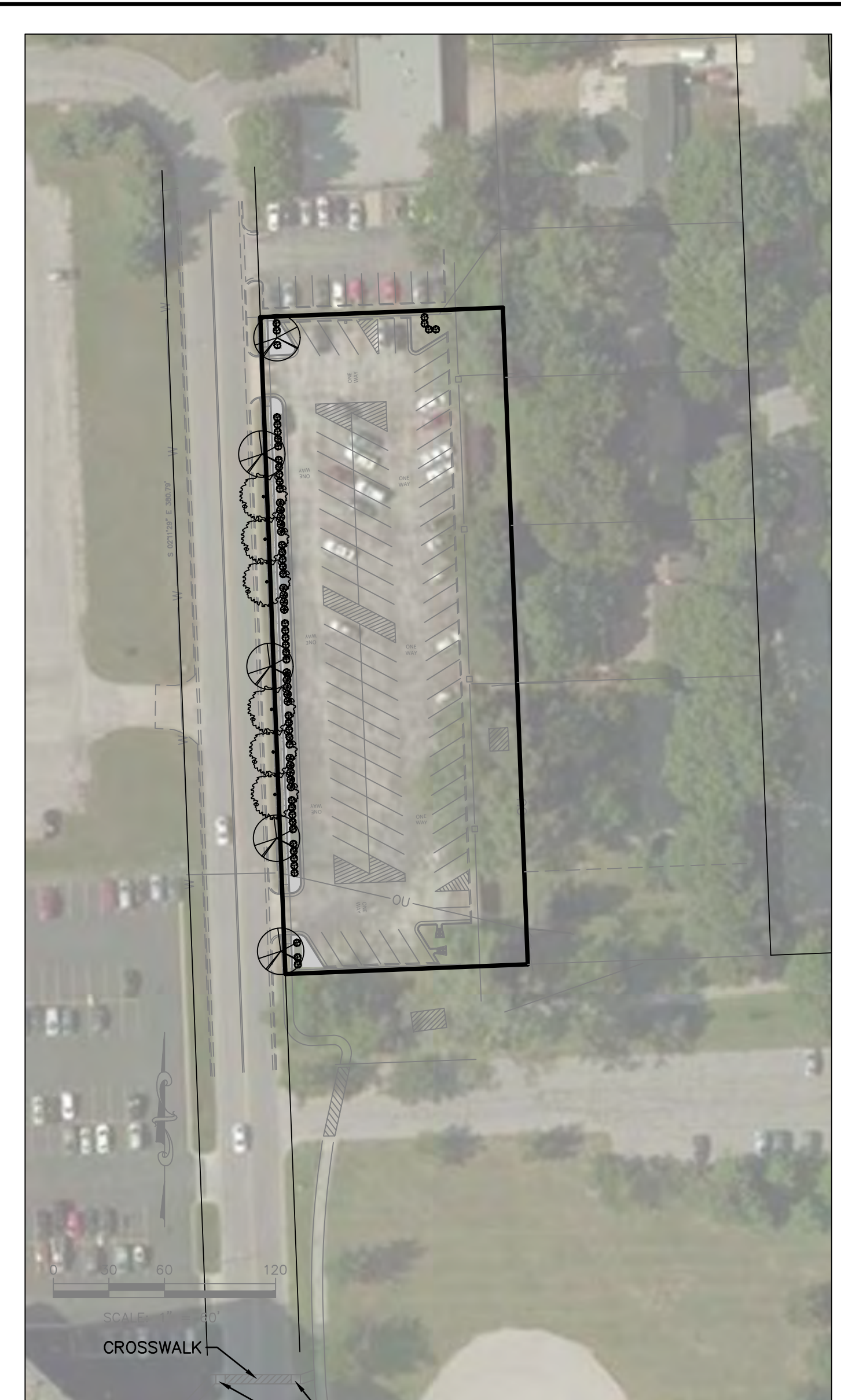


<p>CAPROCQ KC MISSION, LLC 5700 BROADMOOR STREET MISSION, KANSAS 66202</p> <p>PROJ. NO. C17_9526 DESIGNER MRO CFN 9526SP SHEET C100</p>	<p>KAW VALLEY ENGINEERING KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY KANSAS STATE CERTIFICATE OF AUTHORIZATION # E-113. EXPIRES 12/31/18</p> <p>14700 WEST 114TH TERRACE LENEKA, KANSAS 66215 PH. (913) 894-9150 FAX (913) 894-5977 kawvalley.com www.kawvalley.com</p> <p>REV 1 DATE 1/4/18 DESCRIPTION PER CITY COMMENT</p> <p>REV 0 DATE 9/7/17 DESCRIPTION INITIAL ISSUE</p> <p>REV DSN DATE DWN DESCRIPTION CHK</p>
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THIS DRAWING SHALL NOT BE UTILIZED BY ANY PERSON, FIRM, OR CORPORATION IN WHOLE OR IN PART WITHOUT THE SPECIFIC PERMISSION OF KAW VALLEY ENGINEERING, INC.



- LANDSCAPING NOTES:**
1. LOCATE ALL UTILITIES BEFORE LANDSCAPE CONSTRUCTION BEGINS.
 2. NOTIFY OWNER REPRESENTATIVE OF ANY LAYOUT DISCREPANCIES.
 3. ALL EXTERIOR GROUND WITHIN THE LIMITS OF THE CONTRACT, EXCEPT FOR SURFACES OCCUPIED BY BUILDINGS, STRUCTURES, PAVING, AND AS DIRECTED ON THE DRAWINGS AS UNDISTURBED, SHALL BE FILLED WITH SIX INCHES (6") OF TOPSOIL.
 4. ALL DISTURBED AREAS NOT DESIGNATED FOR OTHER PLANTING SHALL BE SODDED. SOD SHALL CONSIST OF 90% TURF TYPE TALL FESCUE 10% BLUEGRASS.
 5. WEED MAT SHALL BE USED UNDER ALL PLANTING AREAS NOT TO BE SODDED OR AS DIRECTED ON THE DRAWINGS. THE MAT SHALL BE COVERED WITH MULCH AND SECURED IN-PLACE BY A SOIL ANCHOR.
 6. QUANTITIES INDICATED IN PLANT LIST ARE FOR CONVENIENCE ONLY. CONTRACTOR IS RESPONSIBLE FOR PLANT QUANTITIES AS ILLUSTRATED ON THE PLAN.
 7. SHREDDED HARDWOOD MULCH SHALL BE USED AS THREE INCH (3") TOP DRESSING IN ALL PLANT BEDS AND AROUND ALL TREES. SINGLE TREES OR SHRUBS SHALL BE MULCHED TO THE OUTSIDE EDGE OF SAUCER OR LANDSCAPE ISLAND (SEE PLANTING DETAILS).
 8. PLACE STEEL EDGING AROUND ALL LANDSCAPE BEDS.
 9. STEEL EDGING SHALL BE 1/8" x 4" WITH CLIPS AND STAKES FIVE FEET(5') ON CENTER MIN.
 10. FERTILIZE ALL PLANTS AT THE TIME OF PLANTING WITH TIME-RELEASE FERTILIZER (3-4 SLOW-RELEASE TABLETS/PELLETS) AND APPLY MYCORRHIZAE TREATMENT TO EACH ROOT BALL.
 11. IF LEANING OCCURS WITHIN ONE YEAR, TREES SHALL BE RE-STAKED (SEE PLANTING DETAILS).
 12. CONTRACTOR SHALL STAKE ALL PLANT MATERIALS PRIOR TO INSTALLATION FOR THE PURPOSE OF DETERMINING CONFLICTS WITH ROCK, UTILITIES, ETC. NO PLANTS CAN BE PLANTED DIRECTLY ON ROCK OR UTILITIES. NOTIFY ARCHITECT/ENGINEER/OWNER AT ONCE IF ANY CONFLICTS OCCUR. CONTRACTOR WILL BE REQUIRED TO ADJUST PLANT LOCATIONS AT NO ADDITIONAL COST.
 13. CONTRACTOR IS RESPONSIBLE FOR WATERING ALL SOD UNTIL ROOTS HAVE KNITTED INTO SOIL AND OWNER HAS OCCUPIED THE BUILDING.
 14. PROVIDE "GATOR" BAGS ON ALL TREES. REFILL AS NECESSARY UNTIL OWNER OCCUPIES THE BUILDING. BAGS TO REMAIN FOR USE BY OWNER.



PLANT SCHEDULE				
KEY	QTY.	BOTANICAL NAME	COMMON NAME	SIZE/REMARKS
TREES				
RM	6	ACER RUBRUM 'AUTUMN BLAZE'	AUTUN BLAZE RED MAPLE	2.5" CAL. B&B
RO	5	QUERCUS RUBRA	RED OAK	2.5" CAL. B&B
SHRUBS/GRASSES/GROUNDCOVER				
SGJ	19	JUNIPERUS 'CHINENSIS' 'SEA GREEN'	SEA GREEN JUNIPER	30" HT.
DY	46	TAXUS x MEDIA 'DENSIFORMIS'	DENSIFORMIS YEW	30" HT.

NOTES:
 EXISTING PARKING: 108 STALLS
 PROPOSED PARKING: 86 STALLS

	REQUIRED				EXISTING		PROVIDED	
	TREE	SHRUB	TREE	SHRUB	TREE	SHRUB	TREE	SHRUB
BROADMOOR ROAD (355 LF)	TBD		0				11	
PARKING - PERIMETER								
CONTINUOUS HEDGE				0				SEE PLAN

1	01/03/18	CITY COMMENTS	VSR	VSR	VSR	CHK
0	11/13/17	CITY SUBMITTAL	VSR	VSR	VSR	DSN
REV	DATE	DESCRIPTION				

14700 WEST 114TH TERRACE
 LENEXA, KANSAS 66215
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 kawvalley.com | www.kawvalley.com

KAW VALLEY ENGINEERING
 KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY KANSAS STATE CERTIFICATE OF AUTHORIZATION # E-113. EXPIRES 12/31/18

CAPROCQ KC MISSION, LLC
 5700 BROADMOOR STREET
 MISSION, KANSAS 66202

SITE PLAN

PROJ. NO. C17_9526
 DESIGNER VSR DRAWN BY VSR
 CFN 9526LP
 SHEET L100 REV 00

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GENERAL NOTES:

