

**STAFF REPORT**  
**Planning Commission Meeting March 27, 2017**

**AGENDA ITEM NO.:** 4

**PROJECT NUMBER / TITLE:** Application # 17-01

**REQUEST:** Final Site Development Plan for The Gateway Development

**LOCATION:** Property located on the south side of Johnson Drive between Roeland Dr, Shawnee Mission Pkwy, and Roe Ave

**APPLICANT:** Korb Maxwell, Polsinelli PC

**PROPERTY OWNER:** Aryeh Realty, LLC  
140 Broadway, FL 41  
New York, NY 10005

**STAFF CONTACT:** Danielle Sitzman



**PUBLIC HEARING: NA**

**Property Information:**

The subject property is the site of the former Mission Mall and is zoned Planned Mixed Use District "MXD". This district is intended to encourage a variety of land uses in closer proximity to one another than would be possible with more conventional zoning districts, and to encourage building configurations that create a distinctive and memorable sense of place. Developments in this district are allowed and expected to have a mixture of residential, office and retail uses, along with public spaces, entertainment uses and other specialty facilities that are compatible in both character and function. Developments are also expected to utilize shared parking facilities linked to multiple buildings and uses by an attractive and logical pedestrian network that places more emphasis on the quality of the pedestrian experience than is generally found in typical suburban development.

Buildings are intended to be primarily multi-story structures with differing uses organized vertically rather than the horizontal separation of uses that commonly results from conventional zoning districts. The property is also subject to the Mission, Kansas *Design Guidelines* for the Johnson Drive Corridor.

Surrounding properties are zoned and developed as follows:

North:Roeland Park “OB” Office Building District-small offices, “PUB” Public Services-vacant, and “MXD” Mixed Use District-vacant

West: Mission “RP-3” Planned Townhome District-Roeland Court Townhomes, “MS2” Main Street District 2-vacant, restaurant, “R-1” Single Family Residential District-detached dwelling units,

South: Mission “RP-6” Planned High Rise Apartment District-vacant “C-1” Restricted Business District-bank, “C-O” Office Building District-dentist and other office uses.

East: Fairway “R-1” Single Family Residential District-detached dwelling units.

Comprehensive Plan Future Land Use Recommendation for this area:

The Comprehensive Plan indicates this area is appropriate for Mixed Use High-Density to be composed of a pedestrian friendly mix of neighborhood and community office uses, retail-commercial and service-commercial uses, institutional, civic, and medium to high density residential.

### **Project Background:**

In 2005 The Cameron Group, LLC, a development company from East Syracuse, New York, purchased the Mission Mall property with plans to build a mixed-use development on the site. In 2006 the Planning Commission reviewed and approved the rezoning and preliminary site plan for the redevelopment of the subject property for urban development composed of retail, office, hotel, restaurant, and residential uses (Ordinance #1203). Since the “MXD” zoning and preliminary site plan was first approved the project has evolved through several revisions reflected in revised plans presented to the Planning Commission and City Council in 2007, 2008, 2012, and 2015. Each of these plan approvals included a range of stipulations for site development issues, and requirements for additional details to be provided with final plan reviews. A preliminary site plan was approved by the City Council on January 20, 2016 after a public hearing before the Planning Commission on September 28, 2015.

At this time the applicant is requesting final site plan approval for the entire development. The applicant intends to proceed to construction of the entire project in three sequential phases. The timing of these phases will be considered by the City Council as part of their review of the Development Agreement.

### **Plan Review**

The proposed plan identifies six buildings around the perimeter of the site surrounding a partially free-standing three level parking garage. Building “B” on the southwest corner of the site is a 200 room, 7-story hotel. To the north buildings “C”, “D” and “E” along Roeland Drive and Johnson Drive will contain 168 apartments over ground floor retail in 4-story buildings. Building “A” remains a single story building with three retail tenant spaces defined. Building “F” is a 58,000 3-story office building. A boardwalk system is proposed to connect a surface parking lot on Roeland Drive to a green space adjacent to Buildings “C”, “D” and “E”. This courtyard is proposed for the benefit of residents and for use by the public. It includes seating and a small performance area.

A comparison of the modifications between the approved preliminary plan and the submitted final site plan is shown below. The total floor area of this version has been reduced 19,792 square feet and 5 parking stalls have been removed. The number of hotel rooms remains the same while the square footage of the hotel increased slightly. Fourteen (14) fewer dwelling



units are proposed. Office increased by 3,976 square feet. These changes are not considered significant per Section 440.175 and the site plan is therefore in substantial compliance.

Use	Approved Prelim 2015 (SqFt)	Final
Retail	382,974*	166,991
Office	54,540	58,516
Residential	(182 units)*	177,812 (168 units)
Service/ Mechanical	Not itemized	12,109
Hotel	138,610 (200 rooms)	140,904 (200 rooms)
<b>Total</b>	<b>576,124 square feet</b>	<b>556,332 square feet</b>
Parking Provided	1,533 parking spaces	1,528 parking spaces

\*2015 tally combines Residential and Retail

The City’s development review team conducted a review of the final site plans as well as special studies of traffic and stormwater impacts. Review comments were provided to the applicant’s representatives which were addressed in a revised submittal. Some issues do remain and are discussed below in the staff comments. Other unresolved comments are construction details in nature and best addressed at the time of review of those documents. Conditions relating to each are included in staff’s recommendation.

The following is a summary of issues that were identified by the city’s development review team to be addressed with final plans and/or development agreements.

Sustainable design and construction practices

The City’s development review team initially suggested the project pursue and demonstrate a commitment to achieve LEED for Neighborhood Development (LEED-ND). This certification by the U.S. Green Building Council addresses a wide range of design and construction practices related to site design and green infrastructure and building components, such as energy and water efficiency, storm water management, civic spaces, and relationship to the surrounding community. The applicant is opposed to certifying the project through this agency due to the increased upfront certification costs; however as indicated with previously approved plans will “...entertain and implement prudent design principles that follow a ‘green’ protocol in the spirit of a LEED project in the form of a Sustainable Design and Construction Implementation Plan (SDCIP).”

**Staff Notes-Sustainability-** The Mission Sustainability Commission has developed a rating and certification system for development projects. The proposed revised plans have been reviewed by the Sustainability Commission who have endorsed the sustainable elements of the project. Their review scorecard is attached.

Johnson Drive Design Guidelines & Municipal Code Standards

The Johnson Drive Design Guidelines provide a wide range of recommended and required design elements applicable to the development. These and the site development standards of the municipal code are reviewed below.

Building Design and Material Palette

The proposed building materials and architectural style are described in reflected in sheets FDP-A200-A211 and A300. A modern architectural theme is proposed. According to the project architect this style views the structures as larger singular elements whose mass is pushed close to the street and articulated in facade material color variation, balcony insets, and interesting window placement that respond to the particular use housed in the structure. The

ground floor of the buildings fold back at the street level forming a protected pedestrian experience.

A materials board has also been submitted with samples of the proposed materials. The predominant materials on building "A" are painted pre-cast concrete with thin brick and thin block at their base. The north elevation of this building contains 75% glazing at ground level in the form of storefront display boxes. Details of the display boxes are included on sheet FDP-A200. A corner entrance which addresses both Johnson Drive and the west side of the building has been included. The predominant materials of Buildings "B-F" are various metal panels, glass, board formed concrete, stained woods, and stucco.

**Staff Notes-Buildings:** The intent of the Johnson Drive Guidelines is to encourage detailed and articulated building elevations that create interesting facades, complementary massing, human scale elements, and high quality appearance materials. It acknowledges that Mission benefits from a diversity of architectural styles and would not prohibit modern styles that are compatible in form and proportion to buildings with their immediate context on Johnson Drive. The Gateway development is a large site on the far east end of Johnson Drive. It demonstrates a unified design approach within its boundaries. A mix of complementary building materials that are maintenance free are encouraged such as stone, stucco, brick, tile, concrete masonry units and clear glass. Pre-cast concrete is not preferred. Building "A" does make use of different materials that are in similar color to the rest of the site. The proposed design and materials are generally in conformance with the intent of the Design Guidelines. Ground floor percentage minimums of glazing and stucco maximums per story requirements have been met.

#### Public Open Space

A boardwalk and courtyard area between the buildings along Roeland Drive and the parking structure provides 49,000 square feet of public open space. This area is proposed for the benefit of residents of the apartments and visitors to the ground floor restaurants or retailers. This new courtyard does include built in seating and a small performance area. Shade trees and ornamental plantings are included in this space.

Pedestrian routes which are ADA accessible are shown throughout the development on sheet FDP-C1.1

**Staff Notes-Public Open Space:** The intent of the "MXD" zoning district is to encourage public spaces that are compatible in character and function with the other uses surrounding them. The design of the public open space interior to the site is appropriate for its proposed use and accessible.

#### Parking and Loading

The submitted plan provides 1,528 parking spaces for the mix of retail, residential, office and hotel uses. This includes angled parking spaces along Johnson Drive adjacent to street-level retail in Building "E", a surface parking lot is adjacent to Roeland Drive for Buildings "C" and "D", and a multi-level parking structure located in the center of the development.

The parking structure is L-shaped with two floors of parking above one slightly larger level of surface parking. A single circulator to the upper floors is located on the south end of the structure. The lowest level is no longer below grade and is intended for retail visitors. The upper levels will be for hotel guests and the residential units. The exterior of the garage will be wrapped in an articulated perforated aluminum panel system that allows for ventilation of an

open air garage and screens parked vehicles from view. See sheet FDP-A204 and FDP-A211. The material will extend approximately 8' beyond the top deck of the garage. Vignette views on sheets FDP-004-008 demonstrate how parking decks are screened from the view of surrounding roadways.

The surface parking lot along Roeland Drive will be screened from view by a 3' tall concrete wall with wood plan formliner finish on both sides. A planting bed with hardwood mulch is combined with this wall along the south half of the parking field.

**Staff Notes-Parking:** The amount and design of parking provided meets city ordinances. Adequate screening of the parking structure and surface parking lot has been provided. The perimeter concrete wall must be a minimum of 3' in height, not a maximum. Revise the note on sheet FDP-L106 to reflect this. Truck turning movements have been evaluated and are sufficient.

### Screening

Screening of undesirable areas of the development such as rooftop units, trash dumpsters, loading docks, utility pad sites and surface parking lots is required. Rooftop screening is noted on the elevation sheets to be the same as the building cladding and detailed on sheet FDP-A212. The loading dock is screened through the curved pre-cast concrete and brick wall and evergreen plantings. Pad-mounted utilities are to be screened with landscape materials as shown on sheet FDP-L210. All retaining walls and perimeter concrete walls will have a board form finish including the surface parking lot along Roeland Drive. The majority of trash storage will occur within the buildings.

**Staff Notes-Screening:** The proposed screening is adequate to block undesirable views. Add trash enclosure details for the exterior enclosure behind building "A" as noted on sheet FDP-A000.

### On and Off Site Public Improvements

The developer is responsible for installation of streetscaping around the perimeter of the site which includes sidewalks, street trees, benches, bike racks, and street lights and for providing public improvements off-site (crosswalks, modifications to traffic signals, turn lanes, etc). A sample streetscape improvement plan is included on FDP-L107 and other details are shown on the landscape plans.

A minimum 8-foot wide sidewalk clear zone along Johnson Drive must be provided in addition to adequate space for a streetscape amenity zone (street trees, tree wells, street lights, signage, etc.) and a seating area zone for any proposed outdoor restaurant space. The ideal minimum width for all zones combined is 20-feet from the street curb to the building wall. A sidewalk approximately 8'-8.5' wide is shown for a portion of this block to be 60% concrete and 40% specialty paving. Numerous intrusions into the clear zone are shown on the plans including the street lighting along Johnson Drive and vehicle overhangs from the on-street parking. A choke point appears to exist at the far west end of the on-street parking stalls. The sidewalk width shown along Building "A" is only 5' in width. A minimum 5-foot wide sidewalk clear zone is required and provided along Roeland Drive (except Rock Creek Trail segments) and Roe Avenue.

Street trees should be planted between the curb and walking path of the sidewalk space 50' on center. Trees may be clustered to work about other streetscape features but should be

provided at a rate of 1:50' of frontage. Irrigation will be provided for all street trees. Qualifying trees are shown in the table below.

Frontage	Required Street Trees	Provided	Notes
Johnson Drive	21	15	On-street parking and limited width along Building E reduce the number of trees provided. Adequate width should be provided and the number of trees increased.
Roe Avenue	10	4	Additional evergreen trees are proposed in place of shade trees. Screening is a priority in this corridor.
Roeland Drive	21	21	Trees must be located between back of curb and sidewalk.

Street lights are provided meeting the East Gateway Streetscape design standard. Lighting levels have been evaluated by GBA. Site lighting has also been reviewed.

**Staff Notes-Public Improvements:** Increase the width of the paved sidewalk along Johnson Drive to eliminate the numerous intrusions such as street lighting and vehicle overhangs into the required 8' clear zone, remove the choke point that narrows the sidewalk in the proximity of the far west end of the on-street parking stalls, and maintain an 8' clear width path sidewalk along the entire Johnson Drive frontage. Additional benches and trash receptacles should be added to both the Johnson Drive and Roeland Drive frontages.

The curb line along Roeland Drive is unclear. On future replats, right-of-way should be dedicated including all on-street parking areas, sidewalks, and public infrastructure along Roeland Drive, Johnson Drive, and Roe Avenue. The property line should conform with the build-to standard. The applicant has indicated that a plat will be submitted upon approval of the Final Site Plan.

The proposed street trees meet city code for species selected. Street trees along Roeland Drive should be shifted to the west side of the sidewalk. A reduction of street trees along Roe Avenue is acceptable given that the priority in this corridor is screening.

### Signs

The City's Sign Code does not specify any signs by right in the "MXD" Planned Mixed Use District. Instead, the Code requires shopping centers to establish private sign criteria governing all exterior signs in the development and that the Planning Commission review and approve these criteria as part of a final site plan approval. The intent of city's sign code is to ensure harmony and visual quality throughout the development. After approval, no sign permit will be issued by the City for a sign that does not conform to the criteria.

The applicant has provided a sign criteria document for consideration as summarized below:



<b>Permitted Signs</b>				
<b>Type</b>	<b>Style</b>	<b>Location</b>	<b>Illumination</b>	<b>Size</b>
Wall	Halo type/reverse channel letter	Coordinate w. Tenant's storefront entry location. If main sign then immediately above the tenant's storefront on the base building facade	Neon or LED	Proportional to overall storefront TBD by architect
Wall	Panel sign or pin mounted letters	"	Non-internally illuminated/accent lighting	Panels must be a minimum of 3" thick
Wall	Metal cabinet with routed out copy and push through plexiglas letters	Must be fully recessed or incorporated into the tenant's storefront design	Internally illuminated	Proportional to overall storefront TBD by architect
Projecting	Blade sign	Coordinate w. Tenant's storefront entry location. If main sign then immediately above the tenant's storefront on the base building facade. May be considered decorative secondary sign. 9' clearance, Max projection 4', Min projection 1'	Not specified	Proportional to overall storefront TBD by architect
Window	Adhesive or painted lettering	On storefront glazing, located low on the window	Not specified	Max ht 4"
Wall	Open faced channel letters	Coordinate w. Tenant's storefront entry location. If main sign then immediately above the tenant's storefront on the base building facade	Visually exposed neon tube	Proportional to overall storefront TBD by architect
Detached	Directional	At major entrances to the project, for tenants 3,000 or larger, must be multiple tenants listed.	Not specified	Proportional to overall storefront TBD by architect
Detached	Monument	Limit of 2 for development-Northeast and southwest corners of site	Not specified	Not specified-See FDP-L106
Temporary	Temporary	4 weeks max duration for any one sign. May inc. Vinyl banners allowed on storefront or within storefront glass	Not specified	Proportional to overall storefront TBD by architect
<b>Prohibited Signs</b>				
Wall	Exposed or surface mounted box or cabinet or exposed raceway signs			
Window	Suspended internally illuminated panel sign behind storefront glass			
Window	Neon tube sign in front or behind storefront. Specifically single line neon lettered signs.			
Detached	Freestanding, moving, rotating, flashing, noise making or odor producing signs			
Roof	Roof mounted signs			
Window	Cloth, paper, cardboard and large stickers, decals or other temporary looking signs on or around the storefront.			

**Staff Notes-Signs:** The proposed criteria does not specify the size and number of signs allowed instead leaving it up to a subjective review by the architect. Additional exhibits might help to narrow down the proposed locations for tenant main and secondary signs. Several sheets in the site plan indicate a freestanding sign in the island at Drive 4. This location would block pedestrians from view of turning vehicles and should not be allowed.

#### Public Transportation

A transit stop area has been installed along Johnson Drive near the intersection of Roe Avenue.

**Staff Notes-Transit Stop:** The transit stop along the northeast side of the Gateway site is part of the system of newly enhanced bus facilities installed in the Metcalf Ave/Shawnee Mission Parkway corridor as part of a federal TIGER grant. The transit stop is served by proposed sidewalks along the south side of Johnson Drive and west side of Roe Avenue.

#### Rock Creek Trail Extension

The continuation of the Rock Creek Trail from Martway Street to the Roeland Drive/Johnson Drive intersection is required. The submitted plans identify crosswalks at both intersections and a 10-ft wide sidewalk along the east side of Roeland Drive north of the Martway intersection, continuing north across Johnson Drive to Roeland Park. Directional trail marking signs are indicated on sheet FDP-C1.2.

**Staff Notes-Trail:** There is ample room for street trees, streetlights and street furnishings such as benches and trash cans to be located adjacent to the trail without constricting the 10-ft wide path. Rock Creek Trail layout will be reviewed again as part of the city approval of construction drawings.

#### Traffic & Access Management

Access into the site is proposed from six access points, three on Roeland Drive, one on Johnson Drive, and two on Roe Avenue. The driveway access to the load docks of Building "A" has been narrowed. All street intersections surrounding the subject property are currently signalized.

The applicant has submitted an update to the previous traffic study analyzing current conditions and proposed conditions after development. The applicant has proposed improvements to accommodate the expected daily trips the development will generate. On public streets these include restriping turn lanes along Roeland Drive at all three driveway locations to create queueing capacity to prevent blockages of the through lanes or congestion of the intersections; traffic control signs, modify the timing or phases of existing traffic signals, and improve pedestrian accommodations at intersections.

The Kansas Department of Transportation (KDOT) will also need to review and accept any changes proposed to the intersection of Shawnee Mission Parkway at Roeland Drive as this is a US Highway under their jurisdiction.

The City's on-call traffic engineer, George Butler Associates (GBA), has reviewed the applicant's revised Traffic Impact study and the final site plans. GBA accepts the applicant's proposed improvements as adequate for the expected traffic impacts of development of the site. At the time of construction plan review the following issues will need to be addressed: provision of a safe north-south pedestrian crossing at the intersection of Shawnee Mission Parkway and

Roeland Drive, regulatory signs at Drive 2 and 3, and definition of the vehicular pathway at Drive 6. At the the time of platting GBA has asked for an additional exhibit depicting curb lines, lanes utilization, and pavement markings. An excerpt of the Traffic Impact Study is attached along with GBA's memo.

### Stormwater Management

A multi-barrel reinforced concrete box (RCB) drainage system was installed across the site underground for this portion of Rock Creek. The RCB's were designed to convey the 100 year storm event and a letter of map revision (LOMR) has been approved by FEMA taking the property out of the flood zone. Therefore a floodplain permit is not required. Storm sewers for the site will direct water into this system at various locations and surface grading will direct overflows.

The City's on-call engineer at GBA has reviewed the Drainage Study and the proposed final site plans for storm water control. This included consideration of the amount of impervious surface in the development scenario, peak water flows after rain storms, and the location of below ground development features in relation to existing storm sewers. A reduction in the amount of impervious surface has been demonstrated by the addition of green space compared to the existing (pre-demolition) condition. The layout of any piers footings for the new buildings will be reviewed against the pier plan used during the construction of the RCB's and venting for the proper function of the RCB's will be taken into consideration with the design of Building "A" as part of building permit review.

### Consideration of Final Site Plans (440.160 & 440.190)

Final site plans which contain modifications from the approved preliminary development plan but which are in substantial compliance with the preliminary plan, may be approved by the Planning Commission without a public hearing, provided that the Commission determines that the landscaping and screening plan is adequate and that all other submission requirements have been satisfied. In addition the site plan shall be approved by the Planning Commission if it determines that:

1. The site is capable of accommodating the building(s), parking areas and drives with appropriate open space.

-The building, parking area, driveways, and open space have been designed to meet codes and guidelines and have been reviewed by the City's engineers.

2. The plan provides for safe and easy ingress, egress and internal traffic circulation.

-There is adequate space on the site to allow for on-site circulation of customer traffic and design vehicles. Impacts to traffic on adjacent public streets has been studied Traffic Impact Statement (TIS) and endorsed by City's engineers with stipulations.

3. The plan is consistent with good land planning and site engineering design principles.

-The proposed plan is consistent with the City's zoning and site development standards with the stipulations noted.

4. An appropriate degree of harmony will prevail between the architectural quality of the proposed building(s) and the surrounding neighborhood.

-The proposed project is of high quality design and adds to the diverse architecture of the

surrounding area.

5. The plan represents an overall development pattern that is consistent with the Comprehensive Plan and other adopted planning policies.

-The proposed mixed use development is consistent in density and design with the City's adopted plans and policies.

6. Right-of-way for any abutting thoroughfare has been dedicated pursuant to the provisions of Chapter 455.

-A plat reflecting the proposed development pattern has not been submitted. One will be required prior to development. Any required right-of-way changes for this site will be addressed at that time.

### **Staff Recommendation**

While the development is generally in conformance with the approved preliminary site plan and site planning requirements, several details do remain unresolved.

Therefore, Staff recommends the Planning Commission approve the Final Site Development Plan for Case # 17-01 The Gateway with the following conditions:

1. Submit a revised final site for staff review and approval showing the following:
  - a. Correct minor typos including: street names on sheet FDP-L101 and tables on sheets FDP-001, FDP-A050-055.
  - b. Show the minimum height of the perimeter concrete wall on sheet FDP-L106 to be 3' in height.
  - c. Add trash enclosure details for the exterior enclosure behind building "A" as noted on sheet FDP-A000.
  - d. Increase the width of the paved sidewalk along Johnson Drive to eliminate the numerous intrusions such as street lighting and vehicle overhangs into the required 8' clear zone.
  - e. Remove the choke point that narrows the sidewalk in the proximity of the far west end of the on-street parking stalls.
  - f. Maintain an 8' clear width path sidewalk along the entire Johnson Drive frontage.
  - g. Remove the freestanding sign from the island at Drive 4.
  - h. Increase the number of street trees provided along Johnson Drive.
2. Prior to the approval of construction drawings by staff:
  - a. Provide revisions to the streetlight layout and revised site light levels as noted by GBA.
  - b. Locate street trees between back of curb and sidewalk. Street trees along Roeland Drive should be shifted to the west side of the sidewalk.
  - c. Add benches, bike racks, and trash receptacles to both the Johnson Drive and Roeland Drive frontages.
  - d. Provide a safe north-south pedestrian crossing at the intersection of Shawnee Mission Parkway and Roeland Drive.
  - e. Provide regulatory signs at Drive 2 and 3.
  - f. Provide definition of the vehicular pathway at Drive 6.



3. Prior to the issuance of any building permits, a revised final plat must be approved by the City. Right-of-way should be dedicated including all on-street parking areas, sidewalks, and public infrastructure along Roeland Drive, Johnson Drive, and Roe Avenue.
4. Prior to building permit issuance for any building spanning the RCB's, demonstrate venting for the proper function of the RCB's will be taken into consideration and that any piers or footings will not impact the facility.

Staff recommends the Planning Commission reject the private sign criteria and direct the applicant to resubmit a version with edits showing the following:

- a. The size and number of signs allowed by type, tenant, or building.
- b. Additional exhibits to illustrate proposed locations for tenant main and secondary signs and freestanding signs.



Signage is an integral design feature of the Tenant's storefront. Tenant signage shall be designed to complement the specific tenant's storefront and general building design and support the master plan for The Gateway experience.

# THE GATEWAY

## SIGNAGE CRITERIA MANUAL

## **Signage Criteria Overview**

Signage is an integral design feature of the Tenant's storefront. Tenant signage shall be designed to complement the specific tenant's storefront and general building design and support the master plan for The Gateway experience.

The purpose of this section is to define and specify all exterior signage criteria for The Gateway. Each Tenant shall provide a signage package for its space.

All sign packages shall be submitted in triplicate or electronic format for approval at least sixty (60) days of Lease Commencement Date to Landlord and its designated agent prior to fabrication and installation. If the plans are disapproved by Landlord, Tenant shall resubmit them within fifteen (15) days from date of the notice of any disapproval by Landlord or its Architect until such plans are finally approved by Landlord.

The cost of the fabrication, permitting, and installation shall be the responsibility of each individual Tenant.

Sign construction is to be completed in compliance with local building code requirements and sign ordinances, and the instructions, limitations, and criteria contained in criteria manual.

Upon written notice from Landlord, Tenant agrees to take such actions as may be necessary to comply at Tenant's expense, with applicable requirements.

The Landlord must approve all Tenant signage intended to be visible from outside of the Tenant Premises.

The use of a corporate logo or other established corporate insignia shall be permitted only if specifically approved in writing by the Landlord.

All signage is to be of the highest quality construction. Shop fabrication and painting is required. All attachments, labels, fasteners, mounting brackets, wiring, clips, transformers, disconnects, lamps, and other required mechanisms must be concealed from view. Light leaks are not permitted and sign company labels and stamps must be concealed from view, unless Code requires labels to be visible from below. The Tenant's electrical contractor must perform all electrical connections for Tenant signs.

## **Municipal Signage Requirements**

All signs shall comply with applicable codes regarding materials, electrical connections, overall size, and general signage construction. Any permits that may be required by the Jurisdictional Authority shall be the responsibility of the Tenant.

The Tenant's storefront signage shall be proportional to the overall storefront and building facade design.

To encourage design creativity, no maximum letter size has been established, however, the Landlord will closely review all signage to confirm the proper relationship between signage and facade.

### **Signage Location**

Since the Landlord approved location for Tenant signage will depend on specific design conditions and varying base building facade details, the approved signage location shall be determined based on consultation with the Landlord's Architect.

The location of the signage shall specifically coordinate with the Tenant's storefront entry location, the character of the base building facade, and other conceptual design features.

The Tenant shall locate the main sign on the Tenant's storefront construction or immediately above the Tenant's storefront on the base building facade in accordance with Landlord's approved location. Tenant signage shall not be placed on top of, or over, base building design elements (i.e., second level windows).

### **Secondary Signage**

Decorative secondary signage, such as blade signs, decorative icons, and small repetitive window signage, is encouraged provided such signage compliments the overall Tenant facade design and the base building district design theme without violating any municipal ordinances.

### **Landlord's Approval**

Tenant shall provide design information for all signage on all preliminary and working drawings submitted to Landlord for review. Final approval of signage shop drawings.

### **Signage Shop Drawings**

Signage shop drawings submitted by the Tenant shall provide complete information for the Landlord to understand the signage design and appearance. Fabrication or installation of the Tenant's signage shall not commence before the Landlord's approval of the signage shop drawings is secured.

Submitted signage shop drawings shall provide the following information:

- Type and size of all signage and lettering
- Scaled and dimensioned elevation of the store illustrating each sign location
- Section or sections through the sign



- Identify the materials, color scheme, fabrication techniques, illumination, and mounting system
- Photographs of similar wherever possible

### **Permitted Signage Types**

- Halo type illuminated signs (reverse channel letters) with neon or LED illumination. LED colors and suppliers must be reviewed by the Landlord - particularly white since the brightness levels can be highly variable.
- Non-internally illuminated panel sign or individual pin mounted dimensional letters with accent lighting where required (letters or panel shall be minimum 3" thick).
- Internally illuminated metal cabinet sign with routed out copy and push through Plexiglas letters. The sign cabinet must be fully recessed or otherwise incorporated into the Tenant's storefront design. Such cabinet signs shall not be installed on or into the base building facade.
- Decorative blade signs - such signs are required to be dimensional. Flat panels with only painted or vinyl graphics are not allowed.
- Small, adhesive or painted lettering on storefront glazing
- Open-face channel letters with a visually exposed neon tube illumination. Lexan faces may be used to protect channel letters from nesting birds.
- Exposed illuminated neon tubing as part of an intricate, themed, and stylistically appealing sign

### **Signage Not Permitted**

- Exposed or surface mounted box or cabinet type signs, or any signage with an exposed raceway or electrical connections
- Suspended internally illuminated panel signs behind storefront glass
- Exposed neon tube signs, either in front of or behind the Tenant storefront, where the bare neon tube is fully exposed and not an integrated part of a more intricate, thematic sign are not permitted. Specifically, single line neon lettered signs such as "open" signs are not permitted.
- Freestanding, moving, rotating, flashing, noise making or odor producing signs
- Roof-mounted signs

- Signs that are not professional in appearance
- Cloth, paper, cardboard and other large stickers, decals, or other temporary looking signs on or around the storefront surfaces

## **SPECIFIC SIGNTYPE CRITERIA**

### **Internally Illuminated Channel Letters Signs**

- The background surface of the sign is to be opaque and not reflect the illumination or image of the neon lamps behind letters.
- The color of the channel returns must match the color of the letter faces.
- All neon tubing or LED must be fully concealed within the letter so it is not visible to the public.
- The rear and sides of the letter shall be opaque.
- Horizontally-mounted flanges/brackets shall be mounted above provided raceway invisible to public to support the letters.
- All connections and fasteners visible within the sign dimension between the horizontal shade surface and the back of the letters shall be painted to match the shade.
- Halo-type signs used in outdoor locations will have solid backing on the rear of the letters to prevent birds nesting in the letters.

### **Non-Internally Illuminated Panels or Individual Letter Signs**

- Included types are dimensional panels with a textured design, such as a carved wood sign OR dimensional letters or graphics pin mounted to the face of the storefront area or base building facade with external, indirect illumination.
- Non-dimensional letters or graphics less than 3" thick are generally not permitted. Non-dimensional techniques include painting, silk-screening, pressure sensitive vinyl, metal applique or glass etching. Exceptions to this requirement may be allowed if such signage compliments the overall store facade design.
- Signs shall not be placed on a background material that detracts from the appearance of the sign.
- Supplemental lighting must be provided for appropriate signage visibility. The supplemental lighting must be incorporated into the facade design either as a concealed feature or as a design

element (i.e. decorative accent fixtures or concealed fixtures mounted on a Tenant facade that lights back towards the wall mounted signage). Such supplemental lighting shall be compatible with the existing base building facade lighting and is subject to Landlord's approval.

- Dimensional letter signs applied directly to transparent storefront glass must have matching dimensional letters on each side of the glass to create a finished appearance from both sides of the storefront glazing.

### **Internally Illuminated, Fully Integrated Cabinet Signs**

- Signs shall be fully integrated into the storefront design concept. The cabinet sign shall be recessed so that the face of the cabinet sign is flush with the surrounding storefront material, or otherwise incorporated into the design.
- The size of the sign cabinet and the design of the face materials are to be integrally designed into the appearance of the store facade.
- Only letters shall be translucent on the sign face. The background must be opaque. Plastic faced sign panels will not be allowed.
- Signs must be of the highest quality design and construction. Seams are not permitted in the face of the sign cabinet.
- Light leaks are not permitted.
- Cabinet signs with the face panel routed out with Plexiglas laminated behind are not permitted. Push-through dimensional translucent Plexiglas letters that extend through the routed opaque sign face must be provided. Illumination is to be provided by neon, LED, or fluorescent lighting.
- The face panel is to be hinged to provide access for maintenance, or an access panel is to be located in an alternate concealed location. Where rear access is possible, a rear access hatch is preferred to reduce visibility.

### **Open Face Channel Letters With Exposed Tube Neon**

In certain cases, the Landlord may approve visually exposed neon signs where the neon is placed in a channel letter. Other creative and thematic utilization of neon tube may also be considered where the exposed neon tube is integral to a more intricate and thematic sign, such as a dimensional blade sign.

### **Face Illuminated Channel letters**

Face illuminated channel letters shall only be allowed for signage facing a public right-of-way, or parking areas where such signage is specifically controlled by the Municipal Signage Requirements. All such letters shall be evenly illuminated and directly mounted to the building facade in the required location.

## **Blade Sign**

Store facade blade signs are allowed where such signage adds to the design environment of the public walkway area and to the building facades overall. Blade signs are required to have a decorative appearance that compliments the building facade and enhances the Tenant's storefront image. The blade sign size and location must comply with the maximum projection requirements stated below and must be generally proportional to the Tenant facade.

- Minimum height to bottom: 9' -0" of the projecting element
- Horizontal projection requirements: Maximum Projection 4'-0", Minimum Projection 1'-0"

Horizontal projection requirements are measured from the Lease Line. No projection will be permitted to impede the flow of emergency vehicles.

The Tenant blade sign may be an iconic representation of the products and services offered. For instance, a bakery might have a large pastry as an iconic blade sign, provided that the design is approved by the Landlord in writing.

Decorative elements such as iron brackets or three dimensional sculptural panels are encouraged unless otherwise standardized. Flat panels with painted or vinyl graphics are not allowed. The Tenant's blade sign can be internally illuminated. An electrical raceway may be provided from which Tenant can wire to sign. All criteria that apply to other internally illuminated sign types apply in this case. The mounting height of the blade sign can extend higher than the Tenant's leased premises storefront, provided that the base building elevation can accommodate the location and there is no interference with base building architectural features or glazing.

## **Show Window Graphics**

Small-scaled, pedestrian level, adhesive window graphics are desired at the Tenant's show windows. Such store identification graphics shall be no more than 4" in height and located low on the window.

## **Storefront Address Signs**

In addition to show window graphics, a standardized address sign may be required by the Jurisdictional Authorities and/or the Landlord to provide a means of identifying store addresses for emergency purposes. Tenants are not permitted to place an address sign that does not meet this standard in material, type, size, and location.



## **Freestanding Signage**

### **Directional Signage**

Freestanding signs with trade names on them are permitted in the project so long as they are for purposes of directing patrons to specific stores, restaurants, the hotel, residential, or office tenants (“Directional Signs”, “Directional Signage”). The Directional Signs shall have multiple trade names on them, but no tenants or users under 3,000 square feet shall be permitted on the Directional Signs. The Directional Signs shall be located at major entrances into the project.

Directional Signs shall be constructed of board form concrete bases with pin mounted letters on two sides, and remotely illuminated by ground light fixtures. Directional Signs shall not be higher than five (5’) feet from the ground level in height and not more than eight (8’) feet in width.

### **Project Identification Monuments Signs**

The Developer intends to design, construct, and operate two (2) monument signs identifying the Project as “The Gateway” (the “Monument Signs”). The Monument Signs shall be constructed, operated, insured, and maintained consistent with the operation of a first-class property. The locations of the Monument Signs shall be at the northeast corner of the Project at the corner of Johnson Drive and Roe Avenue, and the southwest corner of the project at the corner of Shawnee Mission Parkway and Roeland Drive.

## **Temporary Signage**

For tenants and other users of the project, temporary signage will be allowed for a maximum of 4 weeks for any one temporary sign posting. Temporary signs may include professionally done vinyl banners temporarily affixed to a storefront, or posted within storefront glass. Temporary Signage may include the tenant’s name, “Now Open”, “Grand Opening date” etc., but must be submitted to and approved by Landlord, just as permanent signage.

## **RESTRICTIONS ON SMALL SHOP TENANTS ALONG JOHNSON DRIVE AND ROELAND DRIVE**

For all small shop tenants located in Buildings C, D, & E there shall be no awnings or any additional materials attached to the building that would protrude from the exterior building façade.

Tenants will not be permitted to sell or display merchandise on the exterior of their storefronts.

All public entrances for small shops in Buildings, C, D, & E will be facing the street, with the exception of access points for outdoor seating areas for restaurant tenants located in buildings C & D. Outdoor seating areas for these restaurant tenants shall be separated from the public outdoor space by use of railing or similar structure.



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## MEMORANDUM

To: Danielle L. Sitzman, AICP  
From: Dave Mennenga, P.E, PTOE and Beth Fry, P.E.  
Date: March 23, 2017  
Subject: Mission Gateway – Preliminary Reviews & Recommendations

- GBA has reviewed the Final Stormwater Management Study for The Gateway, dated March 8, 2017, and conditionally approves this submittal. The responses, revised grading plan, and revised calculations provided in the March 8 letter were found to be acceptable under the condition that the vents of the RCB will be taken into consideration during the final design of Building A.
- GBA has also reviewed the revised Traffic Impact Study for The Gateway, dated March 2017, and conditionally approves this submittal. There is one remaining issue regarding the omission of Table 3 and correction of subsequent Table references throughout the document that will be addressed directly with the developer's consultant. Otherwise, the responses provided and revisions made to the TIS report were found to be acceptable. The final approval of the Traffic Impact Study is conditional upon review by the Kansas Department of Transportation (KDOT), which maintains jurisdiction over US-56 (Shawnee Mission Parkway) adjacent to the proposed development.
- GBA will continue to work with the development team through the process of developing construction drawings to address the provision of a safe north-south pedestrian crossing at the intersection of Shawnee Mission Parkway and Roeland Drive. Traffic signal and geometric modifications will likely be needed to ensure that this intersection / crossing is compliant with all ADA and MUTCD requirements. Sheet FDP-C1.1 may also need revision to better define this accessible route across Shawnee Mission Parkway, depending on the future review comments to be provided by KDOT.
- There are still four internal regulatory R1-1 stop signs that should be added onto Sheet FDP-C1.2 (two signs for Drive 2 onto Roeland Drive, and two signs for Drive 3). GBA will continue to work with the development team to address these traffic control needs through the preparation of construction documents.
- GBA has reviewed the public street lighting plans and found the design layouts and fixture details to be generally in compliance with both City

and industry illumination standards. The illumination calculations provided did not include several fixtures along the north side of Johnson Drive and east side of Roe Avenue, or those on the combination poles at the signalized intersections, that would further increase the lighting levels provided.

- GBA has reviewed the on-site lighting plans and noted numerous pedestrian areas where the City's minimum one-half foot-candle illumination level criterion is not satisfied. Also, there seem to be other inconsistencies within the photometric calculation grids that were provided. These items will need to be revised by the development team and reviewed again to ensure that the final on-site lighting design satisfies the City's standards.
- Site plans will need to be modified during the construction document process to ensure that the vehicular pathway is defined with pavement markings, at a minimum, from the Drive 6 location onto Roe Avenue through the truck dock and into the interior parking areas of the site.
- In conjunction with the future re-platting of right-of-way dedication, it would be helpful for the development team to provide a comprehensive exhibit depicting existing and proposed curb lines, R/W and property lines, lane utilizations, and pavement markings on the adjacent public streets, particularly along the Roeland Drive corridor. Specifically, GBA will continue to work with development team regarding the provision of a supplemental northbound right-turn lane for inbound vehicles at either the Drive 1 or Drive 2 location as the construction documents are developed.
- Regarding the constrained width of the Johnson Drive streetscape adjacent to Building E, GBA reviewed the currently proposed configuration of the adjacent on-street parking bay to assess whether any modifications could be accommodated. It appears that the current layout provides a parking stall depth of approximately 20 feet. The Urban Land Institute's (ULI) "Dimensions of Parking" provides guidance that a minimum 19-foot vehicle projection distance is recommended for the proposed 60-degree angle parking. Based on the recommended parking dimension criteria, changing the angle of the proposed parking stalls would be one way to gain additional streetscape width. Minimum vehicle projection distances of 17'-7" and 15'-1" are required for 45-degree and 30-degree parking angles, respectively.

**THE GATEWAY DEVELOPMENT**  
**SHAWNEE MISSION PARKWAY (US-56) AND ROELAND DRIVE**  
**MISSION, KANSAS**

**TRAFFIC IMPACT STUDY**

**REVISED MARCH, 2017**

**OA Project No. 2015-2039**



**RECEIVED**  
**MAR 17 2017**  
**CITY OF MISSION**  
**COMMUNITY DEVELOPMENT**





## 1.0 INTRODUCTION & OBJECTIVE

This report studies traffic impacts for The Gateway mixed-use development, located on the site of the former Mission Mall at Shawnee Mission Parkway (US-56) and Roeland Drive in Mission, Kansas. The study was based on the site plan dated February 1<sup>st</sup>, 2017. However there have been numerous revisions to both the site plan and traffic study over the years so there are several references to previous work through the report. City of Mission staff was contacted regarding the scope of work for this study.

The objective of this study is to evaluate the existing traffic, roadway conditions and traffic impacts expected from the proposed development. The appropriate intersection geometrics and traffic control improvements necessary to accommodate the increased traffic on the study area roadways were identified. For the purposes of this study the following scenarios will be analyzed for the PM peak hour period for vehicular traffic operations:

- Existing conditions
- Existing plus Development conditions
- Future year 2035 conditions

Specifics regarding each scenario will be discussed in further detail later in the report. The existing study area intersections include the following, maintaining jurisdiction is noted after each intersection:

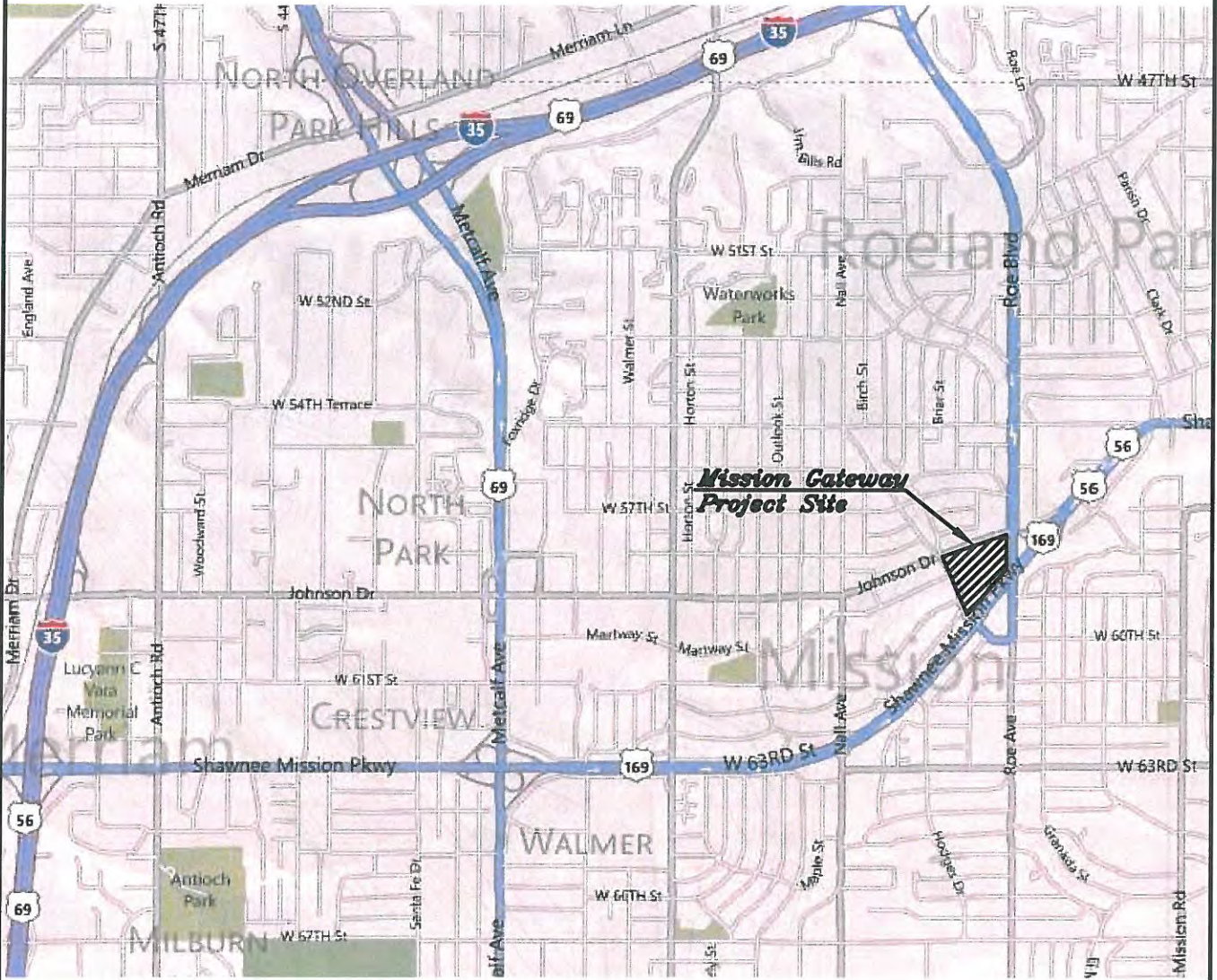
- Shawnee Mission Parkway (US-56) and Roeland Drive (KDOT)
- Roeland Drive and Martway Street (City of Mission)
- Roeland Drive and Johnson Drive (City of Mission)
- Johnson Drive and Roe Avenue (City of Mission)
- Site driveways and access points, as appropriate

US-56 is maintained by Kansas Department of Transportation (KDOT) and borders the proposed project. Thus, the report has been completed consistent with the policies and procedures described in the KDOT *Access Management Policy* where applicable on US-56. Remaining study intersections will be consistent using the City criteria along with engineering judgment. The approximate location of the development area is shown on the vicinity map, **Figure 1**. **Figure 2** illustrates the site plan for The Gateway development.