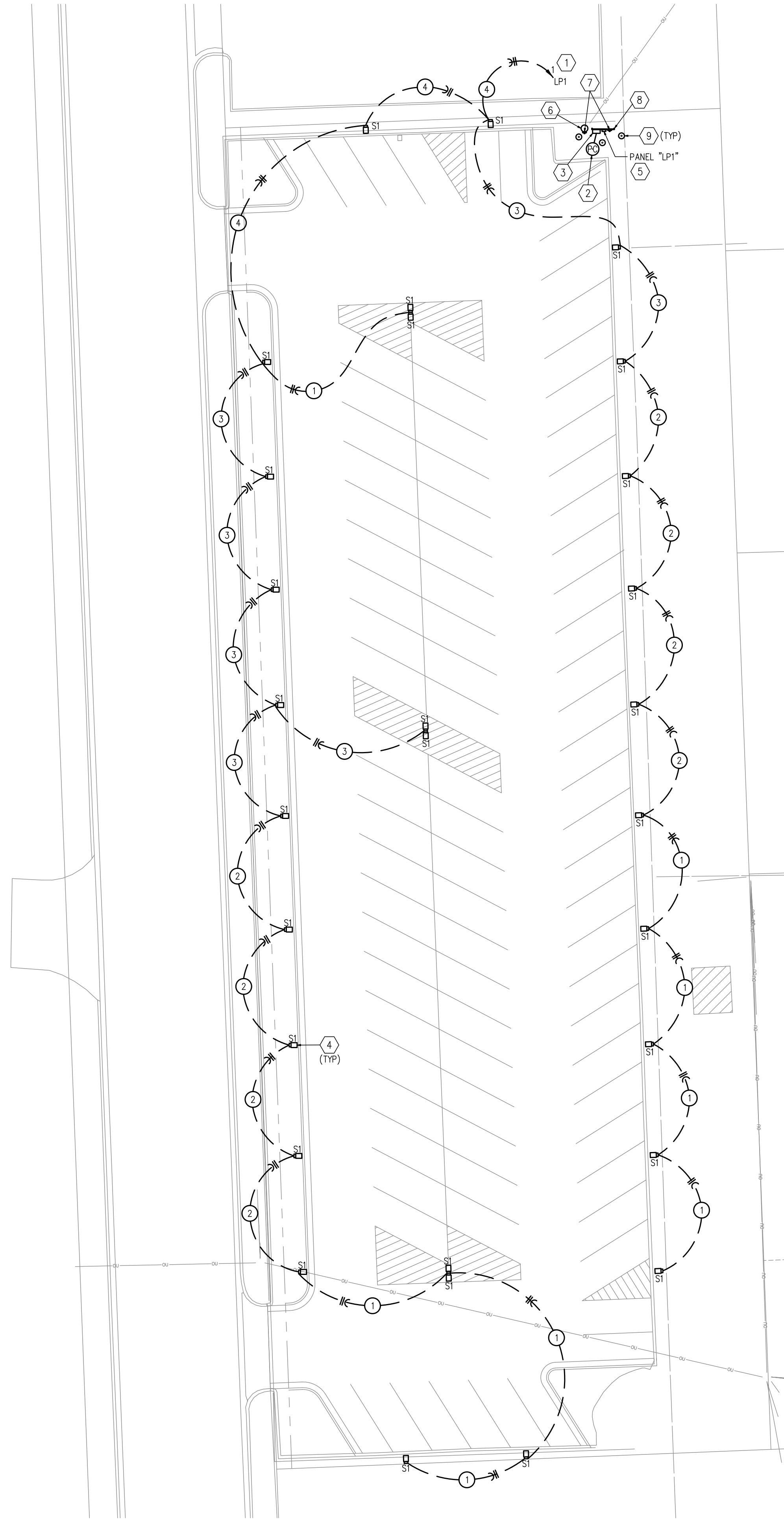
- 1) REFER TO CIVIL DRAWINGS FOR ADDITIONAL SITE INSTALLATIONS.
- 2) REFER TO SHEET SL400 FOR GENERAL NOTES, ABBREVIATIONS AND SCHEDULES.
- 3) REFER TO SHEET SL300 FOR ELECTRICAL DETAILS.

PLAN NOTES:

- ① ROUTE LUMINAIRE HOMERUN CIRCUIT THROUGH THE LIGHTING CONTACTOR PANEL "LCP" AND THEN TO PANEL "LP1".
- ② PROVIDE PHOTOCELL "PC" MOUNTED ON THE NEW EQUIPMENT UNISTRUT RACK. MOUNT AT A MINIMUM OF 8'-0" ABOVE GRADE AND AIMED IN A NORTHERN DIRECTION. REFER TO SHEET SL300 FOR ADDITIONAL INFORMATION AND CONNECTIONS.
- ③ PROVIDE LIGHTING CONTROL PANEL "LCP" ON THE NEW EQUIPMENT UNISTRUT RACK. CONTRACTOR SHALL PROVIDE ALL NECESSARY DEVICES, CONDUIT, WIRING, ETC. AS REQUIRED FOR THE WIRING DETAIL SHOWN ON SHEET SL300. LIGHTING CONTROLS SHALL BE MOUNTED IN A HINGED, LOCKABLE, STEEL NEMA 3R ENCLOSURE.
- ④ NEW PEDESTRIAN TYPE LUMINAIRE TO BE INSTALLED IN APPROXIMATE LOCATIONS SHOWN. REFER TO THE LUMINAIRE BASE DETAIL ON SHEET SL300. LUMINAIRE SHALL BE ORIENTED IN A DIRECTION PARALLEL OR PERPENDICULAR TO THE PARKING LOT CURBS. LUMINAIRE PROTECTION SHALL BE PROVIDED TO HELP PREVENT DAMAGE FROM VEHICLES.
- ⑤ PROVIDE 30AMP, 240V, 1 PHASE, 8 CIRCUIT PANELBOARD MOUNTED ON THE ELECTRICAL EQUIPMENT UNISTRUT RACK. REFER TO THE PANELBOARD SCHEDULE SHOWN ON SL400 FOR PANELBOARD INFORMATION.
- ⑥ DISCONNECT AND REMOVE THE EXISTING LUMINAIRE, POLE AND THE EXISTING POLE BASE. MAINTAIN THE EXISTING UTILITY METER AND CONDUIT AND WIRING SERVING THE METER.
- ⑦ RELOCATE THE EXISTING METER AND FEEDER FROM THE EXISTING POLE TO THE NEW UNISTRUT RACK. EXTEND ALL CONDUITS AND WIRING AS REQUIRED. COORDINATE NEW LOCATION WITH THE UTILITY COMPANY.
- ⑧ PROVIDE NEW GALVANIZED UNISTRUT SUPPORT TO MOUNT NEW ELECTRICAL EQUIPMENT. REFER TO ELECTRICAL EQUIPMENT UNISTRUT RACK DETAIL ON SHEET SL300 FOR ADDITIONAL INFORMATION.
- ⑨ GROUND ROD. REFER TO SHEET SL300 FOR ADDITIONAL REQUIREMENTS AND SIZES OF THE GROUND RODS.

FEEDER SCHEDULE:

- ① (2) #12 AWG, (1) #12 AWG GROUND IN 3/4" CONDUIT.
- ② (2) #10 AWG, (1) #10 AWG GROUND IN 3/4" CONDUIT.
- ③ (2) #8 AWG, (1) #8 AWG GROUND IN 1" CONDUIT.
- ④ (2) #6 AWG, (1) #6 AWG GROUND IN 1" CONDUIT.



1 ELECTRICAL SITE PLAN

1" = 20'-0"



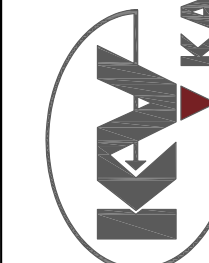
MEP ENGINEER

InSite Group
 DESIGNATION: DESIGN, INTEGRITY,
 2000 W. COMBINE DRIVE, SUITE 100, OMAHA, NE 68122
 PHONE: (402) 426-8200 FAX: (402) 426-8200
 WWW.ISITEGROUP.COM

CAPROCQ KC MISSION, LLC
 5700 BROADMOOR STREET
 MISSION, KANSAS 66202

PROJ. NO.	C17_9526
DESIGNER	JGR
DRAWN BY	JGR
CFN	5381 - SL100
SHEET	SL100
REV	0
ISG PROJECT NUMBER	16-5381-0

14700 WEST 114TH TERRACE
 LENEXA, KANSAS 66215
 PH. (913) 894-5150 | FAX (913) 894-5877
 info@kvweng.com | www.kvweng.com



KAW VALLEY ENGINEERING
 KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY KANSAS STATE CERTIFICATE OF AUTHORIZATION # E-113. EXPIRES 12/31/18

(PLANNING SUBMITTAL ONLY)
 NOT FOR CONSTRUCTION

REV	DATE	DESCRIPTION
0	11/15/2017	PLANNING SUBMITTAL

DSN	JGR	JGR	AWN
CHK	DSN	JGR	AWN

GENERAL NOTES:


- REFER TO CIVIL DRAWINGS FOR ADDITIONAL SITE INSTALLATIONS.
- REFER TO SHEET SL400 FOR GENERAL NOTES, ABBREVIATIONS AND SCHEDULES.
- REFER TO SHEET SL300 FOR ELECTRICAL DETAILS.

CALCULATION SUMMARY

DESCRIPTION	SYMBOL	AVG	MAX	MIN	MAX/MIN	AVG/MIN
PARKING LOT	+	1.4 F.C.	43.7 F.C.	0.0 F.C.	N/A	N/A
20'-0" FROM BACK OF PARKING LOT CURB	*	0.0 F.C.	0.0 F.C.	0.0 F.C.	N/A	N/A

NOTES:

- THE INFORMATION IN THIS CHART IS BASED ON THE LUMINAIRE MOUNTING HEIGHTS AND LOCATIONS SHOWN. CHANGES IN HEIGHTS OR LOCATIONS WILL MODIFY THESE RESULTS.
- THE SYMBOLS IN THIS CHART ARE REPRESENTED ON THE SITE PLAN, AND INDICATE THE LIGHTING LEVELS FOR THAT AREA.
- THE VALUES OBTAINED ARE BASED ON USING A PEDESTRIAN STYLE LUMINAIRE. REFER TO THE LUMINAIRE SCHEDULE ON SHEET SL400 AND THE CUT SHEET SHOWN BELOW FOR LUMINAIRE TYPE AND STYLE USED. PEDESTRIAN STYLE FIXTURES WERE USED PER CITY'S REQUEST, AND TO ENSURE THAT THE TOTAL LUMINAIRE HEIGHT IS LESS THAN THE EXISTING 8'-0" PRIVACY FENCES TO THE EAST. THE INTENT IS TO MINIMIZE LIGHT TRESPASS ON THE ADJACENT RESIDENTIAL PROPERTIES.



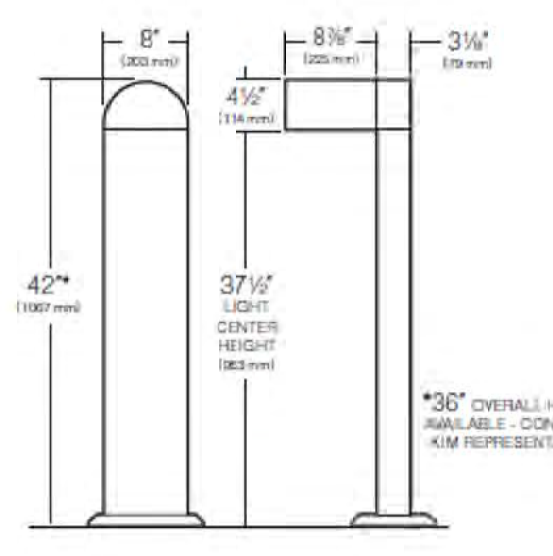
SL1 LED
Site Lightforms
revision 7/16/14 • k_l_sl1led_spec.pdf

Approvals:

Date: Page: 1 of 4

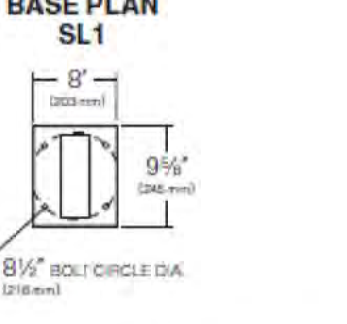
Specifications

SL1 LED
Maximum weight: 37 lb



42" (1067mm)
37 1/2" (941mm)
4 1/2" (114mm)
8" (203mm)
8 1/2" (216mm)
3 1/4" (86mm)

*36" OVERALL HEIGHT AVAILABLE - CONTACT KIM REPRESENTATIVE



8" (203mm)
9 1/2" (241mm)
8 1/2" BOLT CIRCLE DIA (216mm)

BASE PLAN SL1

HUBBELL Lighting

© 2014 KIM LIGHTING • 16555 EAST GALE AVENUE, CITY OF INDUSTRY, CA 91745-1788 • TEL: (926)999-5696 • FAX: (926)999-5716

Head: One-piece cast, low-copper (<0.6% Cu) aluminum, 3/4" wall thickness with external reveal unifying head and shaft. Attachment to shaft is by four bolts: concealed within the head.

Shaft: One-piece extruded aluminum with internal tracks for attaching head and base.

Base: One-piece cast, low-copper (<0.6% Cu) aluminum secured to shaft by four concealed stainless steel bolts.

Lens: Clear flat 1/4" thick tempered glass retained by two stainless steel brackets, and four stainless steel, hex socket cap screws, fully gasketed.

LED Optical System: A total of 27 mid power LED emitters configured in a rectangular array comprised together as a module. Two (2) modules for 18 version and four (4) modules for 36 version. Available in 580nm Amber, 3000K, 4200K and 5100K color temperatures.

LED Driver: Universal voltage from 120 to 277V with a ±10% tolerance, -40°F starting temperature. All drivers are Underwriters Laboratories recognized.

Anchor Bolts: Four 3/4" x 10" + 2" zinc plated L-hooks complete with leveling nut, washers, couplings, rigid pressed board template and black stainless steel bolts.

Finish: Fade and abrasion resistant, electrostatically applied, thermally cured, triglycidyl isocyanurate (TGIC) polyester powdercoat. Standard colors are Black, Dark Bronze, Light Gray, Titanium, Platinum Silver, or White. Custom colors are available and subject to additional charges, minimum quantities and longer lead times. Consult representative.

Listed to: UL 1598 Standard for Luminaires - UL 8750 Standard for Safety for Light Emitting Diode (LED) Equipment for use in Lighting Products and CSA C22.2#250.0 Luminaires. RoHS compliant. Meets Buy American provisions within ARRA.

Warranty: For full warranty see <http://www.hubbellighting.com/resources/warranty>.

CAUTION: Fixtures must be grounded in accordance with national, state and/or local electrical codes. Failure to do so may result in serious personal injury.



1 PHOTOMETRIC PLAN

1" = 20'-0"



MEP ENGINEER



InSite Group
DEDICATION. DESIRE. INTEGRITY.
2000 W. COMB DRIVE, SUITE 100 | P.O. BOX 113 | ST. LOUIS, MO 63103
TEL: (314) 288-6200 | FAX: (314) 288-6200

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MISSION, KANSAS 66202

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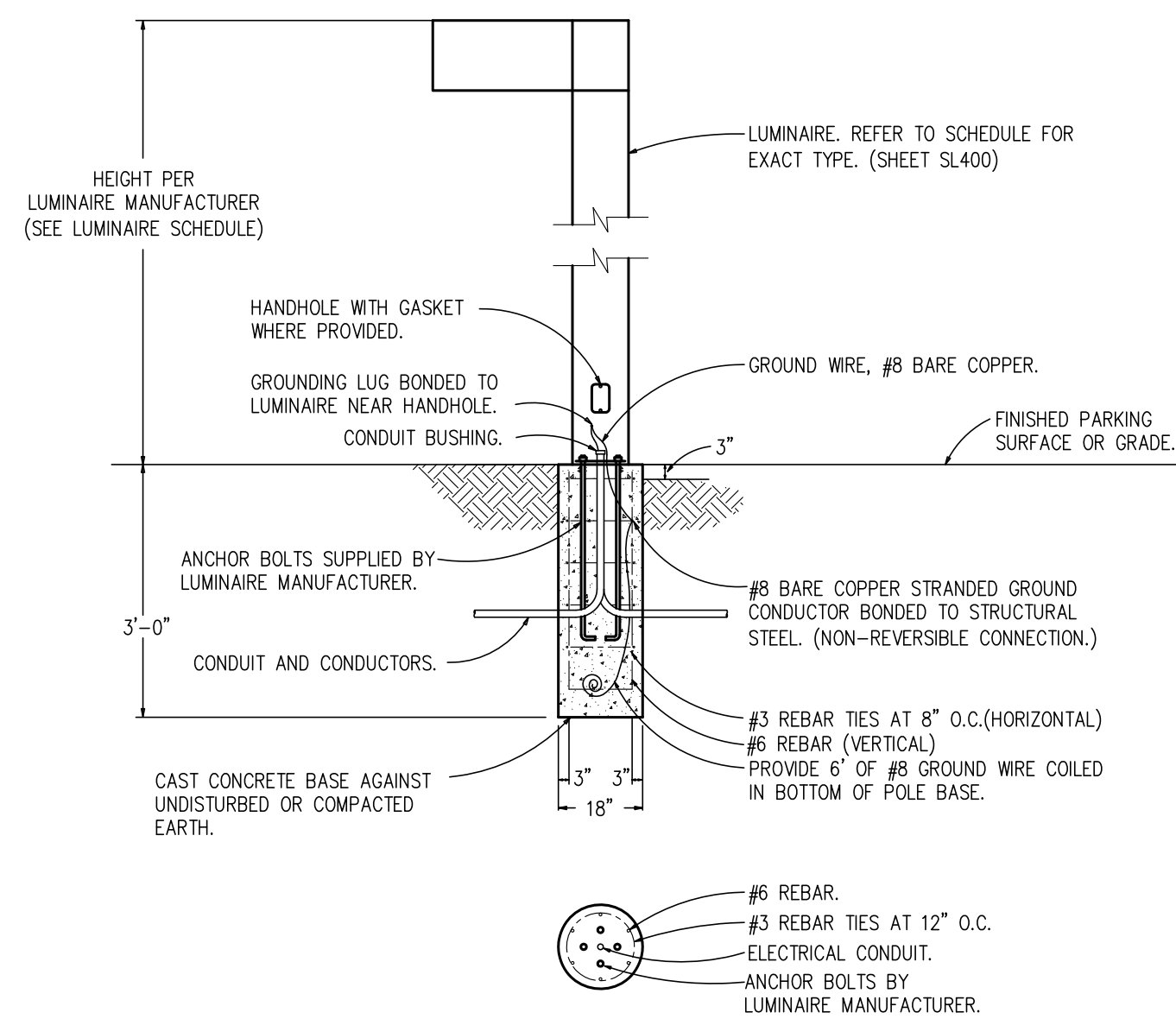
KAW VALLEY ENGINEERING
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PROJ. NO.	C17_9526
DESIGNER	JGR
DRAWN BY	JGR
CFN	5381 - SL200
SHEET	REV
ISG PROJECT NUMBER	16-5381-0

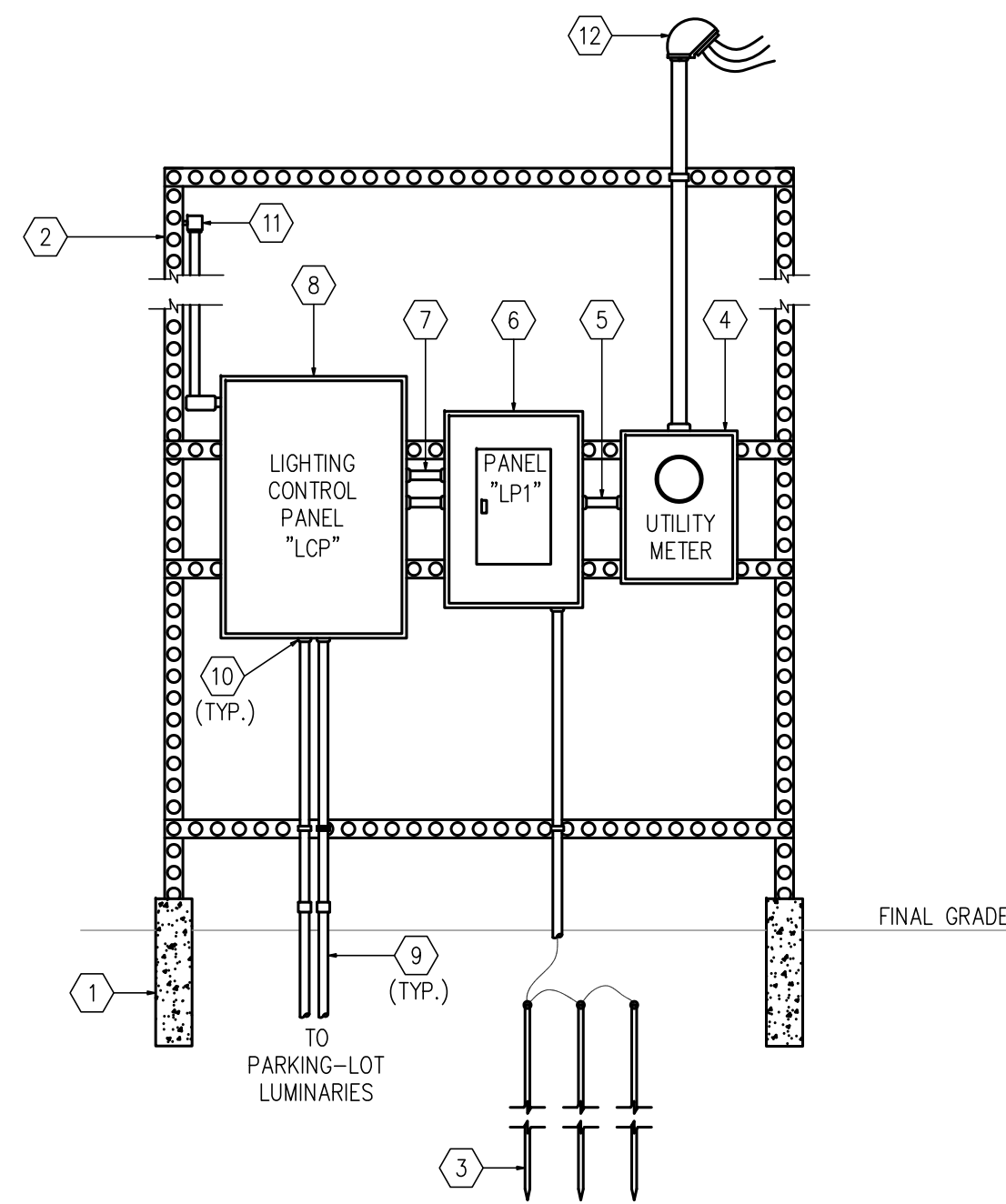
REV	DATE	DESCRIPTION
0	11/15/2017	PLANNING SUBMITTAL
		DSN
		JGR
		JGR
		AWN
		CHK