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Source: Bing Maps



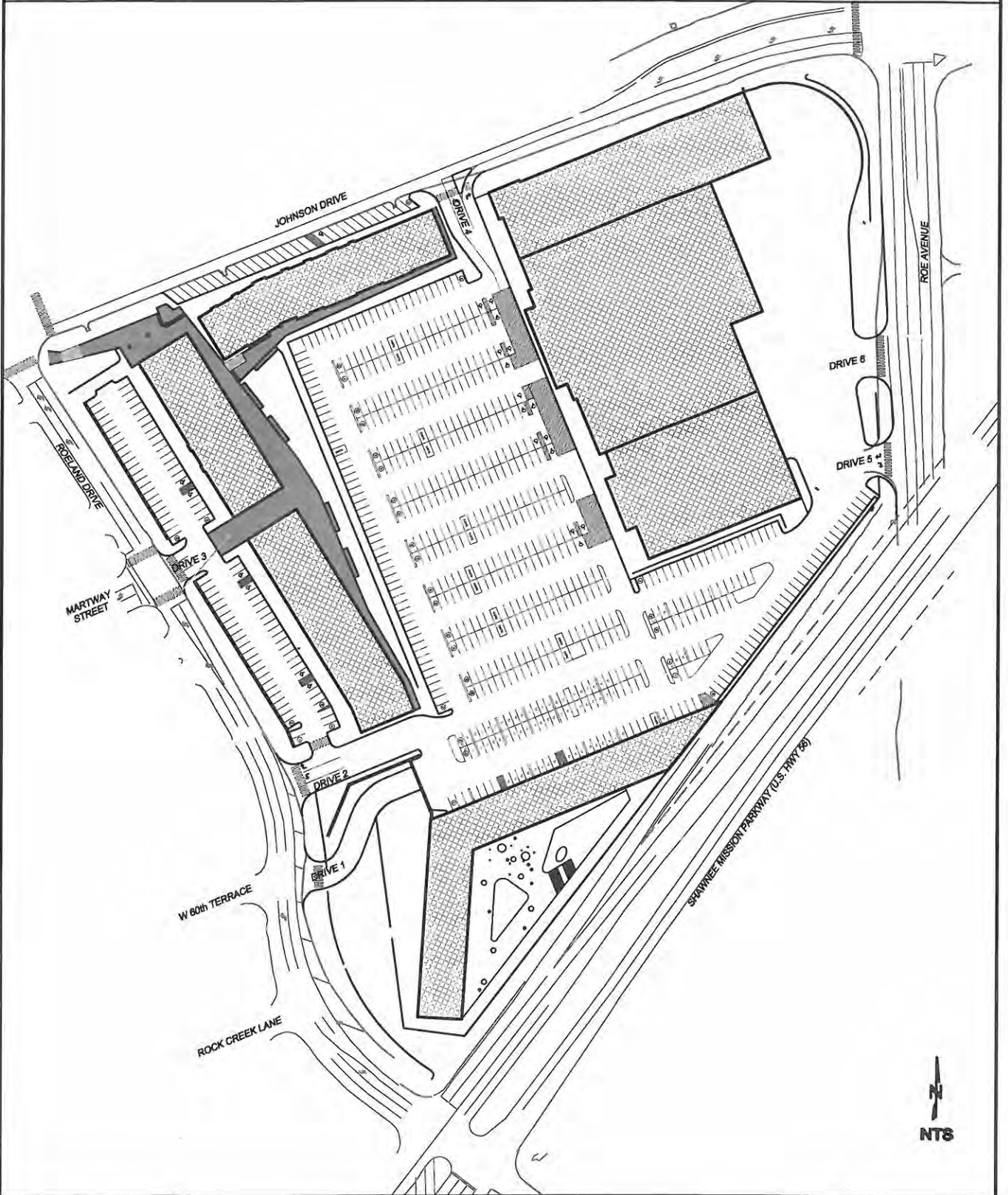
Vicinity Map

FIGURE

1



MISSION GATEWAY  
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**OLSSON**  
ASSOCIATES

Site Plan

FIGURE  
2

## 2.0 DESCRIPTION OF STUDY AREA

### 2.1 Proposed Development

An update to the site plan occurred since the preliminary review comments dated February 24<sup>th</sup>, 2017. At present, the site plan depicts the proposed development as an arrangement of buildings and central parking structure that will include several land-uses. The proposed mixed-use development consists of 175,799 square feet of retail space, 58,516 square feet of office, and 168 units of residential. This is a total decrease of 35,116 square feet in retail space, an increase of 3,970 square feet of office, a decrease of 200 hotel rooms, and a decrease of 14 residential units from the previous site plan. It should be noted that the decrease in square footage of the site will decrease the number of trips entering and exiting the site. To remain conservative, operations of the study area was analyzed with the same trips generated by the previous site plan. The development is bounded by Johnson Drive on the north, US-56 on the south, Roe Avenue on the east, and Roeland Drive on the west.

Access to the development is proposed from five access points, three on Roeland Drive, one on Johnson Drive, and one on Roe Avenue. Spacing for each of the following proposed drives were measured center to center.

Drive 1 is a full access drive which feeds into Level 2 of the main parking structure. Level 2 also provides access to Level 3 of the parking structure. The main parking structure provides parking for all land uses within the development, however Levels 2 and 3 are primarily used for the Hotel, Office, and Residential land uses. This access is located closest to the Hotel and is anticipated to be the major access for that particular land use. Drive 1 is located on Roeland Drive approximately 450 feet north of US-56, aligned across from West 60<sup>th</sup> Terrace.

Drive 2 is a full access drive proposed to be located approximately 120 feet north of Drive 1 along Roeland Drive, which leads into Level 1 of the parking structure. Drive 2 also provides access to frontage parking along the east side of Roeland Drive which will primarily be utilized by the retail land uses.

Drive 3 is proposed to align across from Martway Street and will provide access into the frontage parking along the east side of Roeland Drive. This drive will also primarily serve the retail land uses.

Drive 4 is a full access on Johnson Drive located approximately 560 feet east of Roeland Drive providing access into Level 1 of the parking structure. This drive is anticipated to primarily serve the retail components.

Drive 5 is an existing curb cut right-in/right-out drive on Roe Avenue located approximately 550 feet south of Johnson Drive serving primarily the retail development with access into Level 1 of the parking structure. One additional existing curb cut is a full

access servicing a truck loading dock gains access from Roe Avenue approximately 250 feet north of Drive 5.

## **2.2 Roadway Classification and Characteristics**

US-56 is a four-lane east/west divided major arterial roadway with a posted speed limit of 45 mph near the site. The roadway serves as a regional function connecting I-35 with the Country Club Plaza. US-56 falls under the jurisdiction of the Kansas Department of Transportation (KDOT). US-56 is classified as a Type D route per the KDOT *Access Management Policy* and a Principle Arterial by KDOT's Functional Classification map.

KDOT's Functional Classification map also shows roadway classifications for City streets as follows:

Roeland Drive is a three-lane north/south undivided local roadway with a posted speed limit of 25 mph near the site.

Johnson Drive is a three-lane east/west minor arterial roadway that terminates with US-56 east of Roe Avenue. The posted speed limit is 30 mph near the site.

Roe Avenue is a four-lane north/south divided arterial roadway with a posted speed limit of 35 mph near the site.

Martway Street is a three-lane east/west local roadway that terminates at Roeland Drive. The posted speed limit is 25 mph near the site. Rock Creek Trail currently resides along the south side of Martway Street and is proposed to cross Roeland Drive and continue north on the east side of Roeland Drive once the development is constructed.

## **2.3 Study Intersection Characteristics**

The intersection of Roeland Drive and US-56 is a signalized four-legged intersection. Dedicated left-turn lanes are provided for all movements at the intersection except the south approach. Dedicated right-turn lanes are provided for the southbound and eastbound movements. The intersection is currently split phased for north/south traffic. No Pedestrian accommodations are provided at the intersection.

The intersection of Roeland Drive and Martway Street is a signalized three-legged intersection. Dedicated left-turn lanes are provided for the northbound and eastbound movements at the intersection. Pedestrian accommodations are provided for the southbound movement at the intersection, including marked crosswalks, pedestrian indications, and push buttons. Currently, this signalized intersection operates in "Flash" mode for all periods of the day. During "Flash" operation mode, no pedestrian accommodations are provided.

The intersection of Roeland Drive and Johnson Drive is a signalized four-legged intersection. Dedicated left-turn lanes are provided for the northbound, eastbound, and westbound directions at the intersection. Dedicated right-turn lanes are provided for the

eastbound and westbound movements. Pedestrian accommodations are provided along the west and south legs of the intersection and include marked crosswalks, pedestrian indications, and push buttons.

The intersection of Roe Avenue and Johnson Drive is a signalized four-legged intersection. Dedicated left-turn lanes are provided for all movements at the intersection with dual left-turn lanes allocated for the eastbound movement. A dedicated right-turn lane is provided for the southbound movement. A channelized right-turn lane is provided for the northbound movement. Pedestrian accommodations are provided on the west and north legs of the intersection and included marked crosswalks, pedestrian indications, and push buttons.





### 3.0 DATA COLLECTION

Traffic count data was collected at the study intersections of US-56 and Roeland Drive, Roeland Drive and Martway Street, Roeland Drive and Johnson Drive, and Roe Avenue and Johnson Drive with the previous study and utilized within this study. AM and PM peak hour turning movement counts for study area intersections were collected from 6:30 to 8:30 AM and 4:30 to 6:30 PM respectively. Counts were taken between December 13<sup>th</sup>, 2011 and January 10<sup>th</sup>, 2012. This count data was determined to be suitable for existing analysis based on KDOT area count maps. Historical KDOT count maps were reviewed to determine if volumes along US-56 experienced significant change since 2011 when the data was collected. Based on KDOT count maps along US-56, traffic volumes have been flat or had a slight decrease since 2011. KDOT historical count maps from 2011, 2012, and 2013 can be found in the **Appendix**.

Considering traffic conditions, the highest volume of traffic occurs during the PM peak hour period. Reviewing the trip generation calculations discussed later in **Section 5.1**, the PM peak hour period is expected to generate the highest volume of new trips to the roadway network. Based on previous traffic impact studies for this development location, it was determined that the PM peak hour period conditions and associated improvements necessary for satisfactory operations would be more critical than the AM peak hour period conditions. Thus, operational analysis and review was completed for only the PM peak hour period.

The peak hour of traffic fluctuated between study intersections. To provide a baseline of traffic volumes, a consistent peak hour period was chosen for the study intersections. Based on the traffic count data collected the PM peak hour period is from approximately 5:00 to 6:00 PM.

Data sheets for the traffic counts are provided in the **Appendix**.

## 4.0 EXISTING TRAFFIC CONDITIONS

The analysis of existing conditions is based on the traffic counts collected for the study intersections. **Section 2.2** details roadway classification and intersection characteristics for the existing network. Existing traffic volumes used for analysis are illustrated in **Figure 3**. The existing intersection geometrics and traffic control for the study area intersections are illustrated in **Figure 4**.

### 4.1 Capacity Analysis

Signalized intersection capacity analyses were performed using SYNCHRO, version 9.1, based on the Highway Capacity Manual (HCM) delay methodology. Unsignalized capacity analyses were performed in accordance with chapter 17 of the HCM using the Highway Capacity Software (HCS2010), version 6.1. For simplicity, the amount of delay is equated to a grade or Level of Service (LOS) based on thresholds of driver acceptance. A letter grade between A and F is assigned, where LOS A represents the best operation. **Table 1** represents the LOS associated with intersection control delay, in seconds per vehicle (sec/veh), for signalized and unsignalized intersections.

**Table 1: Intersection Level of Service Summary**

Level of Service (LOS)	Level-of-Service Criteria	
	<u>Stop Control</u> Approach Delay sec/veh	<u>Signal Control</u> Control Delay sec/veh
A	≤ 10	≤ 10
B	>10 and ≤ 15	>10 and ≤ 20
C	>15 and ≤ 25	>20 and ≤ 35
D	>25 and ≤ 35	>35 and ≤ 55
E	>35 and ≤ 50	>55 and ≤ 80
F	>50	>80

Typically, the LOS for traffic signal controlled intersections should be LOS D or better. A LOS D or better at traffic signal controlled intersections is in regards to the overall intersection LOS; some individual movements may operate at a lower LOS. If an individual movement LOS is D or worse, queuing results will be reviewed to determine if queuing for the movement is accommodated. LOS D is desirable for stop controlled intersections; however, LOS E and F are often accepted during peak periods due low side street volumes waiting for gaps in the heavy mainline volume stream.

The Mid-America Regional Council (MARC) supplied timings for the signalized intersection of US-56 and Roeland Drive. Olsson Associates recently completed timing adjustments at the intersection of Roe Avenue with Johnson Drive for the City of Roeland Park, these timing adjustments were included in Mission Gateway analysis scenarios. Remaining signal timings and phasing for signalized intersections were acquired from the prior traffic study. As previously mentioned, the signalized intersection of Roeland Drive

with Martway Street is currently operating in “flash” mode with northbound/southbound movements observing the yellow, “caution” indication and the eastbound movement having the red, “stop” indication flashing, representing unsignalized stop control along Martway Street. The intersection was analyzed with both control methods of stop and signal control for comparative purposes.

Capacity analysis was completed as discussed above for the signalized study area intersections. **Table 2** details level of service for each signalized study intersection.

**Table 2: Existing Signalized Intersection Capacity Analysis**

Intersection	PM Peak Hour*
US-56 and Roeland Drive	C (28.5)
Roeland Drive and Martway Street	A (9.6)
Roeland Drive and Johnson Drive	B (14.0)
Roe Avenue and Johnson Drive	C (29.7)

\*LOS (Delay, in seconds)

During the PM peak hour period the overall operations at the signalized intersection are acceptable with a LOS C or better. All individual movements operate at LOS D or better during the peak hour periods with the following exceptions:

US-56 & Roeland Drive:

- During the PM peak hour period the westbound left-turn movement is operating at LOS F and the eastbound left-turn, southbound through, and northbound movements are operating at LOS E.
  - The low level of service for the westbound left-turn movement is attributed to the abrupt arrival rate of vehicles which may be a result of the westbound through movements’ queue length extending beyond the adjacent left-turn storage bay.
  - Remaining movements operating at LOS E are attributed to heavy main-line traffic volumes being accommodated. These movements’ 95<sup>th</sup>-percentile queue lengths are contained within the available storage.

Unsignalized capacity analysis was conducted for the intersection of Martway Street and Roeland Drive to replicate existing “flash” operations of the signalized intersection. Under the current conditions, the intersection operates acceptably as an unsignalized intersection.

Capacity analysis sheets are included in the **Appendix**. **Figure 5** illustrates the existing level of service for study intersections.

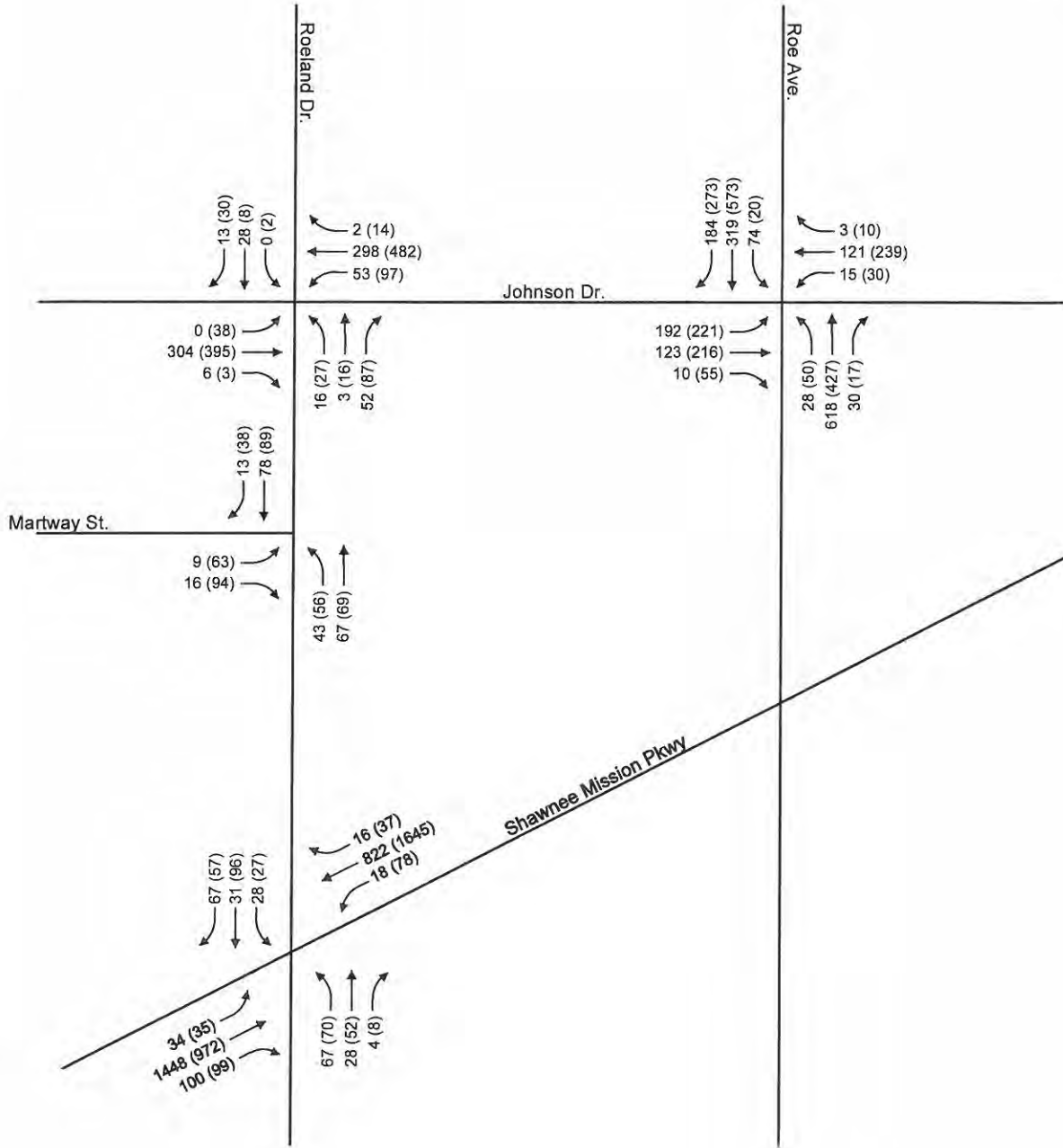


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#### **4.2 Existing Recommendations**

Study intersections are currently operating overall at acceptable levels of service. The intersection of Martway Street and Roeland Drive operates acceptably with signal timings in place as well as in “flash” operation. The need for a signal will be analyzed in the Existing plus Development scenario to determine if the intersection conditions warrant a signal.

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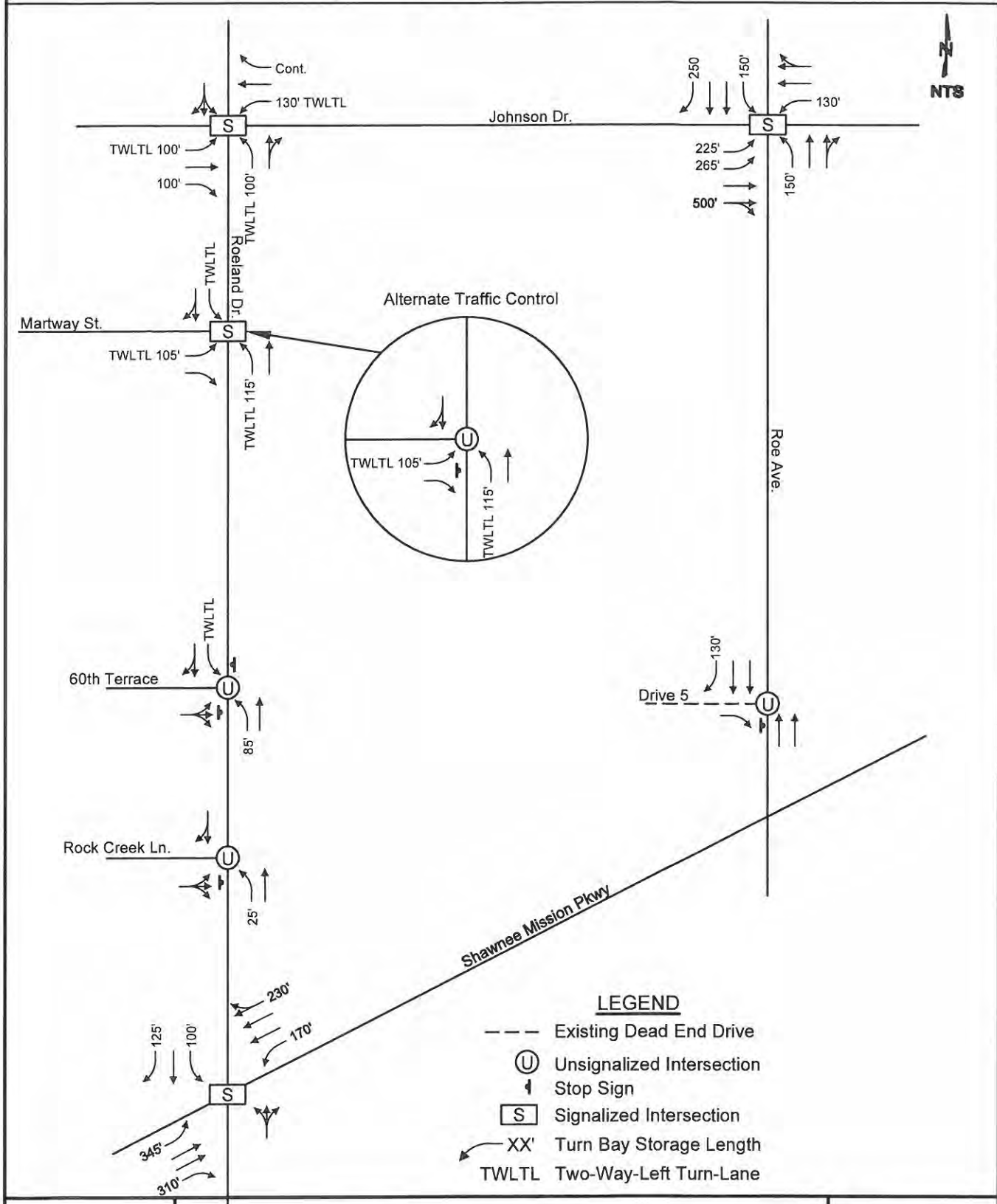
**LEGEND**  
XX (XX) - AM PM Peak Hour Volumes



Existing Peak Hour Volumes

FIGURE  
3

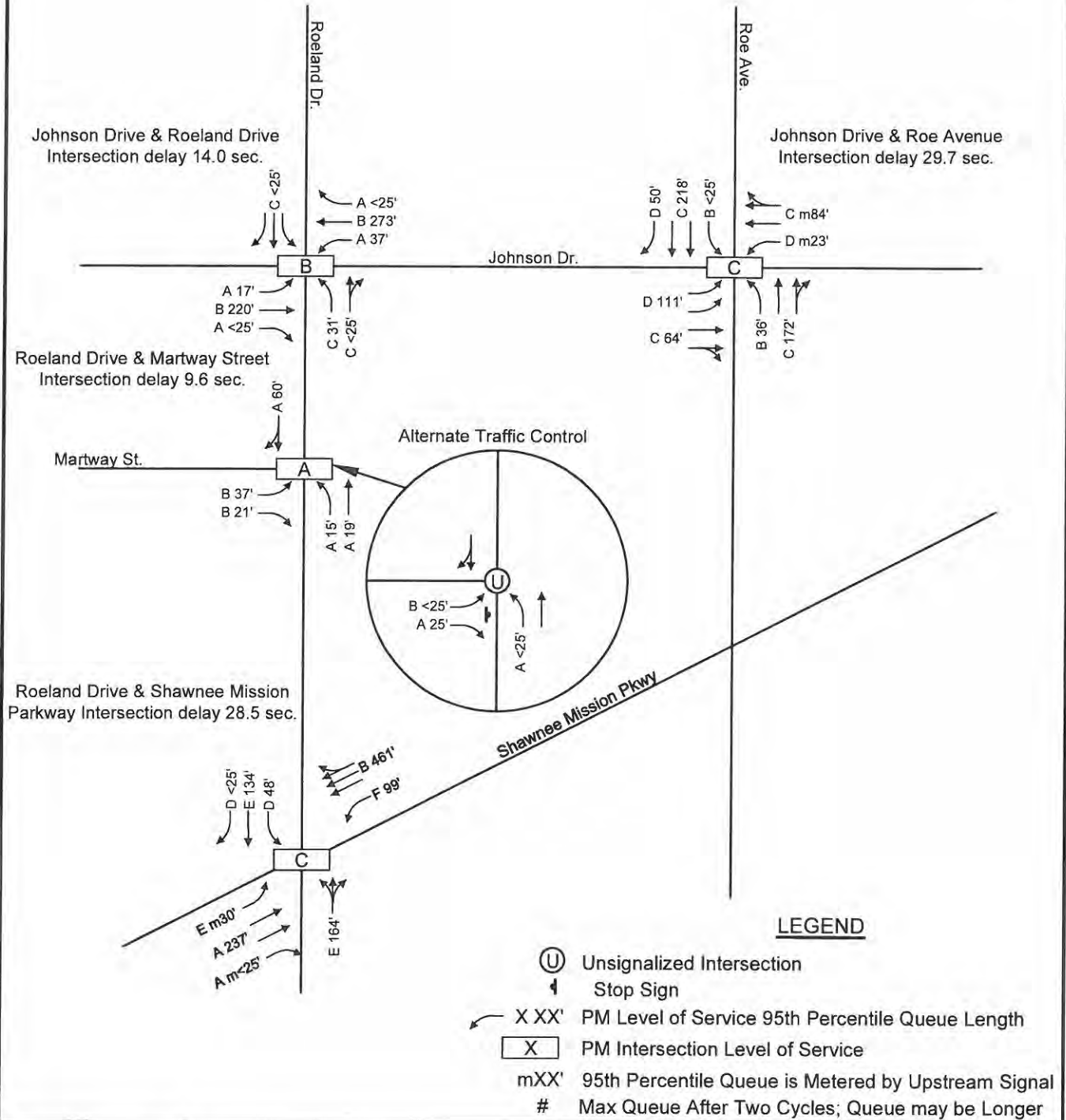
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Existing Lane Configurations & Traffic Control

FIGURE  
4

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## 5.0 EXISTING PLUS DEVELOPMENT CONDITIONS

It should be noted, an update to the site plan has occurred since the preliminary review comments dated February 24<sup>th</sup>, 2017. The proposed development is an arrangement of buildings and central parking structure that will include several land-uses. The proposed mixed-use development consists of 175,799 square feet of retail space, 58,516 square feet of office, and 168 units of residential. This is a total decrease of 35,116 square feet in retail space, an increase of 3,970 square feet of office, a decrease of 200 hotel rooms, and a decrease of 14 residential units from the previous site plan. It should be noted that the decrease in square footage of the site will decrease the number of trips entering and exiting the site. Operations of the study area was analyzed with the same trips generated by the previous site plan. Given the overall decrease in trips generated by the site compared to the previous site plan, the capacity analysis of this section is considered conservative. Actual operations with the study area is expected to be better than what is analyzed in this report.

Access to the development is described in **Section 2.1**. Access spacing, throat length, and geometrics will be reviewed considering the policies and procedures described in the *KDOT Access Management Policy* where applicable on US-56. Remaining study intersections will be consistent with the City of Mission standard guidelines and engineering judgment.

**Figure 2** illustrates the proposed site plan.

### 5.1 Trip Generation Comparison

An updated site plan was provided on February 1<sup>st</sup>, 2017. Changes to the site plan depict a land use change to the building in the northeast section of the property. In the previous October 2015 study, the original use for the facility was intended to be a 158,800 square foot *Free Standing Discount Superstore*. The current site plan shows the building divided into three separate retail stores totaling 123,684 square feet. To accurately describe the buildings functionality, the land use was updated from *Free Standing Discount Superstore* to *Shopping Center*. Other updates include a total increase in 3,970 square feet of office, the removal of 200 hotel rooms, and removal of 14 residential units as stated in **Section 5.0**.

Trips are expected to decrease by 1,316 vehicles during an average weekday and a decrease of 115 vehicles is expected during the PM peak hour, compared to trips generated by the previous site plan. Trip distribution remained unchanged from the previous site plan. A comparison between both land uses are shown in **Table 3**. The updated site plan can be found in **Figure 2**.

---

### Table 3: Trip Generation Update

#### **5.2 Trip Generation and Distribution**

Trip generation characteristics expected for the development are shown in **Table 3**. These characteristics are based on trip generation data included in the Institute of Transportation Engineers (ITE) *Trip Generation Manual* (9<sup>th</sup> Edition). For trip generation determination, the site's land uses were classified as a mixed-use development including Shopping Center, Apartment, General Office Building, Hotel, and Specialty Retail Center. Trip generation was based on the square footage of the retail and office developments while residential and hotel trip generation was based on the dwelling units.

The proposed development is expected to generate 11,270 daily trips on an average weekday. The site is expected to generate 436 trips during the AM peak hour period, and 1,216 trips during the PM peak hour period.

As discussed in **Section 3.0** the PM peak hour period was determined most critical and operational analysis and review was completed for this period.



**Table 4: Proposed Development Trip Generation**

Daily Trip Generation									
ITE Code/Page	Land Use	Size		Trip Gen. Avg. Rate/Eq.	Daily Trips	Trip Distribution		Daily Trips	
						Enter	Exit	Enter	Exit
820/1561	Shopping Center	123,684	SF	Equation	7798	50%	50%	3,899	3,899
220/333	Apartment	182	DU	Equation	1,227	50%	50%	614	613
710/1259	General Office Building	54,540	SF	Equation	828	50%	50%	415	413
310/613	Hotel	200	Rooms	Equation	1,417	50%	50%	709	708
826/1579	Specialty Retail Center	52,115	SF	Equation	2,268	50%	50%	1,134	1,134
<b>Total</b>					<b>11,270</b>			<b>6,771</b>	<b>6,767</b>

AM Peak Hour Trip Generation (Adjacent Street)									
ITE Code/Page	Land Use	Size		Trip Gen. Avg. Rate/Eq.	AM Peak Hour Trips	Trip Distribution		AM Peak Hour Trips	
						Enter	Exit	Enter	Exit
820/1562	Shopping Center	123,684	SF	Average	119	62%	38%	74	45
220/336	Apartment	182	DU	Equation	93	20%	80%	19	74
710/1260	General Office Building	54,540	SF	Equation	118	88%	12%	104	14
310/614	Hotel	200	Rooms	Equation	106	59%	41%	63	43
826/**	Specialty Retail	52,115	SF	-	0	50%	50%	0	0
<b>Total</b>					<b>436</b>			<b>260</b>	<b>176</b>

\*\*No ITE AM Trip Estimation Available

PM Peak Hour Trip Generation (Adjacent Street)									
ITE Code/Page	Land Use	Size		Trip Gen. Avg. Rate/Eq.	PM Peak Hour Trips	Trip Distribution		PM Peak Hour Trips	
						Enter	Exit	Enter	Exit
820/1563	Shopping Center	123,684	SF	Equation	691	48%	52%	332	359
220/337	Apartment	182	DU	Equation	118	65%	35%	77	41
710/1261	General Office Building	54,540	SF	Equation	140	17%	83%	24	116
310/581	Hotel	200	Rooms	Average	120	51%	49%	62	58
826/1580	Specialty Retail	52,115	SF	Equation	147	44%	56%	65	82
<b>Total</b>					<b>1,216</b>			<b>560</b>	<b>656</b>

After determination of trip generation for the site, multi-use development (captured trips) and pass-by trips (re-assignment) were determined for the PM peak hour. Based on information in the *Trip Generation Manual*, multi-use trip reductions should be applied first and then pass-by assignment can be considered. When a site consists of multiple land uses, total trips to a development can be reduced due to internal capture on the site. These are trips that stay within the development area and do not leave the site to travel to office or retail sites, for example. The multi-use reduction percentage is determined using various tables included in the *Trip Generation Manual*. Worksheets used to determine the multi-use reduction percentage are included in the **Appendix**. Based on the worksheets, the multi-use percentage was determined to be 11%, 12%, and 34% for retail, office, and residential trips respectively.



After multi-use trip reduction is applied to the total development volumes, pass-by can be applied. Primary, multi-use, and pass-by distribution were performed using SYNCHRO *Traffic Impact Analysis* application. When considering pass-by, trips to a site can be classified as pass-by or primary. Primary trips account for those drivers making a specific trip to a site. For example, a driver traveling from work directly home would be a primary trip. Pass-by trips are those trips which divert from their intended destination. For example, a driver is traveling from work to home on a roadway adjacent to the site, but decides to stop and visit a retail site. These pass-by trips are re-assigned within the network via roadways adjacent to the project site. According to the ITE Trip Generation Handbook, the pass-by trips for a retail shopping center vary from 12% to 89% during the PM peak hour period. To be conservative, 15% pass-by trips during the PM peak hour was used for this study.

Considering multi-use (internal capture) calculations, 464 trips are expected to enter the site and 560 trips are expected to exit the site during the PM peak hour period. For the re-assignment of pass-by trips, the total PM peak hour primary trips for the development is expected to be 411 trips entering and 501 trips exiting. The pass-by trips are expected to be 53 trips entering and 59 trips exiting. Complete trip generation data is illustrated in Table 3, Table 4 illustrates site internal interaction with primary or pass-by trip information.

**Table 5: Trip Generation with Internal Interaction and Pass-by Trips**

Land Use	Total Development Plan													
	Intensity	ITE Code	Daily	AM Peak Hour				PM Peak Hour						
				Total	% In	% Out	In	Out	Total	% In	% Out	In	Out	
Shopping Center	123,684	SF	820	7,798	119	62%	38%	74	45	691	48%	52%	332	359
Apartment	182	DU	220	1,227	93	20%	80%	19	74	118	65%	35%	77	41
General Office Building	54,540	SF	710	828	118	88%	12%	104	14	140	17%	83%	24	116
Hotel	200	DU	310	1,417	106	59%	41%	63	43	120	51%	49%	62	58
Specialty Retail	52,115	SF	826	2,268	0	50%	50%	0	0	147	44%	56%	65	82
Fast-Food - Without Drive-Thru	0	SF	933	0	0	60%	40%	0	0	0	51%	49%	0	0
	<b>Total Trips</b>		<b>13,538</b>	<b>436</b>				<b>260</b>	<b>176</b>	<b>1,216</b>			<b>560</b>	<b>656</b>
	*Internal Trips			—				—	—	192			96	96
	*External Trips			436				260	176	1,024			464	560
	**15% Pass By Trips			0						112			53	59
	**Non Pass By Trips			436				260	176	912			411	501

A traffic distribution was developed for the proposed site considering the usage of the site, access to the adjacent roadway network, and the distribution used in the approved study. Traffic distribution has changed slightly from the previously approved study in that more trips have been assigned to Roe Avenue traveling south and less trips traveling south on Roeland Drive. Trip generation and distribution was provided to the City's traffic consultant and approved with the previous study. The distribution for trips generated from the site is illustrated in Table 5. Slight modifications to the distribution were made when distributing trips to account for the redundant access to the site from the south and east.

**Table 6: Traffic Distribution**

Roadway To/From	Distribution	by Direction
North (Roe Avenue)	15%	20%
North (Roeland Drive)	5%	
South (Roe Avenue)	10%	20%
South (Roeland Drive)	10%	
East (Shawnee Mission Pkwy)	15%	25%
East (Johnson Drive)	10%	
West (Shawnee Mission Pkwy)	20%	35%
West (Johnson Drive)	10%	
West (Martway Avenue)	5%	
<b>Total</b>	<b>100%</b>	<b>100%</b>

The PM peak hour period trips for the development, following distribution and assignment to the roadway network, are illustrated in **Figure 6**. Trips associated with the proposed development were added to the existing plus approved development traffic volumes. The resulting existing plus development traffic volumes are illustrated in **Figure 7**.

### **5.3 Driveway Spacing**

Five access points are proposed to provide access to the development. All drive spacing was measured by using center-to-center criteria. Three drives are proposed along Roeland Drive.

Drive 1 is proposed to align with 60<sup>th</sup> Terrace as an unsignalized full access drive.

Drive 2 is proposed to be located approximately 120' north of Drive 1 as an unsignalized full access drive with two exiting lanes.

Drive 3 is proposed to align with the intersection of Martway Street at Roeland Drive as the east leg with two exiting lanes.

Drive 4 is proposed along Johnson Drive approximately 560' east of Roeland Drive as full access drive with two exiting lanes.

Drive 5 is proposed at an existing right-in/right-out curb cut along Roe Avenue. Approximately 150' north of Drive 5, measured center to center, a separate drive is provided for truck traffic.

US-56 is classified as a Class D route per the KDOT *Access Management Policy* and a Principle Arterial by KDOT's Functional Classification map. Per the KDOT *Access Management Policy*, adequate upstream and downstream spacing for the full access Drive 1 is provided.

Remaining sight access points were analyzed based on KDOT's *Access Management Policy*. Based on this policy, all remaining driveway spacing is acceptable for unsignalized accesses.



## **5.4 Signal Warrant Analysis**

As stated previously, the signalized intersection of Martway Street and Roeland Drive currently operates in “flash” mode, effectively making the intersection a stop controlled intersection for eastbound movements along Martway Street. Thus, the warrants for signalization were evaluated to determine if the need for a traffic signal still exists with the proposed development volumes.

A traffic signal may be justified if traffic conditions meet any of the applicable eight signal warrants described in the 2009 Manual on Uniform Traffic Control Devices (MUTCD). The MUTCD provides criteria for conducting an engineering study to determine whether a traffic signal is appropriate at any intersection. Those criteria are embodied in the eight traffic signal warrants. Data collection from 2011 provides enough information to analyze Warrant 3, Peak Hour traffic volumes. Warrant 3 is intended for use at a location where traffic conditions are such that for a minimum of 1 hour of an average day, the minor-street traffic suffers undue delay when entering or crossing the major street.

The results of analyzing Warrant 3 on anticipated Existing Plus Development conditions indicate that the projected traffic volumes at Martway Street and Roeland Drive do not warrant a signal. Although a signal warrant is not met for at Martway Street and Roeland Drive consideration should be given to keep the signal in place due to the signal being a vital connection for pedestrians along the Rock Creek Trail. The signal will allow pedestrians traveling along the trail to cross the street safely. No other warrants for signalization were analyzed due to limited data. The signal will need to be modified to accommodate Drive 3 and for pedestrians crossing Roeland Drive if the signal is to remain. Capacity analysis results will be utilized to determine if the intersection operates efficiently as two-way stop controlled.

## **5.5 Auxiliary Turn Lanes**

US-56 is maintained by Kansas Department of Transportation (KDOT) and borders the proposed project. Thus, the report has been completed consistent with the policies and procedures described in the KDOT *Access Management Policy* where applicable. These methods are used to determine when turn lanes are required at study area intersections, in addition to noting when turning volumes warrant auxiliary lane improvements. The Access Management Policy and queue length information obtained from analysis reports were used to determine the recommended length of turn lanes at existing intersections and at proposed site drives as necessary.

### **5.5.1 Left-Turn Lanes**

The KDOT *Access Management Policy* provides guidance regarding the recommendation of left-turn storage along drives. Queue lengths at study intersections were reviewed to determine if the appropriate turn bay length is provided. The following left-turn lane improvements are recommended at study area intersections based on the minimum recommendations as described in **Section 5.4**:

#### **Roeland Drive and Drive 1**

- Re-stripe the north approach to maximize the length of the southbound left-turn in the existing two-way left-turn lane.

#### Roeland Drive and Drive 2

- Re-stripe the north approach to provide a 75-foot southbound left-turn in the existing two-way left-turn lane.
  - This should provide reverse left-turn lane striping between Martway Street and Drive 2.
- According to the site plan a northbound right turn lane should be installed. The addition of a northbound right turn lane will ensure that inbound vehicle blockages do not impact the northbound through movement.

#### Roeland Drive and Martway Street/Drive 3

- Provide a separate left-turn lane to mirror the west approach and remove left-turn traffic from the shared through/right-turn lane.
- Re-stripe the north approach to provide a 100-foot southbound left-turn in the existing two-way left-turn lane.
  - This should provide reverse left-turn lane striping between Johnson Drive and Martway Street.

### **5.5.2 Right-Turn Lanes**

The KDOT *Access Management Policy* also provides guidance regarding the recommendation of right-turn lanes. Queue lengths at study intersections were reviewed to determine if the appropriate turn bay length is provided. The following right-turn lane improvements are recommended at study area intersections based on the minimum recommendations as described in **Section 5.4**:

#### US-56 and Roeland Drive

- An auxiliary right-turn lane is warranted under Existing conditions primarily due to through volumes, with or without the proposed development. Currently, a third through lane is added on the outside of the westbound US-56 lanes. This lane is existing pavement from the previously removed entrance ramp at Roe Avenue and the third lane is continued for approximately 650 feet west of Roeland Drive. This added through lane is underutilized by through traffic at its current state due to the short distance upstream and downstream of Roeland Drive and acts as a right-turn lane. Thus, no improvements of an additional right-turn lane are recommended at the intersection.

#### Johnson Drive and Drive 4

- Provide separate left and right-turn lanes on the south leg of the intersection exiting the site.
  - The right-turn lane should be made to release traffic into the existing second eastbound through lane on the outside of Johnson Drive. Provisions should be made to eliminate the potential for eastbound through traffic to merge



into the outside through lane across Drive 4. As generally shown on the site plan, **Figure 2**.

#### Roe Avenue and Drive 5

- There is currently a 130' right turn lane depicted in the site plan at Drive 5. This turn lane should be extended 120' for a total of 250' to accommodate both the Truck entrance and Drive 5.

#### Roeland Drive and Drive 2

- The addition of a northbound right turn lane would ensure that inbound vehicle blockages do not impact the northbound through movement. However, due to 20' of throat distance at Drive 2 it is not recommended to place a northbound right turn lane.

#### Roeland Drive and Drive 1

- To ensure that inbound vehicle blockages do not impact the northbound through movement, a northbound 130' right turn lane plus taper is required. Contrary to Drive 2, Drive 1 has an acceptable throat distance which will allow for the installation of an auxiliary right turn lane.

**Figure 8** details recommended lane configurations and storage lengths.

### 5.6 Driveway Throat Length

Driveway throat lengths are important to address during site plan development to provide for adequate room to maneuver when transitioning from City streets to internal site areas. Inadequate throat lengths can result in slow traffic operation, safety, and capacity issues. Queues can block access on the intersection approach or result in delays for right-turning vehicles off City Streets.

Drive 1 provides approximately 200' of uninterrupted throat distance and is expected to be acceptable.

Drive 2, in a more typical parking lot configuration, only provides 20' of throat distance before access to parking is provided north of the Drive. As standard, it is recommended to provide an additional 55' for a total of 75' of throat to accommodate intersection queueing and room for vehicles to maneuver. However, given the constraints of the site, and understanding that additional queueing within an internal parking lot that has other exit options is more acceptable than in public streets, it is suggested, as an acceptable solution, to provide "Do Not Block Intersection" signing for westbound vehicles approaching Roeland Drive at the parking lot drive. It should be noted that with separate left and right-turn lanes for exiting traffic, queueing is anticipated to be less than two vehicles and is not anticipated to block access to the parking lot for entering vehicles.

Drive 3 also provides approximately 20' of throat distance and similar to Drive 2, it is recommended to provide an additional 55' for a total of 75' of throat to accommodate

intersection queueing and room for vehicles to maneuver. Also similar to Drive 2, it is understood that queueing within the internal parking lot is more acceptable, and given the site constraints, other exit points, and as noted that with separate left and shared through/right-turn lanes provided, queueing is anticipated to be approximately 1 vehicle with signal control or stop controlled approaches and would be contained within the given throat length. In addition to throat distance recommendations at Drive 3, it is suggested to provide signing to allow entering vehicles the right-of-way at the intersection of Drive 3 with the internal parking lots.

Drive 4 provides approximately 125' of uninterrupted throat distance and is expected to be acceptable as the queue length expected for both the northbound left and right-turn movements is one vehicle.

Drive 5 is proposed to provide approximately 60' of throat distance and is recommended to be reconfigured to provide a minimum of 75' of throat distance.

Recommended driveway throat lengths are illustrated in **Table 6**.

**Table 7: Recommended Driveway Throat Lengths**

Drive	Throat Length	
	Proposed	Recommended
Drive 1	200'	200'
Drive 2	20'	75'
Drive 3	20'	75'
Drive 4	125'	125'
Drive 5	60'	75'

**5.7 Truck Movements**

The development plans indicate several possible truck movements. It is essential with a site of this intensity to consider truck and emergency access routing to and through the site. Where raised medians on driveways force wide swings into public streets, raised medians should be cut back or removed. Corner radii at driveways should be sized to accommodate anticipated trucks.

The site plan includes a truck entrance on Roe Avenue just north of Drive 5. The curb cut for Drive 5 and a 130' southbound right turn lane was included with the Roe Avenue roadway design plans and is currently in place. The site plan has since been revised, from previous site plans, to allow trucks entering the site the ability to maneuver internally, without impeding traffic along Roe Avenue. However, since the Truck entrance is located just after the lane taper or the existing right turn lane for Drive 5, we recommend that the existing turn lane be extended 120' for a total of 250' to accommodate both the Truck entrance and Drive 5. It is also recommended to provide edge striping internal to the site between Drive 6 and Drive 5 to delineate the westbound driving path from the dock area.

**5.8 Pedestrian Accommodations**

Access to the Rock Creek Trail is at Roeland drive and Martway Street. As described in **Section 2.3**, although the signal at this intersection is currently in "Flash" operation pedestrian accommodations include marked crosswalks, pedestrian indications, and push buttons. It is recommended to update these facilities in compliance with ADA Title II requirements. Adequate curb ramp design with detectable warnings and vibrotactile pedestrian push buttons should be included in every direction. Due to its close proximity to the Rock Creek Trail, Roeland Drive and Johnson Drive should also be updated with these accommodations.

Pedestrian accommodations should also be provided long the southern leg of Shawnee Mission Parkway and Roeland Drive. This will provide connectivity to the sidewalks ending just before the intersection. As described above, accommodations should be updated to ADA standards, this includes adequate ramp design with detectable warnings and vibrotactile push buttons. In addition, due to the width of the south leg of the intersection, pedestrian refuge should be provided within the medians.

## **5.9 Capacity Analysis**

**Section 4.1** details the methods used for capacity analysis. **Table 7** represents the LOS and delay associated with the signalized study intersections. To complete signalized capacity analysis and provide a direct comparison accounting for changes in traffic volumes at the study intersections, signal timings remained consistent with existing timings, except for the US-56 and Roeland Drive intersection. At the intersection of US-56 and Roeland Drive the southbound movement was given additional green time to account for the vehicles exiting the site during the PM peak hour period.

Capacity analysis sheets for signalized intersections are included in the **Appendix**.



**Table 8: Existing plus Development Signalized Intersection Capacity Analysis**

Intersection	PM Peak Hour*
US-56 and Roeland Drive	C (19.0)
Roeland Drive and Martway Street/Drive 3	B (11.2)
Roeland Drive and Johnson Drive	B (18.0)
Roe Avenue and Johnson Drive	C (30.8)

\*LOS (Delay, in seconds)

The overall LOS for the signalized study intersections is expected to remain a LOS C or better with timing modifications at US-56 and Roeland Drive. Capacity analysis sheets illustrating this improvement are provided in the **Appendix**.

US-56 & Roeland Drive:

- During the PM peak hour period at US-56 and Roeland Drive the existing deficiencies remain with the westbound left-turn and northbound movements. 95<sup>th</sup>-percentile queue lengths are anticipated to increase by less than two vehicles over Existing conditions.
- The southbound left-turn movement’s 95<sup>th</sup>-percentile queue length is expected to exceed the available storage length during the PM peak hour period. The southbound through movement’s 95<sup>th</sup>-percentile queue length is expected to extend past the Roeland Drive and Rock Creek Lane intersection during portions of the PM peak hour period blocking the intersection. If blocking would occur, it is anticipated that the effect on the northbound left-turn movement turning onto Rock Creek Lane would be minimal. This is not anticipated to occur during other periods of the day. The 95<sup>th</sup>-percentile queue represents the queue length that has a 5 percent probability of being exceeded during the peak hour period. Thus the queueing, if it were to occur, would be expected to be a limited occurrence.
  - It is recommended to install signage for southbound traffic at Rock Creek Lane and Roeland Drive to prevent road users from blocking access into and out of Rock Creek Lane during periods of high demand.

With the exception of the intersection of US-56 and Roeland Drive, as discussed above, other study intersections are expected to operate at acceptable LOS.

Unsignalized analysis was conducted for the remaining unsignalized study intersections. All movements at unsignalized intersections are expected to operate at LOS D or better during the PM peak hour period considering Existing plus Development conditions. To minimize the delay and queuing separate left-turn and right-turn lanes for exiting vehicles are recommended at Drives 2, 3 and 4 as depicted in the site plan.

Taking into consideration the signal warrant analysis and current operational state of the intersection of Roeland Drive with Martway Street/Drive 3 this intersection was analyzed

as a two-way stop controlled intersection with stop control along Martway Street and Drive 3. The resulting LOS analysis indicate that the intersection will work at acceptable levels with side-street delay being minimal. Although signal warrant analysis indicates that a signal is no longer warranted and capacity analysis suggests that the signal would perform adequately under unsignalized conditions, consideration should be given to keep the signal in place due to the signal providing connection for pedestrians along the Rock Creek Trail.

Figure 9 illustrates the Existing plus Development level of service for study intersections.

### **6.0 Existing plus Development Recommendations**

Study intersections are expected to operate at acceptable levels of service for the Existing Plus Development scenario considering the following recommended improvements:

#### **Roeland Drive and Drive 1:**

- Re-stripe the north approach to maximize the length of the southbound left-turn in the existing two-way left-turn lane.
- To ensure that inbound vehicle blockages do not impact the northbound through movement, a northbound 130' right turn lane plus taper is required. Contrary to Drive 2, Drive 1 has an acceptable throat distance which will allow for the installation of an auxiliary right turn lane.

#### **Roeland Drive and Drive 2:**

- Provide a separate left-turn and right-turn lane for exiting traffic.
- Re-stripe the north approach to provide a 75-foot southbound left-turn in the existing two-way left-turn lane.
  - This should provide reverse left-turn lane striping between Martway Street and Drive 2.
- It is recommended to provide 75' of throat distance at the intersection to allow entering vehicles to safely maneuver into the parking area.
- Provide "Do Not Block Intersection" signing for westbound vehicles approaching Roeland Drive at the parking lot drive.
- The addition of a northbound right turn lane would ensure that inbound vehicle blockages do not impact the northbound through movement. However, due to 20' of throat distance at Drive 2 it is not recommended to place a northbound right turn lane.

#### **Roeland Drive and Martway Street/Drive 3:**

- Provide a separate left-turn lane to mirror the west approach and remove left-turn traffic from the shared through/right-turn lane.
- Re-stripe the north approach to provide a 100-foot southbound left-turn in the existing two-way left-turn lane.
  - This should provide reverse left-turn lane striping between Johnson Drive and Martway Street.



- It is recommended to provide 75' of throat distance at the intersection to allow entering vehicles to safely maneuver into the parking area and store queued vehicles exiting the site to City streets.
- Provided adequate pedestrian accommodations in compliance with ADA standards.
- Provide signing to allow entering vehicles the right-of-way at the intersection of Drive 3 with the internal parking lots. This eliminates the potential for entering traffic to queue back into City streets.
- Although a signal warrant is not met for the intersection, consideration should be given to keep the signal in place due to the signal providing connection for pedestrians along the Rock Creek Trail. The signal will allow pedestrians traveling along the trail to cross the street safely.
  - Modify signal to accommodate Drive 3 and for pedestrians crossing Roeland Drive if the signal is to remain.

#### Johnson Drive and Drive 4:

- It is recommended to provide 125' of throat distance at the intersection to store queued vehicles exiting the site to City streets.
- Provide separate exiting left and right-turn lanes at the intersection.
  - The right-turn lane should be made to release traffic into the existing second eastbound through lane on the outside of Johnson Drive. Provisions should be made to eliminate the potential for eastbound through traffic to merge into the outside through lane across Drive 4.


#### Roe Avenue and Drive 5:

- As stated in Existing plus Development conditions, there is currently a 130' right turn lane depicted in the site plan at Drive 5. This turn lane should be extended 120' for a total of 250' to accommodate both the Truck entrance and Drive 5.
- It is also recommended to provide edge striping internal to the site between Drive 6 and Drive 5 to delineate the westbound driving path from the dock area.

#### US-56 and Roeland Drive:

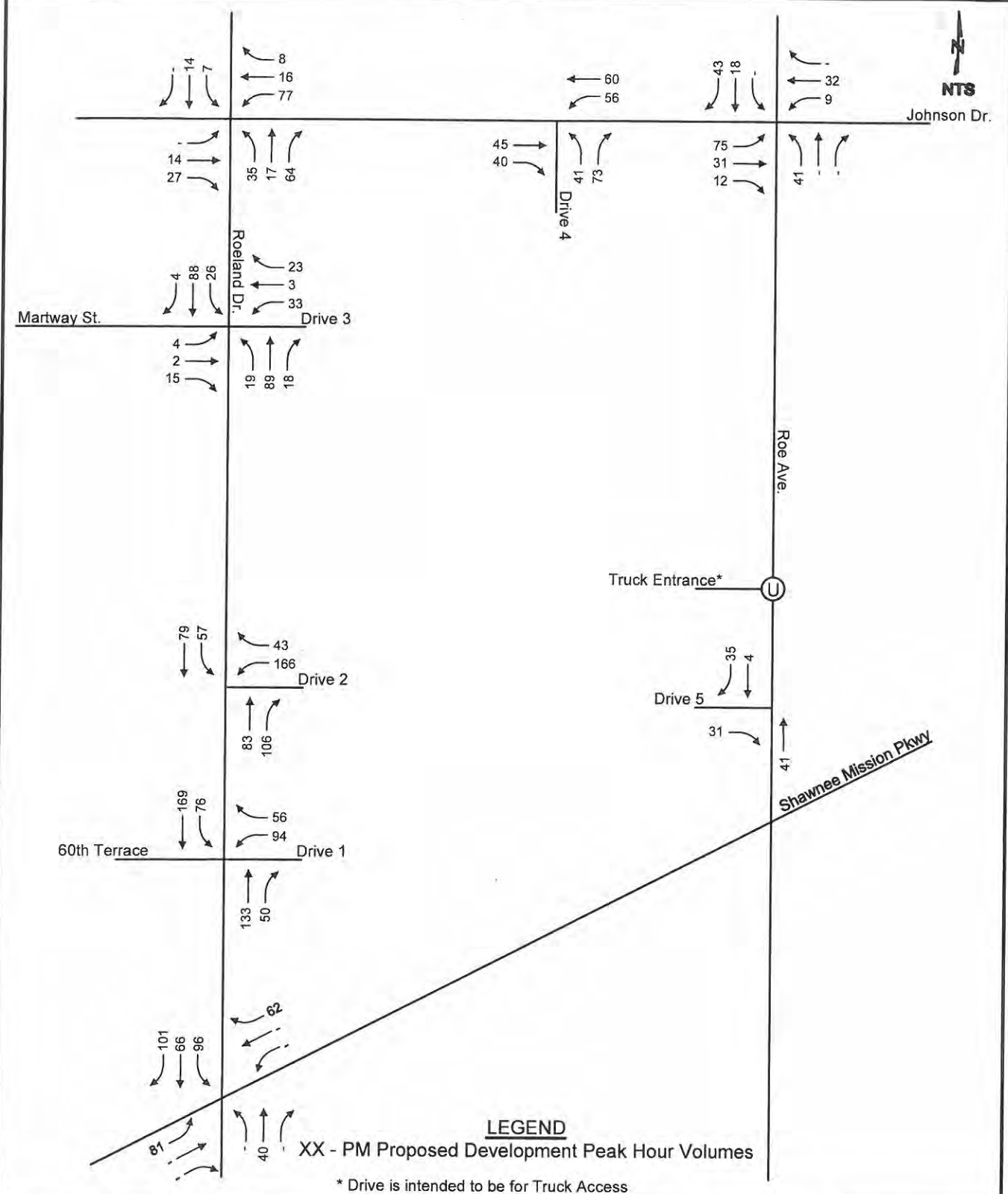
- Due to the increased demand on the north approach of the intersection, it is recommended to provide additional green time of up to 5 seconds to the southbound movements. This additional time will relieve congestion and reduce potential queue lengths on the approach while only minimally impacting the mainline traffic along US-56.
  - It is recommended to install signage just north of Rock Creek Lane along Roeland Drive to prevent road users from blocking access into and out of Rock Creek Lane during periods of high demand.
- Provided adequate pedestrian accommodations along the south leg of the intersection in compliance with ADA standards.

#### Johnson Drive and Roeland Drive:

- 
- It is recommended to provide adequate pedestrian accommodations along the south leg of the intersection in compliance with ADA standards.



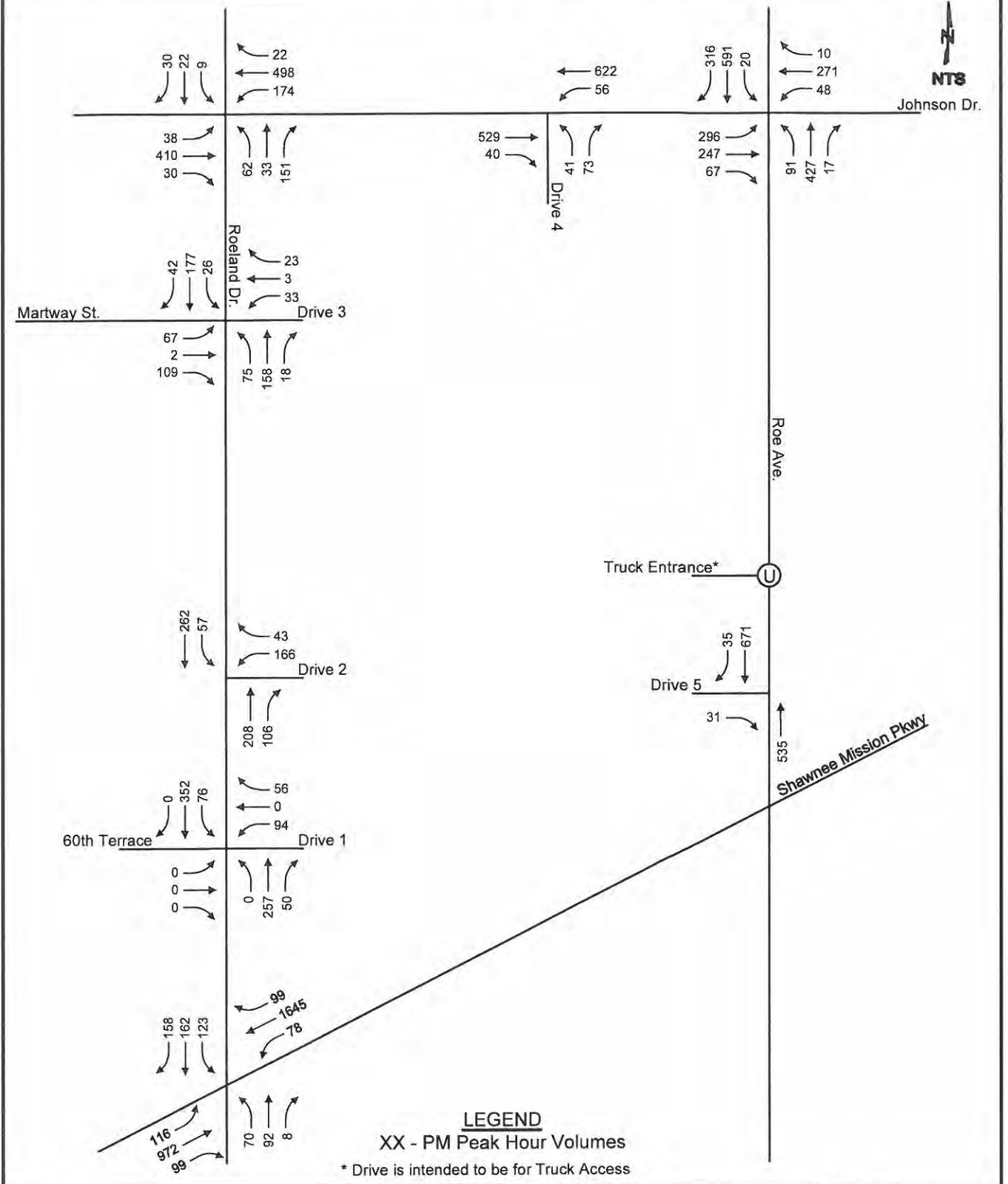
MISSION GATEWAY  
MISSION, KANSAS



Development Peak Hour Volumes

FIGURE  
6

MISSION GATEWAY  
MISSION, KANSAS

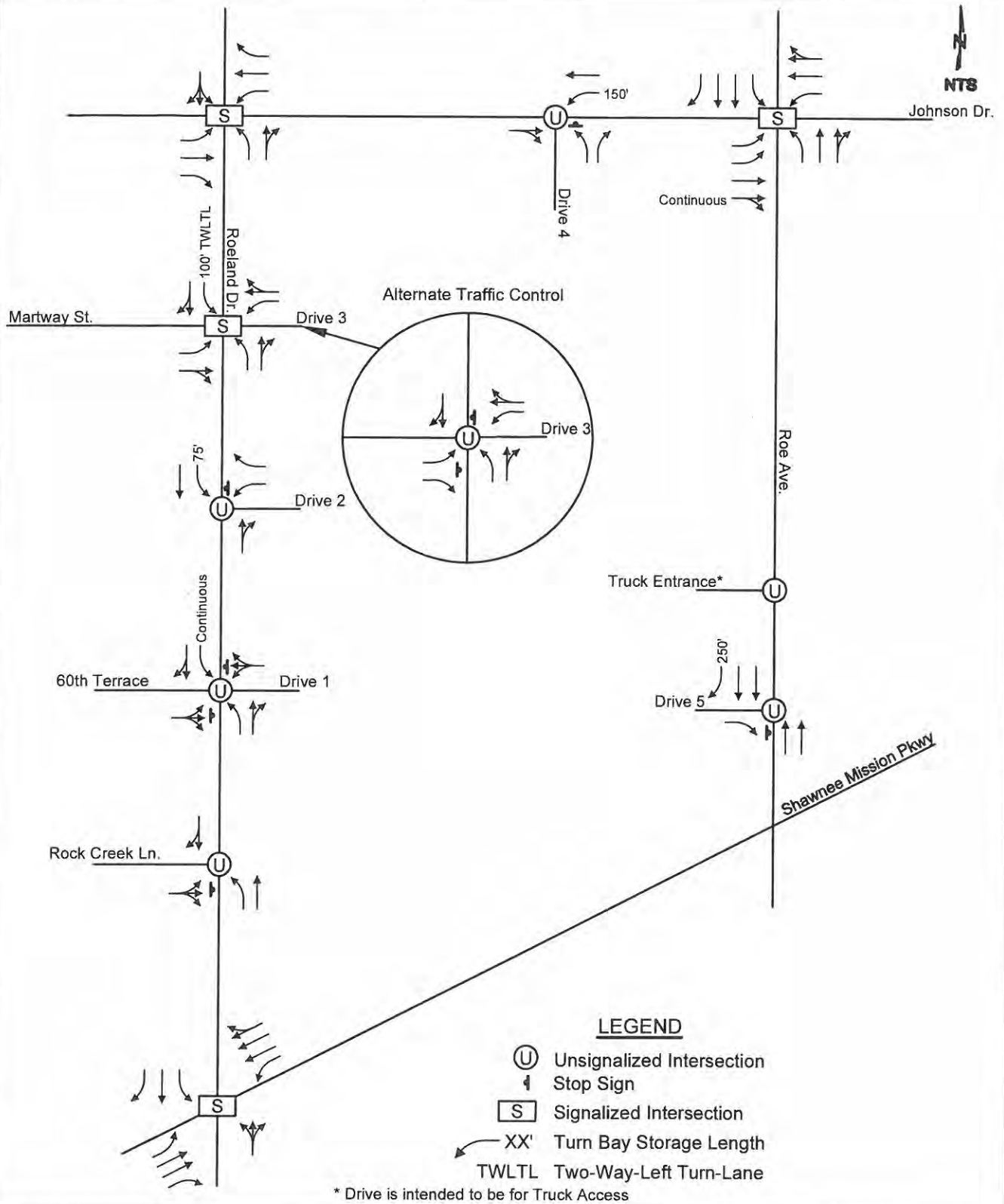


Existing Plus Development  
Peak Hour Volumes

FIGURE  
7



MISSION GATEWAY  
MISSION, KANSAS



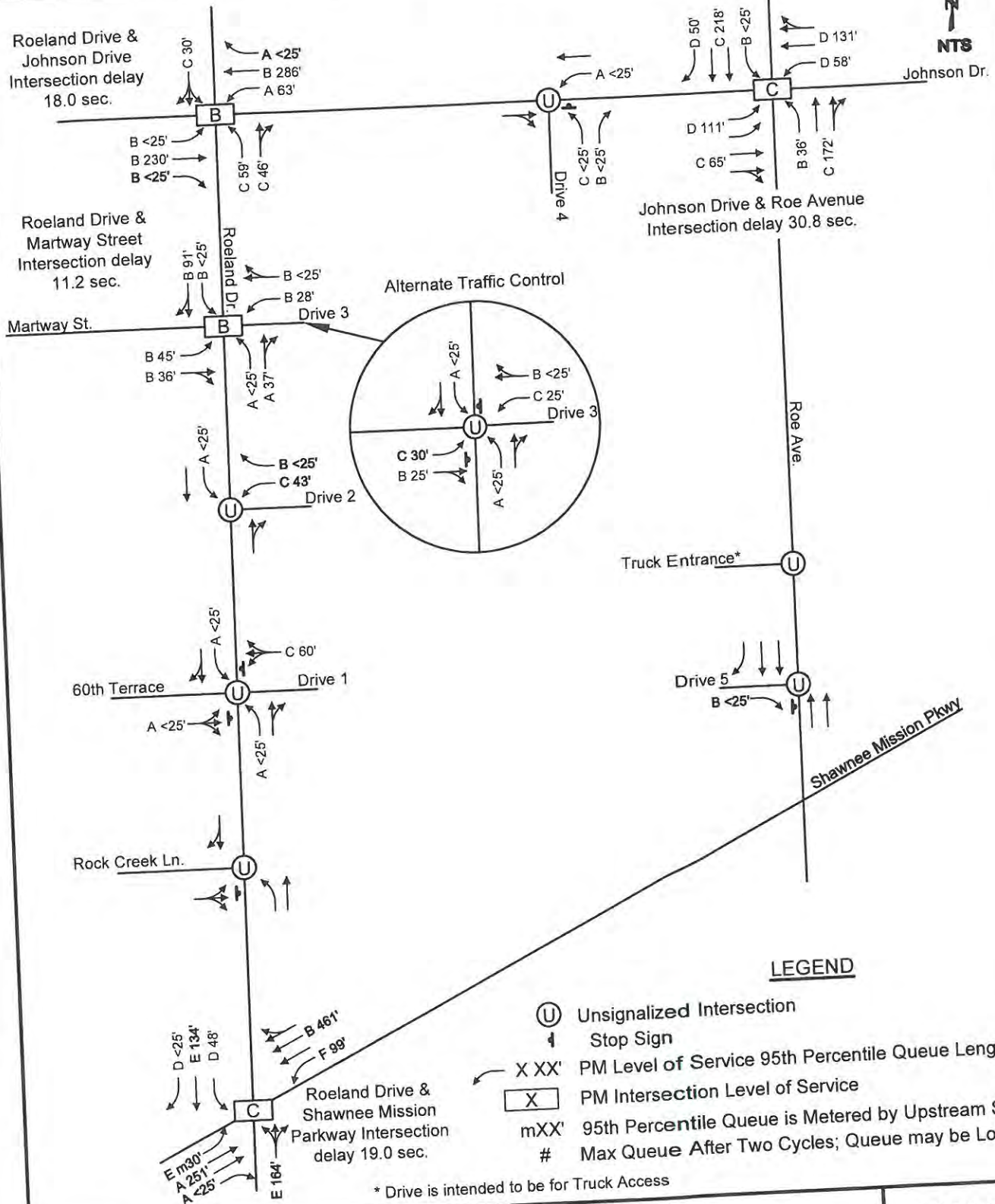
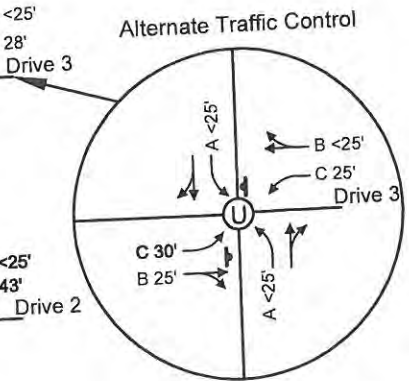
# MISSION GATEWAY MISSION, KANSAS



Roeland Drive & Johnson Drive  
Intersection delay  
18.0 sec.

Roeland Drive & Martway Street  
Intersection delay  
11.2 sec.

Johnson Drive & Roe Avenue  
Intersection delay 30.8 sec.



### LEGEND

- Unsignalized Intersection
- Stop Sign
- PM Level of Service 95th Percentile Queue Length
- PM Intersection Level of Service
- 95th Percentile Queue is Metered by Upstream Signal
- Max Queue After Two Cycles; Queue may be Longer

\* Drive is intended to be for Truck Access



## Existing Plus Development Level of Service

## FIGURE 9

## 6.0 FUTURE YEAR 2035 TRAFFIC CONDITIONS

The future year 2035 conditions consider the Mission Gateway development plus growth of background traffic volumes. KDOT Historical count maps from 2011, 2012, and 2013 were reviewed to determine an annual growth rate for the study area. Based on these maps, no significant growth has occurred in the previous 3 years. For conservative analysis a 0.5% annual growth rate was utilized along roadways classified greater than collector streets to account for any potential growth in the study area. The assumed growth rate is consistent with the previously completed traffic impact study completed by Olsson Associates and greater than previous impact studies in this area. The growth rate was applied to the existing through volumes along Shawnee Mission Parkway, Johnson Drive, and Roe Avenue to obtain future year background volumes.

The background additional volumes were added to the existing plus development volumes resulting in future year 2035 traffic volumes, illustrated in **Figure 10**. Future year 2035 intersection geometrics and traffic control for the study area intersections are illustrated in **Figure 11**.

### 6.1 Capacity Analysis

**Section 4.1** details the methods used for capacity analysis. **Table 8** represents the LOS and delay associated with the signalized study intersections. To complete signalized capacity analysis, signal timings were reviewed and updated as necessary to account for changes in traffic volumes at study intersections for the future year 2035.

Capacity analysis sheets for signalized intersections are included in the **Appendix**.

**Table 9: Future Year 2035 Signalized Intersection Capacity Analysis**

Intersection	PM Peak Hour*
US-56 and Roeland Drive	D (35.6)
Roeland Drive and Martway Street/Drive 3	B (11.1)
Roeland Drive and Johnson Drive	B (19.7)
Roe Avenue and Johnson Drive	C (33.5)

\*LOS (Delay, in seconds)

Based on Future 2035 analysis, all signalized intersections are expected to operate at acceptable LOS D or better during the PM peak hour period. Intersection delay and queuing are expected to experience minimal increases over the Existing plus Development scenario.

Based on Future 2035 analysis, the deficiencies at signalized and unsignalized intersections and throat lengths are consistent with the Existing plus Development scenario. No additional improvements are expected to be necessary.



Figure 12 illustrates the future year 2035 level of service for study intersections.

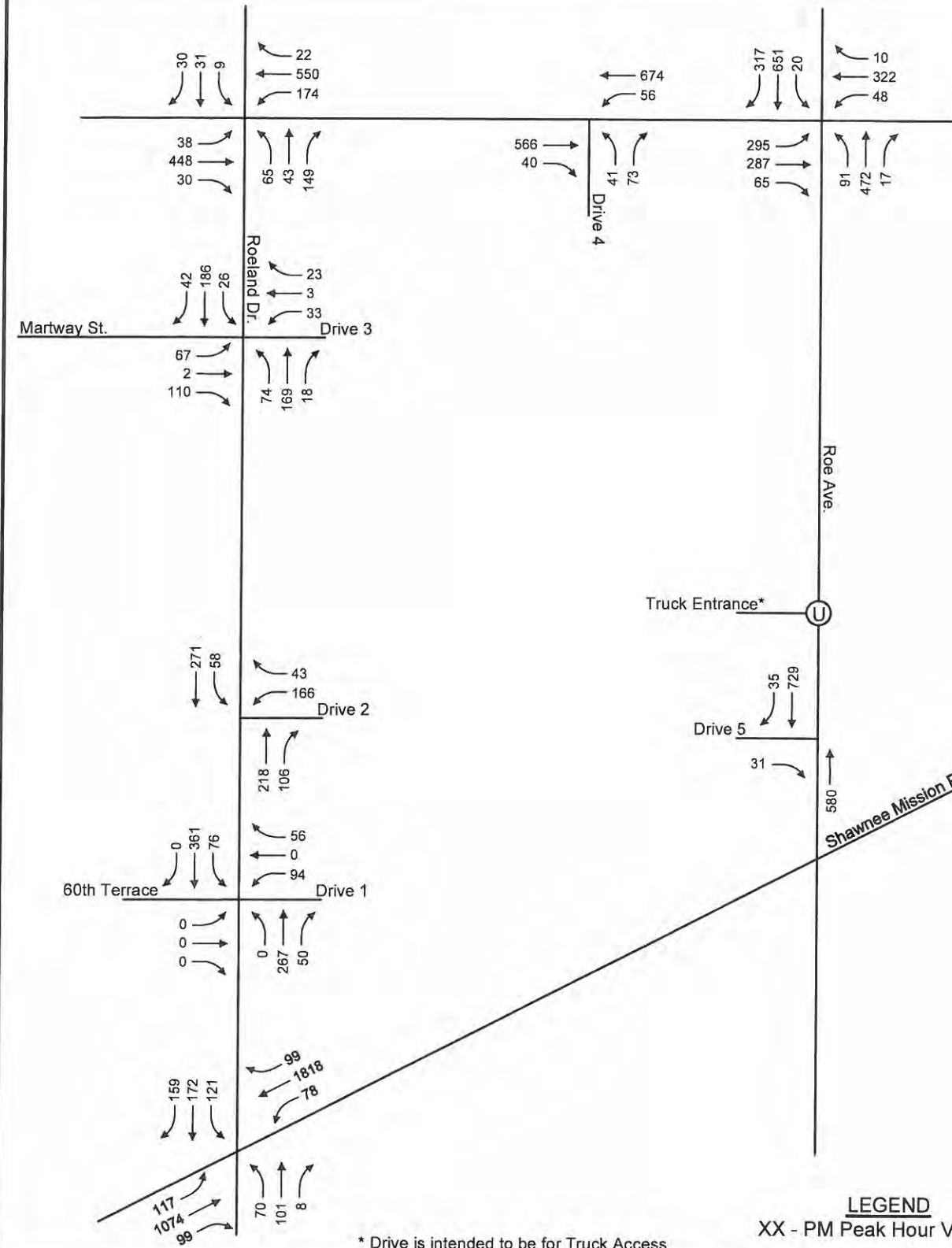
### **6.2 Future Year 2035 Recommendations**

Based on the analysis conducted, study area intersections are expected to operate at acceptable levels of service without additional improvement from the Existing plus Development scenario.

MISSION GATEWAY  
MISSION, KANSAS



Johnson Dr.



\* Drive is intended to be for Truck Access

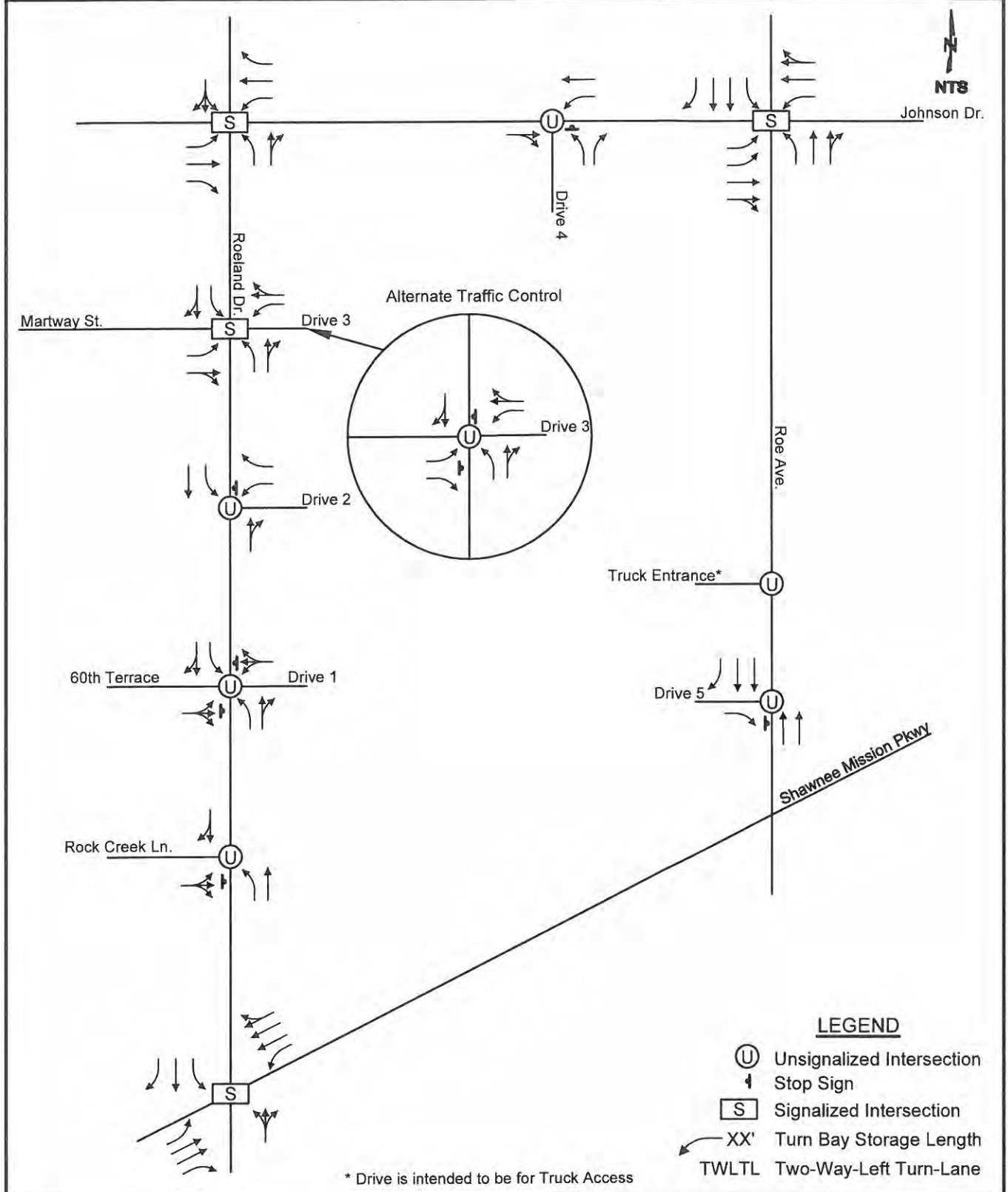


Future Year 2035  
Peak Hour Volumes

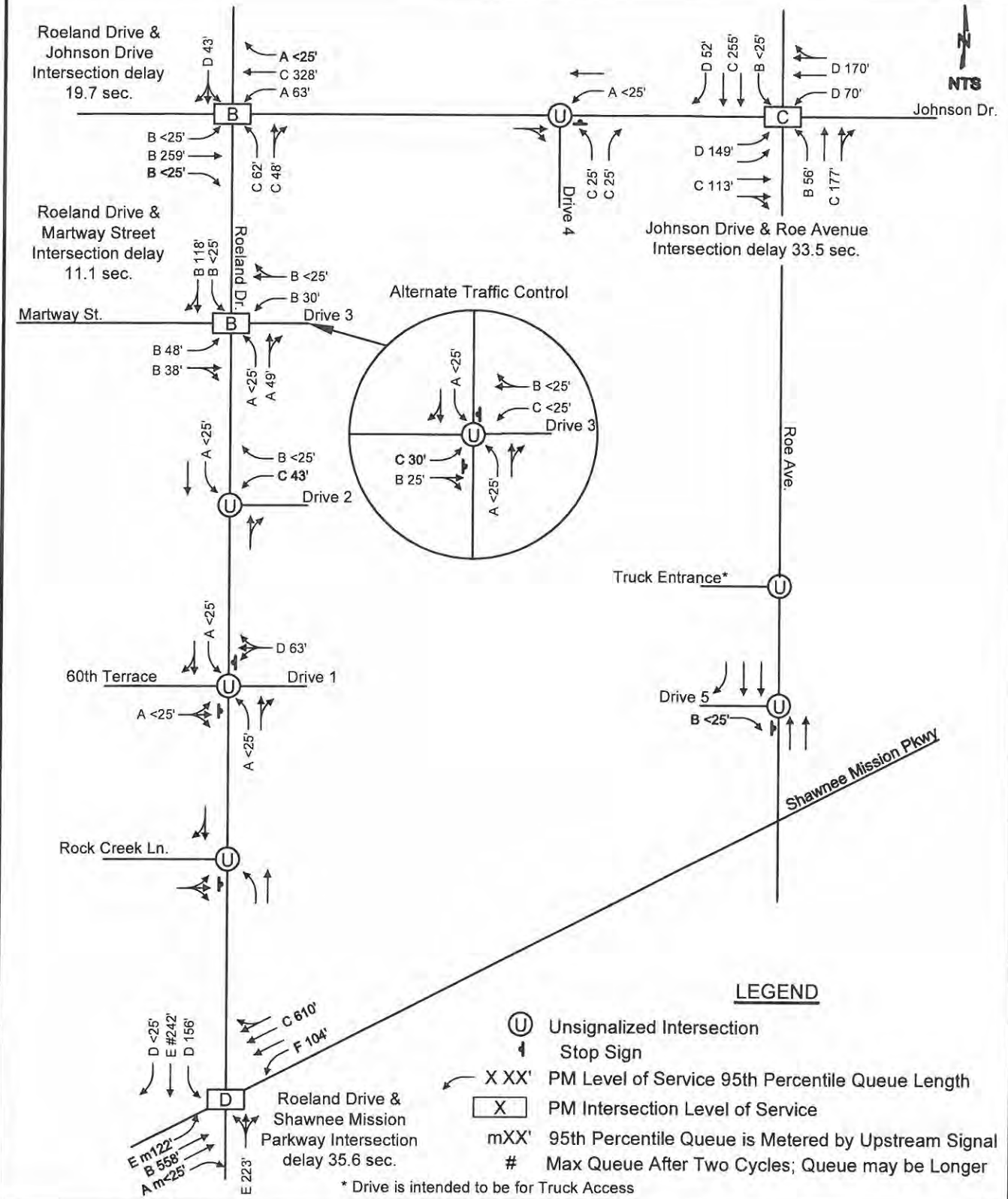
FIGURE  
10



MISSION GATEWAY  
MISSION, KANSAS



# MISSION GATEWAY MISSION, KANSAS







## 7.0 RECOMMENDATIONS & CONCLUSIONS

Based on completion of capacity analysis and review of the proposed project plan, the following improvements are recommended for the project study area for each scenario.

### Existing Conditions

Study intersections are currently operating overall at acceptable levels of service. The intersection of Martway Street and Roeland Drive operates acceptably with signal timings in place as well as in “flash” operation. The need for a signal will be analyzed in the Existing plus Development scenario to determine if the intersection conditions warrant a signal.

### Existing plus Development Conditions

Study intersections are expected to operate at acceptable levels of service for the Existing plus Development scenario considering the following recommended improvements:

#### Roeland Drive and Drive 1:

- Re-stripe the north approach to maximize the length of the southbound left-turn in the existing two-way left-turn lane.
- To ensure that inbound vehicle blockages do not impact the northbound through movement, a northbound 130’ right turn lane plus taper is required. Contrary to Drive 2, Drive 1 has an acceptable throat distance which will allow for the installation of an auxiliary right turn lane.

#### Roeland Drive and Drive 2:

- Provide a separate left-turn and right-turn lane for exiting traffic.
- Re-stripe the north approach to provide a 75-foot southbound left-turn in the existing two-way left-turn lane.
  - This should provide reverse left-turn lane striping between Martway Street and Drive 2.
- It is recommended to provide 75’ of throat distance at the intersection to allow entering vehicles to safely maneuver into the parking area.
- Provide “Do Not Block Intersection” signing for westbound vehicles approaching Roeland Drive at the parking lot drive.
- The addition of a northbound right turn lane would ensure that inbound vehicle blockages do not impact the northbound through movement. However, due to 20’ of throat distance at Drive 2 it is not recommended to place a northbound right turn lane.

#### Roeland Drive and Martway Street/Drive 3:

- Provide a separate left-turn lane to mirror the west approach and remove left-turn traffic from the shared through/right-turn lane.
- Re-stripe the north approach to provide a 100-foot southbound left-turn in the existing two-way left-turn lane.

- This should provide reverse left-turn lane striping between Johnson Drive and Martway Street.
- It is recommended to provide 75' of throat distance at the intersection to allow entering vehicles to safely maneuver into the parking area and store queued vehicles exiting the site to City streets.
- Provide signing to allow entering vehicles the right-of-way at the intersection of Drive 3 with the internal parking lots. This eliminates the potential for entering traffic to queue back into City streets.
- Although a signal warrant is not met for the intersection consideration should be given to keep the signal in place due to the signal providing connection for pedestrians along the Rock Creek Trail. The signal will allow pedestrians traveling along the trail to cross the street safely.
  - Modify signal to accommodate Drive 3 and for pedestrians crossing Roeland Drive if the signal is to remain.
- Provided adequate pedestrian accommodations in compliance with ADA standards.

#### Johnson Drive and Drive 4:

- It is recommended to provide 125' of throat distance at the intersection to store queued vehicles exiting the site to City streets.
- Provide separate exiting left and right-turn lanes at the intersection.
  - The right-turn lane should be made to release traffic into the existing second eastbound through lane on the outside of Johnson Drive. Provisions should be made to eliminate the potential for eastbound through traffic to merge into the outside through lane across Drive 4.

#### Roe Avenue and Drive 5:

- As stated in Existing Plus Development conditions, there is currently a 130' right turn lane depicted in the site plan at Drive 5. This turn lane should be extended 120' for a total of 250' to accommodate both the Truck entrance and Drive 5.
- It is also recommended to provide edge striping internal to the site between Drive 6 and Drive 5 to delineate the westbound driving path from the dock area.

#### US-56 and Roeland Drive:

- Due to the increased demand on the north approach of the intersection, it is recommended to provide additional green time of up to 5 seconds to the southbound movements. This additional time will relieve congestion and reduce potential queue lengths on the approach while only minimally impacting the mainline traffic along US-56.
  - It is recommended to install signage just north of Rock Creek Lane along Roeland Drive to prevent road users from blocking access into and out of Rock Creek Lane during periods of high demand.
- Provided adequate pedestrian accommodations along the south leg of the intersection in compliance with ADA standards.

Johnson Drive and Roeland Drive:

- It is recommended to provide adequate pedestrian accommodations along the south leg of the intersection in compliance with ADA standards.

**Future Year 2035 Conditions**

Based on the analysis conducted, study area intersections are expected to operate at acceptable levels of service without additional improvement from the Existing plus Development scenario.





MISSION GATEWAY - SUSTAINABLE MISSION CERTIFICATION  
11.02.15







# SUSTAINABLE MISSION DOCUMENT CONTENTS

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- > sidewalk and trails
- > transit facilities and service improvements
- > reduced auto dependence
- > transportation demand management
- > connected and open community
- > bicycle network and storage
- > compact development

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- > recycled content in infrastructure
- > number of housing units constructed
- > mixed-use neighborhood centers
- > infrastructure energy efficiency
- > locally/sustainably sourced building materials
- > on-site renewable energy sources
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- > brownfield redevelopment
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- > access to civic and public spaces
- > historic resource preservation and adaptive reuse
- > light pollution reduction
- > access to recreation facilities
- > neighborhood schools
- > minimize site disturbance in design and construction

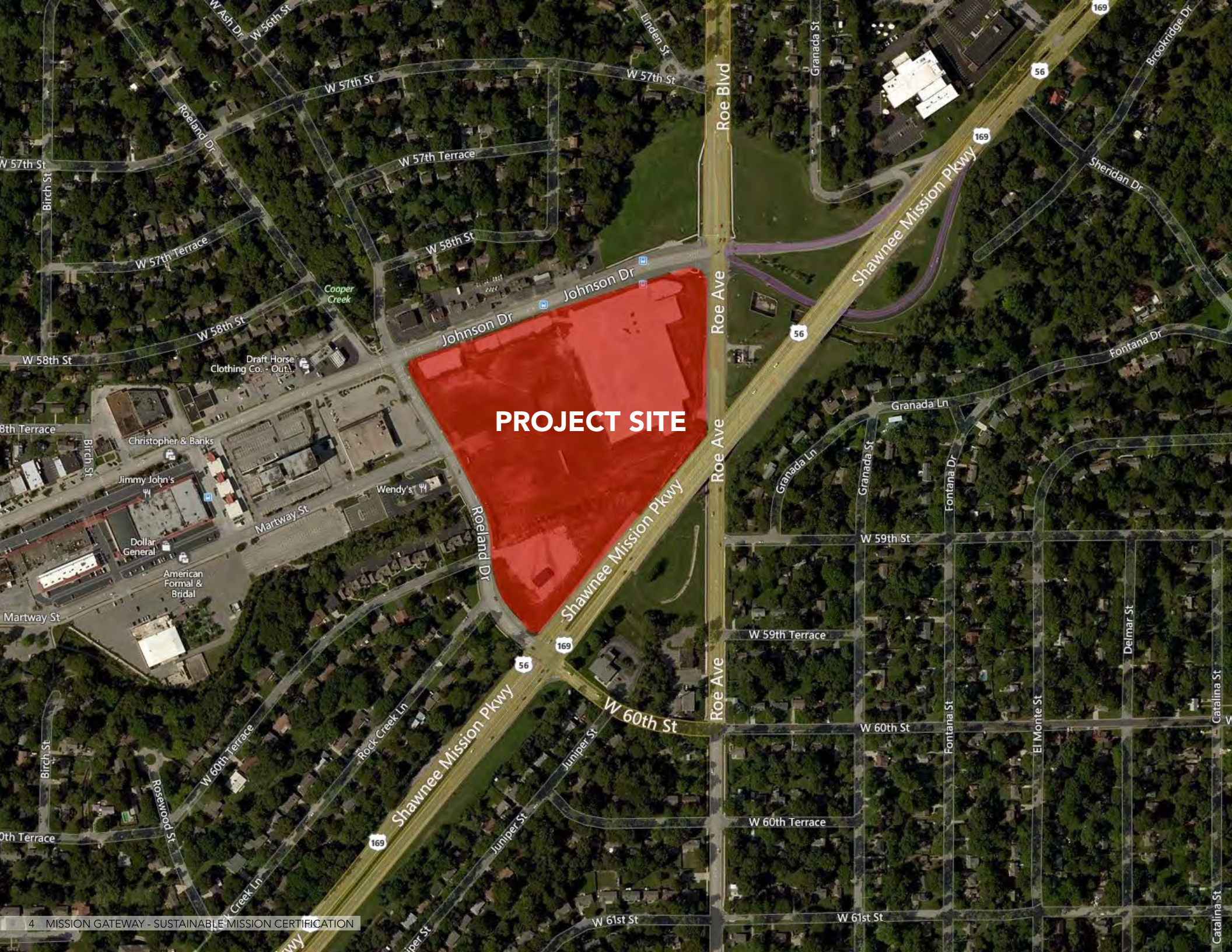
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## OTHER //

- > community outreach and involvement
- > project addresses regional priorities
- > local food production
- > innovation in design LEED accredited professional on project

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**PROJECT SITE**



# PROJECT OVERVIEW

# PROJECT OVERVIEW



Gateway will be a 587,330 SF mixed-use retail / multifamily / hotel development that will be built on a 16.6 acre triangular shaped lot located at the intersection of Shawnee Mission Parkway and Roe Avenue in the well-to-do Kansas City inner-ring suburb of Mission KS. Mission is located 9 miles south / southwest of downtown Kansas City MO.

The design focuses on maximizing the site with a dense assemblage of uses to create an interconnected, community based experience. The project features a substantial amount of landscaping that will transform the existing barren site into a network of pedestrian friendly sidewalks, paths, trails, boardwalks and public gathering spaces.

The Sponsors acquired the Property in August 2005. The Property was then the under-performing 350,000 SF Mission Mall. Mission Mall was in significant disrepair and in foreclosure. The Sponsors vacated the tenants, closed the mall and demolished the site in March 2006. Redevelopment was supposed to commence in 2008 but was delayed due to the upheaval in the financial markets that began in 2008 and extended through 2012. The Sponsors have carried the Project and have invested significant time, effort and equity into crafting a viable development plan. Mass Excavation is mostly complete, pre-development work is ongoing and construction is slated to begin in the first quarter of 2016.

Gateway will include the following components:

- A 154,469 SF Wal-Mart Supercenter on a 20-year NN lease.
- A 3-story 182-unit Class A residential rental project spread over 3 inter-connected buildings.
- 55,629 SF of on-grade small-shop retail space located beneath the 182-unit residential buildings.
- A 200-key 7-story limited-service national flag hotel.
- 15,285 SF of restaurant space and 6000 SF of spa space all located within the Hotel. The restaurant space will be leased to third party operators.
- A 2-story structured parking deck with 794 stalls located above 708 surface parking slots. Total parking capacity is 1502 cars.