(PLANNING SUBMITTAL ONLY)
NOT FOR CONSTRUCTION

SITE PLAN



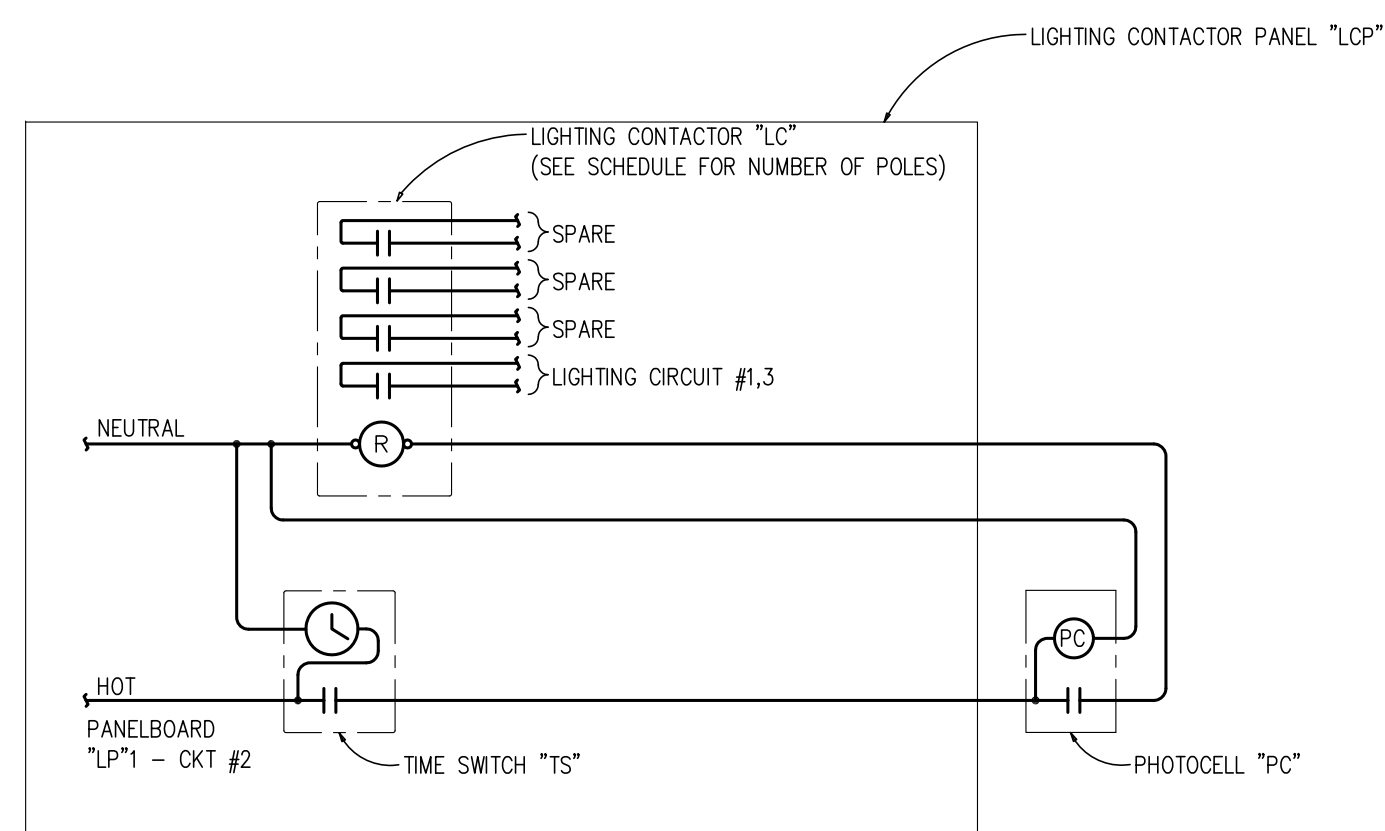
1 LUMINAIRE BASE DETAIL
NO SCALE



3 ELECTRICAL EQUIP. UNISTRUT RACK DETAIL
NO SCALE

ELECTRICAL EQUIP. UNISTRUT RACK NOTES:

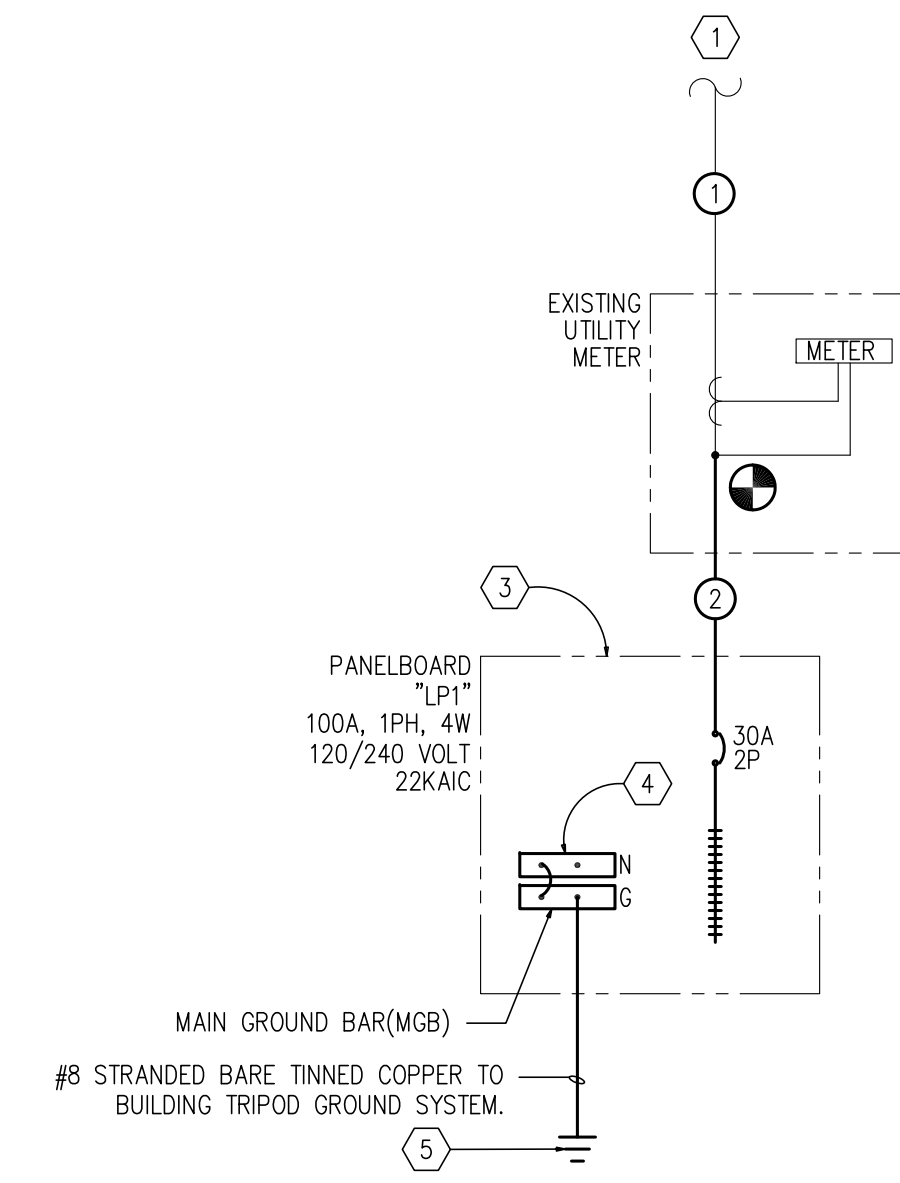
- 1 CONCRETE PIER BELOW UNISTRUT RACK. PROVIDE 8" DIAMETER PIER, 48" DEEP WITH #4 EPOXY COATED REINFORCING STEEL AT 4" ON CENTER VERTICALLY AND HORIZONTALLY. CONCRETE SHALL BE MINIMUM 4000 PSI. REFER TO CIVIL PLANS AND SPECIFICATIONS FOR ADDITIONAL CONCRETE REQUIREMENTS.
- 2 GALVANIZED UNISTRUT RACK. PROVIDE ADDITIONAL SUPPORTS AS REQUIRED TO MOUNT ALL EQUIPMENT SHOWN. PROVIDE CONCRETE FOOTINGS AT RACK LEGS TO ADEQUATELY SUPPORT THE RACK SYSTEM SHOWN. ALL HARDWARE AND CONNECTIONS SHALL BE GALVANIZED. PROVIDE INTERMEDIATE SUPPORTS AND LATERAL BRACING AS REQUIRED.
- 3 PROVIDE THREE(3) 3/4" X 10' LONG COPPER CLAD GROUND RODS. DRIVE THE THREE GROUND RODS INTO THE GROUND, MINIMUM 6' APART FROM EACH OTHER. CONNECT THE THREE RODS TOGETHER USING #3/0 BARE COPPER WIRE. ALL CONNECTIONS SHALL BE NON-REVERSIBLE TYPE CONNECTIONS.
- 4 RELOCATED UTILITY METER MOUNTED ON UNISTRUT. ALL MOUNTING HARDWARE SHALL BE GALVANIZED. LOCATION AND ELEVATION APPROVED BY UTILITY COMPANY. METER SHALL BE INSTALLED SO THAT IT IS ACCESSIBLE TO THE UTILITY COMPANY. EXTEND EXISTING FEEDERS TO THE RELOCATED METER.
- 5 CONDUIT FROM UTILITY METER TO LIGHTING PANEL "LCP".
- 6 PANELBOARD "LP1". REFER TO ONE-LINE DIAGRAM (THIS SHEET) AND PANELBOARD SCHEDULE (SHEET SL400) FOR ADDITIONAL REQUIREMENTS.
- 7 CONDUITS FROM PANELBOARD "LP1" TO LIGHTING CONTACTOR PANEL "LCP". REFER TO SITE PLAN (SHEET SL100) AND PANELBOARD SCHEDULE FOR EXACT CONDUIT AND WIRING REQUIREMENTS.
- 8 LIGHTING CONTACTOR PANEL "LCP". REFER TO "LUMINAIRE CONTROL SCHEMATIC DIAGRAM" DETAIL (THIS SHEET) FOR CONTENTS WITHIN THE LIGHTING CONTACTOR PANEL ENCLOSURE SHALL BE NEMA 3R RATED WITH HINGED AND PAD-LOCKABLE FRONT DOOR.
- 9 CONDUITS BELOW GRADE SHOULD BE SCHEDULE 40 PVC. CONDUITS ABOVE GRADE SHALL BE RIGID GALVANIZED STEEL CONDUIT. PROVIDE CONNECTION BETWEEN PVC CONDUIT AND RIGID GALVANIZED STEEL CONDUIT. ALL 90 DEGREE "TURN UPS" SHALL BE RIGID GALVANIZED STEEL CONDUIT.
- 10 PROVIDE WEATHER-PROOF HUBS WHERE CONDUITS ENTER EQUIPMENT.
- 11 PHOTOCELL MOUNTED A MINIMUM OF 8'-0" ABOVE GRADE.
- 12 PROVIDE OVERHEAD ELECTRICAL SERVICE TO THE RELOCATED UTILITY METER. PROVIDE RIGID GALVANIZED CONDUIT AND WEATHER-HEAD. COORDINATE FINAL HEIGHT OF THE WEATHER-HEAD, AND FINAL CONNECTIONS OF THE FEEDERS WITH THE UTILITY COMPANY. PROVIDE ALL ADDITIONAL CONDUIT, WIRING, HARDWARE AND OTHER EQUIPMENT AS NEEDED FOR RECONNECTION OF THE EXISTING METER TO THE EXISTING SECONDARY UTILITY FEEDERS.