60th Terrace Ln

Walmartway St

Roeland Dr

Roeland Dr

W Ash Dr

W 60th St

Johnson Dr

Shawnee Mission Pkwy

169

56

Roe Ave

56

Roe Ave

169

Shawnee Mission Pkwy

56

169

th St

Gr

PROJECT OVERVIEW







## Walkable and Complete Streets

*The project adds over 2,700 linear feet of sidewalk along the perimeter of the site where none currently exists. These walks provide connectivity to the existing public sidewalk and trail system. A green zone is established between the street and sidewalk to enhance the pedestrian experience. Buildings are pushed to the property line where retail uses open up to the street to enliven the street presence. 8 of 8 points.*

# STREETS



## Trees and Shading

The current site is largely barren and formerly 100% covered by the former Mission Mall complex. The project restores close to 100 shade trees and over 40 ornamental trees to the perimeter and interior. These trees shade sidewalks, streets, parking areas, and building facades. Additionally, the structured parking garage provides a large degree of solar shading over what would otherwise be triple the amount of surface parking. *5 of 5 points.*

## Sidewalks and Trails

The project adds to the existing Rock Creek Trail system with wide paving supporting a variety of uses such as biking, jogging, and walking. Internal to the site, pedestrian pathways connect all parts of the project including elevated pedestrian connections from the parking garage to the residential and hotel. 2,700 linear feet of sidewalk and trail are added at perimeter of the site alone. *5 of 5 points.*

## Transit Facilities & Service Improvements

Pedestrian connectivity to existing transit locations is provided at the perimeter of the site as well as to the nearby transit hub to the west. *4 of 4 points.*

## Reduced Auto Dependence

By integrating living with a mixture of retail and other uses within a pedestrian connected development, the project eliminates the need to maneuver about the site in a vehicle and reduces the need for residents and workers to leave the site for goods and services during the course of a day. *3 of 3 points.*

## Transportation Demand Management

This mixed-use development accommodates many modes of travel (walking, biking, mass transit, auto) and thus reduces dependence on vehicular travel. *3 of 3 points.*

## Bicycle Network and Storage

Connection to the Rock Creek trail and the addition of ample bike racks serve to encourage bike travel. *2 of 2 points.*



## Connected and Open Community

The interconnected site provides ample opportunity for the public to access and utilize the common spaces. This is highlighted by a public park-like plaza through the center of the site that accommodates outside dining, performance, and leisure activities. **3 of 3 points.**

## Compact Development

We have a much more dense assemblage of uses than you would typically find in a suburban development. Buildings hug the property line along the site perimeter where possible and parking stacked to create a tightly packed, more pedestrian oriented environment that preserves green space at the heart of the site. **3 of 3 points.**





## Building Orientation and Energy Efficiency

*The building massing is designed so that upper stories overhang the lower along the street facades creating a continuous protected zone that shades the storefront glazing and provides cover from rain and snow. Building envelopes are enhanced by high insulation values meeting or exceeding energy code. Insulated precast concrete panels are formed with thermal breaks utilizing carbon fiber ties. Energy efficient, yet highly transparent glazing systems are specified. A limited amount of planted roofing is employed around the perimeter of the residential building to help mitigate storm surge, decreases heat gain and urban heat island effect, as well as increases longevity of the roof membrane below it. All other roofs are of high reflectance roof membrane that further help to reduce urban heat island effect.*

*Energy efficient light, much of which will be LED, is employed. Much of the retail square footage is lit by skylight paired with active dimming lighting system to greatly reduce dependence on artificial lighting. Occupancy sensors are also used to automatically turn off lights in unoccupied rooms. High efficiency HVAC systems and low water consumption plumbing fixtures are incorporated. Energy efficient elevators are used. 13 of 13 points.*



# BUILDING



## *Recycled Content in Infrastructure*

*The project employs recycled content materials wherever feasible. Structural steel, insulation, concrete and building claddings will contain high levels of recycled content. Additionally, Low VOC coatings and sealants are specified for all parts of the development. 4 of 4 points.*

## *Number of Housing Units Constructed or Renovated*

*The development brings 182 rental apartments comprised of studio, one bedroom, and two bedroom, as well as a 200 room hotel to the site. 3 of 3 points.*

## *Mixed-Use Neighborhood Centers*

*The project consists of over 500,000 sf of mixed uses such as hotel, residential, and retail including upscale and fast casual restaurants and array of goods and services. 3 of 3 points.*

## *Infrastructure Energy Efficiency*

*The site employs high efficiency LED lighting both on the perimeter and internal to the site. 3 of 3 points.*

## *Locally/Sustainably Sourced Building Materials*

*Concrete, landscape material, and potentially insulation, are to come from local sources. 2 of 3 points.*

## *On-Site Renewable Energy Sources*

*No measures are currently incorporated. However the development team continues to search for budget conscious opportunities. 0 of 2 points.*

## *Construction Activity Pollution Prevention*

*Stormwater systems are to be protected from runoff during earth moving activities such as site prep, foundations and rough and finished grading / planting. 1 of 1 points.*





Building

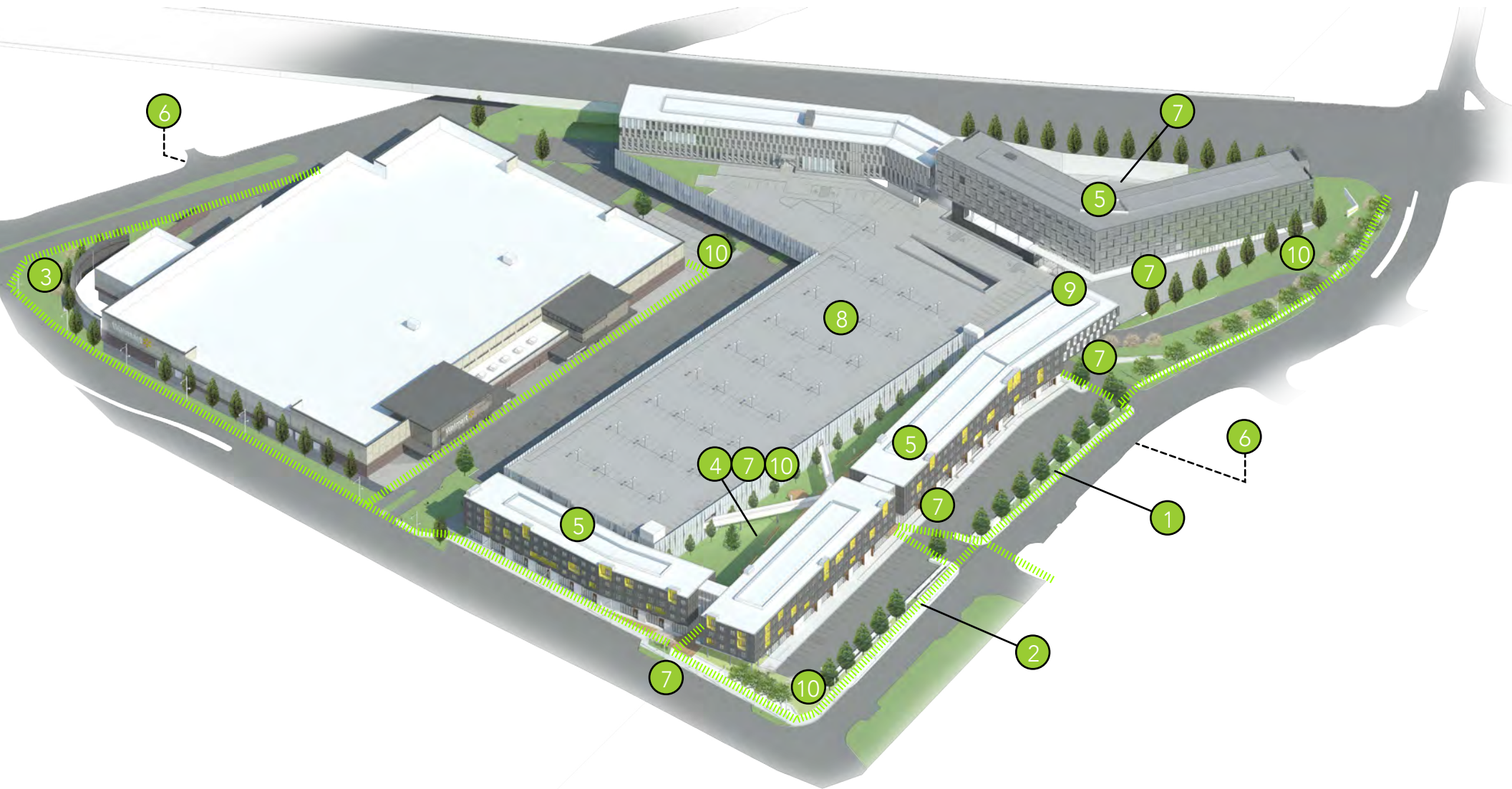




**REDEVELOPMENT**



# REDEVELOPMENT



## 1 Walkable and Complete Streets.

These walks provide connectivity to the existing public sidewalk and trail system.

## 2 Sidewalk and Trails.

The project adds to the existing Rock Creek trail system with wide paving that can support variety of uses such as biking, jogging, and walking.

## 3 Transit Facilities and Service Improvements.

Existing transit locations.

## 4 Connected and Open Community.

Highlighted by a public park-like plaza through the center of the site that accommodates outside dining, performance, and leisure activities.

## 5 Units Constructed.

The project will include the construction of 182 rental apartments comprised of studio, one bedroom, and two bedrooms as well as a 200 room hotel.

## 6 Stormwater Management.

The project also includes a large reconstruction of stormwater culvert that crosses the site from west to east.

## 7 Access to Civic and Public Spaces.

The design of the project creates civic and public space both at the interior and at the perimeter of the site as well as provides connection via sidewalks to the nearby community amenities.

## 8 Light Pollution Reduction.

The project is designed with high efficiency LED parking lot, perimeter and pedestrian lighting.

## 9 Access to Recreation Facilities.

The project contains a fitness facility that can be shared by residents, hotel guest, and potentially employees via memberships.

## 10 Project Addresses Regional Priorities.

Water efficient landscaping

# REDEVELOPMENT



## Water-efficient Landscaping and Stormwater Management

The project incorporates a large amount of landscaping. Plantings are selected to perform well in our climate, such as water efficient native species. The project also includes the construction of a large stormwater culvert crossing the site from west to east. This infrastructure will help protect the site from flooding in extreme weather and brings the site as well as significant portions of the city out of the 100-year flood plane zone enabling its development. *7 of 7 points.*

## Mixed-Income Diverse Communities

The residential component includes a variety of apartment sizes to appeal to a range of budgets. No subsidized apartments are being considered at this time. *4 of 6 points.*

## Brownfield Redevelopment

The project site, while potentially not a brownfield by the strict EPA definition, is certainly blighted by former uses (Mission Mall was 70% vacant at time of purchase). Without the significant investment in stormwater infrastructure it would be virtually unbuildable, as area would remain in the 100-year flood plane. This project represents a substantial improvement to the area. *2 of 3 points.*

## Square feet of Commercial Space Renovated

There is no renovated space with the project. *0 of 3 points.*

## Access to Civic and Public Spaces

The design of the project creates civic and public space both at the interior and at the perimeter of the site as well as provides connection via sidewalks to the nearby community amenities. Meeting space is included as well. *3 of 3 points.*

## Historic Resource Preservation and Adaptive Reuse *N/A. 0 of 3 points.*

## Light Pollution Reduction

The project is designed with high efficiency LED parking lot, perimeter, and pedestrian lighting. All lighting will utilize shields and cutoff that prevent unnecessary spill and adhere to dark sky initiatives. Lighting levels are kept minimal. *2 of 2 points.*





## Access to Recreation Facilities

The project contains a fitness facility that can be shared by residents, hotel guests, and potentially workers and offsite individuals via memberships. The project provides connectivity via sidewalks to nearby community recreation amenities. *1 of 1 points.*

## Neighborhood Schools

The project is positioned within 2 miles of two elementary schools affording residents close access to elementary education. *1 of 1 points.*

## Minimize Site Disturbance in Design and Construction

Due to the former development of the site and the planned high density, most areas will be disturbed during construction – n/a. *0 of 1 points.*





**OTHER**

# OTHER



## Community Outreach and Involvement

The project creates an active street presence and a dynamic interior courtyard that is connected to the perimeter pedestrian experience and welcomes the community in for a variety of experiences that may include a variety of arts related performances. There is an opportunity for the Mission Farmers Market and other events to be hosted on the top deck of the parking structure. Meeting space is also included in the project. *4 of 6 points.*

## Project Addresses Regional Priorities

The following is based on LEED ND v2009 for area code 66202.

- Water efficient landscaping.
- Heat island reduction – high reflectance roof membranes used. Covered and shaded parking areas.
- Compact development – high density development.
- Reduced parking footprint – 2 levels of structured parking.
- Location w/ reduced automobile dependence – close proximity to on and off site amenities, mass transit, pedestrian and bike connectivity. *4 of 4 points.*

## Local Food Production

No measures being pursued at this time. *0 of 3 points.*

## Innovation in Design LEED Accredited Professional on Project

Both the design and construction team have access to in-house LEED accredited professionals as part of the project. *2 of 2 points.*



## Score

Total points 97 of 116

50 points - Sustainable Mission Certified

72 points – Sustainable Mission Green Project

84 points – Sustainable Mission Silver Project

96 points – Sustainable Mission Gold Project

**el dorado** INC





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FDP-A101	BUILDING B/F - FLOOR PLANS ( LEVELS 3-4)	FDP-L103	HARDSCAPE PLAN	FDP-C1.1	ACCESSIBLE PEDESTRIAN CIRCULATION PLAN	FDP-SL104	SITE LIGHTING CUTSHEETS

# Mission Gateway

A Multi-Use Retail Development  
Johnson Drive and Roe Avenue, Mission, Kansas

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

CONSULTANTS:

CIVIL / LANDSCAPE: OLSSON ASSOCIATES 7501 WEST 153RD ST. SUITE 200 OVERLAND PARK, KANSAS 66213 TEL: 913-381-1170 FAX: 913-381-1174	MEP/ P&ME ENGINEERS 13300 W. 98TH ST. LENEXA, KANSAS 66215 TEL: 913-492-2400 FAX: 913-492-2437	STRUCTURAL: BOB D. CAMPBELL & CO., INC. 4338 BELLEVUE AVE. KANSAS CITY, MISSOURI 64111 TEL: 816-531-4144 FAX: 816-531-8572
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MISSION GATEWAY

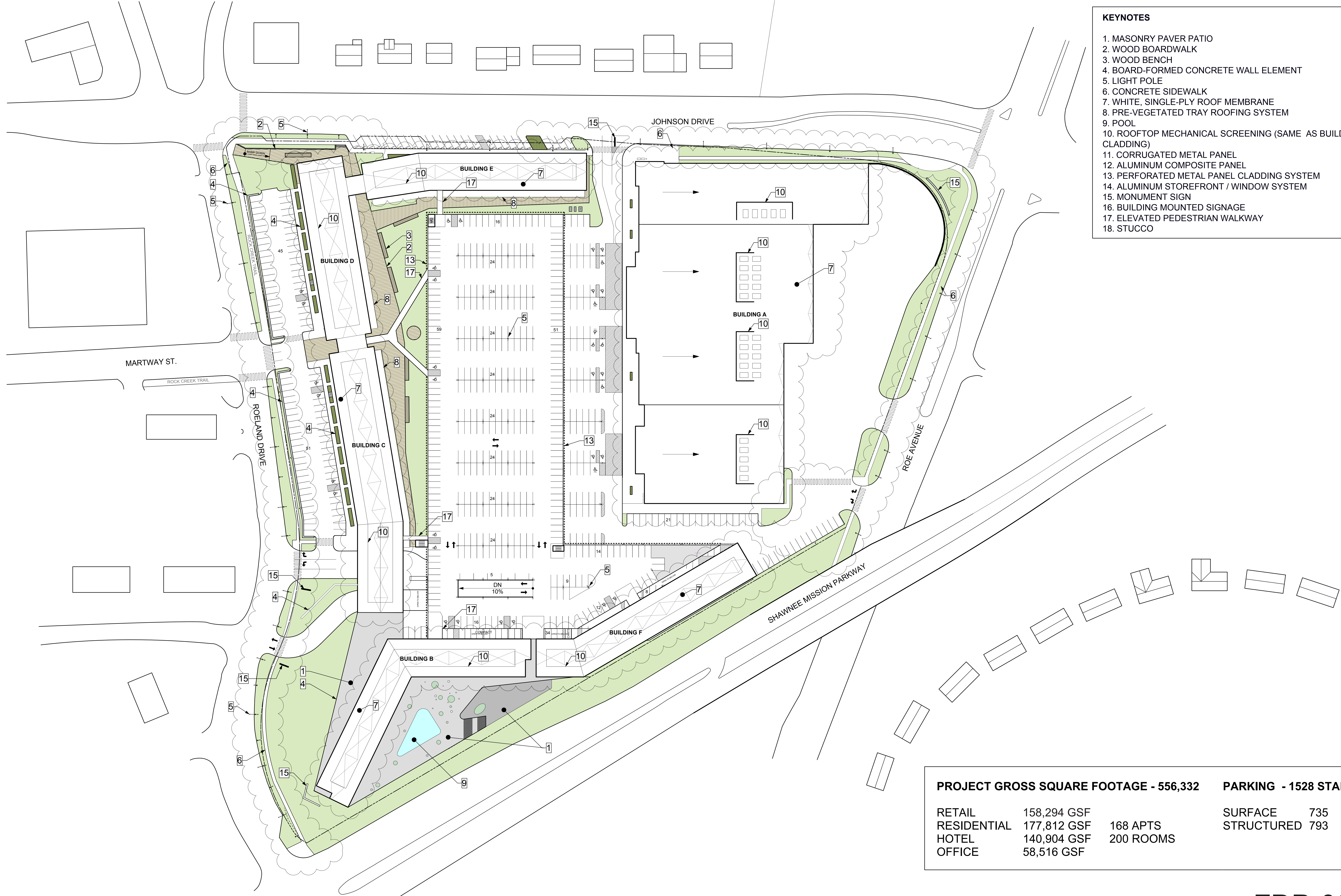
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- KEYNOTES**
1. MASONRY PAVER PATIO
  2. WOOD BOARDWALK
  3. WOOD BENCH
  4. BOARD-FORMED CONCRETE WALL ELEMENT
  5. LIGHT POLE
  6. CONCRETE SIDEWALK
  7. WHITE, SINGLE-PLY ROOF MEMBRANE
  8. PRE-VEGETATED TRAY ROOFING SYSTEM
  9. POOL
  10. ROOFTOP MECHANICAL SCREENING (SAME AS BUILDING CLADDING)
  11. CORRUGATED METAL PANEL
  12. ALUMINUM COMPOSITE PANEL
  13. PERFORATED METAL PANEL CLADDING SYSTEM
  14. ALUMINUM STOREFRONT / WINDOW SYSTEM
  15. MONUMENT SIGN
  16. BUILDING MOUNTED SIGNAGE
  17. ELEVATED PEDESTRIAN WALKWAY
  18. STUCCO



<b>PROJECT GROSS SQUARE FOOTAGE - 556,332</b>		<b>PARKING - 1528 STALLS</b>	
RETAIL	158,294 GSF		
RESIDENTIAL	177,812 GSF	168 APTS	SURFACE 735
HOTEL	140,904 GSF	200 ROOMS	STRUCTURED 793
OFFICE	58,516 GSF		

01 SITE PLAN

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

CONSULTANTS:

CIVIL / LANDSCAPE: OLSSON ASSOCIATES 7301 WEST 153RD ST. SUITE 200 OVERLAND PARK, KANSAS 66213 TEL: 913-381-1170 FAX: 913-381-1174	ME/P ENGINEERS: PKMR ENGINEERS 13300 W. 98TH ST. LENEKA, KANSAS 66215 TEL: 913-492-2400 FAX: 913-492-2437	STRUCTURAL: BOB D. CAMPBELL & CO., INC. 4338 BELLEVUE AVE. KANSAS CITY, MISSOURI 64111 TEL: 816-531-4144 FAX: 816-531-8572
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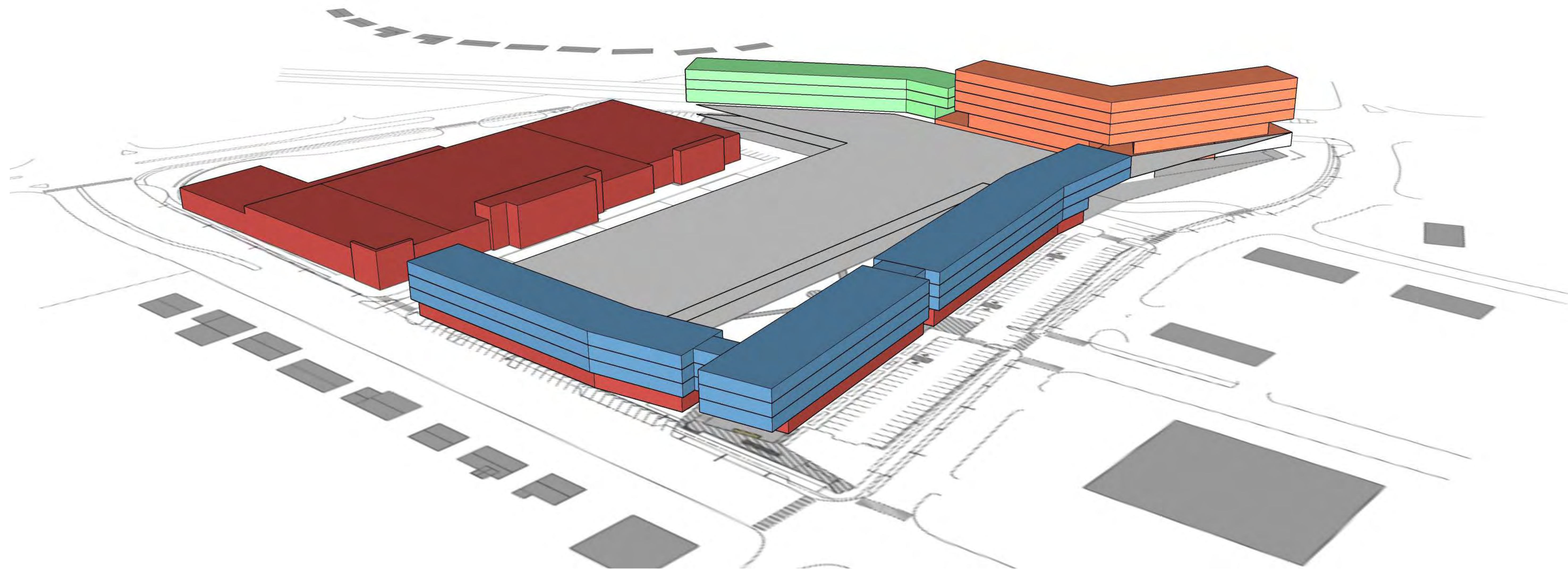
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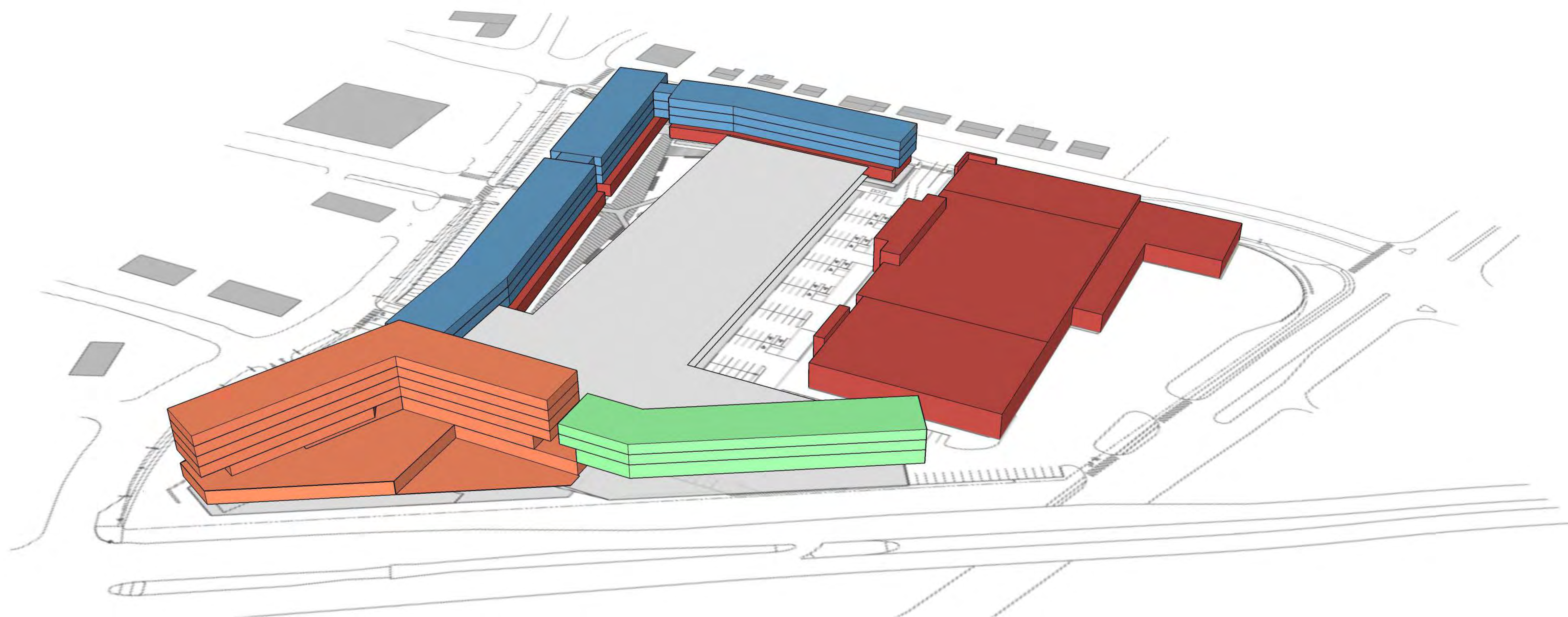
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**FDP-001**  
**SITE PLAN**





STACKING DIAGRAM



- RETAIL
- RESIDENTIAL
- HOTEL
- OFFICE

STACKING DIAGRAM

# FDP-002

## STACKING DIAGRAMS

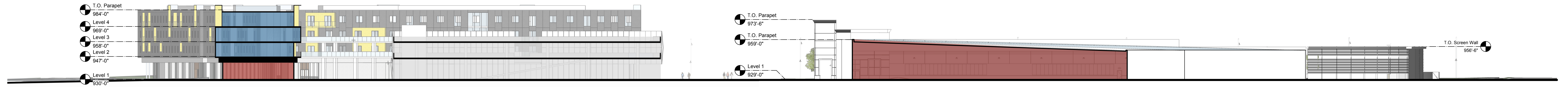
CONSULTANTS:

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7501 WEST 153RD ST. SUITE 200  
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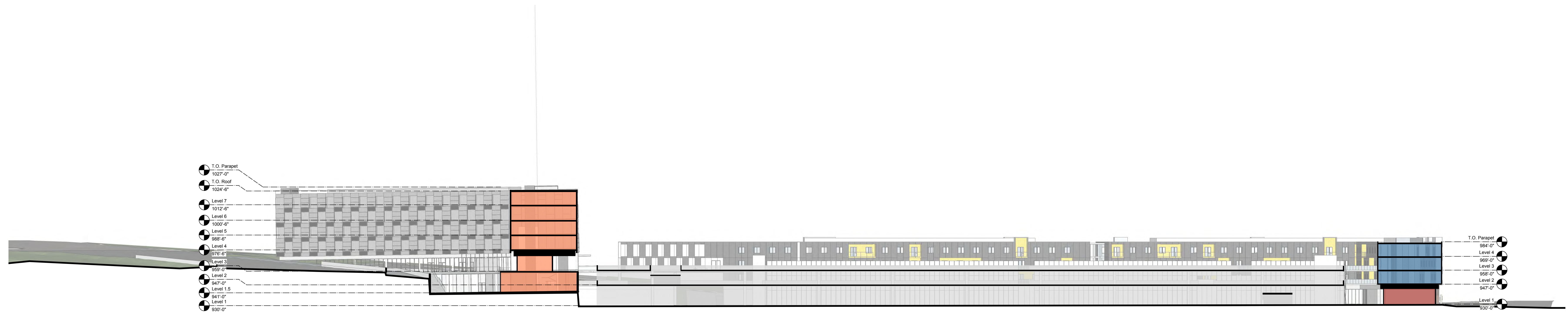
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02 SITE SECTION



- RETAIL
- RESIDENTIAL
- HOTEL
- OFFICE

01 SITE SECTION

# FDP-003

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

CONSULTANTS:

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MISSION GATEWAY

**el dorado**

**OLSSON ASSOCIATES**

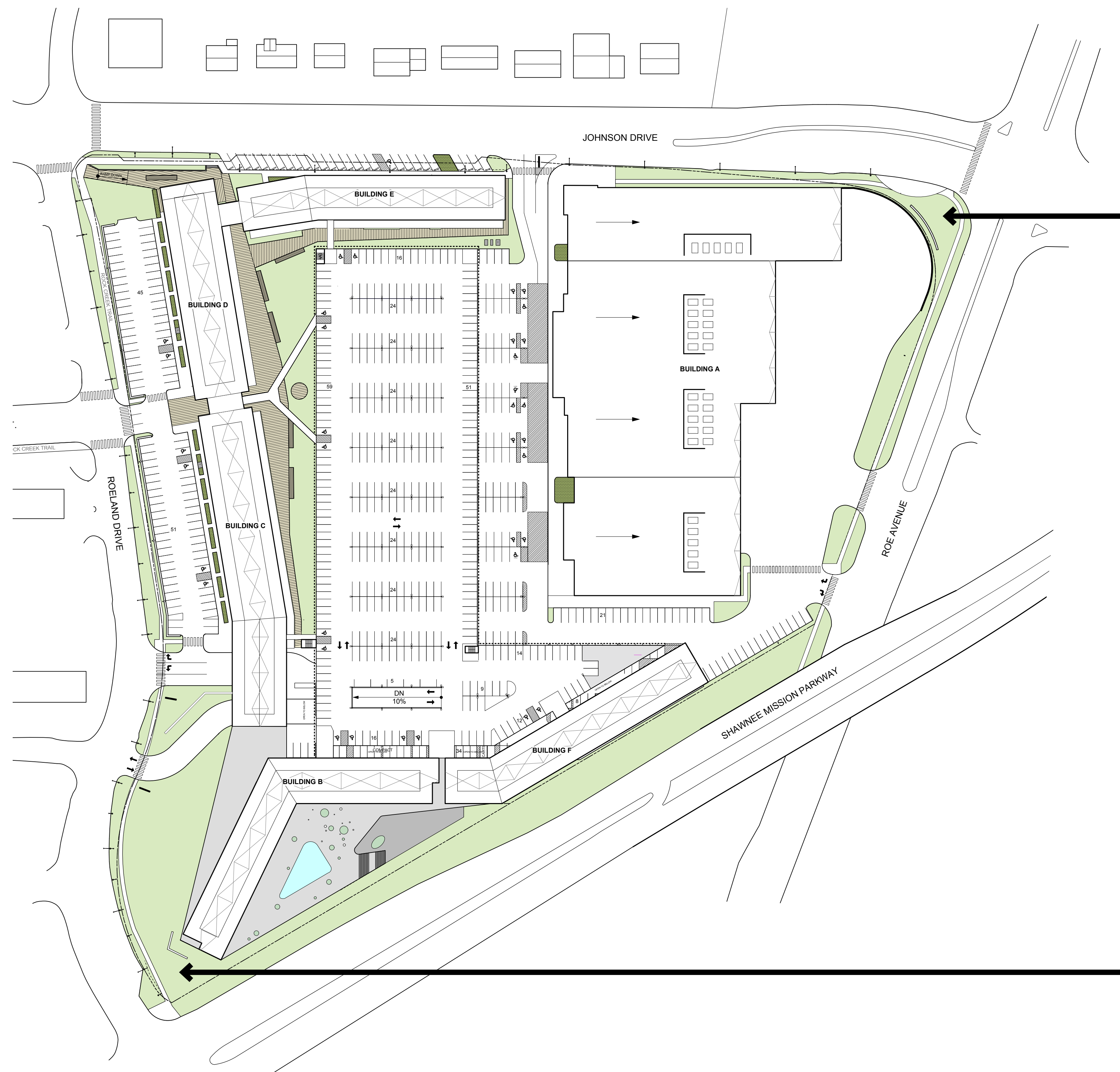
**pkmr**

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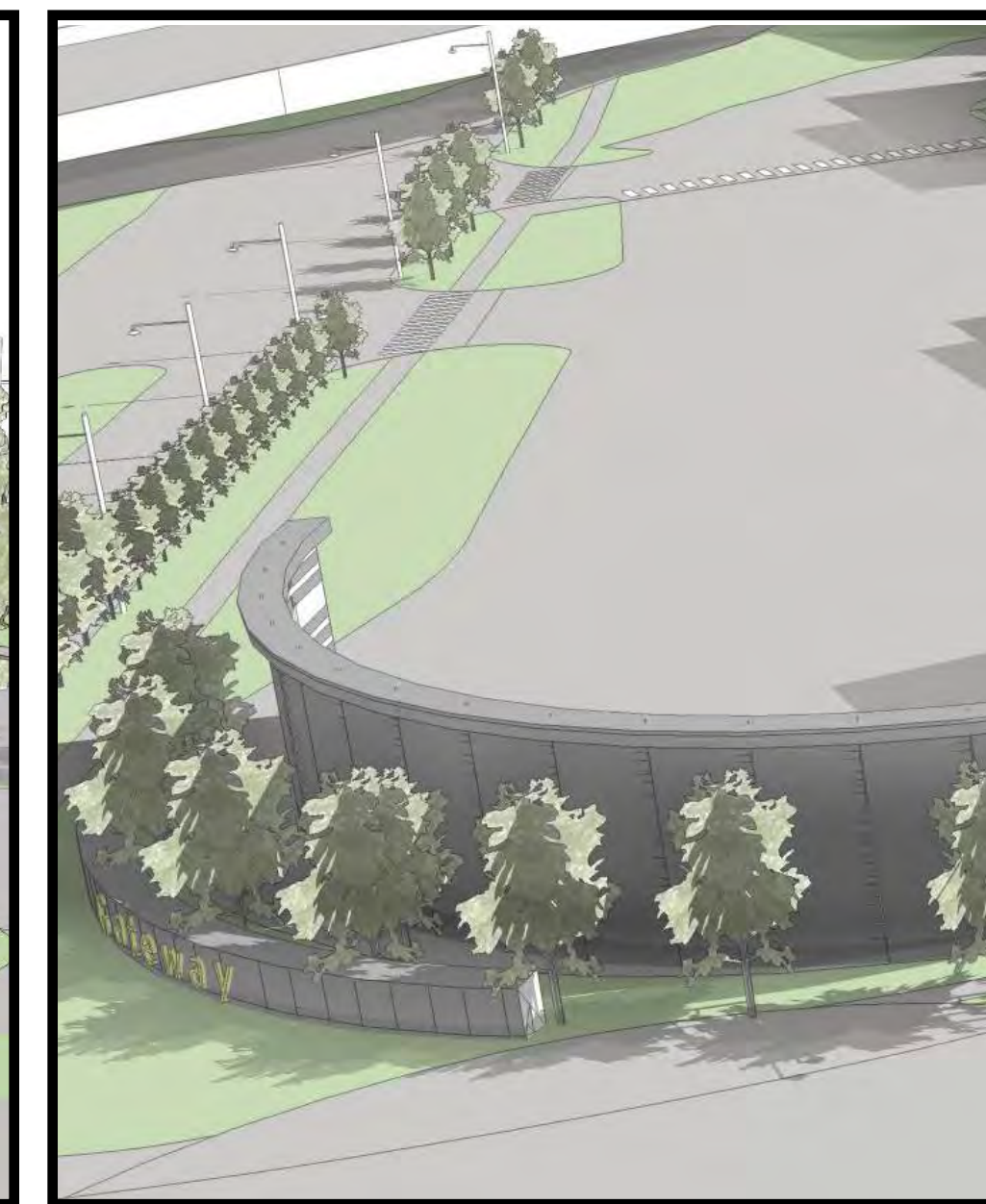
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## BUILDING & SITE SECTIONS





Northeast Monument Sign



Southwest Monument Sign



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MISSION GATEWAY

**el dorado**

**OLSSON**  
ASSOCIATES

**pkmr**  
ENGINEERS

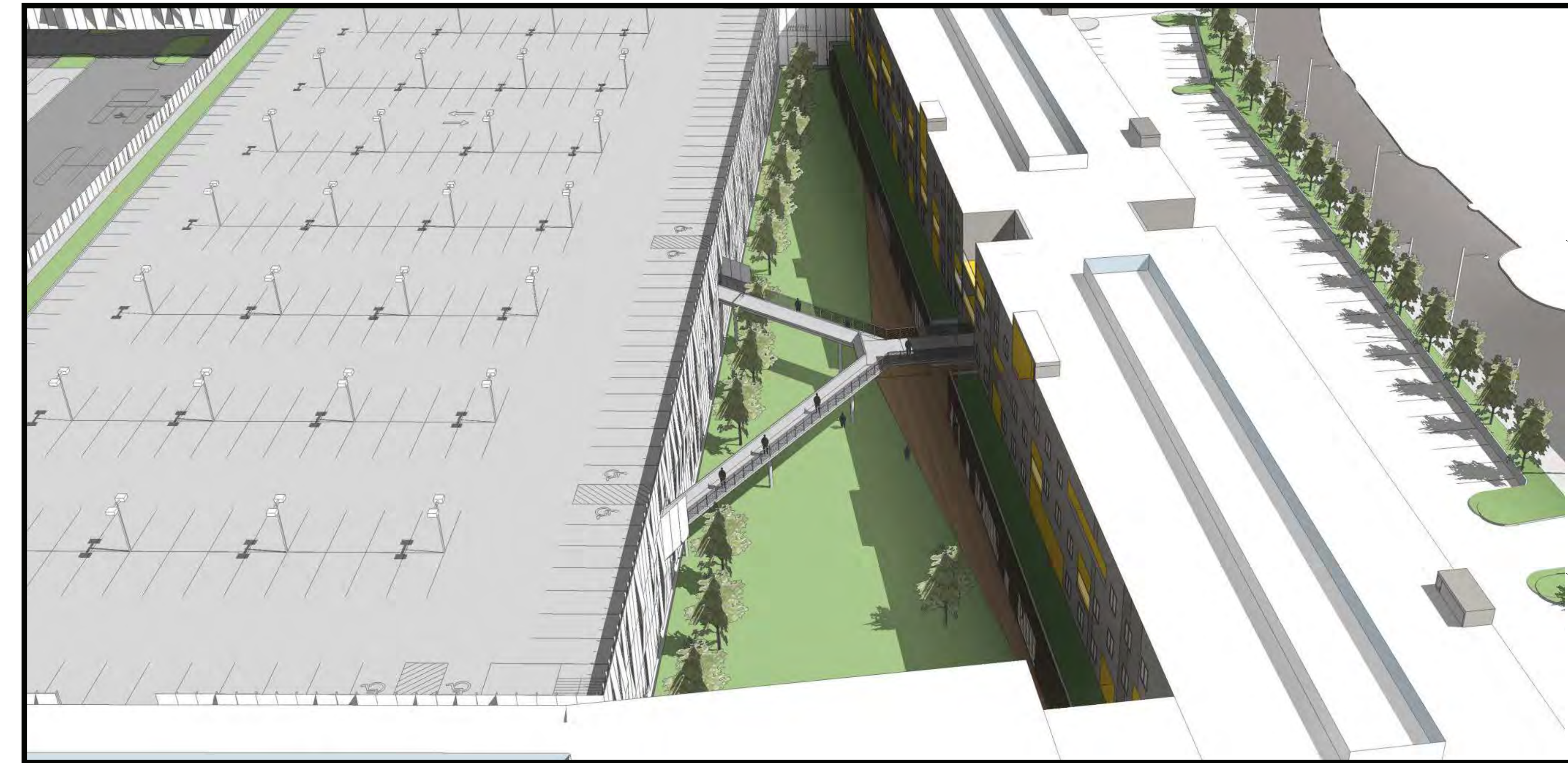
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VIEW FROM NORTHWEST OF RETAIL / RESIDENTIAL



VIEW OF RETAIL / RESIDENTIAL COURTYARD



AERIAL FROM JOHNSON DRIVE ENTRY



VIEW OF RETAIL / RESIDENTIAL COURTYARD

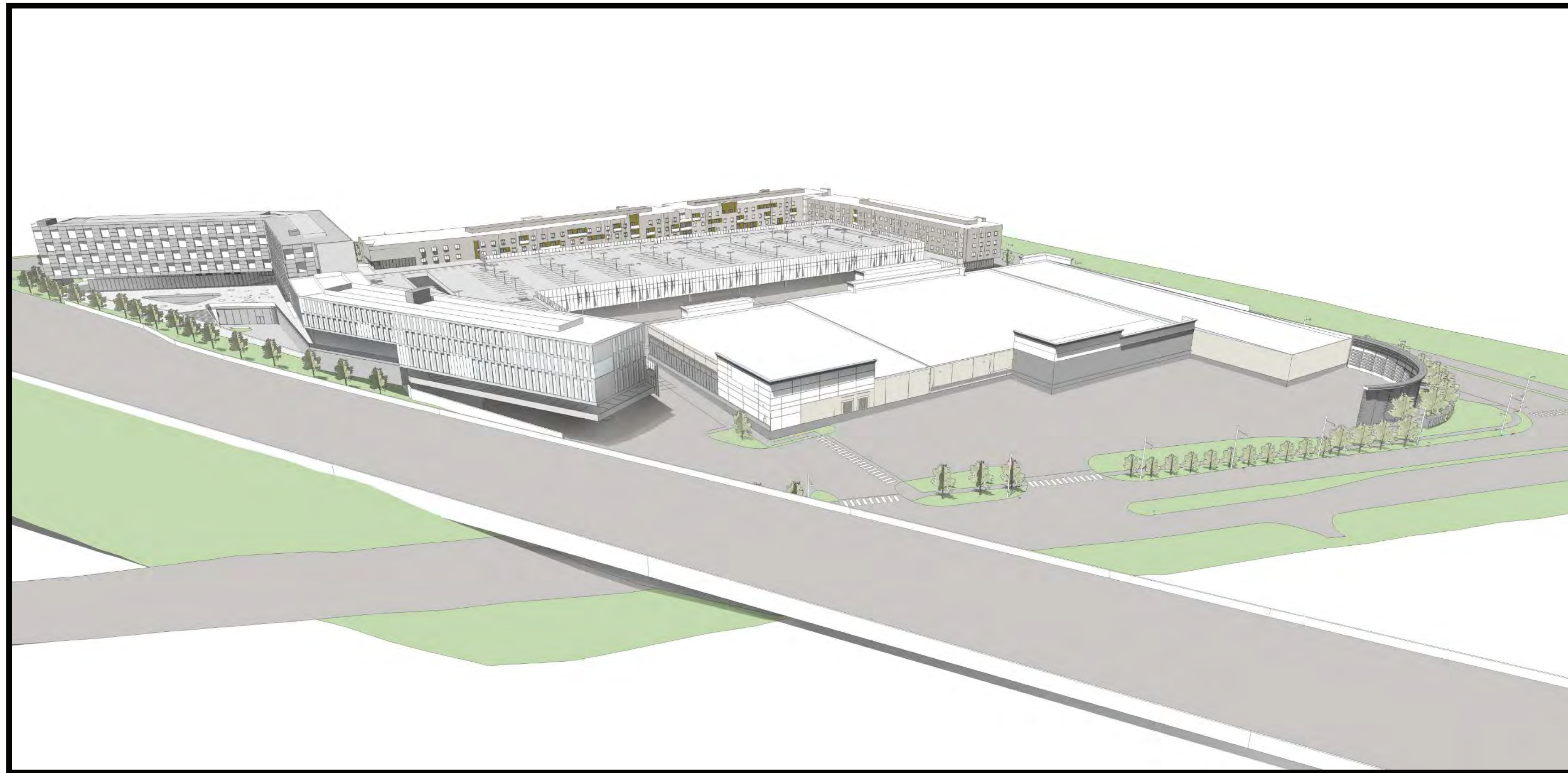
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OLSSON ASSOCIATES  
7301 WEST 153RD ST. SUITE 200  
OVERLAND PARK, KANSAS 66213  
TEL: 913-381-1170  
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LENEXA, KANSAS 66215  
TEL: 913-492-2400  
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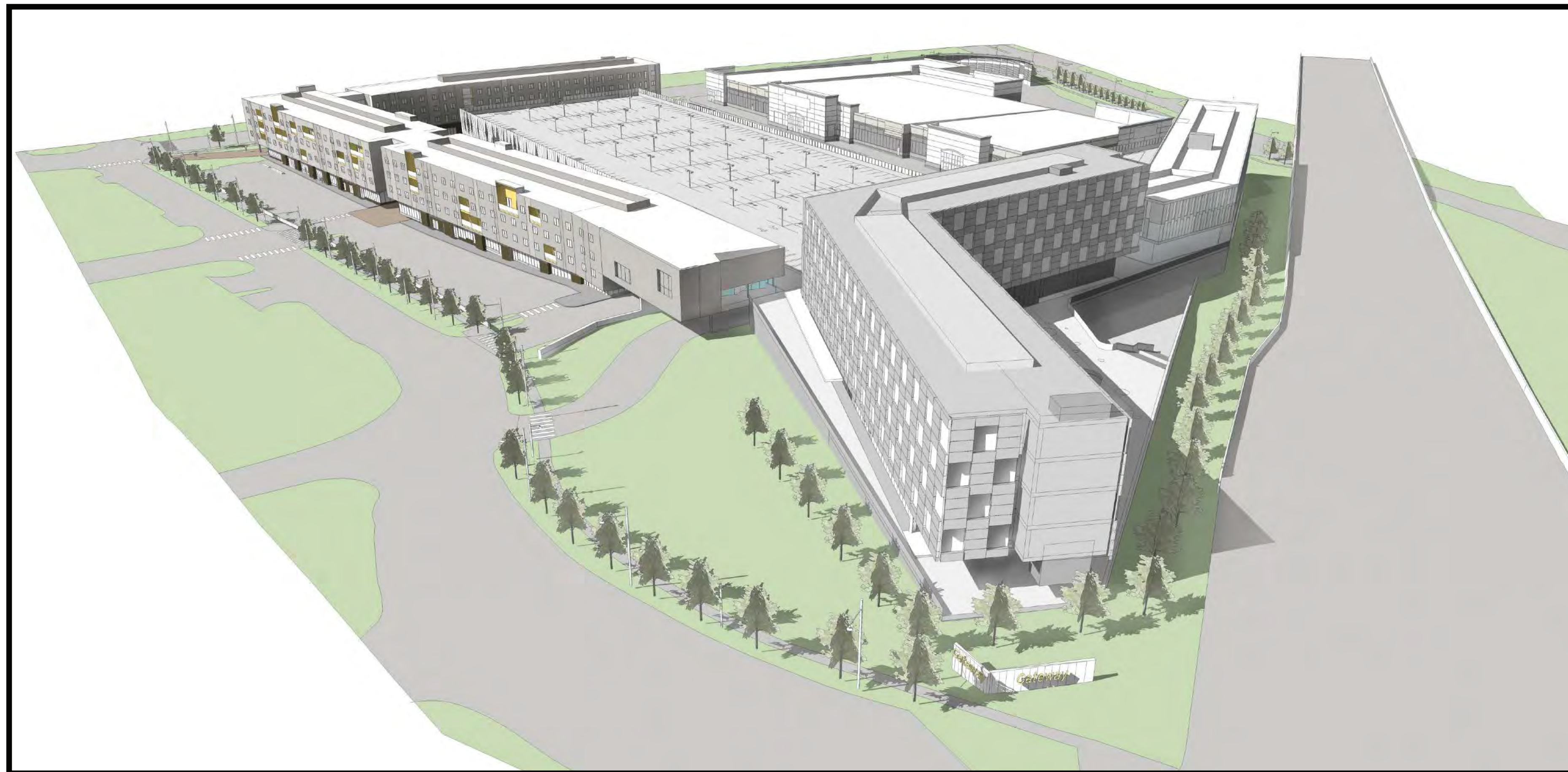




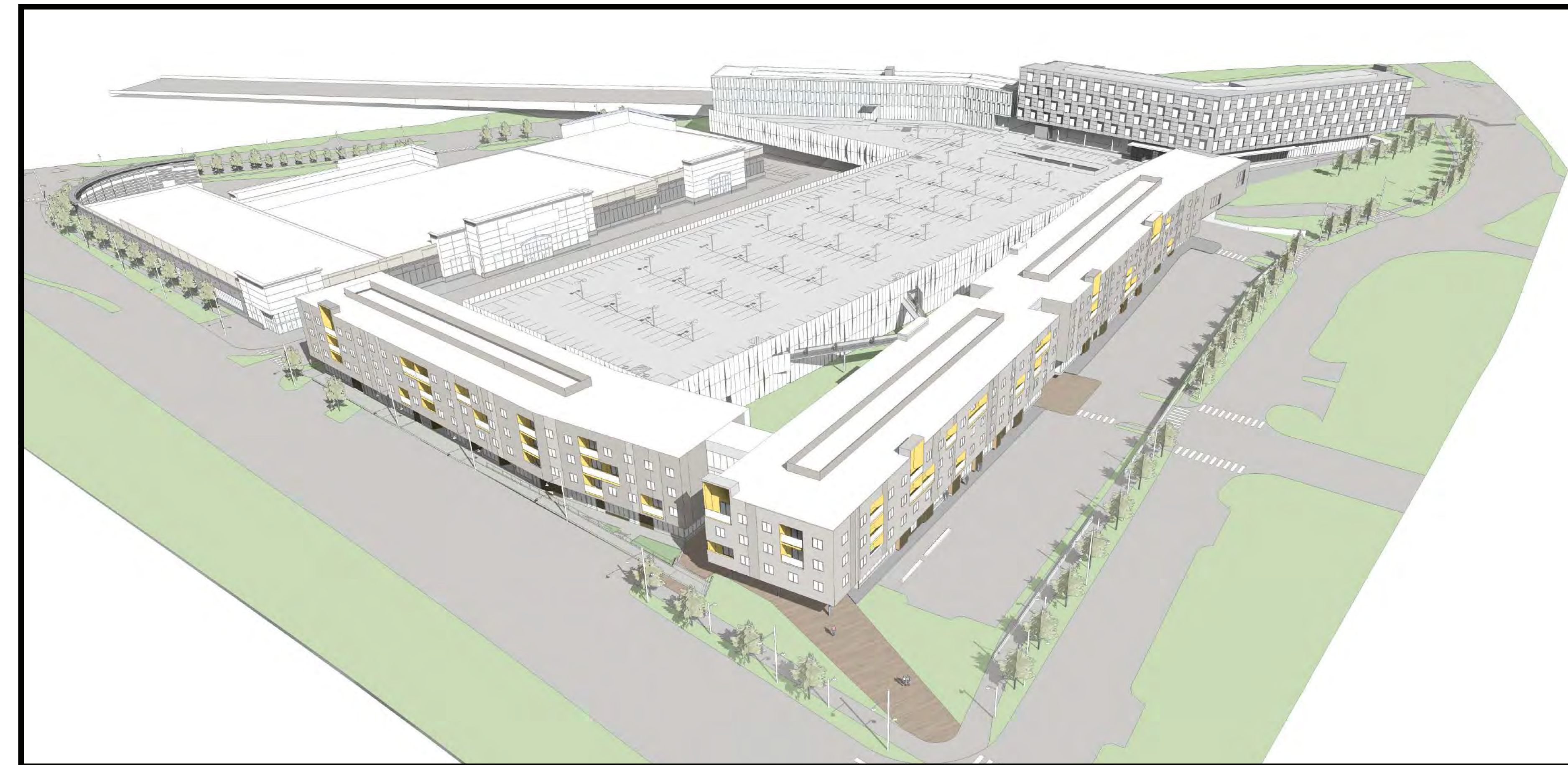
AERIAL FROM SOUTHEAST - SHAWNEE MISSION PARKWAY



AERIAL FROM NORTHEAST - ROE AVE. JOHNSON DRIVE



AERIAL FROM SOUTHWEST - ROELAND DRIVE AND SHAWNEE MISSION PARKWAY



AERIAL FROM NORTH - JOHNSON DRIVE AND ROELAND DRIVE

CONSULTANTS:

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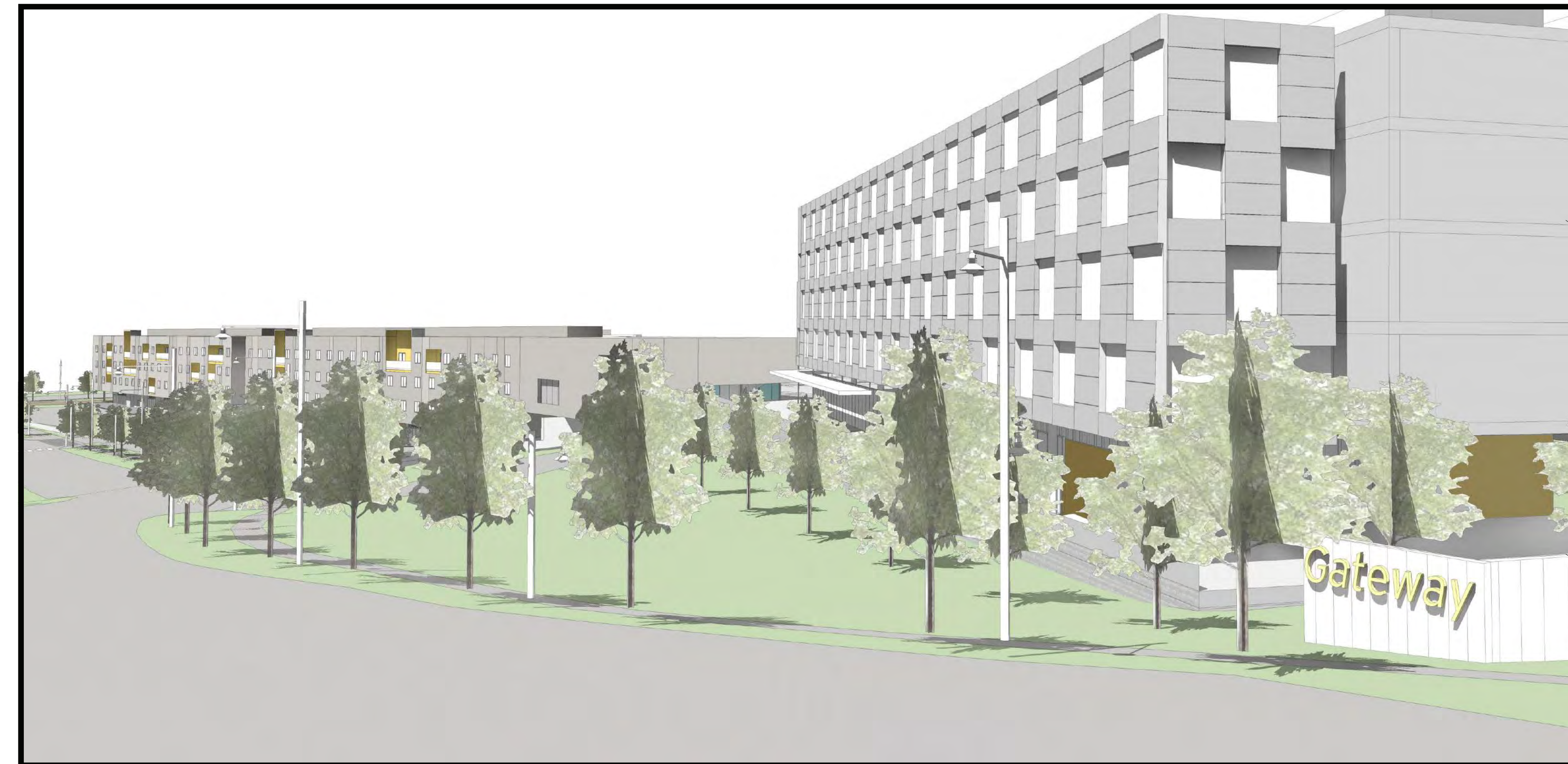
STREET LEVEL VIEW - ENTRY AT JOHNSON DRIVE



STREET LEVEL VIEW - VIEW OF SITE FROM SHAWNEE MISSION PARKWAY LOOKING WEST



STREET LEVEL VIEW - VIEW OF SITE FROM CORNER OF JOHNSON DRIVE AND ROE AVE,



STREET LEVEL VIEW - VIEW OF SITE FROM CORNER OF ROELAND AND SHAWNEE MISSION PARKWAY

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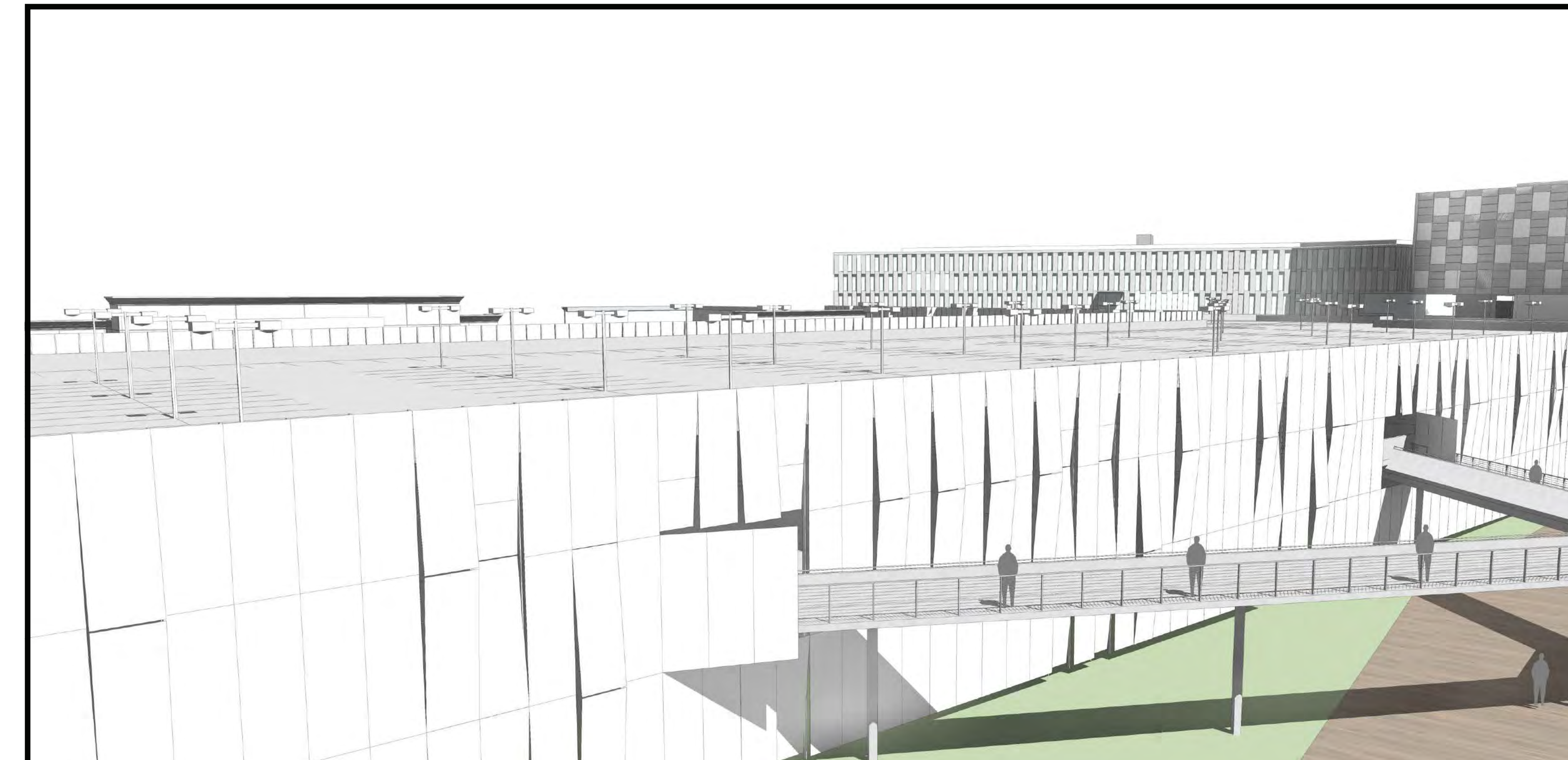
STREET LEVEL VIEW - VIEW OF SITE LOOKING EAST ON ROCK CREEK TRAIL



STREET LEVEL VIEW - VIEW OF SITE FROM ROCK CREEK TRAIL LOOKING SOUTH



STREET LEVEL VIEW - VIEW OF SITE FROM ROCK CREEK TRAIL LOOKING NORTH



VIEW FROM 4TH LEVEL OF BUILDING 'D' AT ROOFS & PARKING LEVEL 03

CONSULTANTS:

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- KEYNOTES**
1. MASONRY PAVER PATIO
  2. WOOD BOARDWALK
  3. WOOD BENCH
  4. BOARD-FORMED CONCRETE WALL ELEMENT
  5. LIGHT POLE
  6. CONCRETE SIDEWALK
  7. WHITE, SINGLE-PLY ROOF MEMBRANE
  8. PRE-VEGETATED TRAY ROOFING SYSTEM
  9. POOL
  10. ROOFTOP MECHANICAL SCREENING (SAME AS BUILDING CLADDING)
  11. CORRUGATED METAL PANEL
  12. ALUMINUM COMPOSITE PANEL
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  14. ALUMINUM STOREFRONT / WINDOW SYSTEM
  15. MONUMENT SIGN
  16. BUILDING MOUNTED SIGNAGE
  17. ELEVATED PEDESTRIAN WALKWAY
  18. STUCCO



<b>PROJECT GROSS SQUARE FOOTAGE - 556,332</b>		<b>PARKING - 1528 STALLS</b>	
RETAIL	179,100 GSF		
RESIDENTIAL	177,812 GSF	168 APTS	SURFACE 735
HOTEL	140,904 GSF	200 ROOMS	STRUCTURED 793
OFFICE	58,516 GSF		

01 SITE PLAN

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

**FDP-A050**  
SITE PLAN

CONSULTANTS:

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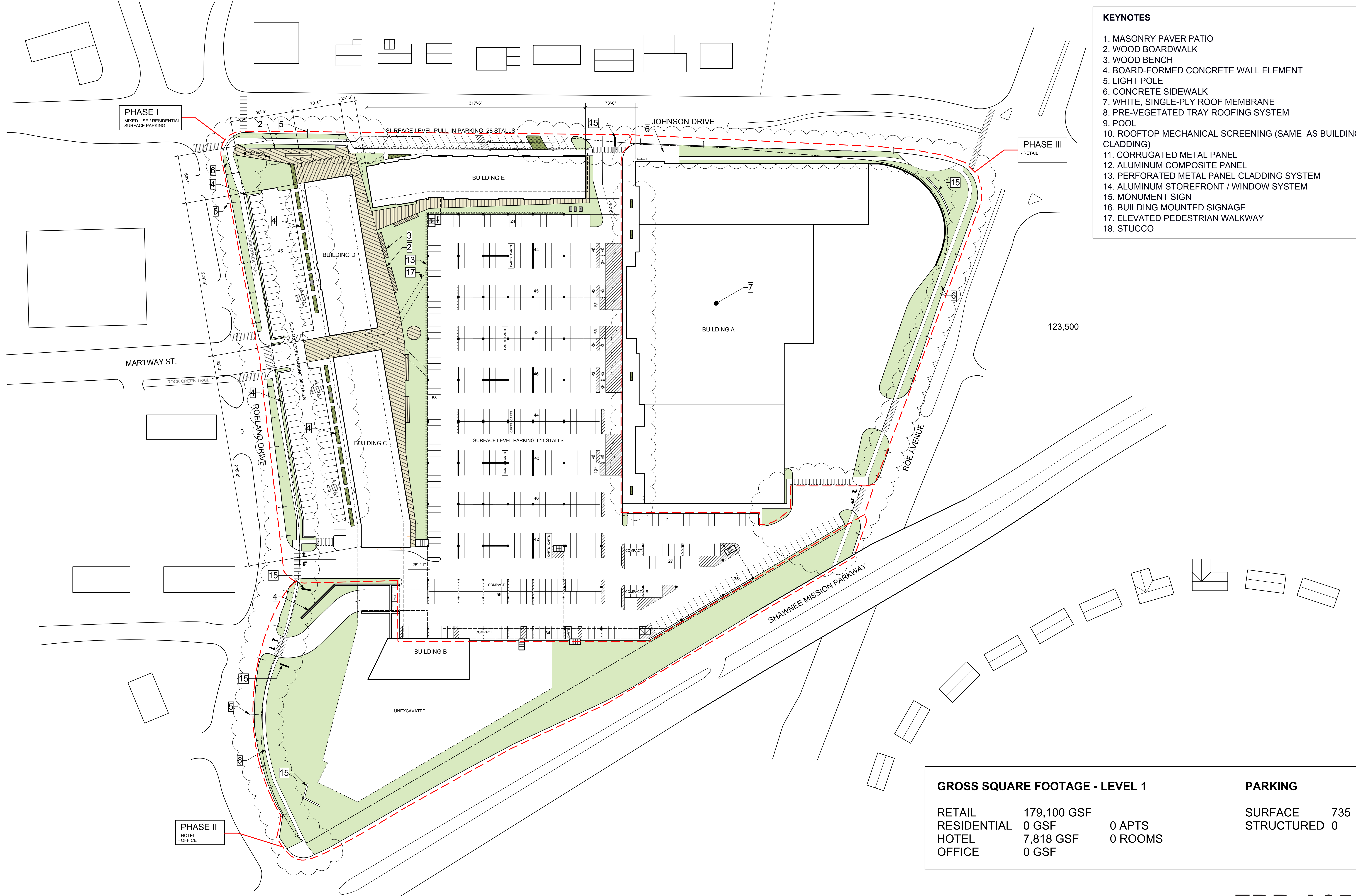
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  18. STUCCO

GROSS SQUARE FOOTAGE - LEVEL 1			PARKING	
RETAIL	179,100 GSF		SURFACE	735
RESIDENTIAL	0 GSF	0 APTS	STRUCTURED	0
HOTEL	7,818 GSF	0 ROOMS		
OFFICE	0 GSF			

01 SITE PLAN - LEVEL 1

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

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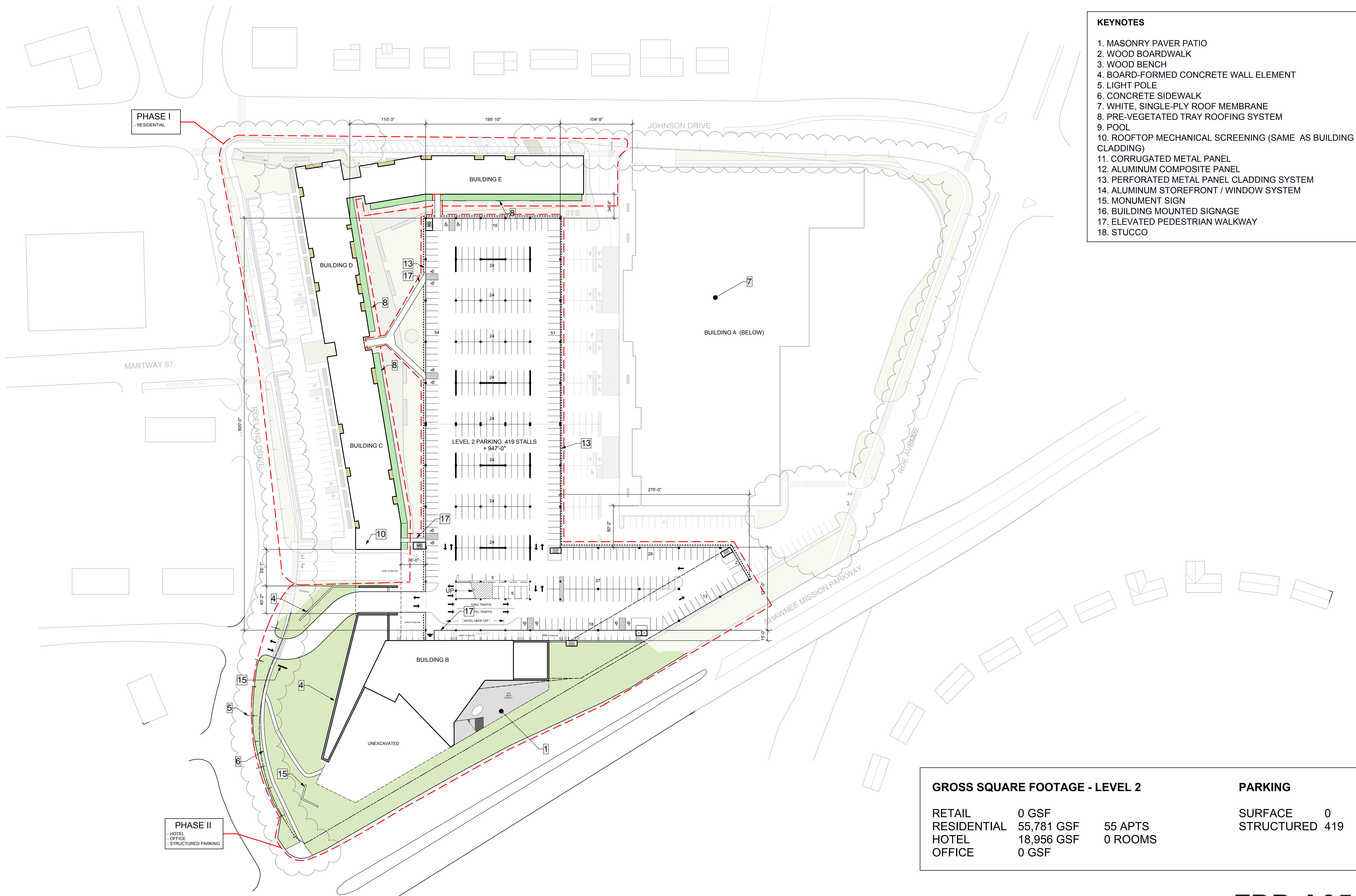
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**FDP-A051**  
**SITE PLAN (LEVEL 1)**



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  18. STUCCO



**PHASE I**  
- RESIDENTIAL

**PHASE II**  
- HOTEL  
- OFFICE  
- STRUCTURED PARKING

GROSS SQUARE FOOTAGE - LEVEL 2			PARKING	
RETAIL	0 GSF		SURFACE	0
RESIDENTIAL	55,781 GSF	55 APTS	STRUCTURED	419
HOTEL	18,956 GSF	0 ROOMS		
OFFICE	0 GSF			

01 SITE PLAN - LEVEL 2

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

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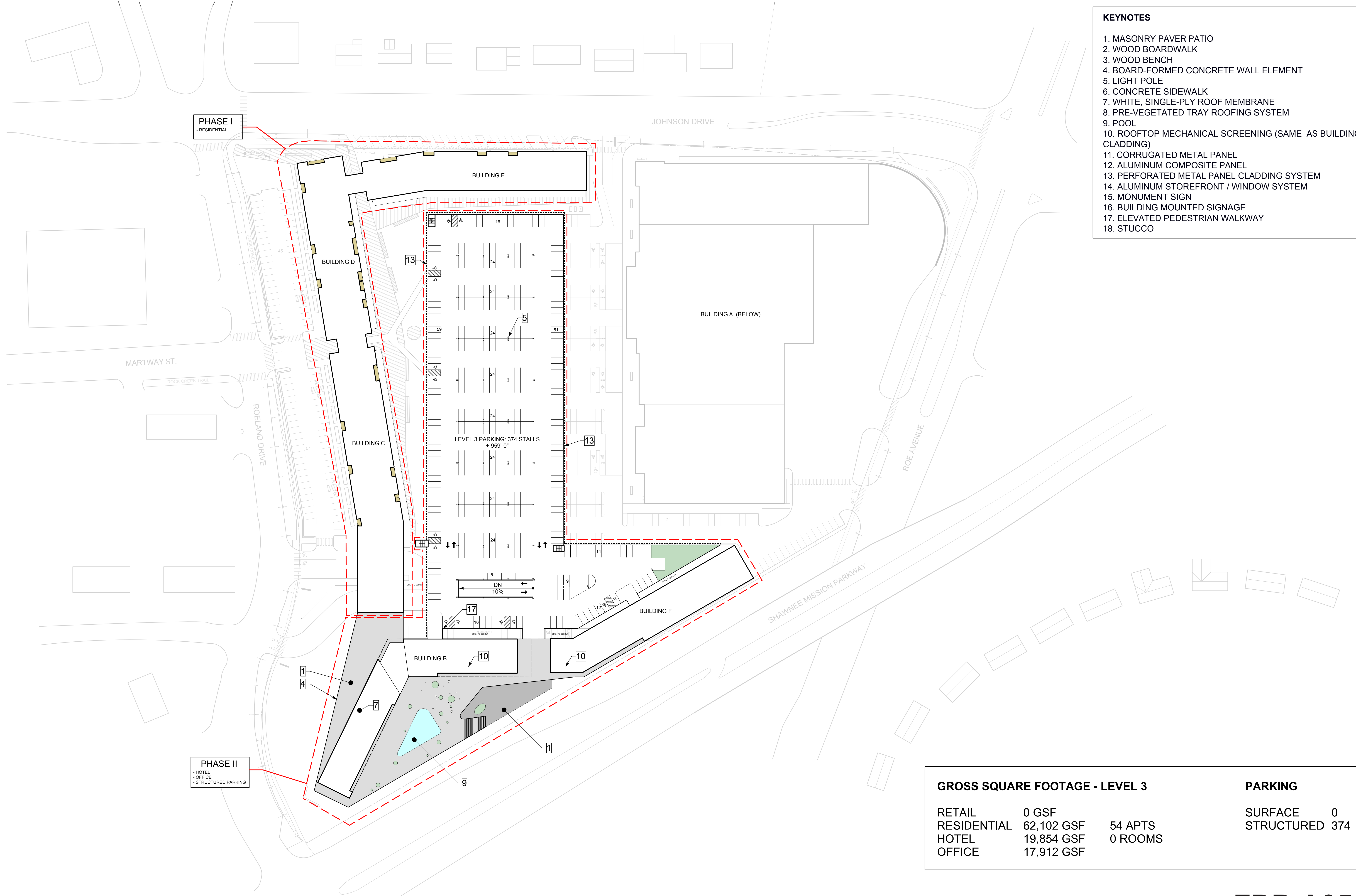
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**FDP-A052**  
SITE PLAN (LEVEL 2)



- KEYNOTES**
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  16. BUILDING MOUNTED SIGNAGE
  17. ELEVATED PEDESTRIAN WALKWAY
  18. STUCCO



GROSS SQUARE FOOTAGE - LEVEL 3			PARKING	
RETAIL	0 GSF		SURFACE	0
RESIDENTIAL	62,102 GSF	54 APTS	STRUCTURED	374
HOTEL	19,854 GSF	0 ROOMS		
OFFICE	17,912 GSF			

01 SITE PLAN - LEVEL 3

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

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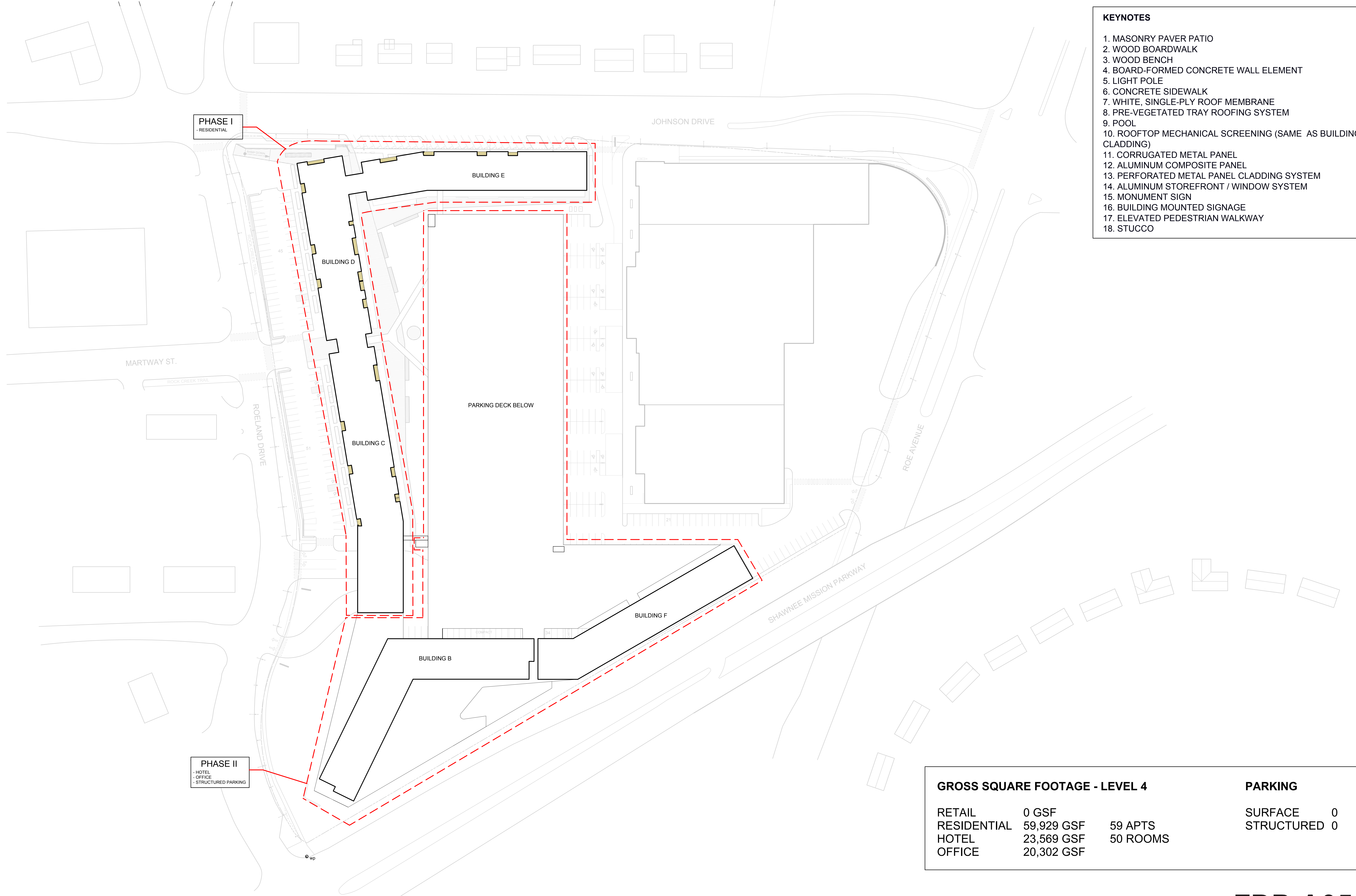
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**FDP-A053**  
**SITE PLAN (LEVEL 3)**





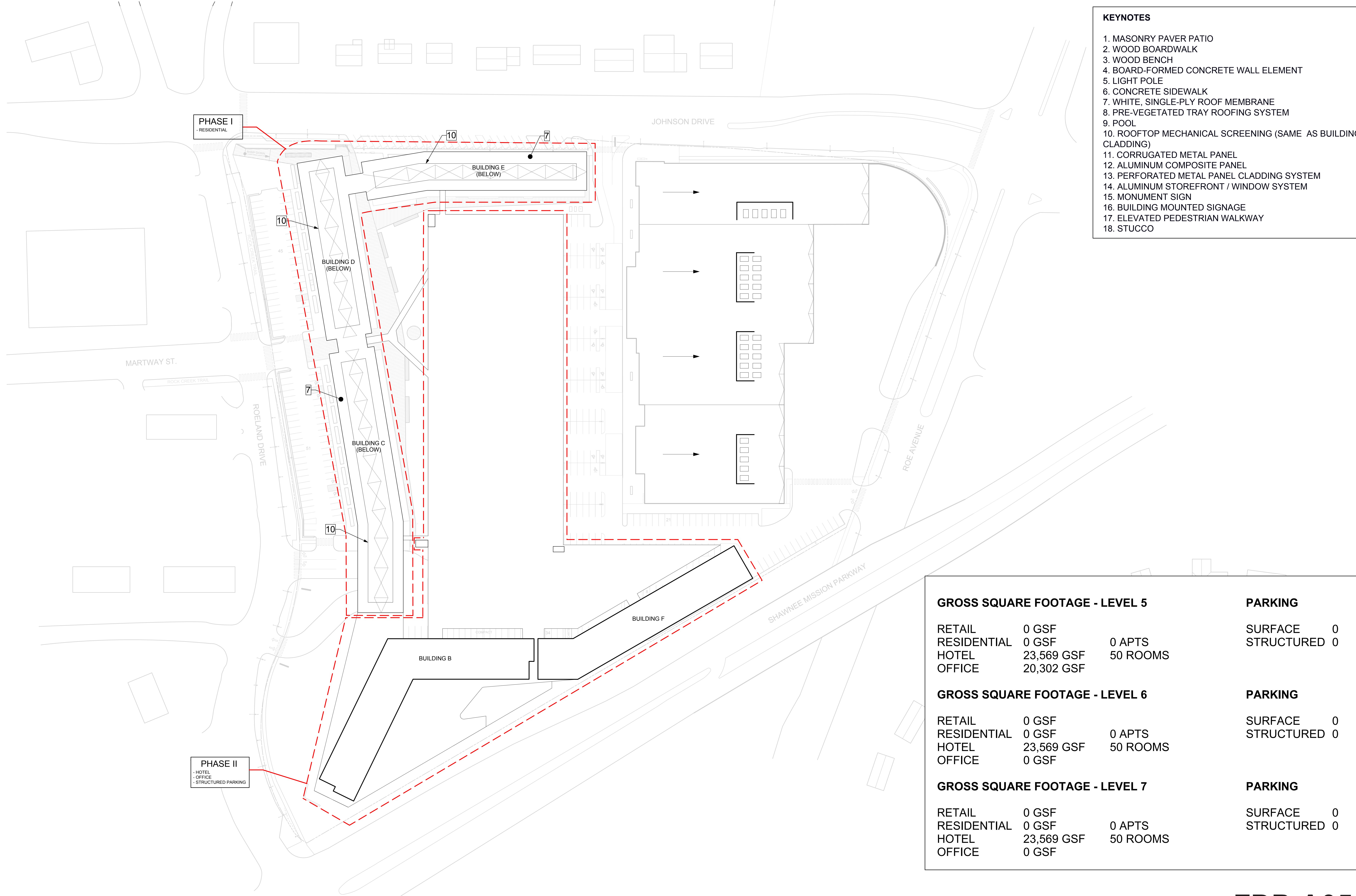
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**PHASE I**  
- RESIDENTIAL

**PHASE II**  
- HOTEL  
- OFFICE  
- STRUCTURED PARKING

GROSS SQUARE FOOTAGE - LEVEL 4			PARKING	
RETAIL	0 GSF		SURFACE	0
RESIDENTIAL	59,929 GSF	59 APTS	STRUCTURED	0
HOTEL	23,569 GSF	50 ROOMS		
OFFICE	20,302 GSF			



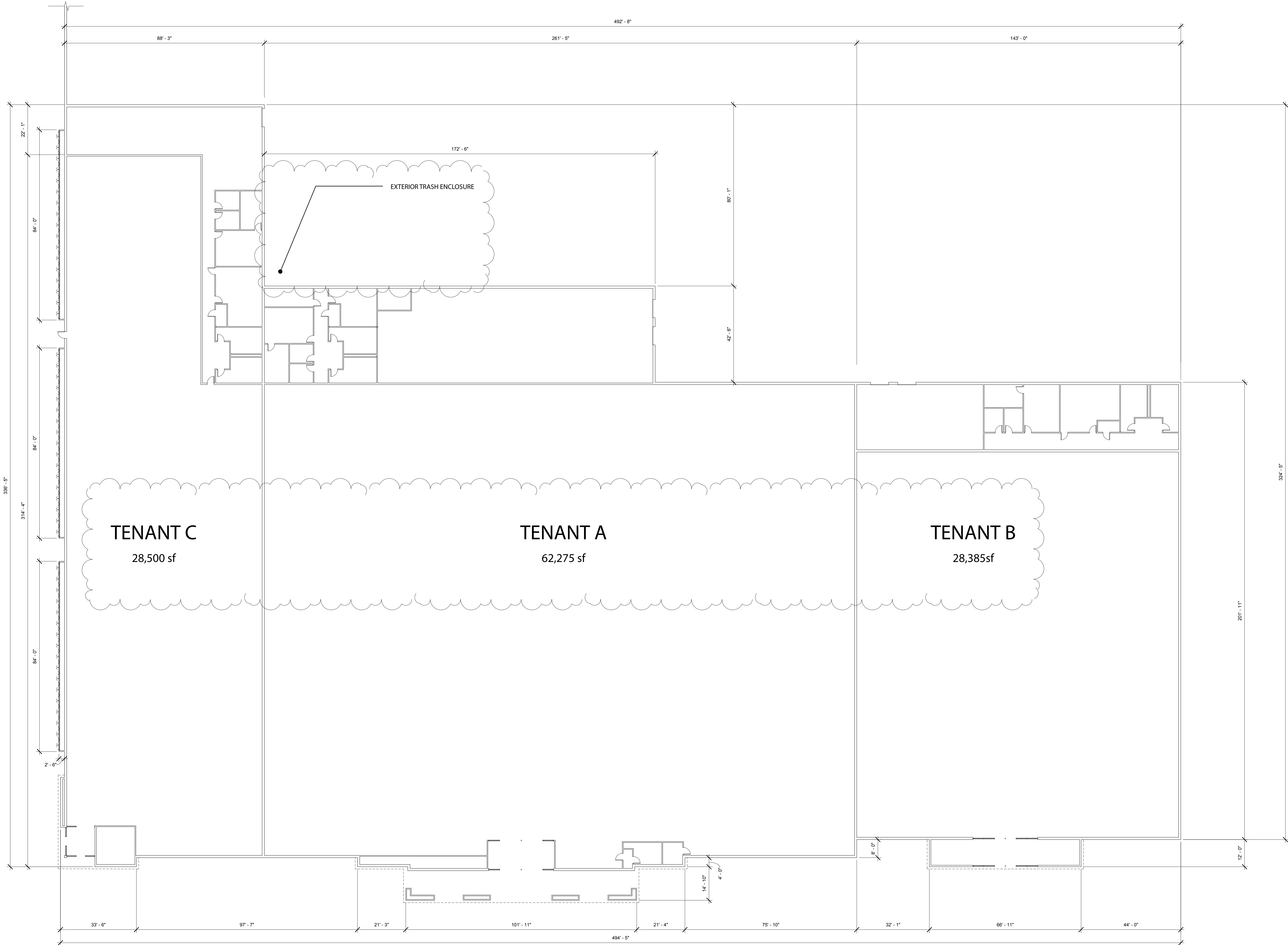


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**PHASE I**  
- RESIDENTIAL

**PHASE II**  
- HOTEL  
- OFFICE  
- STRUCTURED PARKING

GROSS SQUARE FOOTAGE - LEVEL 5			PARKING	
RETAIL	0 GSF		SURFACE	0
RESIDENTIAL	0 GSF	0 APTS	STRUCTURED	0
HOTEL	23,569 GSF	50 ROOMS		
OFFICE	20,302 GSF			
GROSS SQUARE FOOTAGE - LEVEL 6			PARKING	
RETAIL	0 GSF		SURFACE	0
RESIDENTIAL	0 GSF	0 APTS	STRUCTURED	0
HOTEL	23,569 GSF	50 ROOMS		
OFFICE	0 GSF			
GROSS SQUARE FOOTAGE - LEVEL 7			PARKING	
RETAIL	0 GSF		SURFACE	0
RESIDENTIAL	0 GSF	0 APTS	STRUCTURED	0
HOTEL	23,569 GSF	50 ROOMS		
OFFICE	0 GSF			



01 FLOOR PLAN - LEVEL 1

# FDP-A000

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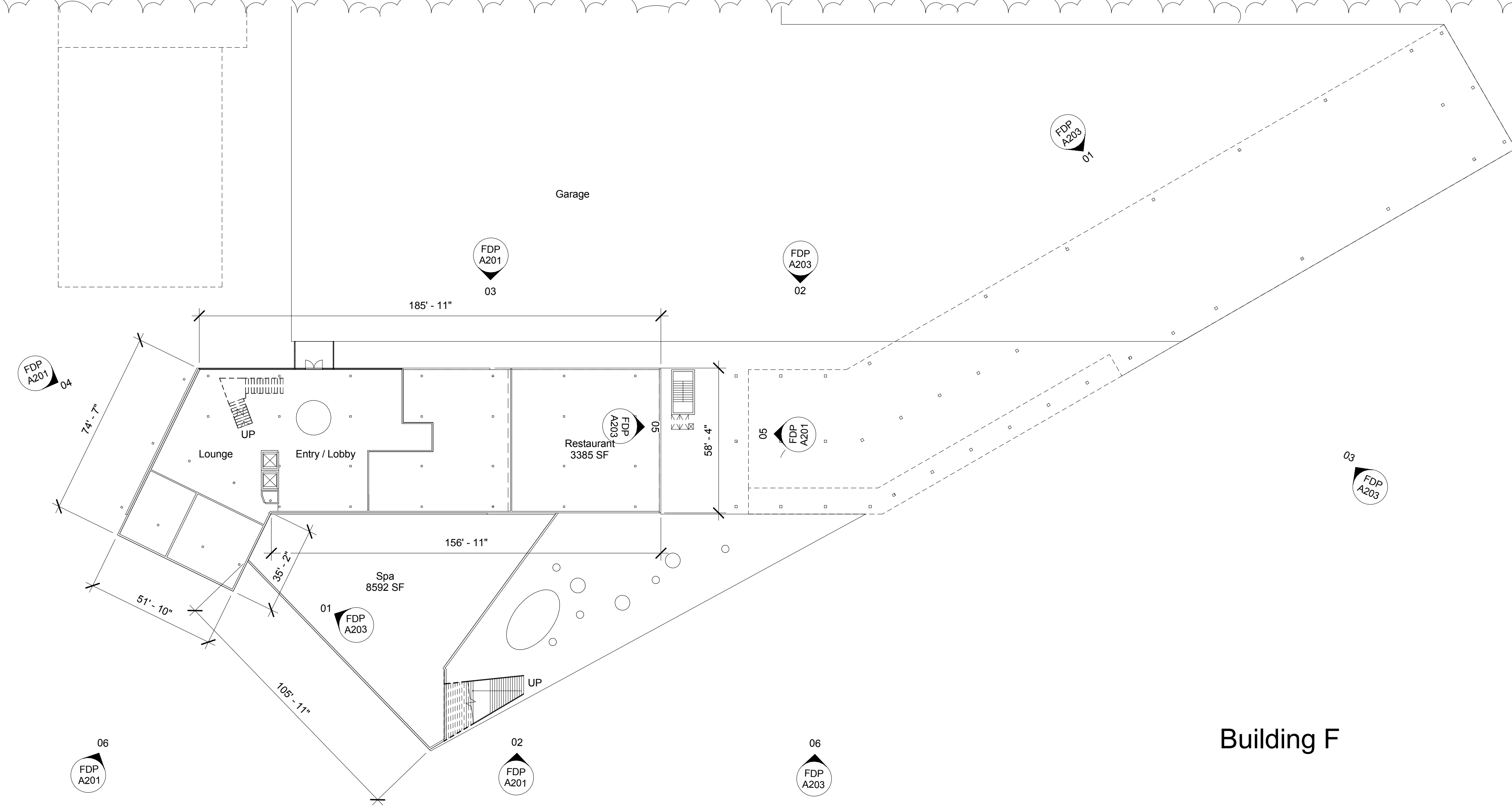
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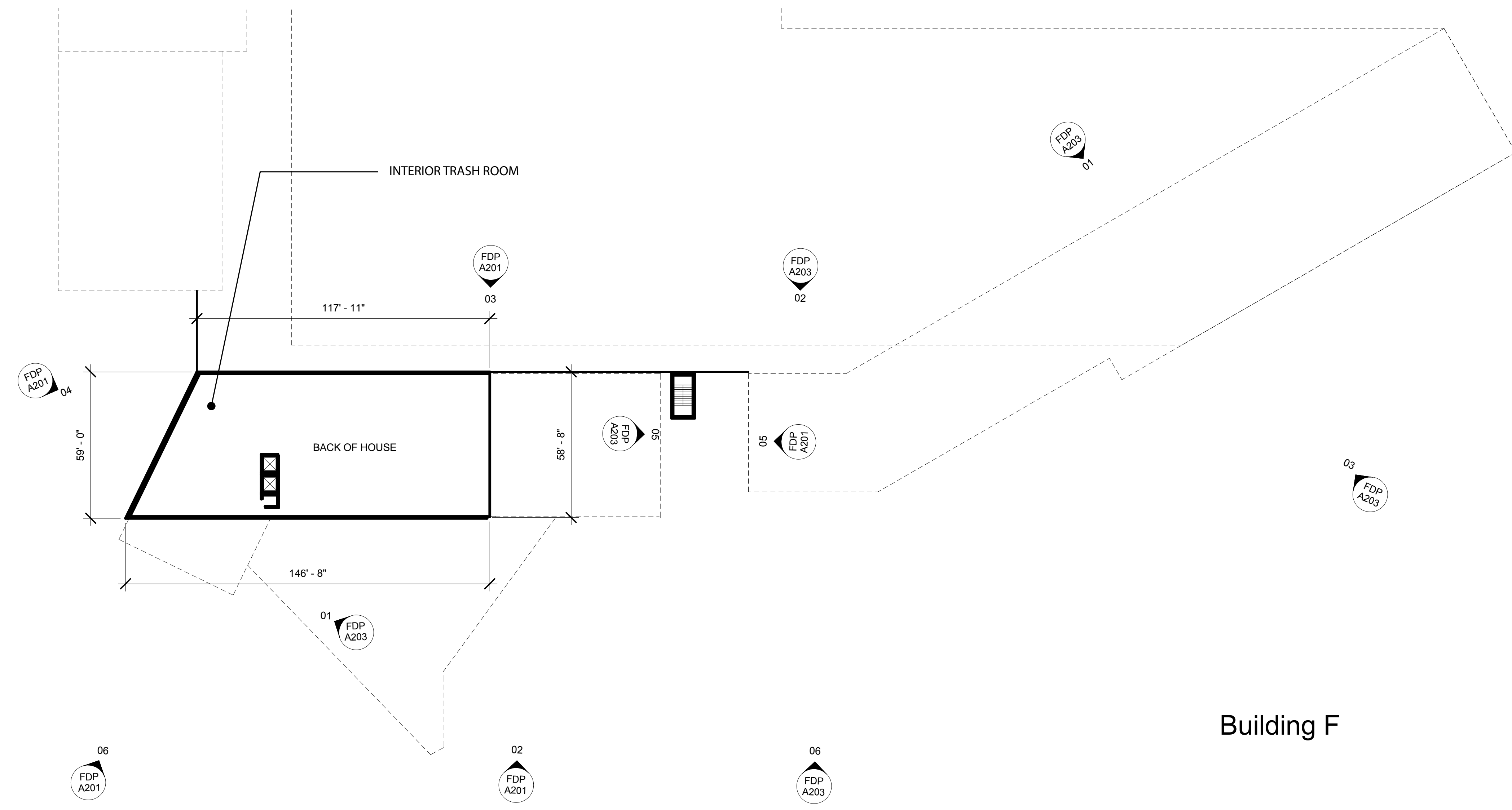


Building B



02 FLOOR PLAN - LEVEL 2

Building B



01 FLOOR PLAN - LEVEL 1

# FDP-A100

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

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## BUILDINGS B/F - FLOOR PLANS (LEVELS 1-2)

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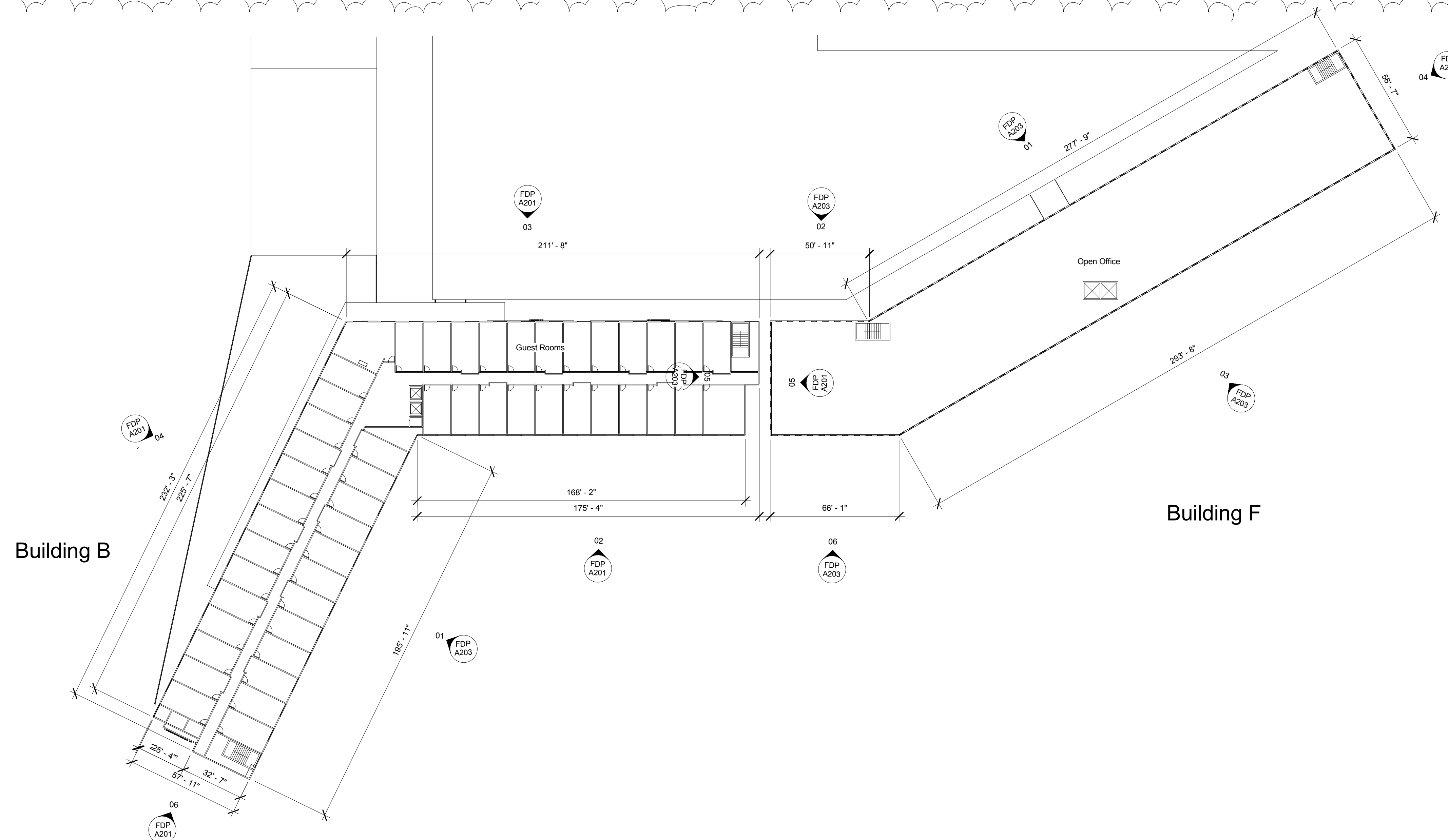
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ENGINEERS