2 LUMINAIRE CONTROL SCHEMATIC DIAGRAM
NO SCALE

LUMINAIRE CONTROL NOTES:

- 1) REFER TO SCHEDULES ON SHEET SL400 FOR EQUIPMENT SHOWN IN THIS DIAGRAM.



4 ELECTRICAL ONE-LINE DIAGRAM
NO SCALE

ONE-LINE DIAGRAM GENERAL NOTES:

- 1) CONTRACTOR SHALL INSTALL ALL EQUIPMENT PER MANUFACTURER'S WRITTEN INSTRUCTIONS AND ALL APPLICABLE LOCAL, STATE, AND NATIONAL CODES.
- 2) 20% SPARE BREAKER SPACE SHALL BE PROVIDED IN ALL PANELBOARDS.
- 3) ALL EXTERIOR CONDUIT CONNECTIONS SHALL BE WEATHER-PROOF TYPE UNLESS NOTED OTHERWISE.
- 4) ALL PANELS SHALL HAVE THEIR DESIGNATION AND VOLTAGE IDENTIFIED ON THE FRONT COVER. PROVIDE PLASTIC LAMINATE LABELS ON ALL EQUIPMENT.
- 5) EQUIPMENT SHOWN SHALL BE EATON/CUTLER HAMMER, SQUARE D/SCHNIEDER ELECTRIC, OR GE BRAND. ALL OTHER MANUFACTURER'S SHALL OBTAIN PRIOR APPROVAL IN ORDER TO BID THE PROJECT.

ONE-LINE DIAGRAM NOTES:

- 1) EXISTING UTILITY FEEDER FROM UTILITY COMPANY TO EXISTING METER.
- 2) EXISTING METER, EXISTING METER TO BE RELOCATED FROM THE EXISTING LIGHT POLE TO THE NEW ELECTRICAL EQUIPMENT RACK.
- 3) 100 AMP RATED, 120/240 VOLT, 1 PHASE, 3 WIRE, 8 POLE PANELBOARD. PROVIDE WITH 30 AMP MAIN BREAKER. PANEL SHALL BE RATED FOR 22KAIC, NEMA 3R ENCLOSURE WITH HINGED AND LOCKABLE FRONT DOOR. PANELBOARD SHALL HAVE FULL BUSSING.
- 4) MAIN GROUNDING BUS BAR MOUNTED WITHIN NEW PANELBOARD "LP1". ALL CONDUCTORS THAT ARE CONNECTED TO THE BUS BARS SHALL BE TWO HOLE TYPE CONNECTORS AND SHALL BE PROPERLY LABELED. CONNECT THE GROUND AND NEUTRAL BUS BARS AT THIS LOCATION.
- 5) COPPER CLAD GROUND RODS.

ONE-LINE DIAGRAM WIRING LEGEND

- 1) EXISTING FEEDER TO UTILITY COMPANY.
- 2) (4) #10 AWG, AND (1) #10 AWG GROUND IN 1" CONDUIT.

			JGR	AWN
			JGR	DSN
			JGR	DWN
			JGR	CHK

(PLANNING SUBMITTAL ONLY)
NOT FOR CONSTRUCTION

KAW VALLEY ENGINEERING

KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY KANSAS STATE CERTIFICATE OF AUTHORIZATION # E-113. EXPIRES 12/31/18

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SITE PLAN

PROJ. NO. C17_9526

DESIGNER JGR DRAWN BY JGR

CFN 5381 - SL300

SHEET SL300 REV 0

ISG PROJECT NUMBER 16-5381-0

MEP ENGINEER

InSite Group

DEDICATION. DESIRE. INTEGRITY.

2000 W. COMB DRIVE, SUITE 1000 | PHOENIX, AZ 85042
800 SPRING VALLEY ROAD, SUITE 100 | FAYETTEVILLE, NC 28404

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LUMINAIRE SCHEDULE

DESIGNATION	MANUFACTURER	FIXTURE DESCRIPTION	MODEL NUMBER	MOUNTING	COLOR (AVG)	LAMP		DRIVE CURRENT (QUANTITY/TYPE)	VOLTS	WATTS	NOTES
						QTY	CODE				
S1	KIM OR APPROVED EQUAL	42" TALL, ONE-PIECE CAST ALUMINUM, 36 LED PEDESTRIAN BOLLARD.	SL1-36L-4K-UV-LG	BASE	4200K	36 PER POLE	LED	(1) 450mA	240	57	1.2

NOTES:
 1) CONTRACTOR SHALL COORDINATE LUMINAIRE COLOR WITH OWNER.
 2) LUMINAIRE SHALL BE RATED FOR WET LOCATIONS.

LIGHTING CONTACTOR SCHEDULE

CONTACTOR DESIGNATION	LOAD			SWITCH					INTERLOCK	
	EQUIPMENT SERVED	MAX WATTS	VOLTS	TYPE	AMP	POLE	ENCLOSURE	ACCESSORIES	CONTROLLED BY	CONTROLS
"LC"	SITE LIGHTING	-	240	NORMALLY OPEN-ELECTRICAL HELD	30	4	NEMA 3R	-	TS-1 & PC-1	-

NOTES:
 1. ACCEPTABLE MANUFACTURERS ABB, SCHNEIDER ELECTRIC, EATON, AND GE. ALL OTHERS REQUIRE PRE-APPROVAL.

TIME SWITCH SCHEDULE

SWITCH DESIGNATION	LOAD			SWITCH						
	EQUIPMENT SERVED	WATTS	VOLTS	MANUFACTURER	MODEL #	TYPE	AMP	POLE	ENCLOSURE	ACCESSORIES
"TS"	SITE LIGHTING	1800	120	INTERMATIC	ET1105C	SINGLE POLE SINGLE THROW	30	1	NEMA 3R	-

NOTES:
 1. PROVIDE WITH STEEL NEMA 3R ENCLOSURE.
 2. 24 HOUR, 7 DAY AND 7-DAY SUNRISE TO SUNSET VERSION.
 3. BATTERY BACK-UP.
 4. AUTO ADJUST FOR DAYLIGHT SAVING TIME.

PHOTOCELL SCHEDULE

PHOTOCELL DESIGNATION	LOAD			DEVICE	
	EQUIPMENT SERVED	WATTS	VOLTS	MANUFACTURER	MODEL #
"PC"	SITE LIGHTING	MIN. 1800	120	INTERMATIC	K4236CM

NOTES:
 1. DELAYED ACTION TO PREVENT SWITCHING FROM LIGHTNING OR HEADLAMPS.
 2. PROVIDE WITH HEAVY DUTY DIE-CAST HOUSING.