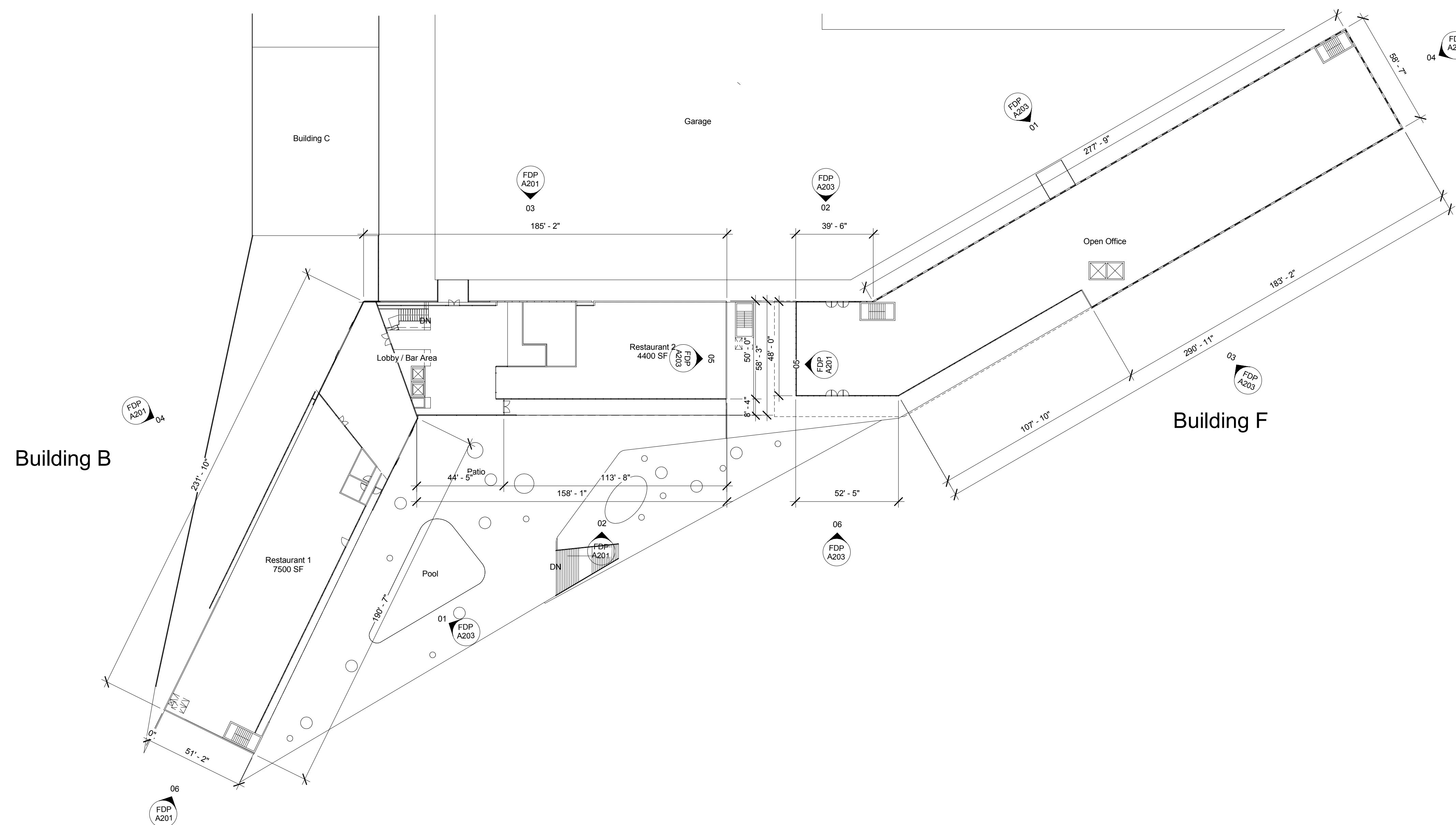
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02 FLOOR PLAN - LEVEL 4



01 FLOOR PLAN - LEVEL 3



# FDP-A101

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

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## BUILDINGS B/F - FLOOR PLANS (LEVELS 3-4)

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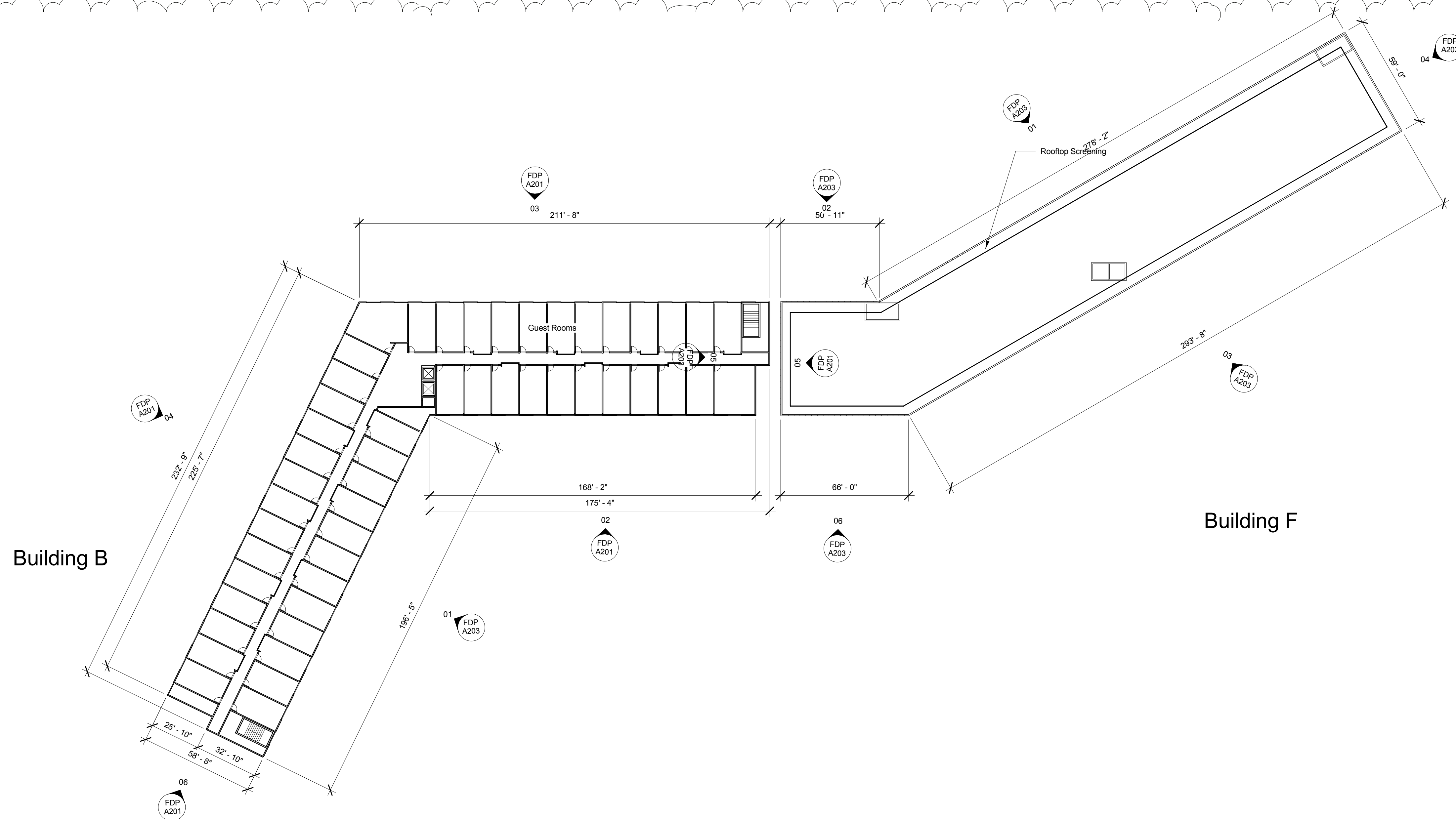
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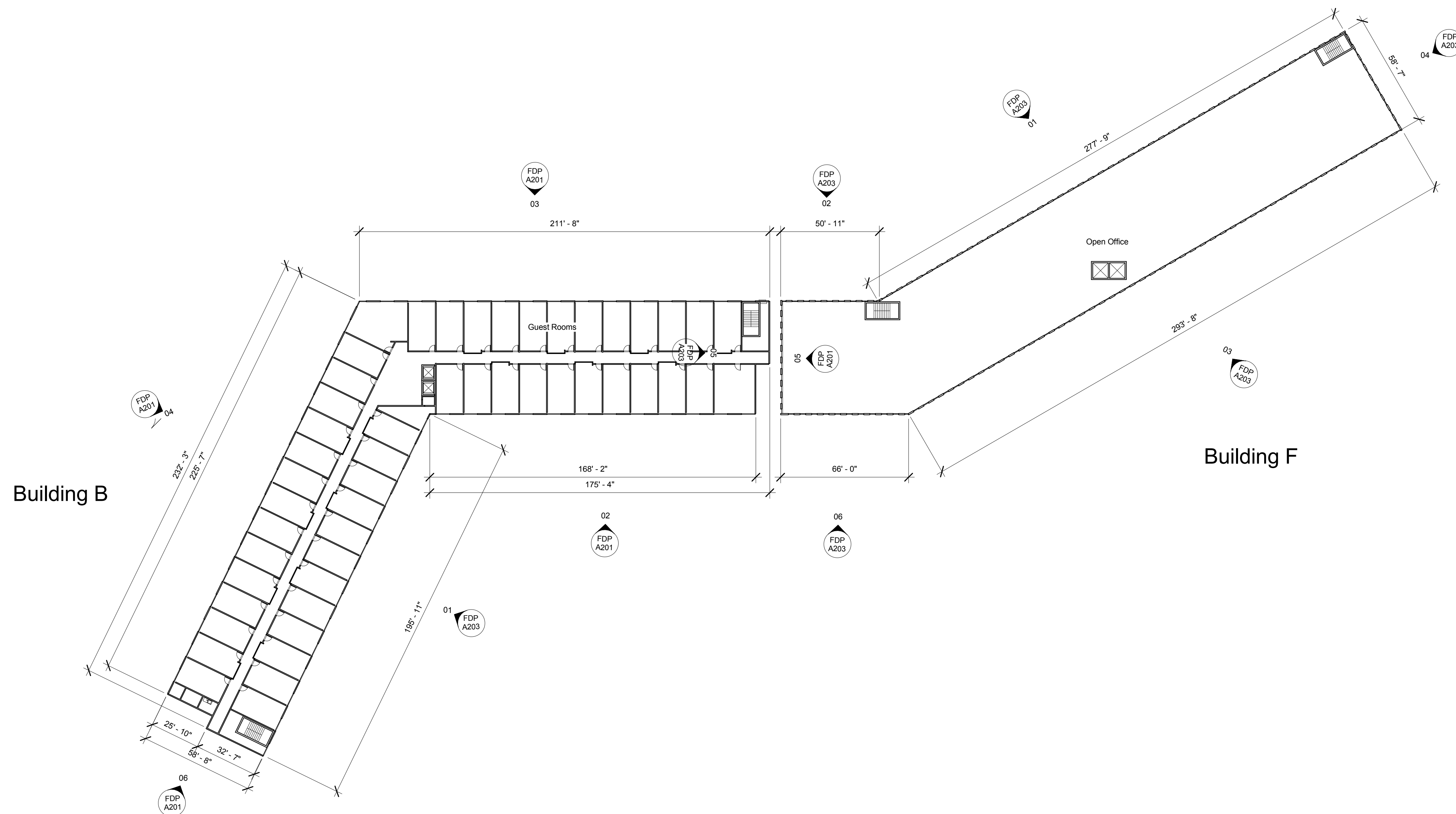
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02 FLOOR PLAN - LEVEL 6



01 FLOOR PLAN - LEVEL 5



# FDP-A102

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

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## BUILDINGS B/F - FLOOR PLANS (LEVELS 5-6)

MISSION GATEWAY

**el dorado**

**OLSSON**  
ASSOCIATES

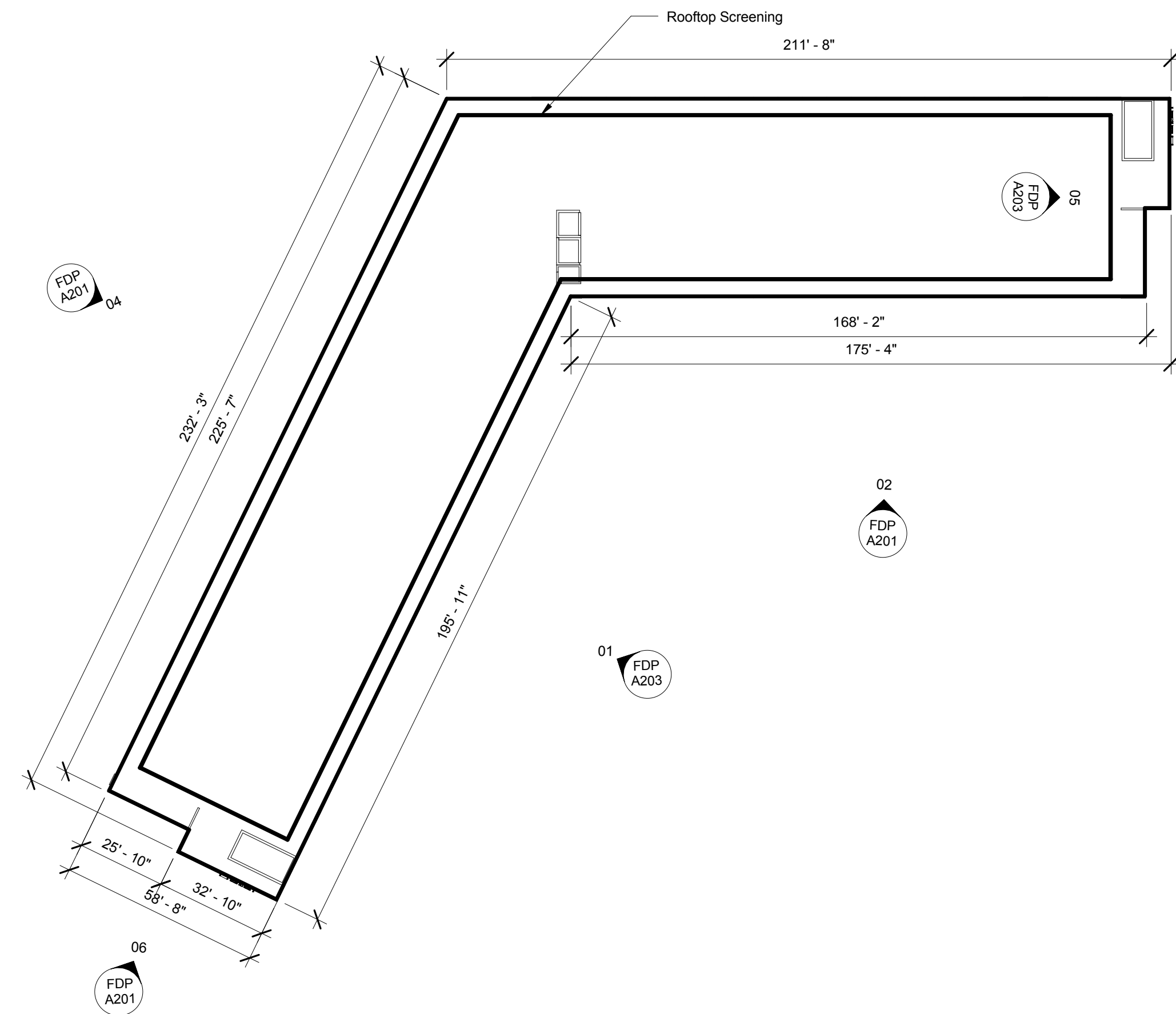
**pkmr**  
INCORPORATED

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02 FLOOR PLAN - ROOF

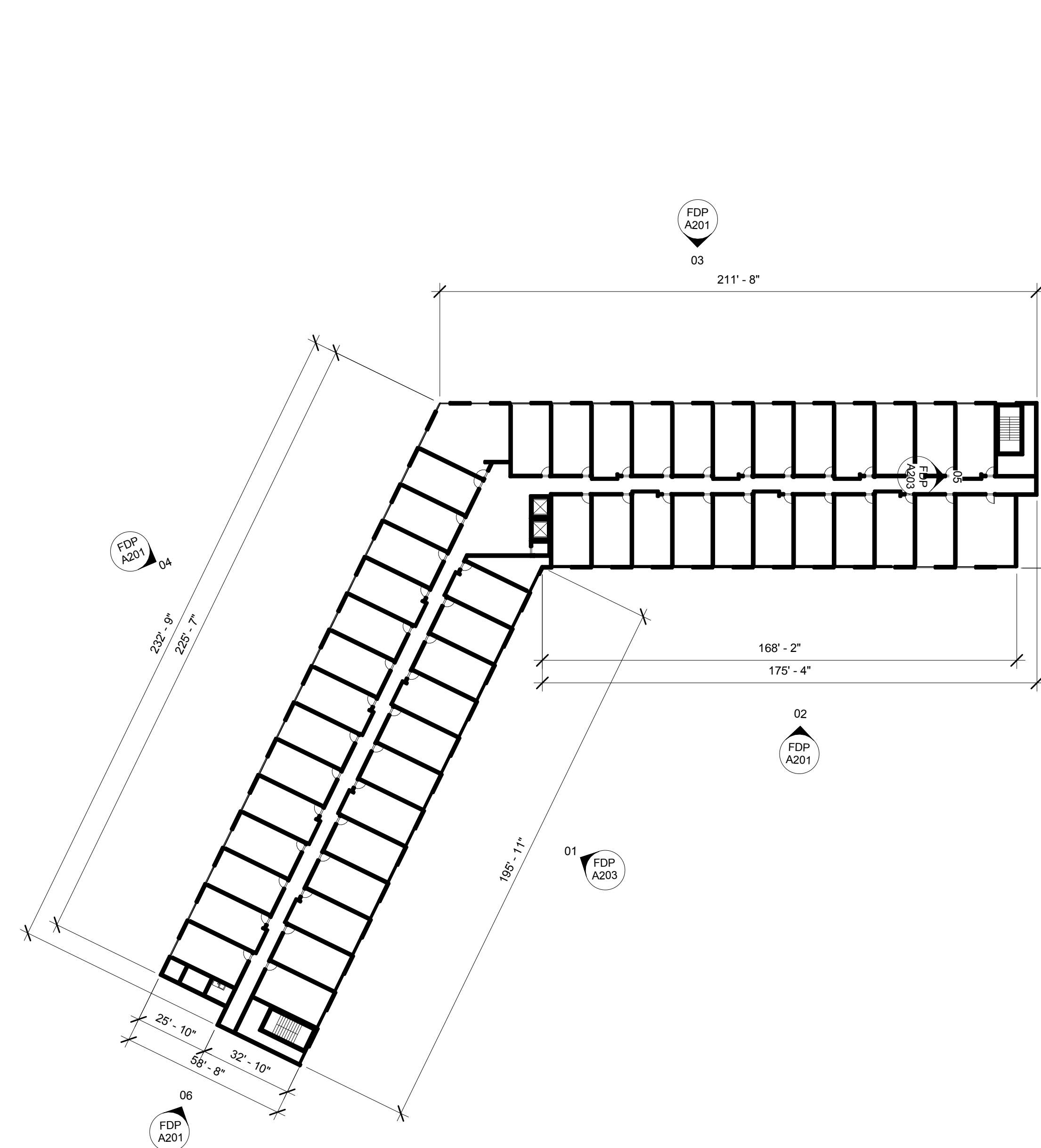
Building B



Building F

01 FLOOR PLAN - LEVEL 7

Building B



Building F

# FDP-A103

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

CONSULTANTS:

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## BUILDINGS B/F - FLOOR PLANS (LEVELS 7-ROOF)

MISSION GATEWAY

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**OLSSON**  
ASSOCIATES

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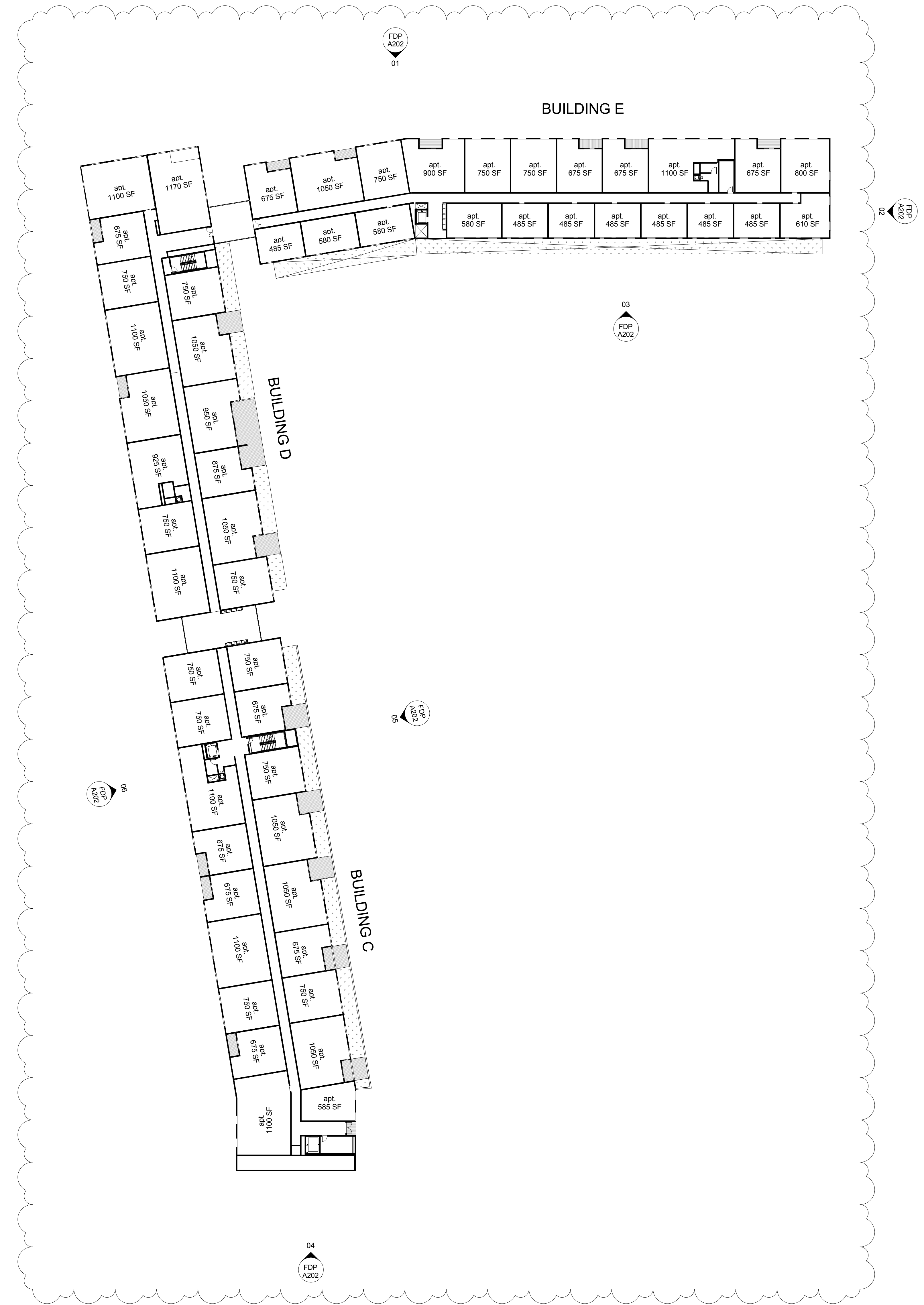
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1 LEVEL 1 PLAN - FDP  
1/32" = 1'-0"



2 LEVEL 2 PLAN - FDP  
1/32" = 1'-0"

CONSULTANTS:

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MISSION GATEWAY

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**OLSSON**  
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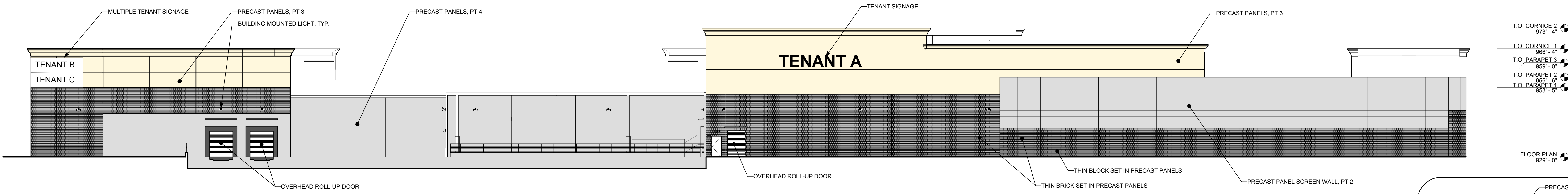
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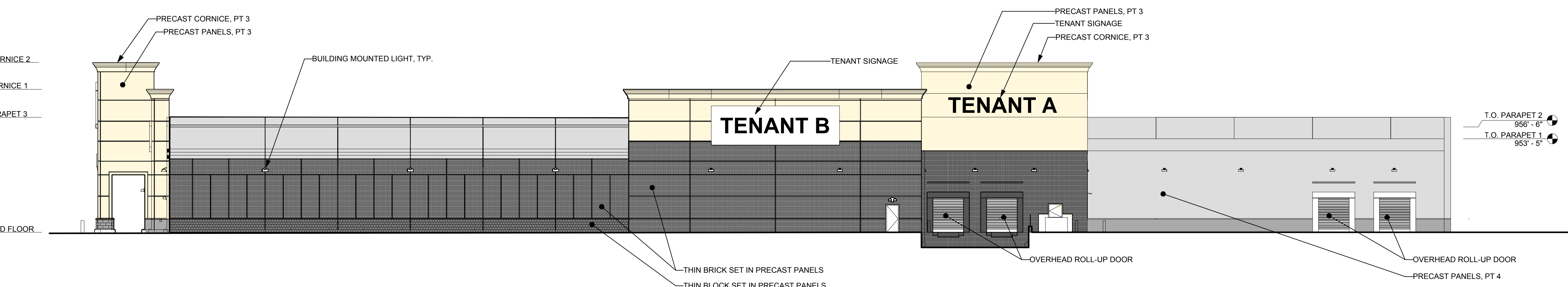




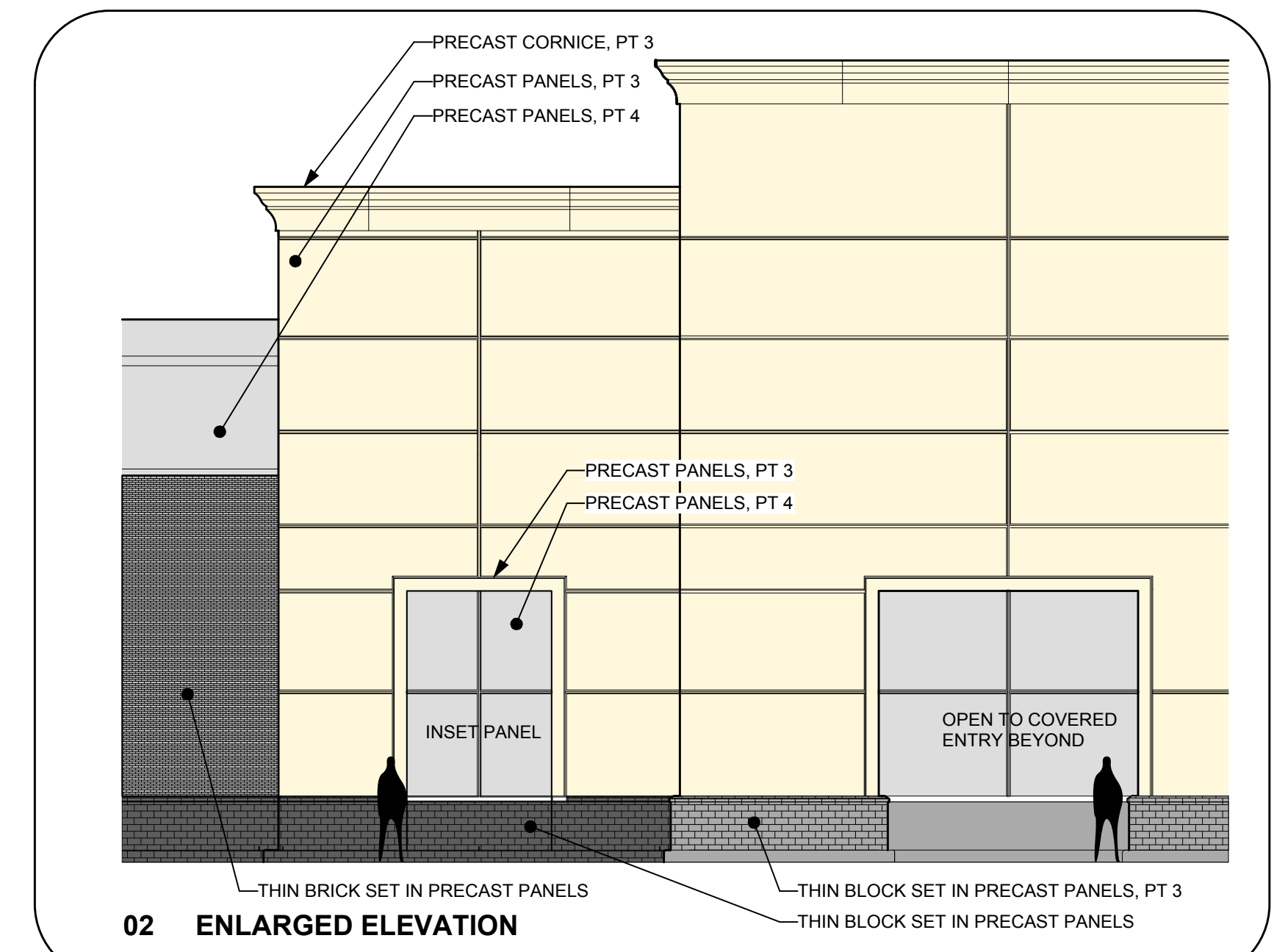




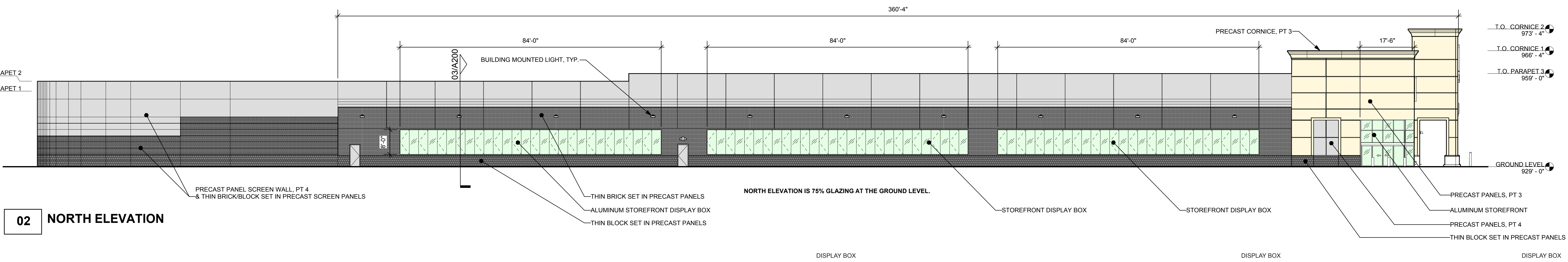
04 EAST ELEVATION



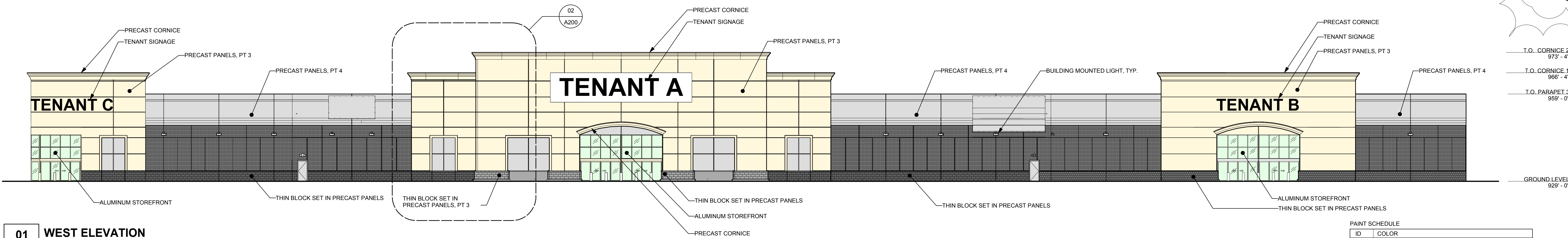
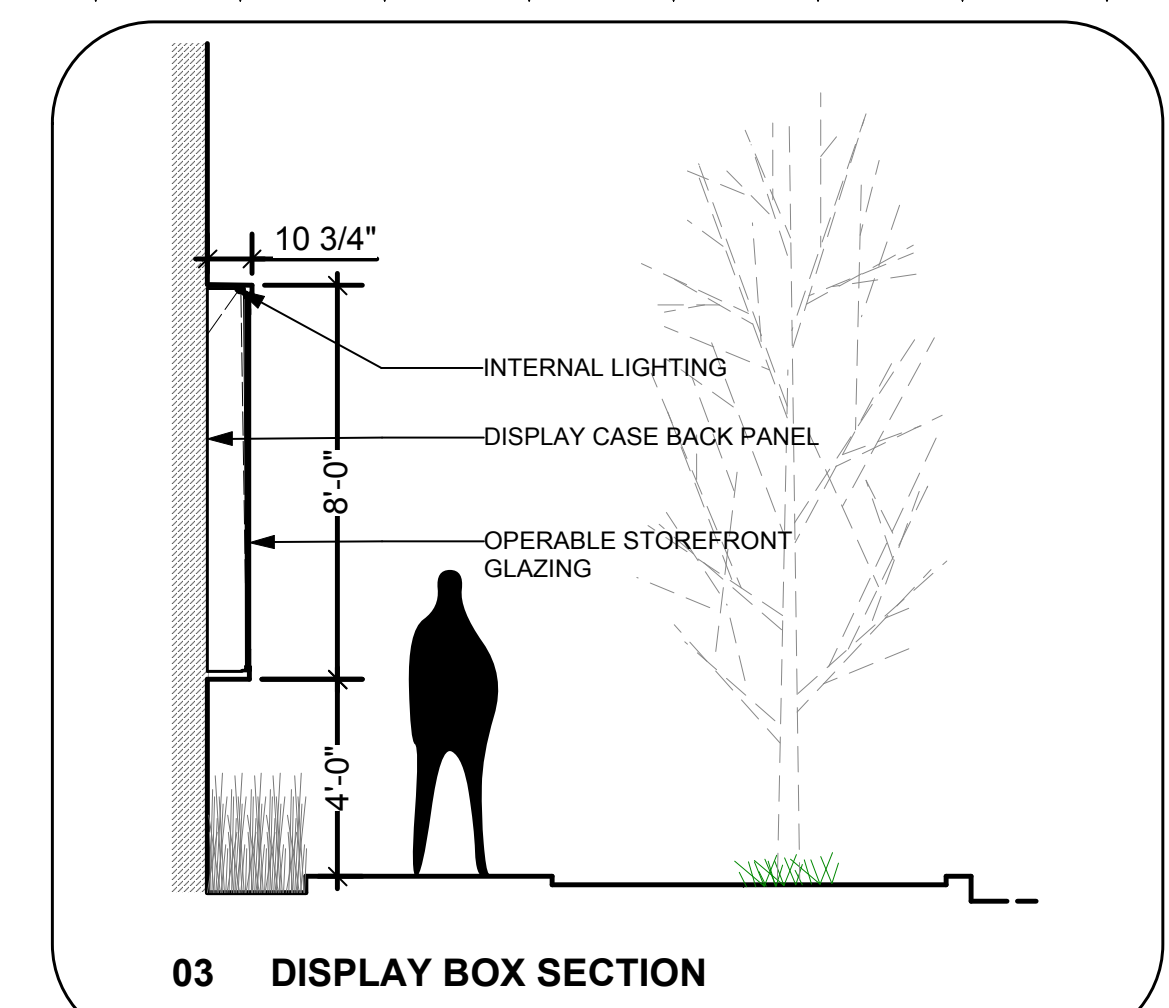
03 SOUTH ELEVATION



02 ENLARGED ELEVATION



02 NORTH ELEVATION

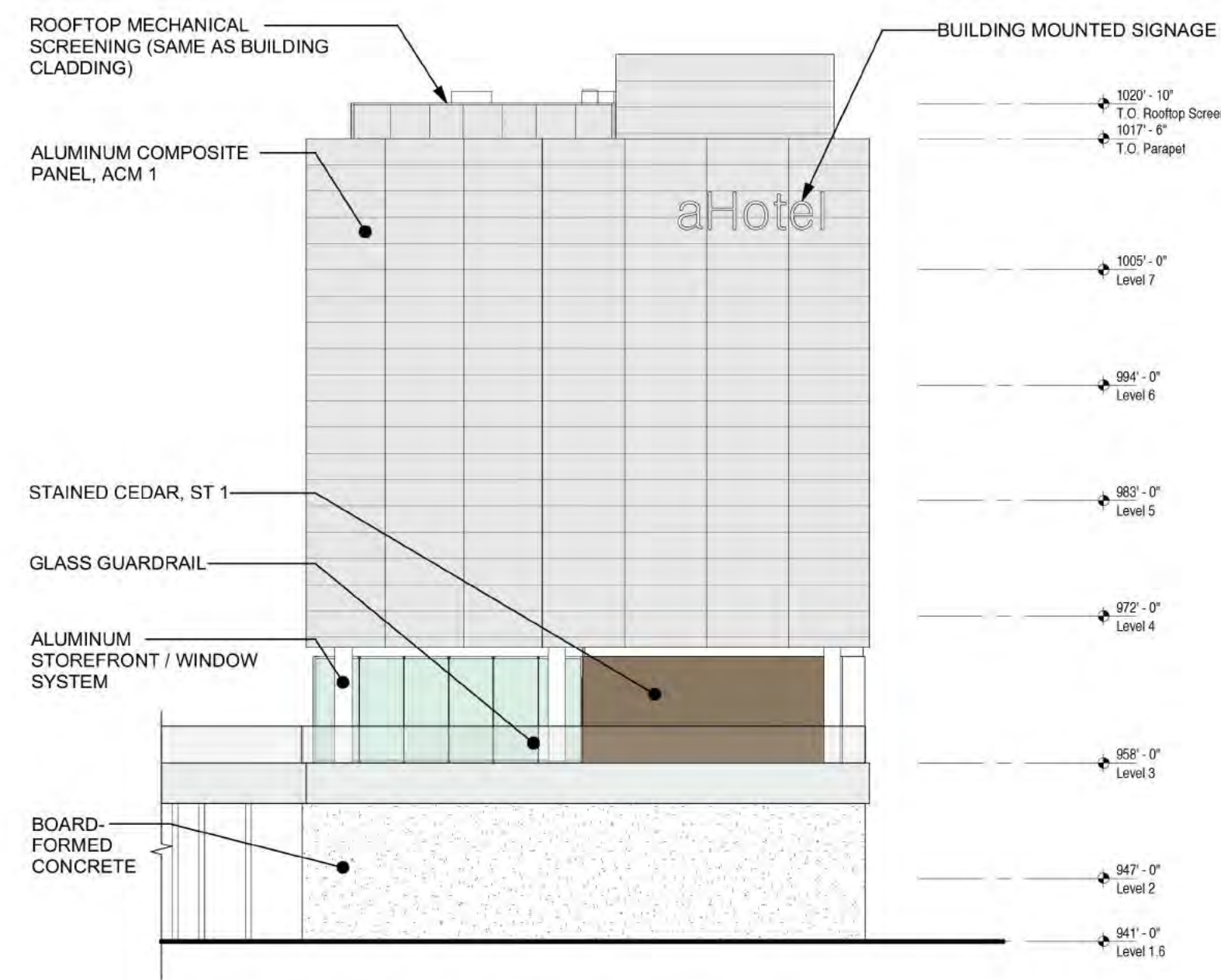


01 WEST ELEVATION

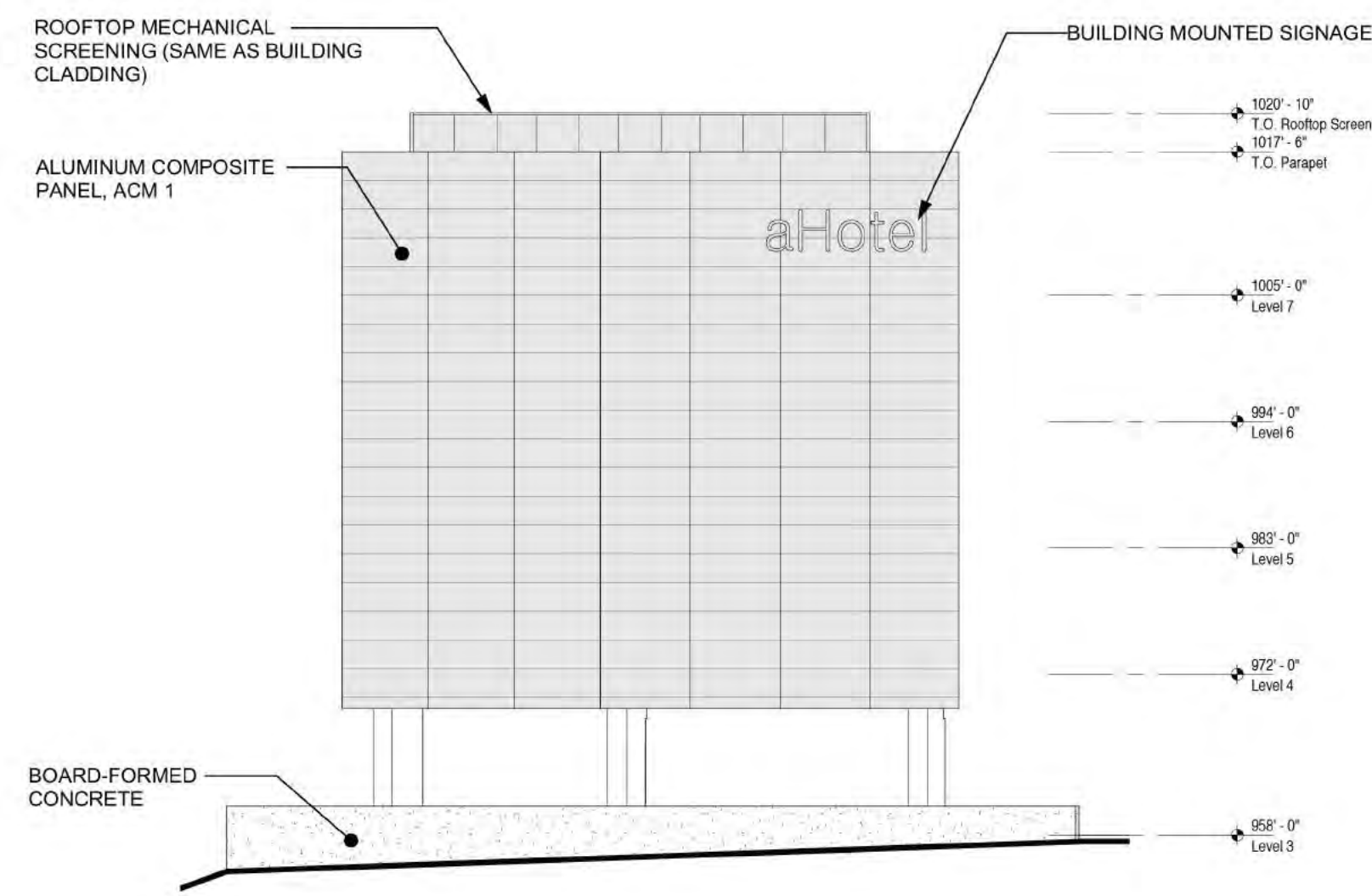
PAINT SCHEDULE

ID	COLOR
PT-1	SW6378 'CRISP LINEN'
PT-2	SW7641 'COLONADE GRAY'
-	THIN BRICK - COAL CREEK - MUTUAL MATERIALS
-	THIN BLOCK - CHARCOAL - MUTUAL MATERIALS



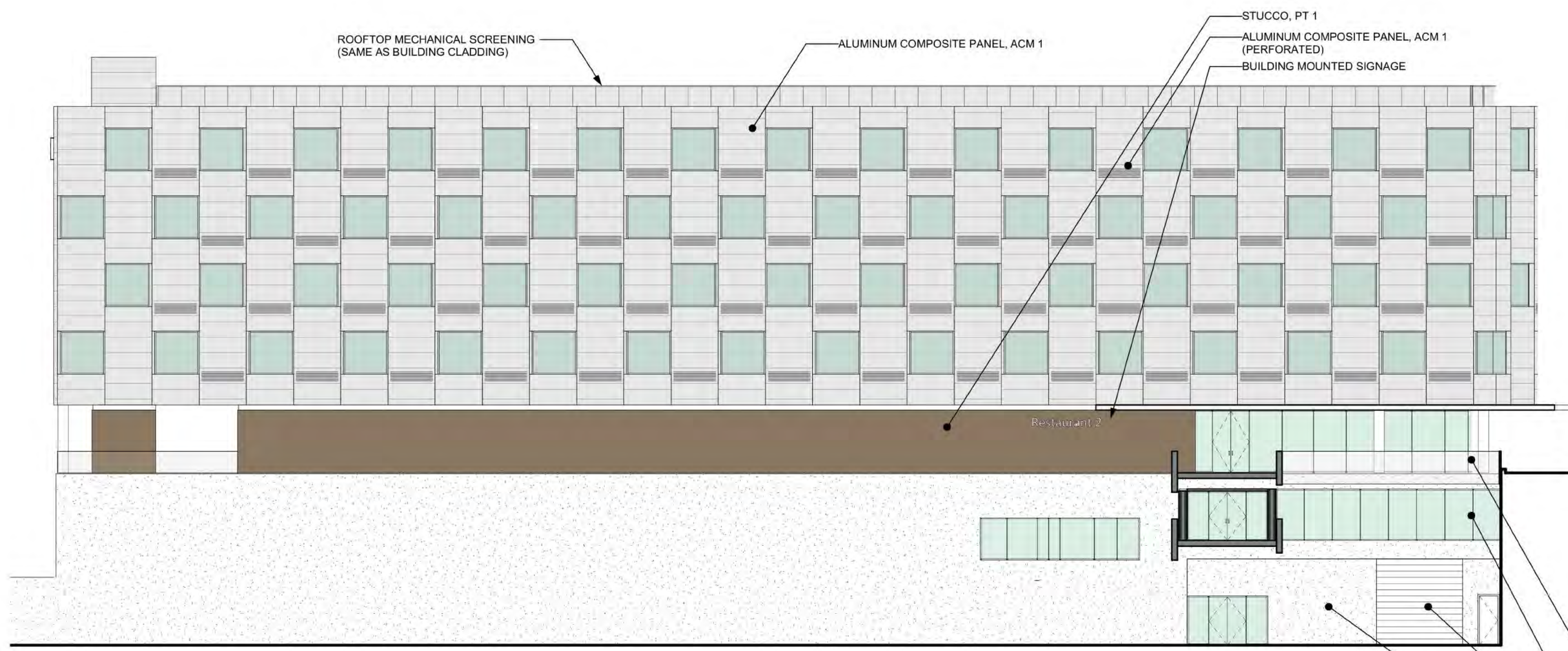
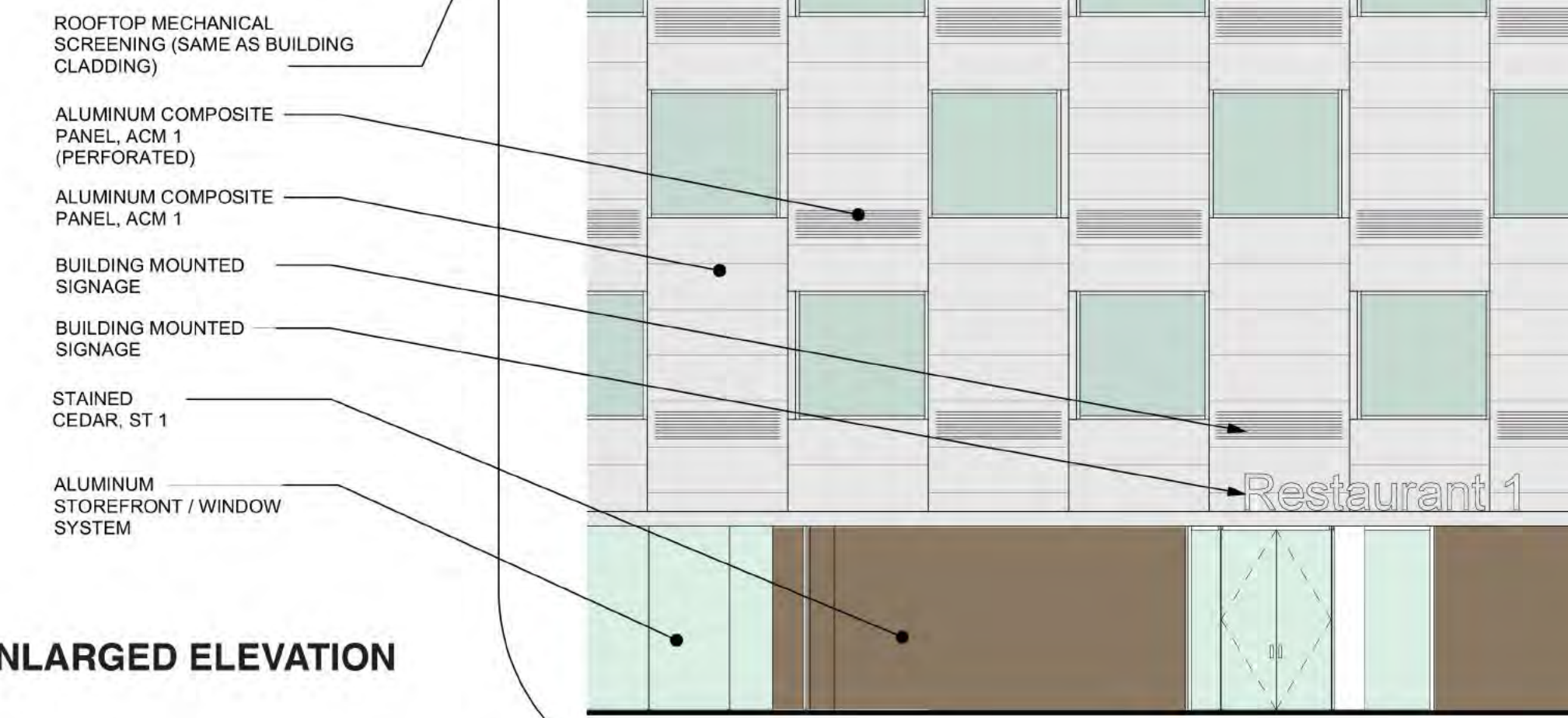


**05 EXTERIOR ELEVATION - EAST**  
11% STUCCO

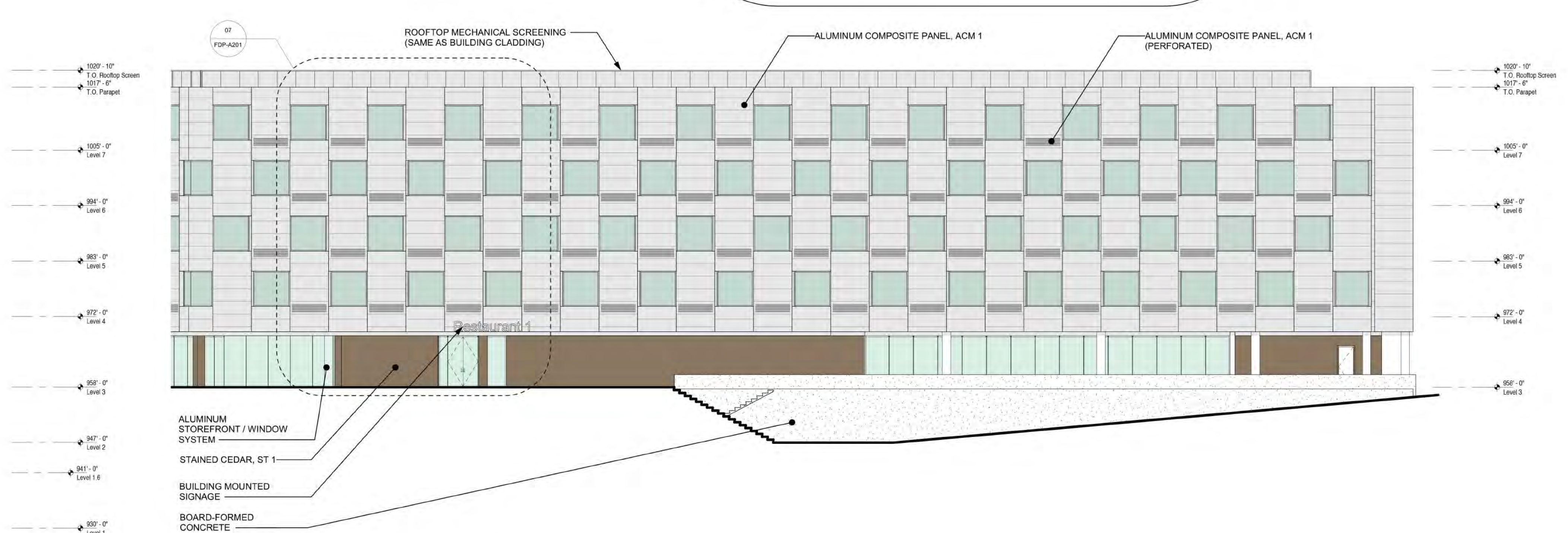


**06 EXTERIOR ELEVATION - SOUTHWEST**  
0% STUCCO

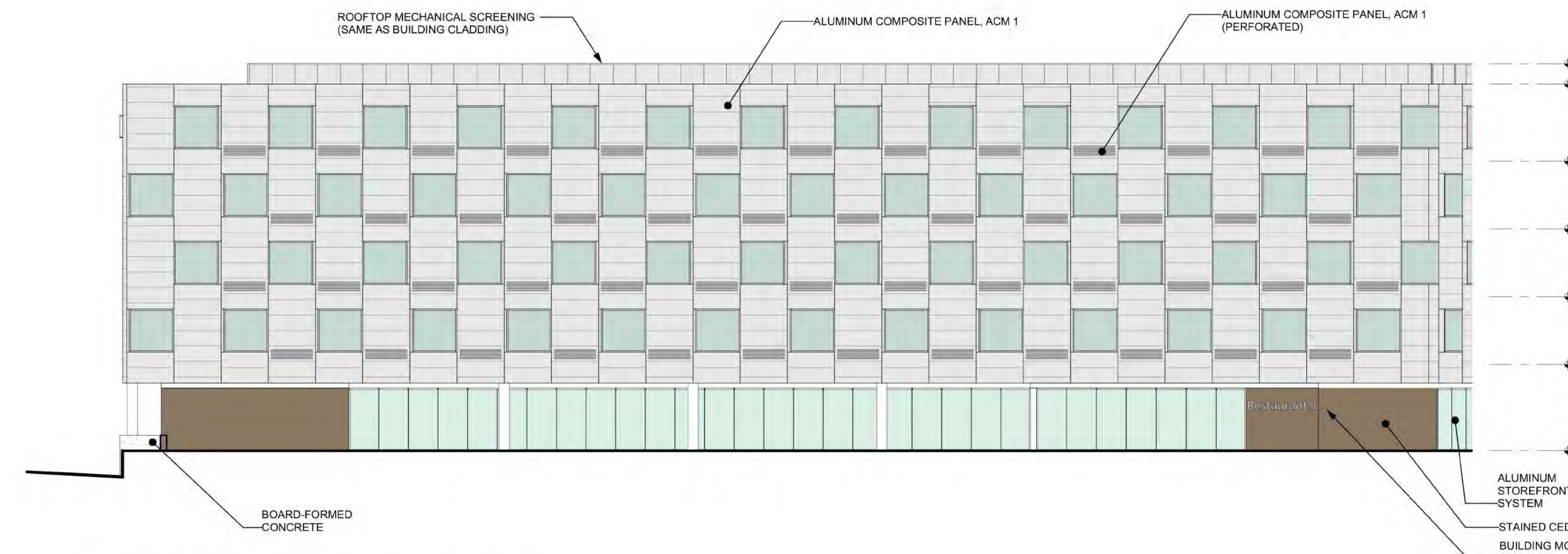
**07 ENLARGED ELEVATION**



**03 EXTERIOR ELEVATION - NORTH**  
8.5% STUCCO



**02 EXTERIOR ELEVATION - NORTHWEST**  
7.8% STUCCO



**01 EXTERIOR ELEVATION - SOUTHEAST**  
5.4% STUCCO



**02 EXTERIOR ELEVATION - SOUTH**  
3.2% STUCCO

**FDP-A201**

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

CONSULTANTS:

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KANSAS CITY, MISSOURI 64111  
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FAX: 816-531-8572

MISSION GATEWAY

**el dorado**

**OLSSON ASSOCIATES**

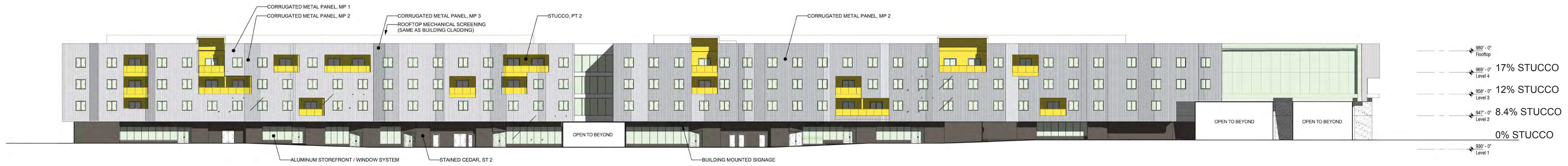
**pkmr**

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**BUILDING B - EXTERIOR ELEVATIONS**





06 EXTERIOR ELEVATION - WEST

76% GLAZING AT GROUND LEVEL



05 EXTERIOR ELEVATION - EAST



03 EXTERIOR ELEVATION - SOUTH

04 EXTERIOR ELEVATION - SOUTH END



01 EXTERIOR ELEVATION - NORTH

82% GLAZING AT GROUND LEVEL

02 EXTERIOR ELEVATION - EAST END

# FDP-A202

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

CONSULTANTS:

CIVIL / LANDSCAPE:  
OLSSON ASSOCIATES  
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MISSION GATEWAY

**el dorado**

**OLSSON**  
ASSOCIATES

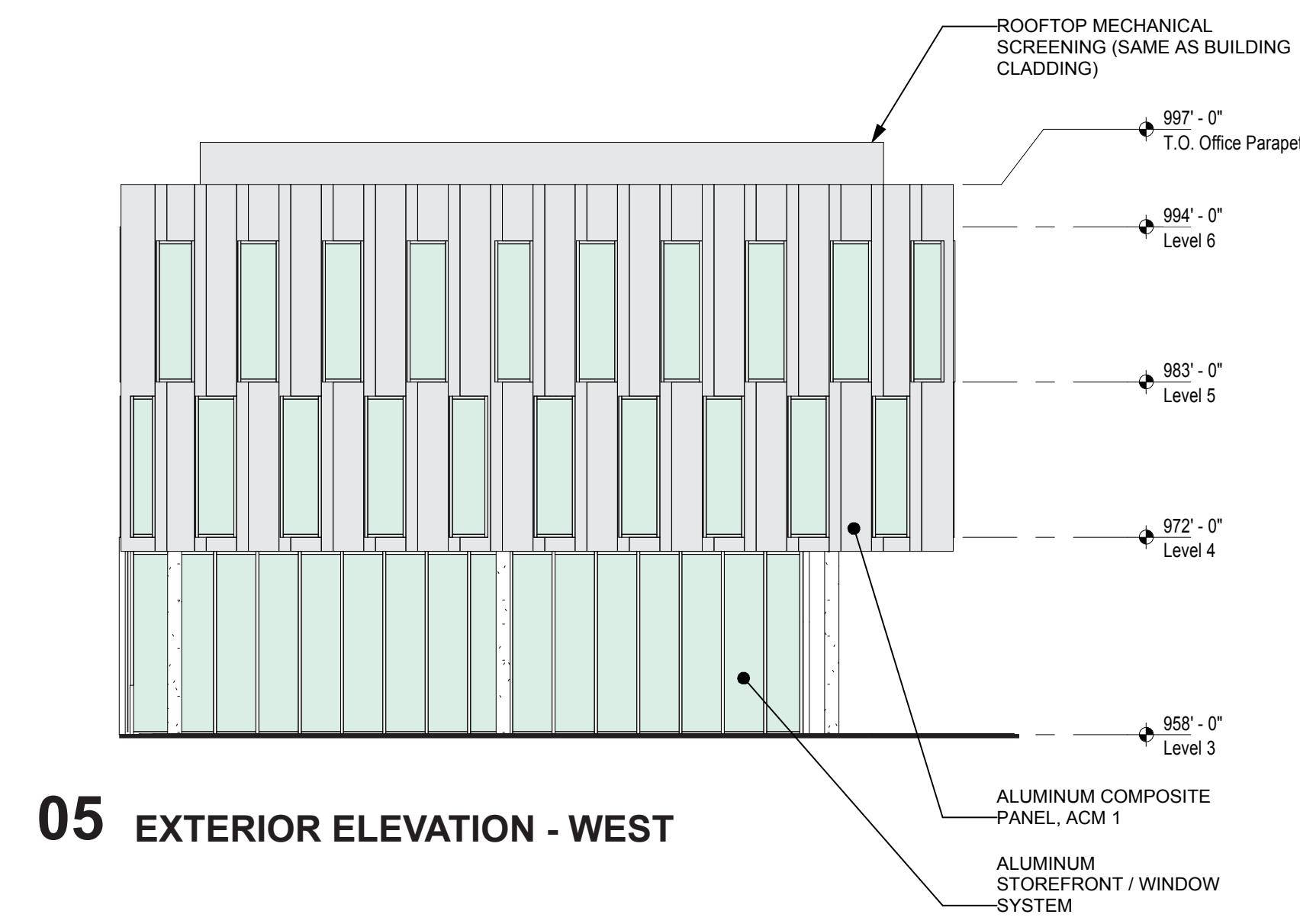
**pkmr**  
ENGINEERS

**BDC**  
BOB D. CAMPBELL AND COMPANY  
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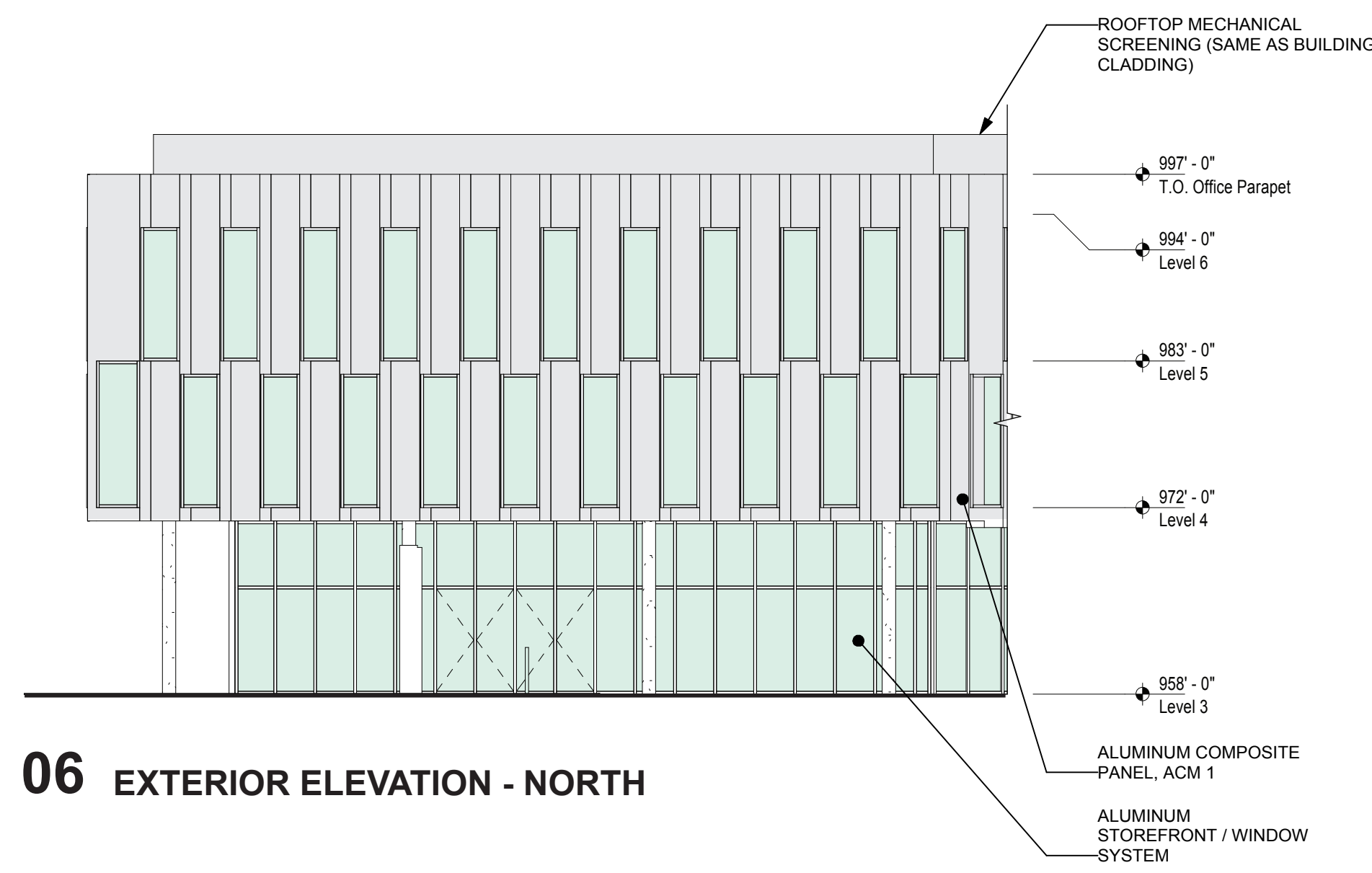
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## BUILDINGS C/D/E - EXTERIOR ELEVATIONS

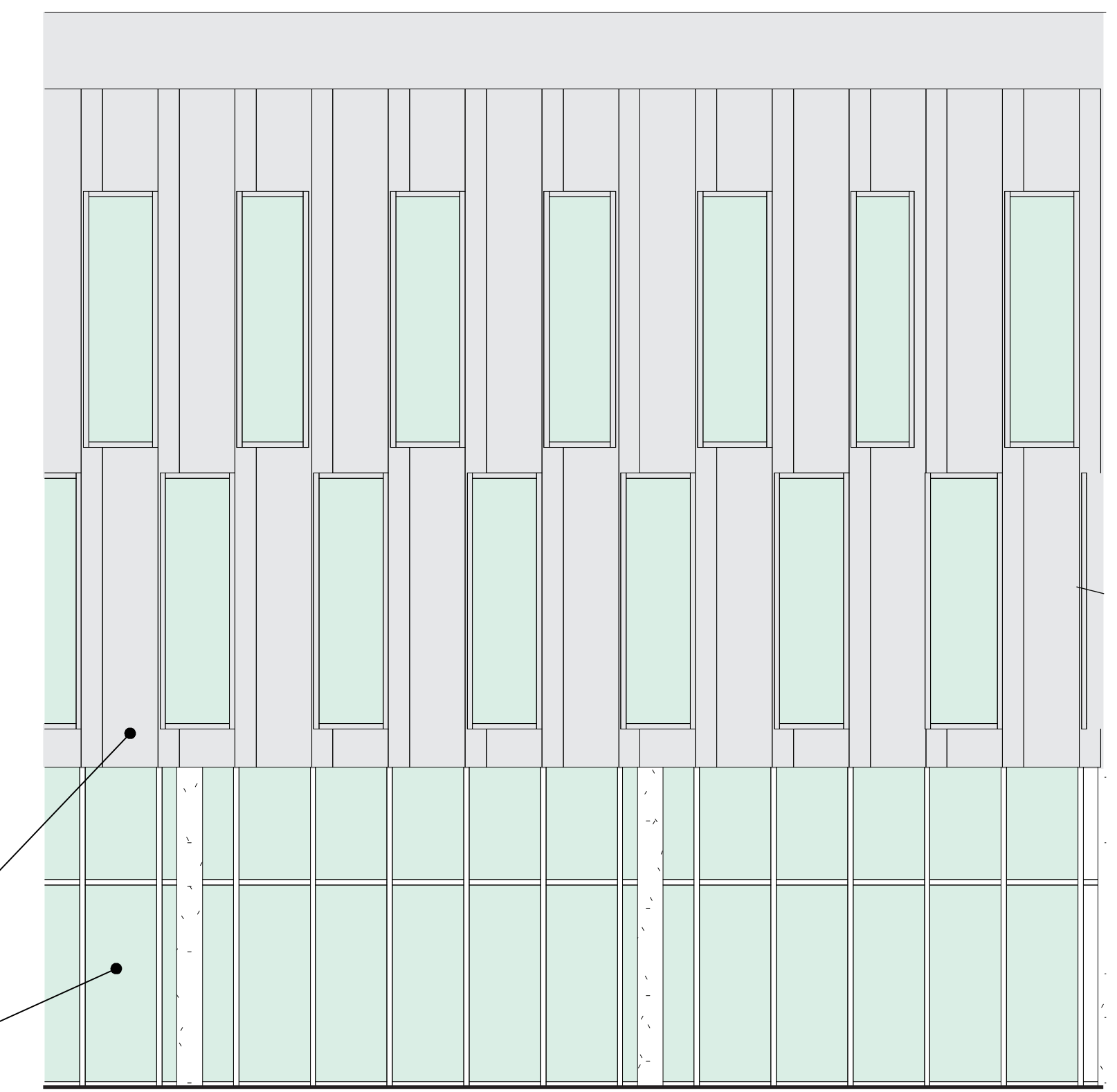




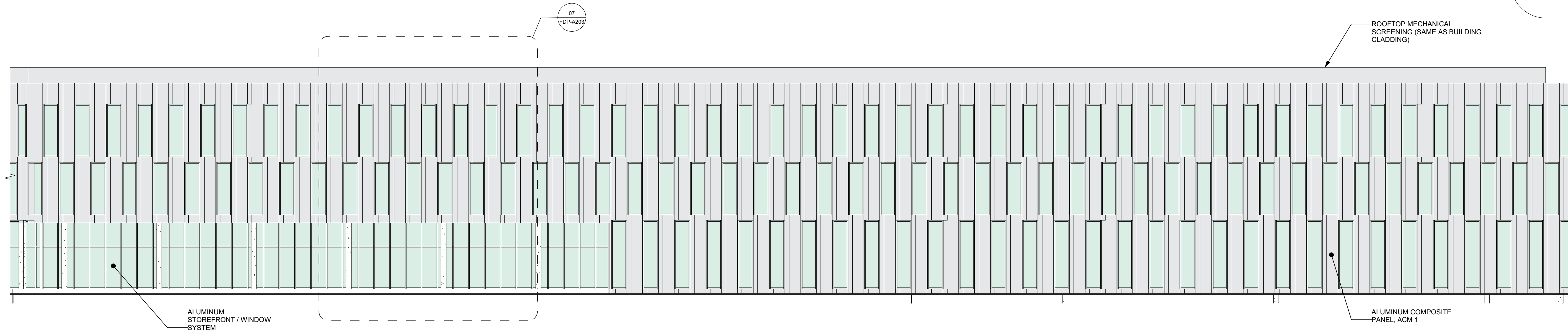
**05 EXTERIOR ELEVATION - WEST**



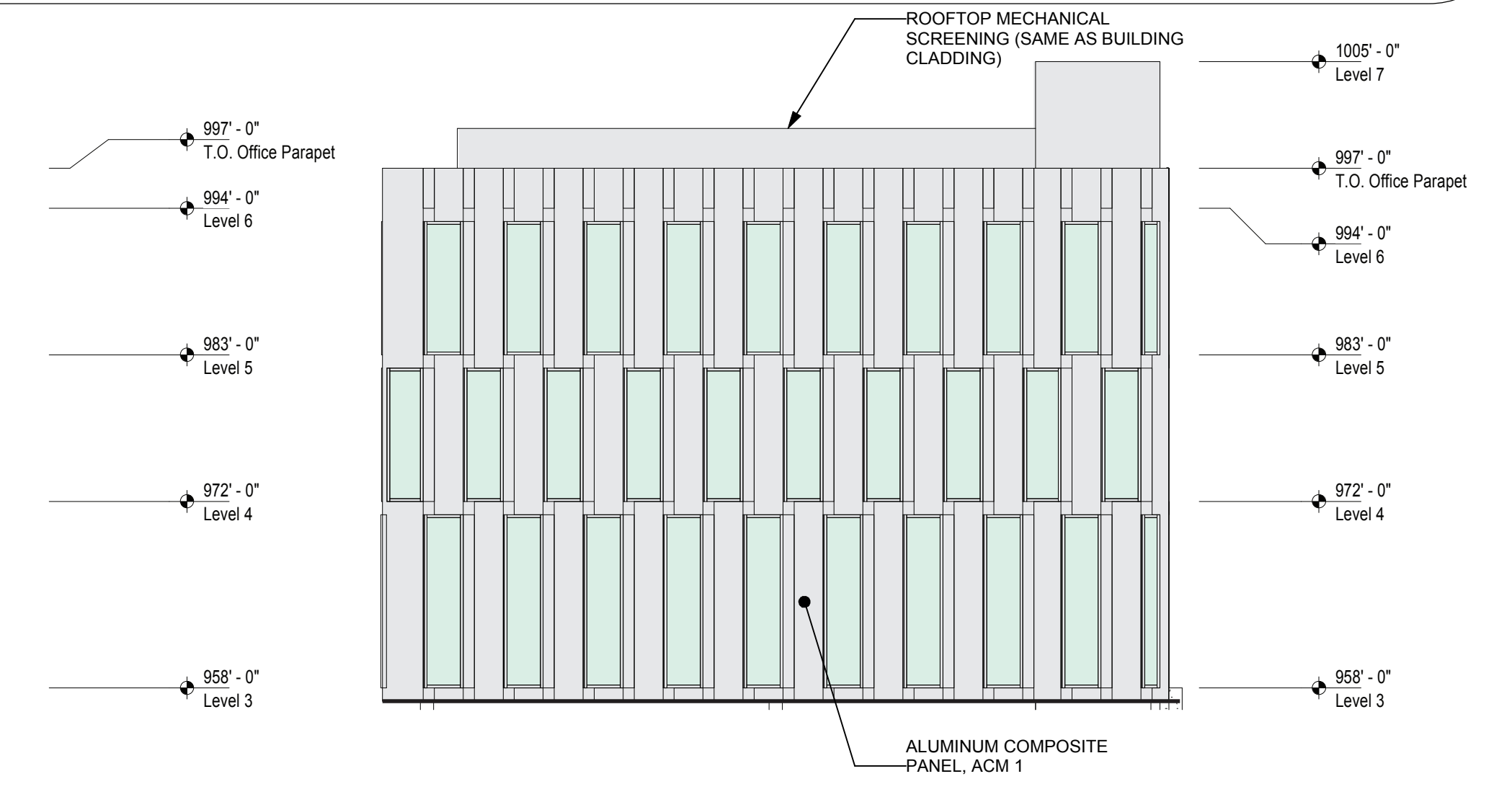
**06 EXTERIOR ELEVATION - NORTH**



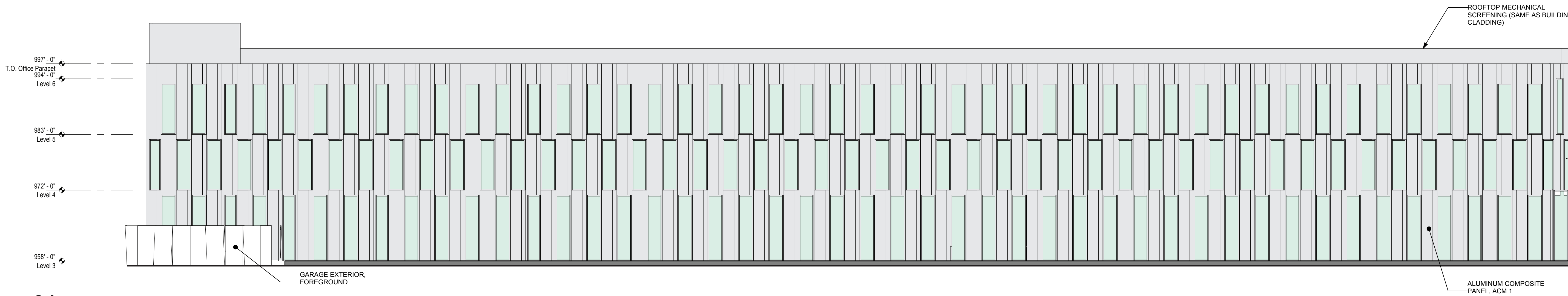
**07 ENLARGED ELEVATION, TYPICAL**



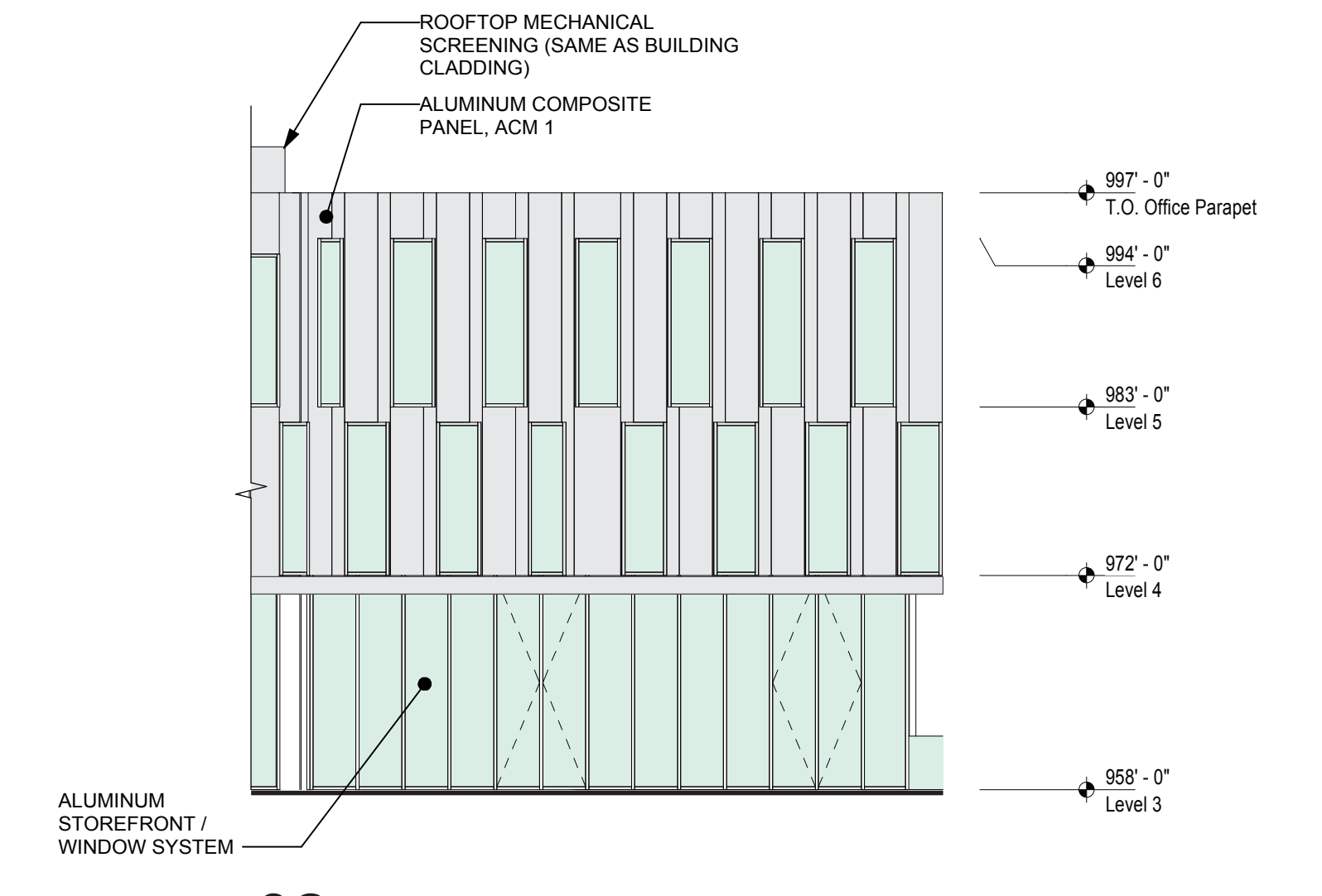
**03 EXTERIOR ELEVATION - NORTHEAST**



**04 EXTERIOR ELEVATION - SOUTHEAST**



**01 EXTERIOR ELEVATION - SOUTHWEST**



**02 EXTERIOR ELEVATION - SOUTH**

**FDP-A203**

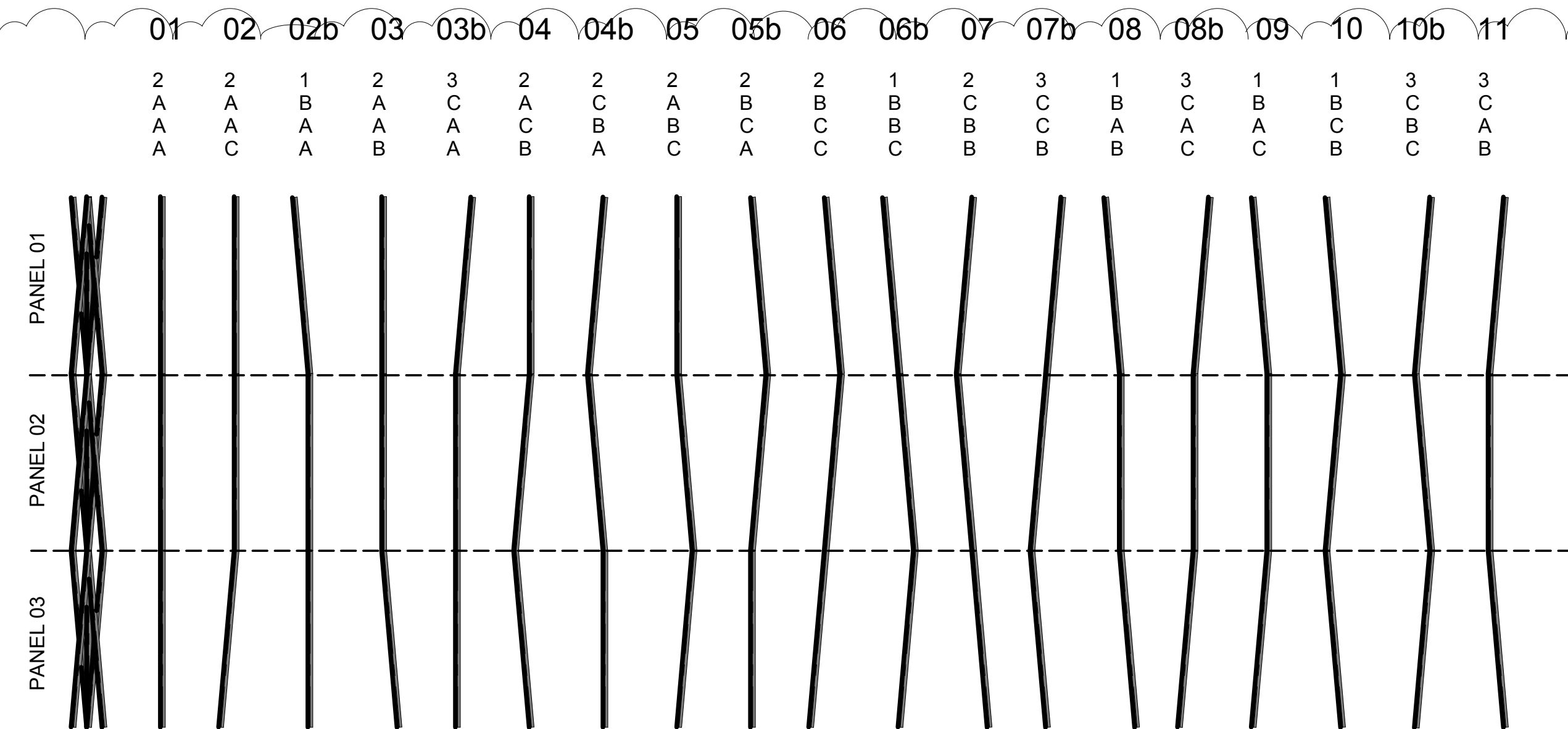
CONSULTANTS:

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OLSSON ASSOCIATES  
7301 WEST 153RD ST. SUITE 200  
OVERLAND PARK, KANSAS 66213  
TEL: 913-381-1170  
FAX: 913-381-1174

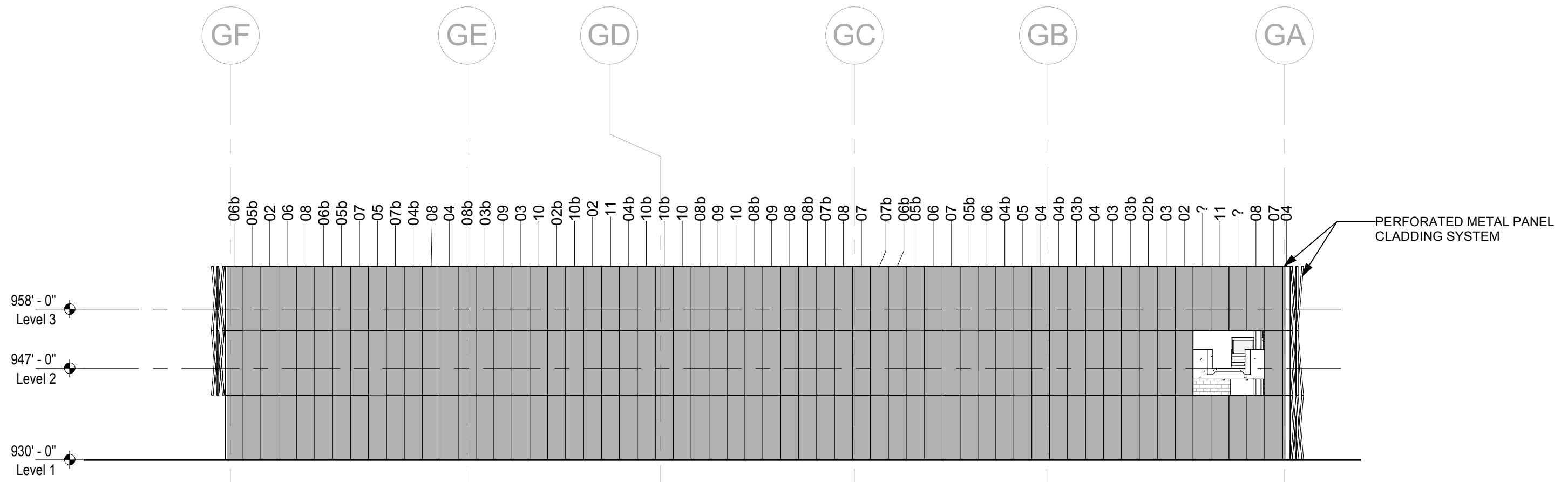
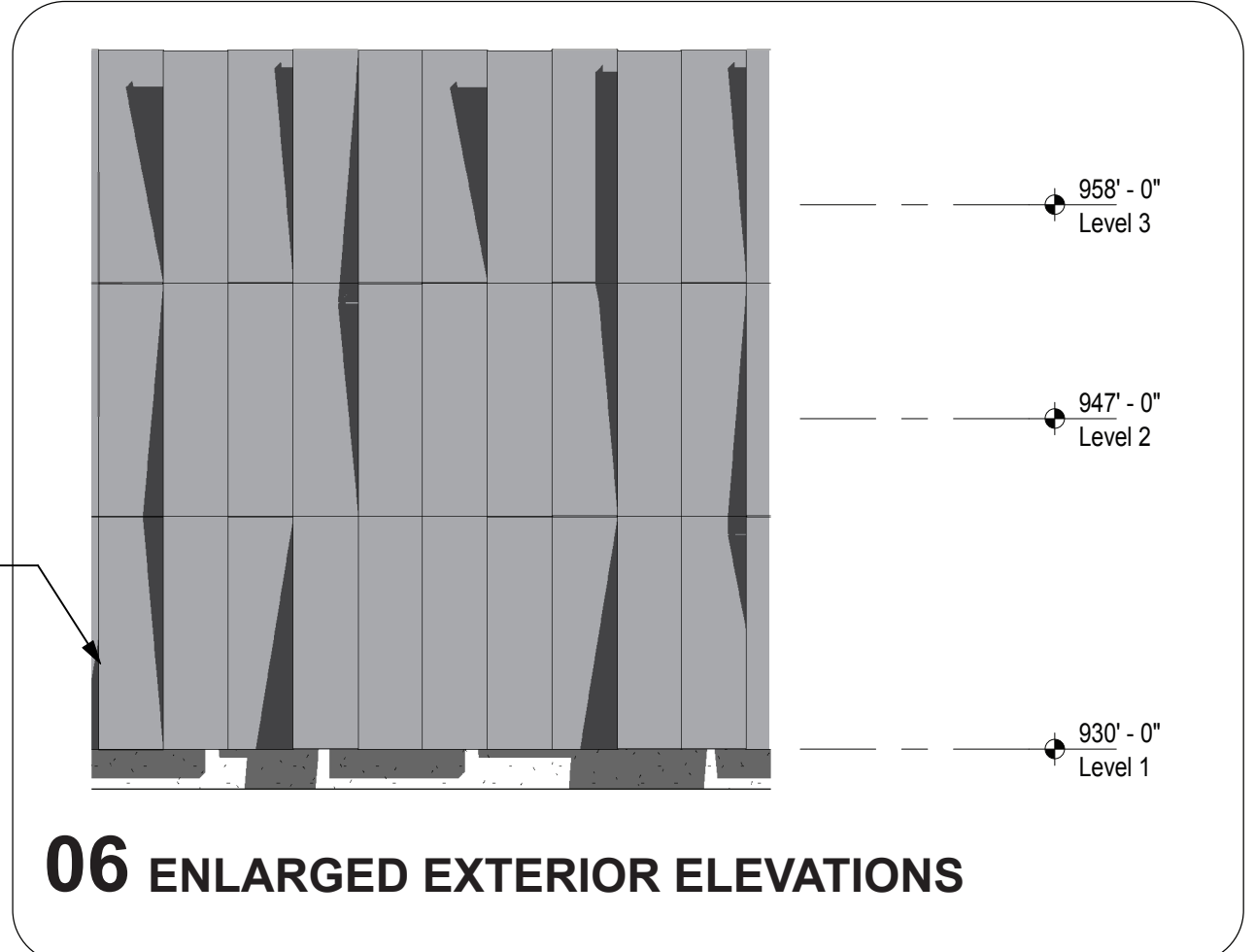
ME/P:  
PKMR ENGINEERS  
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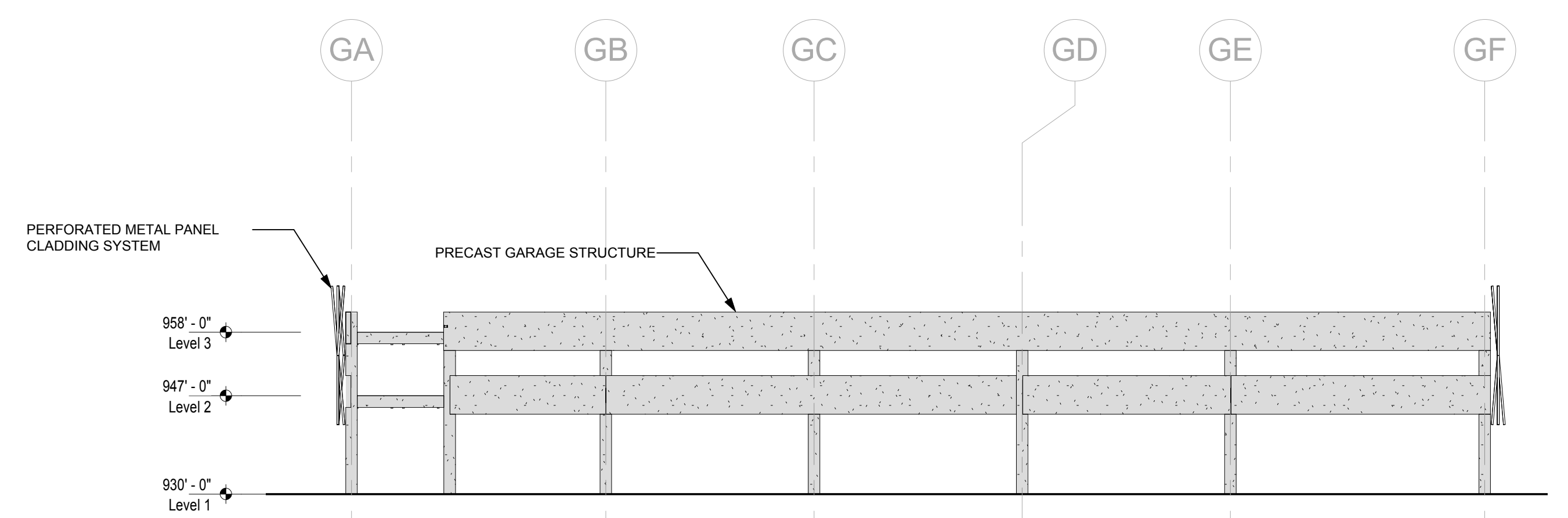




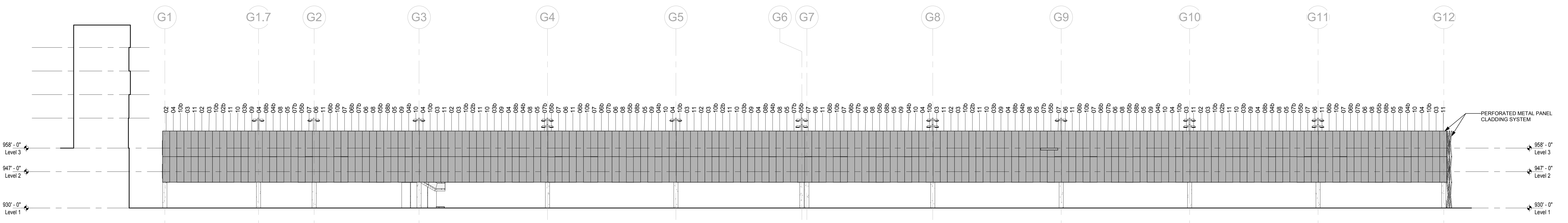
05 GARAGE PANEL INVENTORY



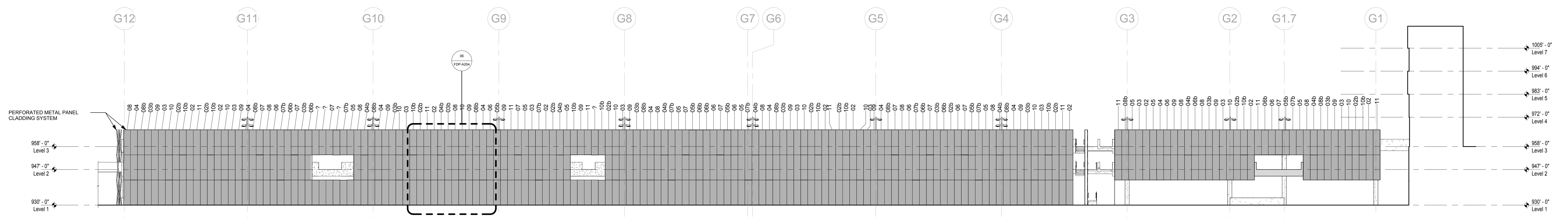
03 EXTERIOR ELEVATION - NORTH



04 EXTERIOR ELEVATION - SOUTH



02 EXTERIOR ELEVATION - WEST



01 EXTERIOR ELEVATION - EAST

**FDP-A204**

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

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MISSION GATEWAY

**el dorado**

**OLSSON**  
ASSOCIATES

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**GARAGE EXTERIOR ELEVATIONS**



PT 1	SW 7028   INCREDIBLE WHITE
PT 2	SW 6696   QUILT GOLD
MP 1	FIRESTONE   CITYSCAPE SR
MP 2	FIRESTONE   SLATE GRAY SR
MP 3	FIRESTONE   CHARCOAL GRAY SR
SF 1	STOREFRONT - CLEAR ANODIZED FRAME
CW 2	CURTAINWALL - BUTT-GLAZED CLEAR ANODIZED FRAME
FR 1	WINDOW & DOOR FRAME - DARK BRONZE
ST 2	DARK STAINED CEDAR



01 ENLARGED EXTERIOR ELEVATION - RESIDENTIAL, TYPICAL

FDP-A210

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

CONSULTANTS:

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FAX: 816-531-8572

ENLARGED EXTERIOR ELEVATIONS - BUILDING C/D/E, TYP.