EXTERIOR LIGHTING POWER ALLOWANCE:

EXTERIOR AREA/SURFACE (EXTERIOR LIGHTING ZONE 3)	SQUARE FEET	LINEAL FEET	ALLOWED W/LF OR W/SF	ALLOWED WATTAGE
BASE SITE ALLOWANCE	-	-	-	750
PARKING AREA	30,852	-	0.10	3,085.2
TOTAL	-	-	-	3,835.2
EXTERIOR FIXTURE	FIXTURE VOLTAGE	NUMBER OF FIXTURES	FIXTURE WATTAGE	TOTAL WATTAGE
"S1"	240V - 1PH	29	57	1653
TOTALS	-	29	-	1653

CALCULATIONS:
 1653 < 3835.2 = [OK]
 *PER 2012 IECC SECTION 405.6.2

ELECTRICAL SYMBOLS

	CONDUIT AND CIRCUITRY TO BE REMOVED.
	BRANCH CIRCUIT WIRING RUN IN CONCEALED CONDUIT WHERE POSSIBLE.
	BRANCH CIRCUIT CONDUCTORS: GROUND, NEUTRAL, HOT (OR SWITCHED HOT).
	BRANCH CIRCUIT HOME RUN WITH PANELBOARD DESIGNATION AND CIRCUIT BREAKER No.
	BRANCH CIRCUIT UNDERFLOOR/BELOW GRADE CONDUIT.
	SITE LUMINAIRE.
	GROUND ROD.
	PHOTOCELL.
	TERMINAL BLOCK (BY OTHERS).
	RELAY COIL.
	KIRK KEY INTERLOCK.
	NORMALLY OPEN CONTACTS.
	NORMALLY CLOSED CONTACTS.
	SWITCH.
	FUSE.
	EARTH GROUND.
	TRANSFORMER.
	PANELBOARD.
	CONTROL PANEL.
	NON-FUSED DISCONNECT SWITCH.
	FUSED DISCONNECT SWITCH.
	COMBINATION MOTOR STARTER/DISCONNECT SWITCH.
	JUNCTION BOX.
	WALL MOUNTED BLANK OUTLET BOX.
	PLAN NOTE DESIGNATION.
	CONNECT TO EXISTING
	EQUIPMENT DESIGNATION.
	SECTION/ELEVATION REFERENCE NUMBER. SECTION/ELEVATION SHEET NUMBER

ELECTRICAL NOTATIONS

ARF	ABOVE RAISED FLOOR.
AFF	ABOVE FINISHED FLOOR.
UNO	UNLESS NOTED OTHERWISE.
IDC	INDEPENDENT CONTROLS CONTRACTOR.
I/C	INDICATES SINGLE CONDUCTOR CABLE.
"A"	THESE LETTERS ADJACENT TO ANY SYMBOL INDICATES DEVICE BOTTOM TO BE MOUNTED 4" ABOVE COUNTERTOP BACKSPASH.
"GF"	THESE LETTERS ADJACENT TO ANY SYMBOL INDICATES GROUND FAULT INTERRUPTER.
"AG"	THESE LETTERS ADJACENT TO ANY SYMBOL INDICATES GROUND FAULT INTERRUPTER AND DEVICE TO BE MOUNTED 4" ABOVE COUNTERTOP BACKSPASH.
"IG"	THESE LETTERS ADJACENT TO ANY SYMBOL INDICATES ISOLATED GROUND DEVICE.
"IL"	THESE LETTERS ADJACENT TO ANY SYMBOL INDICATES LOCKING OR TWIST-LOCK TYPE DEVICE.
"WP"	THESE LETTERS ADJACENT TO ANY SYMBOL INDICATES WEATHER-PROOF ENCLOSURE.
"WG"	THESE LETTERS ADJACENT TO ANY SYMBOL INDICATES WEATHER-PROOF ENCLOSURE.
"YP"	THESE LETTERS ADJACENT TO ANY SYMBOL INDICATES EXPLOSION-PROOF ENCLOSURE.
60"	DIMENSIONS ADJACENT TO ANY SYMBOL INDICATES MOUNTING HEIGHT TO CENTERLINE OF DEVICE.
RE:1E2	REFERENCE DESIGNATION. SHEET NUMBER DETAIL/PLAN NUMBER

WIRING SCHEDULE - VOLTAGE DROP

DISTANCE	CONDUCTOR SIZE
120V-20A BRANCH CIRCUITS	
UP TO 100'	#12
100' - 150'	#10
150' - 200'	#8
250' - 300'	#6
277V-20A BRANCH CIRCUITS	
UP TO 200'	#12
200' - 375'	#10
375' - 500'	#8

- NOTES
- CIRCUITS LENGTHS LONGER THOSE NOTED ABOVE SHOULD BE COORDINATED WITH THE ENGINEER. PROVIDE FEEDER LENGTHS AND PROPOSED CONDUIT AND WIRING SIZE FOR APPROVAL.
 - CONTRACTOR SHALL COORDINATE CONDUCTORS AND CIRCUIT BREAKER LUG REQUIREMENTS WITH MANUFACTURER AND PROVIDE PROPER LUGS TO CONNECT CONDUCTORS TO CIRCUIT BREAKERS.
 - ALL OTHER FEEDER AND BRANCH CIRCUITS NOT NOTED ABOVE SHALL BE COORDINATED WITH THE ENGINEER. PROVIDE FEEDER LENGTHS AND PROPOSED CONDUIT AND WIRING SIZE TO ENGINEER FOR APPROVAL. FEEDER VOLTAGE DROP NOT TO EXCEED 5% AND BRANCH CIRCUITS VOLTAGE DROP NOT TO EXCEED 3% PER NEC.

GENERAL NOTES:

- INSTALL ALL EQUIPMENT WHILE MAINTAINING ALL CLEARANCES PER EQUIPMENT MANUFACTURER'S RECOMMENDATIONS AND PER LOCAL CODES.
- PROVIDE OFFSETS AND TRANSITIONS AS NECESSARY TO AVOID OBSTRUCTIONS.
- THE OWNER AND ENGINEER ARE NOT RESPONSIBLE FOR THE CONTRACTOR'S SAFETY PRECAUTIONS OR TO MEANS, METHODS, TECHNIQUES, CONSTRUCTION SEQUENCES, OR PROCEDURES REQUIRED TO PERFORM HIS/HER WORK.
- DRAWINGS ARE DESIGNED FOR THE MANUFACTURER'S MATERIALS, EQUIPMENT OR SERVICES NAMED ON PLANS AND ANY CHANGES AND THEIR ASSOCIATED COSTS, REQUIRED TO ACCOMMODATE OTHER APPROVED EQUIPMENT MATERIAL OR EQUIPMENT AS WELL AS SPACE REQUIREMENTS FOR THE OTHER APPROVED EQUIPMENT MATERIAL OR EQUIPMENT, MUST BE ASSUMED BY THE CONTRACTOR IN HIS/HER BID.
- THESE PLANS ARE SCHEMATIC IN NATURE AND ARE INTENDED TO DEPICT GENERAL SCOPE OF PROJECT. ALL WORK SHALL BE PERFORMED PER ALL LOCAL AND STATE CODES AND REGULATIONS.
- THE SYMBOLS SHOWN ON THIS SHEET ARE A COMPLETE LIST OF SYMBOLS USED BY InSite Group, Inc. AND NOT ALL SYMBOLS OR ABBREVIATIONS MAY BE USED ON THIS PROJECT.
- THE DRAWINGS, SPECIFICATIONS, REFERENCED STANDARDS, ETC. ARE COMPLEMENTARY OF ONE ANOTHER. IN THE EVENT OF CONFLICT BETWEEN ANY PORTION OF THESE DOCUMENTS, THE ARCHITECT/ENGINEER SHALL BE CONTACTED FOR FORMAL INTERPRETATION OF THE REQUIREMENTS. THE CONTRACTOR SHALL BE DEEMED TO HAVE PROVIDED THE MOST DETAILED AND EXPENSIVE INTERPRETATION OF THE REQUIREMENT IN BID. ANY WORK INSTALLED IN CONFLICT WITH THE ARCHITECT/ENGINEER INTERPRETATION SHALL BE CORRECTED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE AND AT NO EXPENSE TO THE OWNER.
- ALL EXTERIOR PENETRATIONS SHALL BE SEALED WEATHER/WATER TIGHT.
- ALL WORK SHALL BE PERFORMED PER OSHA SAFETY STANDARDS.
- COORDINATE WORK WITH ALL OTHER TRADES.
- CONTRACTOR SHALL PAY ALL PERMITTING COSTS ASSOCIATED WITH THE WORK SHOWN.
- CONTRACTOR SHALL PROVIDE (7) HARD COPIES OF (1) ELECTRONIC COPY OF SUBMITTALS FOR ALL EQUIPMENT TO THE ENGINEER CLEARLY DEPICTING EQUIPMENT SELECTIONS AND DEVICE SELECTIONS. PROVIDE FINAL RED LINE AS-BUILT DRAWINGS TO ENGINEER.
- VERIFY ALL DIMENSIONS & CONDITIONS IN THE FIELD. NOTIFY ARCHITECT/ENGINEER OF ANY DISCREPANCIES. DRAWINGS ARE DIAGRAMMATIC AND DO NOT SHOW ALL OFFSETS, BENDS, ELBOWS, ETC. WHICH MAY BE REQUIRED FOR PROPER INSTALLATION OF WORK. PROVIDE ADDITIONAL BENDS AND/OFFSETS AS REQUIRED TO COMPLETE WORK AT NO ADDITIONAL COST.
- PROVIDE O&M MANUAL AND OWNER TRAINING FOR ALL NEW EQUIPMENT AND DEVICES.
- CONTRACTOR SHALL CONTACT THE LOCAL ELECTRICAL UTILITY COMPANY AND ARRANGE FOR ELECTRIC SERVICE AS INDICATED ON DRAWINGS. INCLUDE ALL COSTS, CHARGES, FEES, ETC. INCURRED BY LOCAL AUTHORITIES INTO BID. PROVIDE ALL MATERIALS AS REQUIRED BY LOCAL AUTHORITIES FOR ELECTRIC SERVICE INSTALLATION. ALL WORK SHALL BE IN ACCORDANCE WITH REQUIREMENTS OF LOCAL AUTHORITIES.
- PERFORM ALL EXCAVATING AND BACK FILLING AS NECESSARY IN COMPLIANCE WITH OSHA SAFETY STANDARDS. ALL EXCAVATIONS SHALL BE PROTECTED BY NECESSARY SHEATHING, SHORING, CRIBBING, TARPULINS, ETC. AS REQUIRED AND REMOVED AT COMPLETION OF WORK.
- REFER TO CIVIL DRAWINGS FOR ADDITIONAL INFORMATION AND REQUIREMENTS ASSOCIATED WITH THE PROJECT.
- ALL DISTURBED SOIL, SURFACES, AND STRUCTURES DISTURBED AS A RESULT OF ELECTRICAL SERVICES IN THE PROJECT BOUNDARY SHALL BE PUT BACK IN ORIGINAL CONDITION, OR IN THE CONDITION DEFINED ON THE CIVIL AND/OR LANDSCAPING DRAWINGS AT NO ADDITIONAL COST TO THE OWNER.
- PROVIDE SOIL EROSION CONTROL AS DEFINED BY THE CIVIL ENGINEER'S SPECIFICATION AND/OR DRAWINGS. WHERE NONE ARE INDICATED, CONTRACTOR SHALL CONSULT WITH CIVIL ENGINEER AND PROVIDE COST IN HIS/HER BID TO COVER EROSION CONTROL AS INSTRUCTED.
- ALL EFFORTS SHALL BE MADE BY THE CONTRACTOR TO PROTECT TREE AND OTHER VEGETATION ROOTS WHERE TRENCHING OCCURS, AND WHERE THE TREES/VEGETATION IS TO REMAIN. OFFSET ROUTING AS REQUIRED.

ELECTRICAL GENERAL NOTES:

- PROVIDE CONDUCTORS FOR LISTED APPLICATIONS AS FOLLOWS:
 EXTERIOR LIGHTING AND RECEPTACLE CIRCUITS: COPPER, TYPE THWN, 600 VOLT, 90 DEGREES C (194 DEGREES F) THERMOPLASTIC INSULATED BUILDING CONDUCTOR.
 POWER CIRCUITS AND FEEDERS UNDER 100 AMPS: COPPER, TYPE THWN, 600 VOLT, 90 DEGREES C (194 DEGREES F) THERMOPLASTIC INSULATED BUILDING CONDUCTOR.
 LOW VOLTAGE AND LINE VOLTAGE CONDUCTORS SIZES NO. 16 AND 18 AWG: COPPER TYPE TFFN, 600 VOLT, 90 DEGREES C (194 DEGREES F) THERMOPLASTIC INSULATED BUILDING CONDUCTOR.
 POWER CIRCUITS AND FEEDERS OVER 100 AMPS: COPPER, TYPE THWN, 600 VOLT, 75 DEGREE C (167 DEGREES F) THERMOPLASTIC INSULATED BUILDING CONDUCTORS.
 * MINIMUM BRANCH CIRCUIT WIRE SIZE IS #12AWG UNLESS NOTED OTHERWISE.
 - ALL EXTERIOR WIRING SHALL BE IN RIGID GALVANIZED CONDUIT.
 - PANELBOARD REGISTRIES SHALL BE TYPE WRITTEN AND PLACED IN THE PLASTIC COVER ON THE INTERIOR DOOR OF EACH PANELBOARD.
 - FURNISH, INSTALL, AND CONNECT ALL WIRE, WIREWAY, CONDUIT, CONNECTORS, OUTLETS, ETC. NECESSARY TO ACHIEVE A COMPLETE ELECTRICAL INSTALLATION. ALTHOUGH SUCH WORK IS NOT SPECIFICALLY SHOWN OR SPECIFIED EQUIPMENT SHALL BE INSTALLED PER CODE REQUIREMENTS PROVIDING A SOUND, SECURE AND COMPLETE INSTALLATION.
 - COORDINATE AIC RATINGS OF ALL ELECTRICAL EQUIPMENT WITH ELECTRIC UTILITY. PROVIDE PANELBOARDS, BREAKERS, DISCONNECT SWITCHES, ETC. AS REQUIRED PER UTILITY COMPANY.
 - COORDINATE POWER REQUIREMENTS AND FINAL LOCATIONS OF ALL EQUIPMENT, DEVICES, ETC. WITH FINAL EQUIPMENT SELECTIONS AND INSTALL ALL NECESSARY DEVICES ALLOWING FOR END TERMINATION/CONNECTIONS.
 - PROVIDE PERMANENTLY AFFIXED BLACK PLACARD WITH WHITE LETTERING INDICATING EQUIPMENT NAME AND PANELBOARD AND CIRCUIT BREAKER SERVICING IT, ON ALL NEW:
 -ELECTRICAL EQUIPMENT.
 -PANELBOARDS.
 -DISCONNECT SWITCHES, ENCLOSED CIRCUIT BREAKERS.
- LABELS SHALL BE ATTACHED USING A MECHANICAL FASTENER. ADHESIVE IS NOT ACCEPTABLE.

- ALL EXTERIOR CONDUIT SUPPORTS SHALL BE GALVANIZED, ALUMINUM, OR STAINLESS STEEL.
 - CONTRACTOR SHALL VERIFY ALL FINAL FEEDER LENGTHS AND INCREASE WIRE SIZES AS REQUIRED TO MEET NEC MAXIMUM VOLTAGE DROP REQUIREMENTS.
- 3 PHASE VOLTAGE DROP FORMULA:
 $VD = (1.73R \cdot I \cdot L) / CM$
 VD = VOLTAGE DROP (VOLTS)
 R = RESISTANCE OF CONDUCTOR (OHMS)
 L = LENGTH OF CONDUCTOR (FEET)
 I = CURRENT (AMPS)
 CM = CROSS SECTION AREA OF CONDUCTOR
- 1 PHASE VOLTAGE DROP FORMULA:
 $VD = (2R \cdot L \cdot I) / CM$
 VD = VOLTAGE DROP (VOLTS)
 R = RESISTANCE OF CONDUCTOR (OHMS)
 L = LENGTH OF CONDUCTOR (FEET)
 I = CURRENT (AMPS)

PANELBOARD SCHEDULE: "LP1"

NCCOD PANELBOARD TYPE		FEEDER ENTRANCE:		PANEL LOCATION:	
120/240 VOLTAGE	1 PHASE	X 30 MAIN BREAKER	X TOP	EQUIP. RACK	
100 AMP MAIN BUS	3 WIRE	MAIN LUGS	BOTTOM	FEEDER CABLE:	
3R NEMA ENCLOSURE	200% NEUT.	FEED THRU LUGS	MOUNT:	SEE ONE-LINE DIAGRAM	
22K RMS AMPS @	240 VOLTS	SUB-FEED BREAKER	X SURFACE	SOURCE:	
11/15/17 DATE	X CU BUSS	SOLID NEUTRAL	FLUSH	UTILITY	

POLE #	SERVICE	VA LOAD	LOAD	WIRE	C.B.	C.B.	WIRE	LOAD	VA LOAD	SERVICE	POLE #			
		L1	L2	TYPE	TRIP	POLE	TRIP	POLE	L1	L2				
1	SITE LIGHTING	826.5		L	NOTE	20	2	20	1	12	M	600	LTG CNTRL PNL "LC"	2
3			826.5	L	3	20	1	20					SPARE	4
5	SPARE					20	2						SPACE	6
7													SPACE	8
		826.5	826.5						0	600				
CONNECTED VA/PH (LESS FEED THRU & SUB FEED)		L1 - 827		L2 - 1427										
CONNECTED VA/PH FROM FEED THRU AND SUB FEED		L1 -		L2 -										
TOTAL CONNECTED VA/PH		L1 - 827		L2 - 1427										
LOAD TYPE	CODE DEMAND REQUIREMENTS	CONNECTED VA	DEMAND VA	MIN. CODE VA (1.25 x CONT)										
LIGHTING (NEC 220-11)	100%	1653	1653	2066										
RECEPTACLES (NEC 220-13)	1st 10,000VA + 1/2 x REMAINING	0	0	0										
LARGEST MOTOR (NEC 430-24)	1.25 x LARGEST FLA	0	0	0										
REMAINING MOTORS (NEC 430-24)	100% REMAINING MOTORS	600	600	600										
HEATING (NEC 220-15)	100%	0	0	0										
KITCHEN EQUIPMENT (NEC 220-20)	VARIABLES (SEE CODE SECTION)	0	0	0										
WATER HEATER	100%	0	0	0										
MISC.	100%	0	0	0										
SPARE	0 x CODE MN VA			0										
TOTAL LOADS	SIZING LOAD	2253	2253	2253	2253	2666								
		11 AMPS												

NOTES:
 1. PROVIDE PANELBOARD WITH HINGED INNER AND OUTER DOOR. OUTER DOOR SHALL BE LOCKABLE.
 2. VERIFY WIRE SIZES SHOWN ON PLAN WITH BREAKERS SHOWN. PROVIDE LARGER LUGS AS REQUIRED.
 3. REFER TO PLANS FOR WIRE SIZES.

MEP ENGINEER

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PROJ. NO. C17_9526
 DESIGNER JGR
 DRAWN BY JGR
 CFN 5381 - SL400
 SHEET SL400
 REV 0

SITE PLAN

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KAW VALLEY ENGINEERING
 KAW VALLEY ENGINEERING, INC. IS AUTHORIZED TO OFFER ENGINEERING SERVICES BY THE KANSAS STATE CERTIFICATE OF AUTHORIZATION # E-113. EXPIRES 12/31/18

0 11/15/2017 PLANNING SUBMITTAL
 REV DATE DESCRIPTION
 JGR DSN JGR AWW
 JGR DSN JGR AWW

CITY OF MISSION, KANSAS

ORDINANCE NO.

AN ORDINANCE AUTHORIZING CERTAIN PROPERTY WITHIN THE CITY OF MISSION, KANSAS TO BE USED FOR OR OCCUPIED BY A SPECIAL USE.

WHEREAS , an application for the establishment of a Special Use Permit has heretofore been made to occupy or use property located at 5635 Broadmoor Street to be used for the following use or uses: Off-site Parking.

WHEREAS, said property is currently zoned Office Building District wherein such uses are not permitted; and

WHEREAS , notice of said original application was duly given as required by law by publication and mailing; and

WHEREAS , a public hearing was held pursuant to law before the City Planning Commission of the City of Mission, and the recommendation of said Planning Commission was acted upon by the City Council of the City of Mission as required by law:

NOW, THEREFORE BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF MISSION KANSAS:

Section 1 . That the said property described as:
8-12-25 PT NW1/4 BG 50' S OF SW CR LT 14 ALTA VISTA HGTS NO3 N 355' X W 131'
1.068 AC M/L MIC 253T 1
Johnson County parcel
ID# KF251208-1033
In the City of Mission, Johnson County, Kansas

may be occupied or used for the above described special use or uses subject to these conditions and requirements:

1. Limit the use of the subject property to the parking of vehicles to support the daily employee parking needs of 5700 Broadmoor Street.
2. Require that the on-site and off-site improvements as detailed in the submitted site plans to be substantially completed no later than November 1, 2018.
3. Require the platting the property for the dedication of right-of-way be completed prior to the issuance of any permits for improvements.
4. The final location of the crosswalk and sidewalk is to be coordinated with City Staff.

Section 2. That the approval of this Special Use Permit shall not change the zoning currently assigned to the property by the Official Zoning Map.

Section 3. This Ordinance shall take effect and be in force from and after its

adoption and publication according to law.

Passed by the City Council this 21st day of February 2018.

Approved by the Mayor this 21st day of February 2018.

Ronald E. Appletoft, Mayor

ATTEST:

Martha M. Sumrall, City Clerk

APPROVED AS TO FORM BY:

PAYNE & JONES, CHTD

David K. Martin, City Attorney
11000 King St, Ste 200
P.O. Box 25625
Overland Park, KS 66225
Tel: (913) 469-4100
Fax: (913)469-8182

City of Mission	Item Number:	6a.
ACTION ITEM SUMMARY	Date:	January 26, 2018
Administration	From:	Laura Smith

Action items require a vote to recommend the item to full City Council for further action.

RE: Coordination and preparation for transition of responsibility for Mission Magazine and Holiday Adoption Programs

RECOMMENDATION: Approve a contract with Suzie Gibbs to manage, document, and prepare to transition the coordination of the Mission Magazine and the Holiday Adoption Programs.

DETAILS: Since 2010, the City has grown and developed two special programs/projects which distinguish us from other cities not only in Johnson County, but throughout the entire metropolitan region. The first is the *Mission Magazine*, and the second is the Holiday Adoption program. Suzie Gibbs was instrumental in the formation of both, and during her tenure as a City Councilmember, assumed the primary responsibility for their management, coordination, and promotion.

Earlier this year, Mayor Appletoft initiated a conversation with Suzie regarding her interest in continuing to serve in a similar capacity in order to ensure that the City was able to maintain consistency and continuity for these important programs. Suzie has indicated a willingness to do so, and the Mayor has put forth this recommendation for Council consideration and action.

Mission Magazine

Suzie would continue to organize and chair the Magazine Committee, and would work with the Committee, city staff and Metromedia to develop, write and edit, and assist in the distribution of the *Mission Magazine* published 5 times each year. Over the course of the transition period, Suzie, in consultation with the Mayor would ensure that all processes and procedures are documented and that someone is identified and trained to assume these responsibilities going forward. Estimated hours: 130 annually.

Holiday Adoptions

For these programs, Suzie would continue to organize and chair the Family Adoption Committee (FAC). And would fundraise, coordinate with area schools to identify families, and coordinate/execute the shopping and delivery of meals and gifts. Suzie, in consultation with the Mayor would be responsible for documenting all processes and procedures and training a successor(s) for this program. Estimated hours: 70 hours annually.

Related Statute/City Ordinance:	
Line Item Code/Description:	19-30-201-13 MCVB Fund - Magazine
Available Budget:	\$44,389

City of Mission	Item Number:	6a.
ACTION ITEM SUMMARY	Date:	January 26, 2018
Administration	From:	Laura Smith

Action items require a vote to recommend the item to full City Council for further action.

A more detailed description of the responsibilities and expectations is attached to ensure that roles and relationships are clearly established and in place for the future. In recognition of the value both programs add to the services provided to our residents and businesses, it is recommended that this position be compensated in the amount of \$250 per month to account for time, travel and other expenses associated with program administration. The position would be considered an independent contractor, and compensation would be paid quarterly. The responsibilities may be transitioned to one or more individuals going forward.

The funds are recommended to come from carry-over funds transferred from the General Fund to the MCVB Fund. These funds have been used to supplement transient guest tax revenues which are dedicated exclusively to production of the Magazine. Transient guest tax revenues are now exceeding the costs to print and mail the magazine. The budget recommendation will be discussed in more detail at the Committee meeting.

CFAA CONSIDERATIONS/IMPACTS: The *Mission Magazine* is a key tool for reaching residents of all ages with relevant information and entertaining stories, strengthening their connections to their community. Similarly, the Holiday Adoption Program is a chance for community members to come together in a large scale effort to benefit others, strengthen connections and help local families.

Related Statute/City Ordinance:	
Line Item Code/Description:	19-30-201-13 MCVB Fund - Magazine
Available Budget:	\$44,389

CITY OF MISSION SPECIAL PROGRAMS COORDINATOR

POSITION TYPE: Independent Contractor
REPORTS TO: Mayor/City Administrator
DATE: February 2018

POSITION SUMMARY: The City of Mission is seeking a person or persons to facilitate, manage, coordinate and oversee the two special programs sponsored by the City of Mission:

Mission Magazine: A 40-page specialty publication produced 5 times annually and distributed to 12,000+ in the surrounding area. Started in 2010, the purpose of the magazine is to promote the Mission community and attract people to Mission businesses. Editorial content is discussed and approved by the Mission Magazine Committee/Editorial Board. The Magazine is funded in part by the City of Mission. As such the City may exercise some rights in the development of appropriate advertising guidelines. Advertising guidelines will be carefully constructed as not to abridge the rights of any individual or group that would like to advertise in the publication. Writing, editing, photography, advertising sales, production and mailing is coordinated by the magazine's publisher, Metromedia, Inc.

ESSENTIAL FUNCTIONS/RESPONSIBILITIES:

Mission Magazine

- Organize and chair the Mission Magazine Committee/Editorial Board (monthly, except December)
- Facilitate ideas and the selection of articles, balance resident and business features, and incorporate stories which highlight and appeal to all demographics
- Organize and attend meetings with writers and designers to discuss and plan for content and layout issue
- Attend interviews and photoshoots as necessary
- Assist in proofreading, rewriting and editing copy to ensure it is ready to go to press
- Review and adjust distribution lists as necessary
- Assist in distribution of magazines to local businesses
- Review and manage magazine budget in coordination with City Administrator
- Review and recommend potential changes in magazine format, content or distribution with Mayor
- Communicate need and request staff or financial resources as necessary with Mayor

Holiday Adoption Program: Initiated in 2010 to serve eight families, the 2017 program provided Thanksgiving meals to 73 families and Christmas meals to 83 families. The program also provided gifts to 44 families at Christmas. The entire program is funding through private donations and gifts from local residents and businesses. The program coordinates with area schools and other

non-profit, civic and religious organizations to accomplish its goal.

ESSENTIAL FUNCTIONS/RESPONSIBILITIES:

Holiday Adoptions

- Organize and chair the Family Adoption Committee (at least quarterly)
- Develop contacts with local schools to identify families who will be served by the program
- Oversee the distribution and collection of Christmas “wish-lists” for families
- Coordinate and secure donations from residents, businesses and other interested in supporting the Adoption Program (monetary and in-kind)
- Coordinate and oversee purchase, assembly, and delivery/pick-up of food baskets
- Coordinate and oversee purchase, wrapping, and delivery/pick-up of gifts
- Develop volunteer job descriptions and oversee volunteer coordination for various events
- Oversee and facilitate donation and refurbishing of bicycles
- Manage and oversee program budget in coordination with City Administrator
- Review and recommend significant changes in program format with Mayor
- Communicate need and request staff or financial resources as necessary with Mayor
- Present Annual Report to the City Council and provide other updates as necessary.

The preferred individual(s) will serve as an ambassador for the City of Mission, promoting a positive image and understanding that he/she represents the City in all interactions and transactions. Either party shall have the right to terminate the contract at any time with thirty (30) days notice.

City of Mission	Item Number:	6b.
ACTION ITEM SUMMARY	Date:	January 26, 2018
Administration	From:	Emily Randel

Action items require a vote to recommend the item to full City Council for further action.

RE: 2018 Farmers Market Schedule

RECOMMENDATION: Approve the schedule for the 2018 Mission Farm and Flower Market schedule for Thursdays, June-September, from 4:30- 8:00 p.m.

DETAILS: In November 2017, the City Council held a work session to review the market's performance both for the 2017 season, and since its beginning in 2015. Staff shared information on vendor and customer attendance. Both were close to what was seen in past years, but lower than expectations established at the beginning of the 2017 season.

Although not formally adopted at the Market's outset, there were several goals and objectives identified for the Mission Farm & Flower Market including:

- Continue the momentum of vibrancy and activity in downtown Mission following the reconstruction of Johnson Drive
- Provide a community gathering space
- Boost community pride
- Provide access to fresh produce and unique local items

Our primary goal since 2015 has been to build and expand the number of quality vendors in order to provide market-goers with options. That growth has been slower than anticipated. We believe this is occurring for a number of reasons, including:

- Increased availability of organic and local products in grocery stores.
- Competition for time on Saturdays, including children's activities, travel, etc.
- The choice of several nearby markets in the metro area.
- Trends toward home delivery and Community Supported Agriculture (CSA) of fresh produce.
- A relatively lower number of vendors compared to other markets, and fewer shopping options.

In discussing vendor plans for 2018, staff expects to have a lower number returning than in past years, roughly a weekly total of nine. Concerned by this trend, staff sought

Related Statute/City Ordinance:	NA
Line Item Code/Description:	01-09-208-16/Farmers Market
Available Budget:	\$10,000

City of Mission	Item Number:	6b.
ACTION ITEM SUMMARY	Date:	January 26, 2018
Administration	From:	Emily Randel

Action items require a vote to recommend the item to full City Council for further action.

and received direction at the November worksession to contact current vendors to ask about their interest or availability to participate in the market if it moved to a Thursday evening format. This information was shared back with the Community Development Committee in January. The number of vendors expected to participate in a Thursday market is similar to those that would be anticipated at a Saturday market.

Following additional discussion at the January Committee meetings, including a desire to include more food trucks and a beer garden each week, the consensus seemed to be to pursue the Thursday evening market to see if a new format would hold more appeal for the community and the vendors.

Keeping the market on Saturdays is still feasible, assuming a smaller vendor number is acceptable. Staff will continue to actively recruit additional vendors to the market regardless of the final decision on the market schedule.

In order to move ahead to recruit vendors and publicize the Market for the upcoming season, Staff is seeking a final decision by the Council for the 2018 schedule.

CFAA CONSIDERATIONS/IMPACTS: The Mission Farm and Flower Market provides a centrally located option for locally sourced food and a gathering space for people of all ages. The market site is connected to surrounding neighborhoods and nearby shopping areas by the accessible Rock Creek Trail.

Related Statute/City Ordinance:	NA
Line Item Code/Description:	01-09-208-16/Farmers Market
Available Budget:	\$10,000

#	Vendor Name	Product	Saturdays 2018	Thursdays 2018
1	The Jelly Man	Produce, baked goods, jams	Yes	Likely not, customer base prefers Saturdays
2	Dang's Garden	Wildflower bouquets	Yes	Yes
3	GF+1	Gluten free products	Yes	Likely yes
4	New Roots for Refugees	Produce	Yes, June-September	Likely yes, anticipates lower revenues, but staffing will be easier than Saturdays
5	Nowachek Apiary	Honey	Yes, every other week	Likely yes, concerned about the heat
6	Backyard Blossoms	Wildflower bouquets	Yes	Possibly every week, would participate once a month
7	2L Farms	Beef, jam	Occasionally	Yes
8	The Beachery	Baked goods	Once a month	Yes
9	Buds and Berries	Produce	Occasionally	Likely, during berry season
10	Ash & Bleu Cheese Co.	Ready to eat dishes and cheese	Likely not	Yes
11	Peaceful Hills Farm	Pork, eggs	No, expanding family, focusing on one market and two drop-off locations	Likely not every week, might participate once a month
12	Bowlin Farms	Produce	No, expanding family, out for 2018	
13	Rollin' B Produce	Produce	No, reevaluating markets	
14	Tasty Nuggets	Granola	No, business on hold indefinitely	
15	Crumble	Baked goods	No, selling goods in stores and catering	
16	Better Together Pet Bakery	Baked goods for pets	No, doing pre-orders and special events	
17	Smitten Bakery	Baked goods	No, selling goods through catering	