MISSION GATEWAY

el dorado

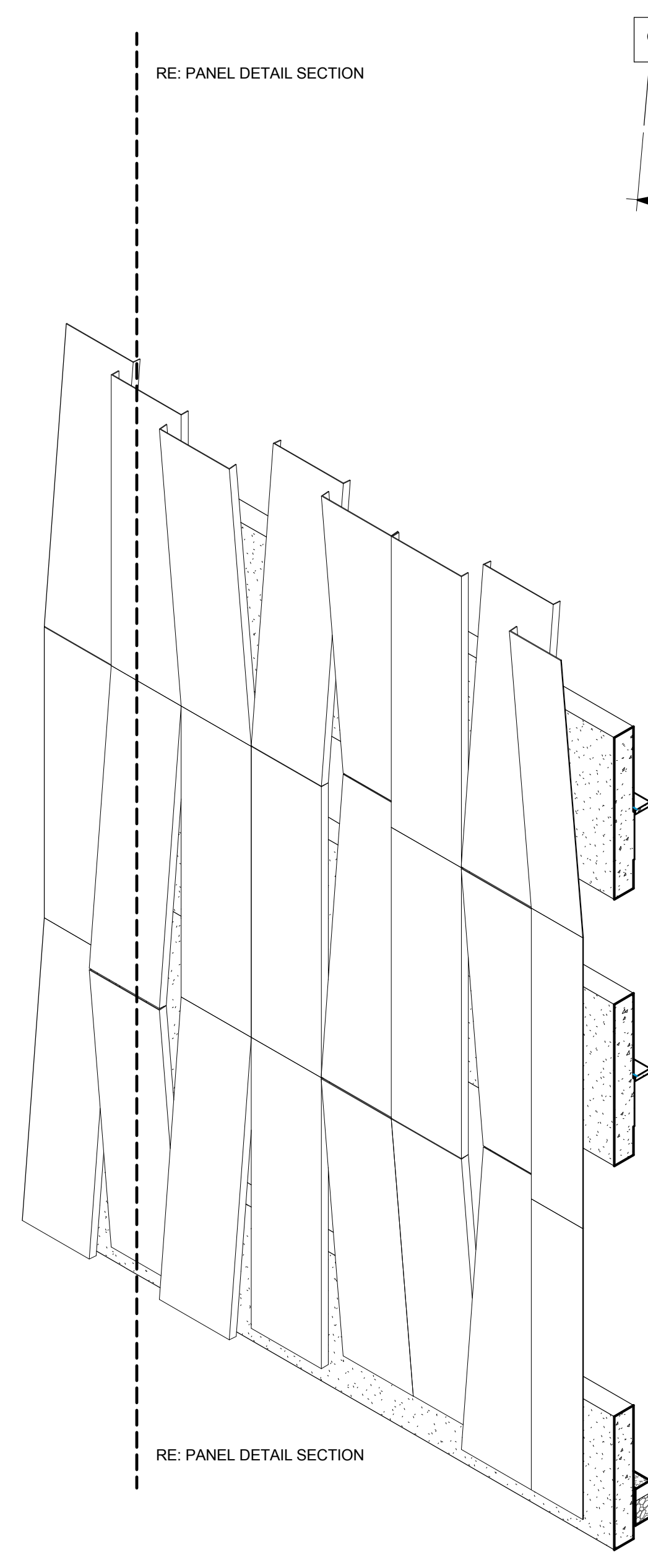
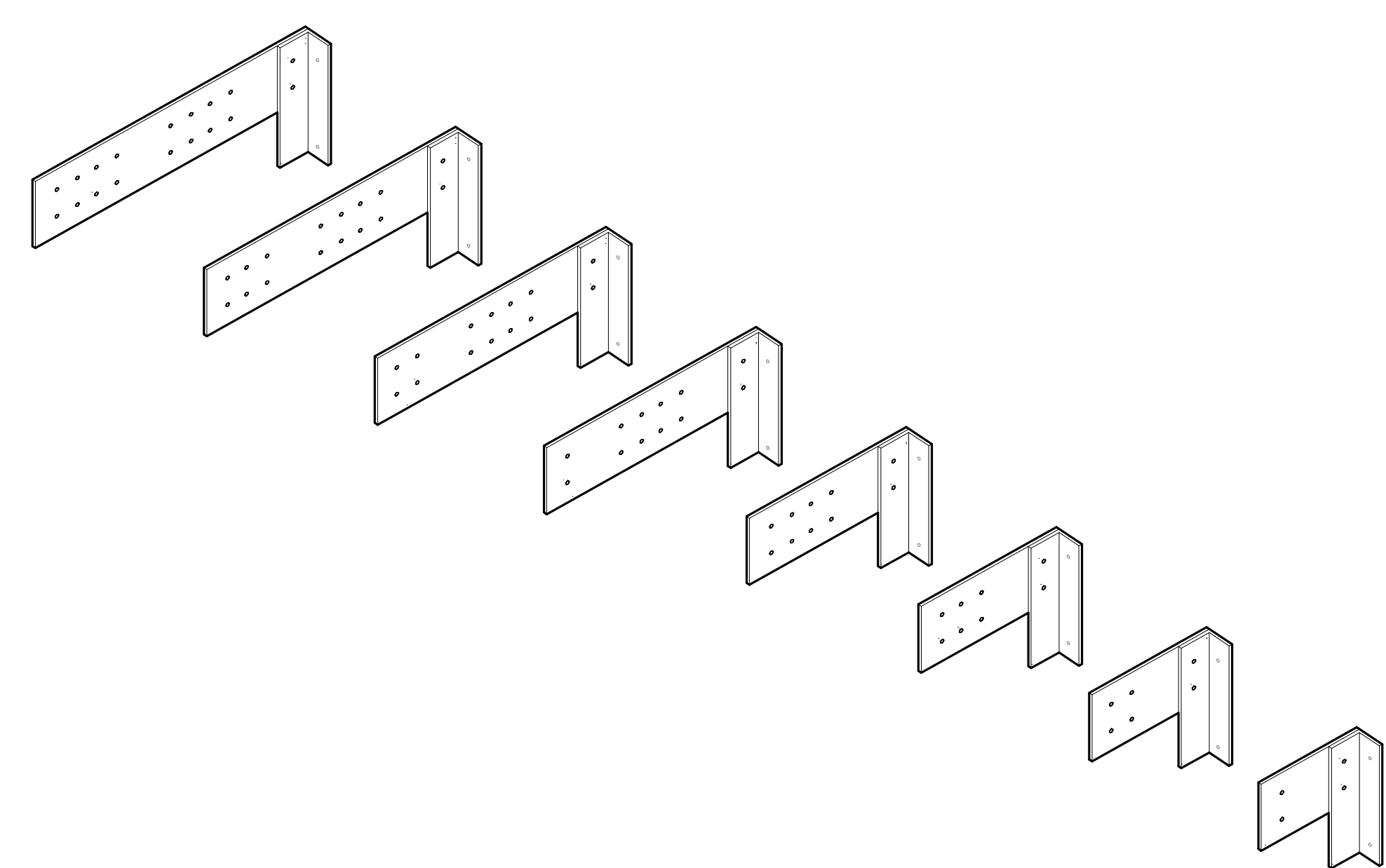
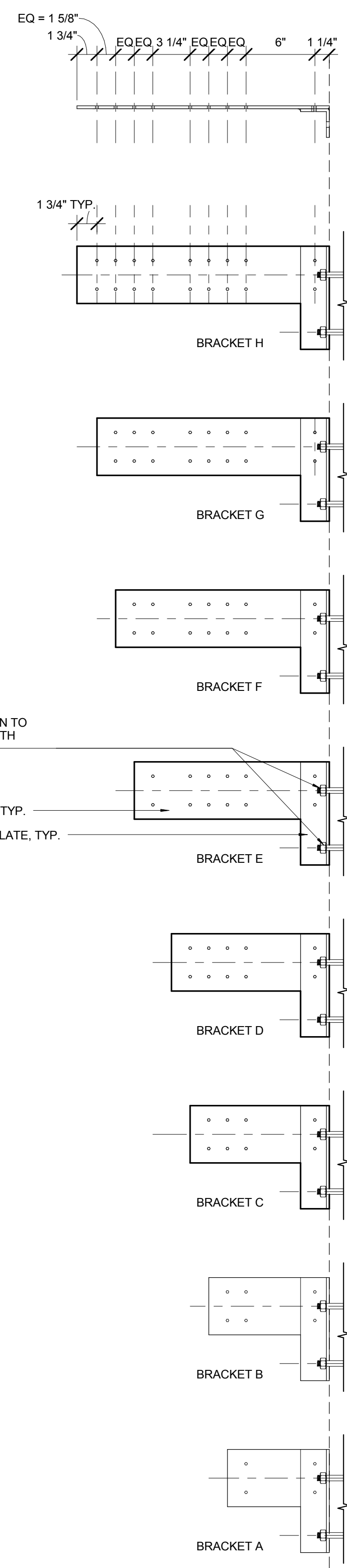
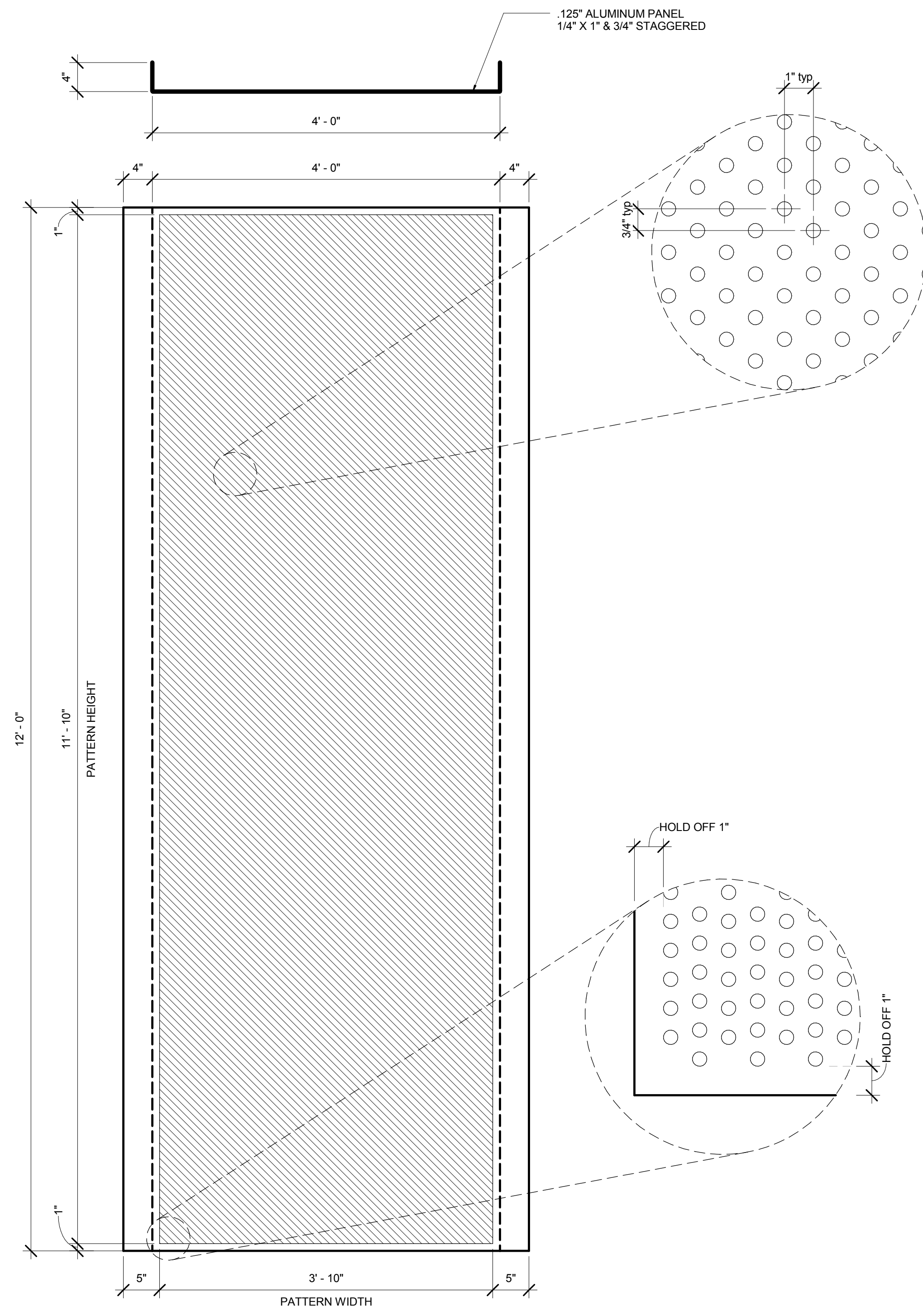
OLSSON ASSOCIATES

pkmr

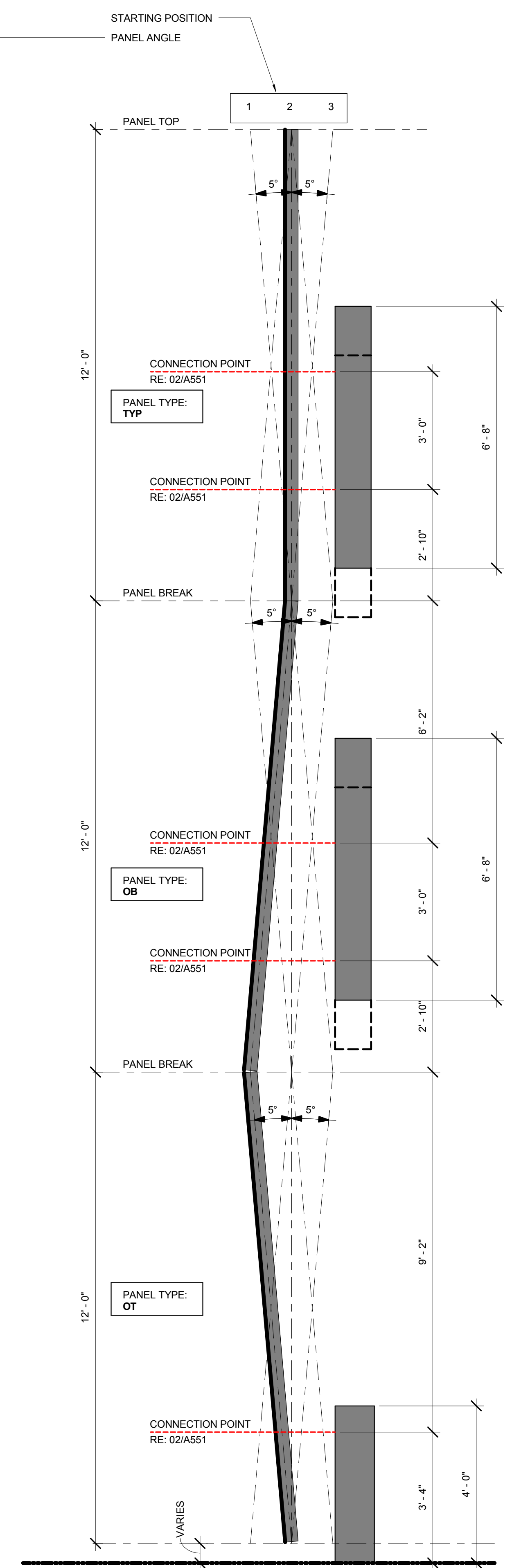
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PANEL #	<b>04</b>
STARTING POSITION	<b>2</b>
PANEL ANGLE	<b>A C B</b>



CONSULTANTS:

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**GARAGE EXTERIOR ENLARGED ELEVATIONS & DETAILS**

MISSION GATEWAY

**el dorado**

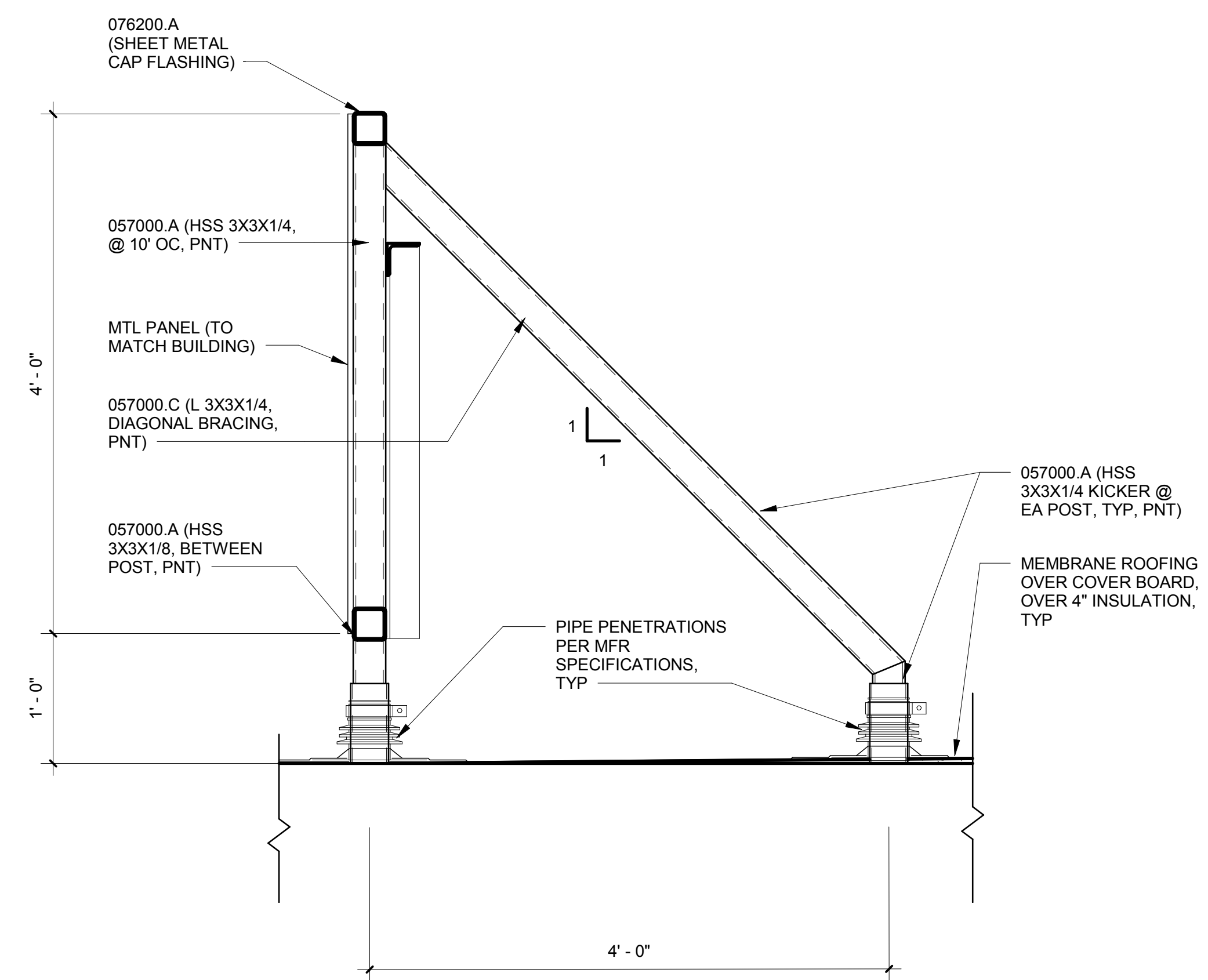
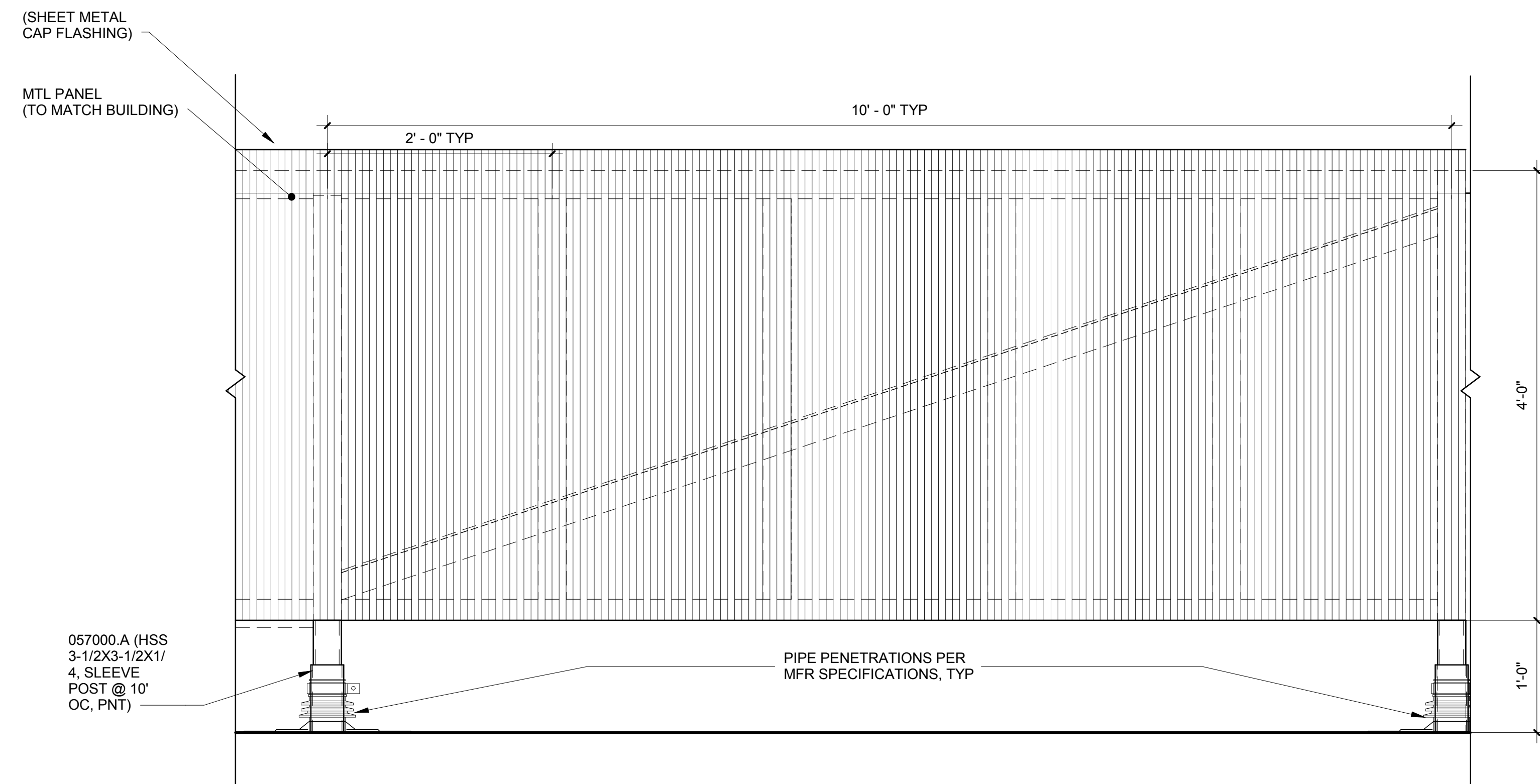
**OLSSON**  
ASSOCIATES

**pkmr**  
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# FDP-A212

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

CONSULTANTS:

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FAX: 816-531-8572

## BUILDINGS B/C/D/E/F ROOF MECHANICAL SCREEN DETAILS

MISSION GATEWAY

**el dorado**

**OLSSON**  
ASSOCIATES

**pkmr**  
ENGINEERS

**B D C**  
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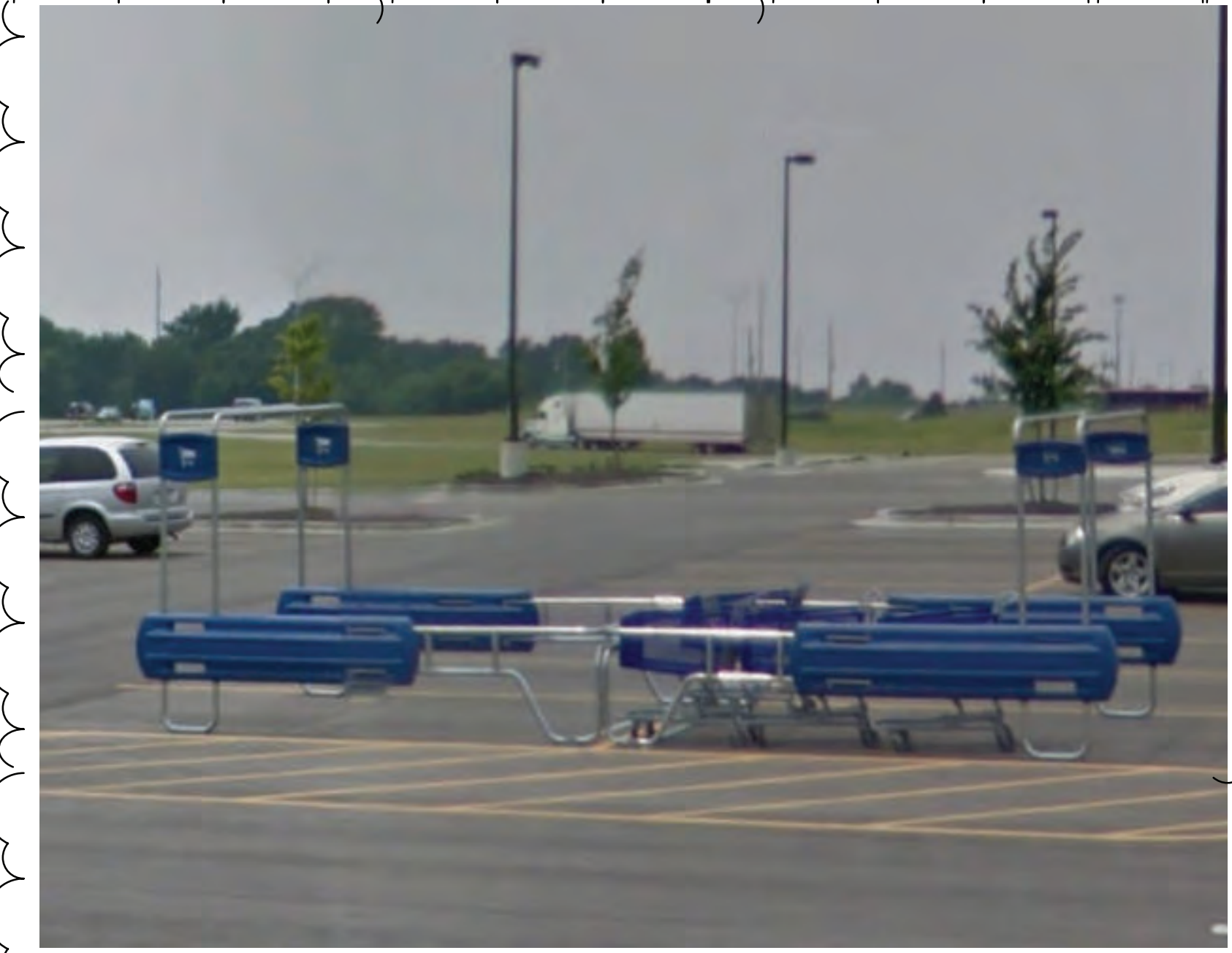




Perforated Metal Screen



Board Formed Concrete



Typical Cart-Corral throughout Ground Level Parking Lot



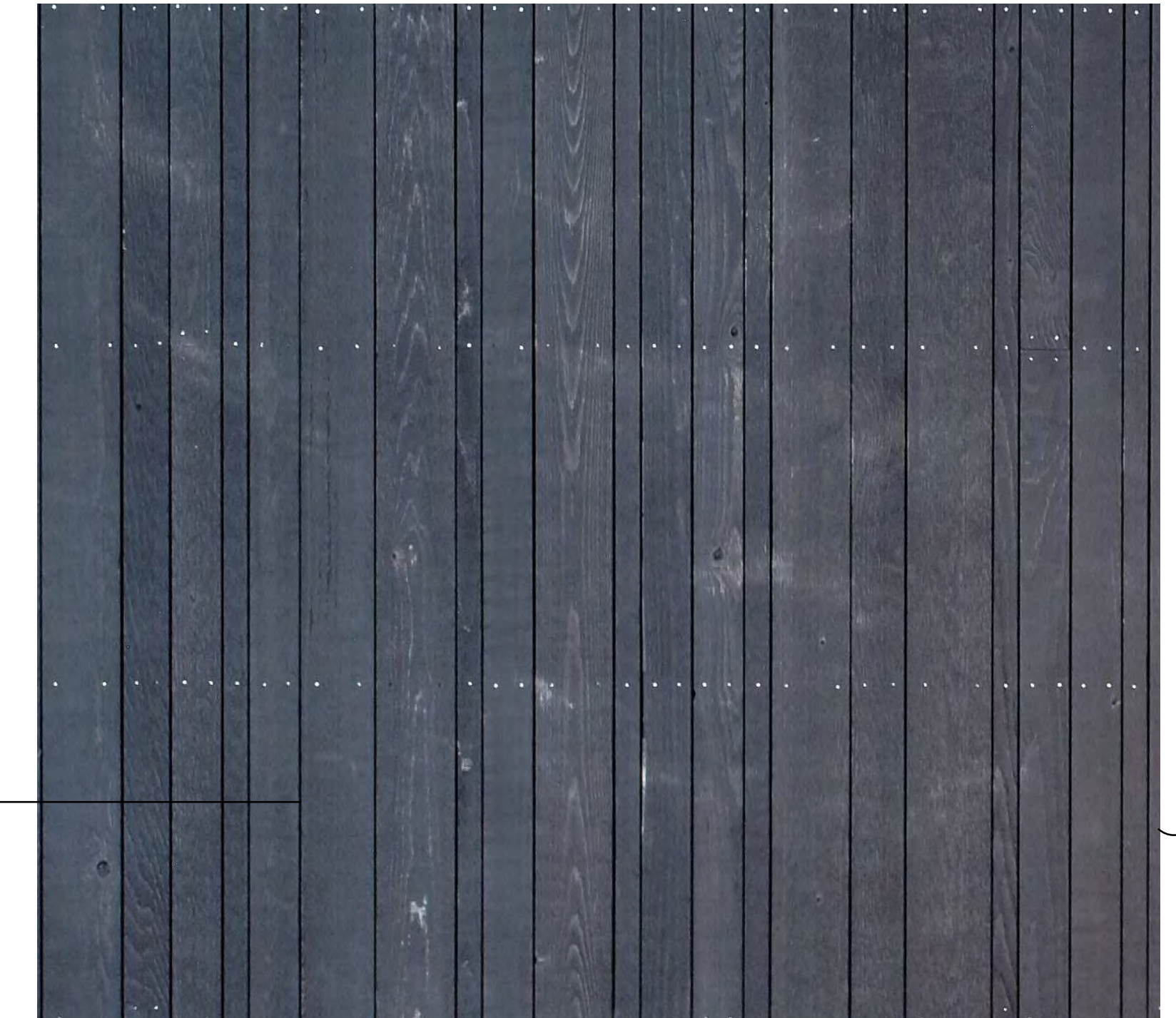
Precast Concrete Panels



Aluminum Composite Panels



Metal Panel



Stained Cedar

# FDP-A300 MATERIAL STUDIES

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

CONSULTANTS:

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MISSION GATEWAY

**el dorado**

**OLSSON**  
ASSOCIATES

**pkmr**  
ENGINEERS

**B D C**  
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Structural Engineers - Since 1957

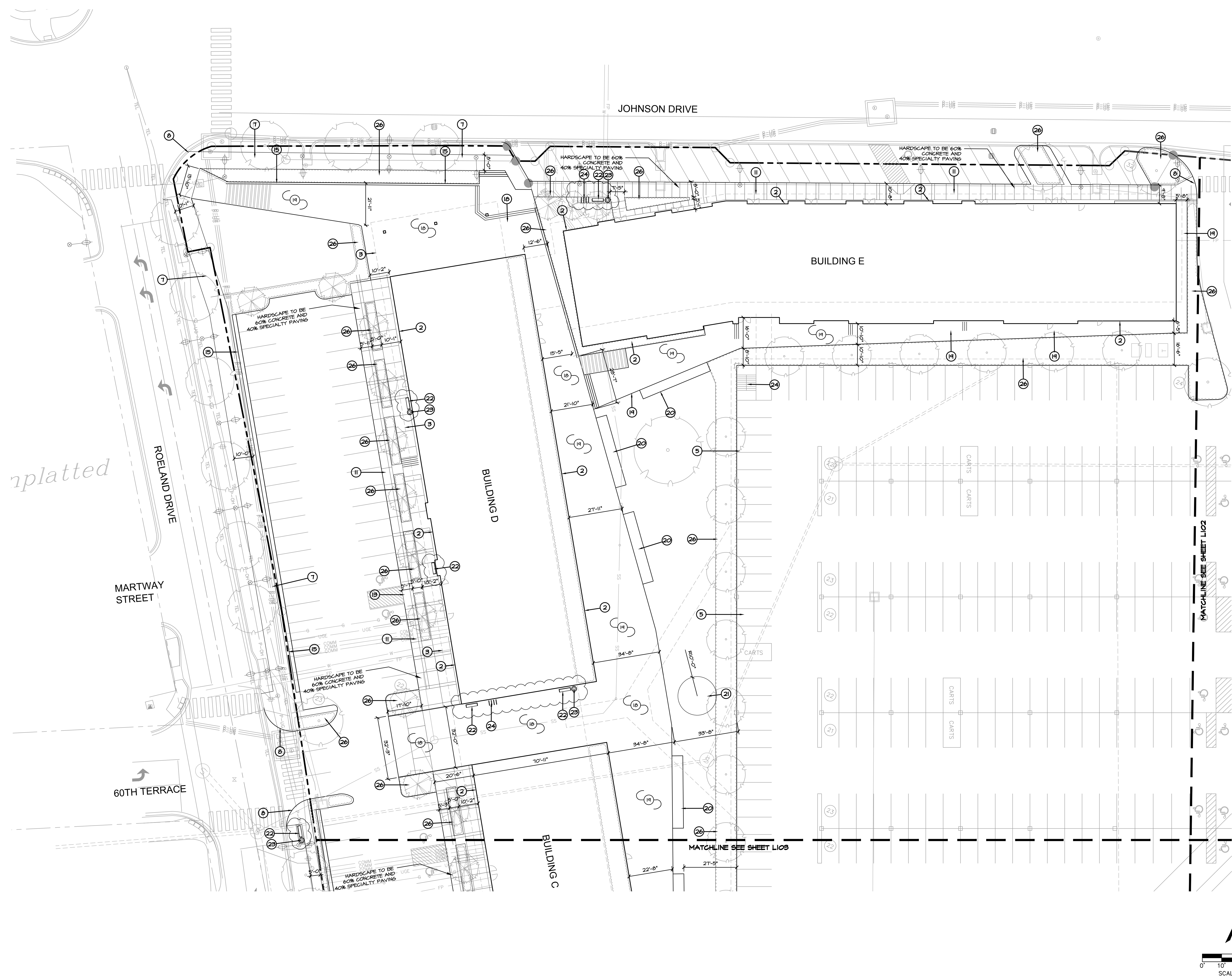
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 DATE: Mar 17, 2017 12:28pm REF: V:\OPD\_452039 C:\BULK\_FDP\_52039 C:\PBASE\_FDP\_52039 C:\PSURF\_52039 C:\LSC\_OVERALL\_52039 C:\LSC\_LEG\_52039 C:\LSC\_PLAN\_NOTES\_52039 C:\LSC\_PBASE\_52039

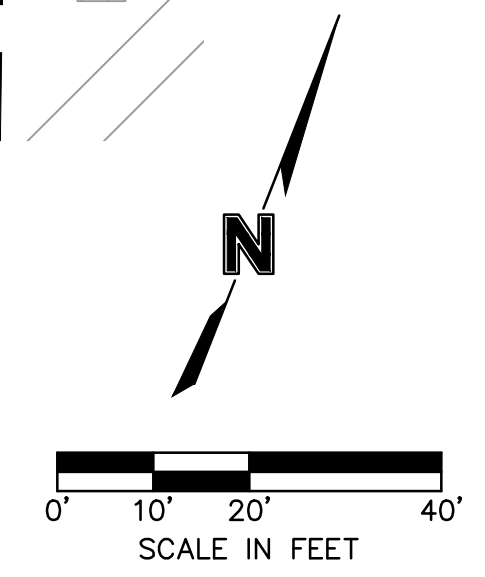


**HARDSCAPE LEGEND:**

- RIGHT OF WAY / PROPERTY LINE
- - - SHEET MATCHLINE

**HARDSCAPE PLAN NOTES:**

- 1 EXISTING WALK
- 2 BUILDING FACADE, REF. ARCHITECTURAL PLANS
- 3 BUILDING CANOPY, REF. ARCHITECTURAL PLANS
- 4 BUILDING COLUMN, REF. ARCHITECTURAL PLANS
- 5 PARKING GARAGE, REF. CIVIL PLANS
- 6 BACK OF CURB, REF. CIVIL PLANS
- 7 CONCRETE SIDEWALK, REF. CIVIL PLANS
- 8 CONCRETE CURB RAMP, REF. CIVIL PLANS
- 9 RETAINING WALL, REF. CIVIL PLANS
- 10 GRAY CONCRETE WALK WITH "V" CUT JOINT
- 11 INTEGRAL COLOR CONCRETE WALK WITH "V" CUT JOINT
- 12 INTEGRAL COLOR CONCRETE CROSSWALK WITH "V" CUT JOINT
- 13 CONCRETE CURB RAMP
- 14 CONCRETE STAIRS & PLANTER WALL
- 15 BOARD FORM PERIMETER CONCRETE WALL
- 16 CONTROL JOINT, TYP.
- 17 EXPANSION JOINT, TYP.
- 18 HARDWOOD DECK OVER CONCRETE, DARK STAIN FINISH
- 19 HARDWOOD DECK OVER FINISH GRADE, DARK STAIN FINISH
- 20 HARDWOOD BENCH, DARK STAIN FINISH
- 21 PERFORMANCE AREA, INTEGRAL COLOR CONCRETE
- 22 LANDSCAPE FORMS MULTIPLICITY BENCHES, LOCATIONS TO BE DETERMINED
- 23 LANDSCAPE FORMS MULTIPLICITY TRASH RECEPTACLE LOCATIONS TO BE DETERMINED
- 24 LANDSCAPE FORMS MULTIPLICITY BIKE RACKS, LOCATIONS TO BE DETERMINED
- 25 COURTYARD LED STRING LIGHTS
- 26 PLANTING BED
- 27 DIRECTIONAL SIGN, REF. 1/L106
- 28 MONUMENT SIGN, REF. 2/L106
- 29 MONUMENT SIGN, REF. 3/L106



01 HARDSCAPE PLAN

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

REVISIONS:  
CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:

CIVIL / LANDSCAPE: OLSSON ASSOCIATES 7301 WEST 133RD ST., SUITE 200 OVERLAND PARK, KANSAS 66213 TEL: 913-381-1170 FAX: 913-381-1174	M/E/P: PKMR ENGINEERS 13300 W. 88TH ST LENEXA, KANSAS 66215 TEL: 913-492-2400 FAX: 913-492-2437	STRUCTURAL: BOB D. CAMPBELL & CO., INC. 4338 BELLEVUE AVE. KANSAS CITY, MISSOURI 64111 66213 TEL: 816-531-4144 FAX: 816-531-8572
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MISSION GATEWAY

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FDP-L101

HARDSCAPE PLAN

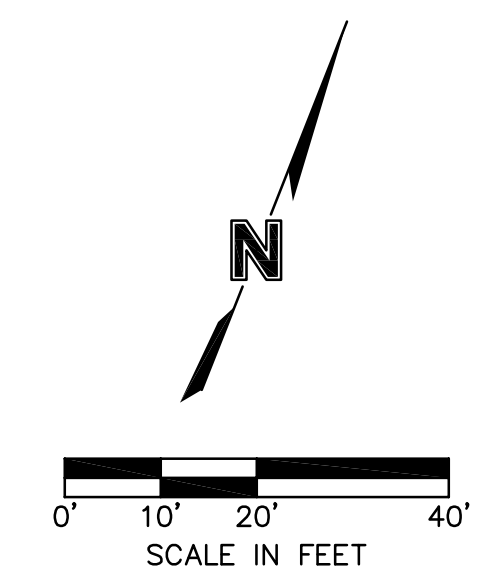
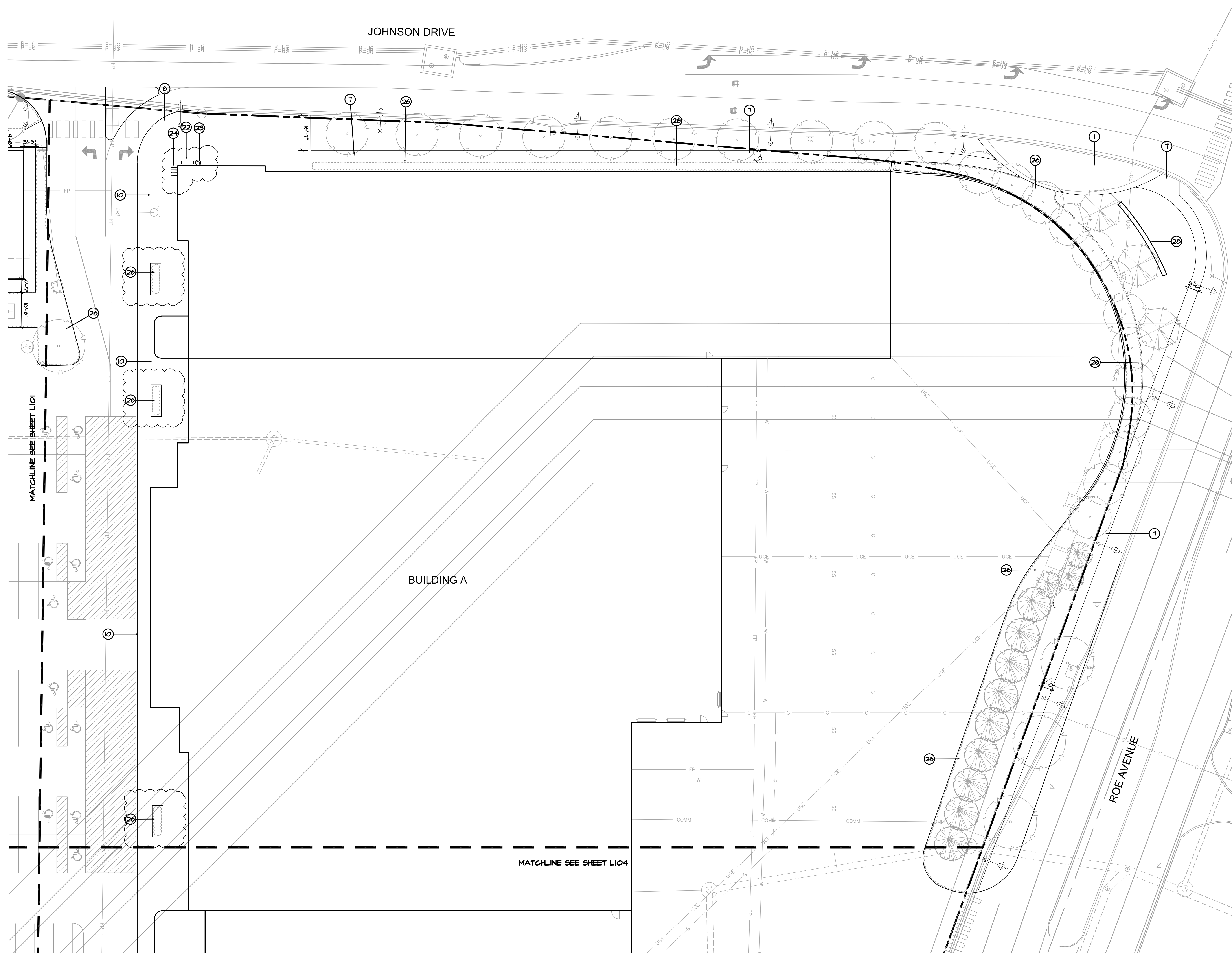


**HARDSCAPE LEGEND:**

- RIGHT OF WAY / PROPERTY LINE
- SHEET MATCHLINE

**HARDSCAPE PLAN NOTES:**

- 1 EXISTING WALK
- 2 BUILDING FACADE; REF. ARCHITECTURAL PLANS
- 3 BUILDING CANOPY; REF. ARCHITECTURAL PLANS
- 4 BUILDING COLUMN; REF. ARCHITECTURAL PLANS
- 5 PARKING GARAGE; REF. CIVIL PLANS
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- 25 COURTYARD LED STRING LIGHTS
- 26 PLANTING BED
- 27 DIRECTIONAL SIGN, REF. 2/L106
- 28 MONUMENT SIGN, REF. 2/L106
- 29 MONUMENT SIGN, REF. 3/L106



01 HARDSCAPE PLAN

FINAL DEVELOPMENT PLAN  
 MARCH 17, 2017  
 REVISIONS:  
 CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:  
 CIVIL / LANDSCAPE:  
 OLSSON ASSOCIATES  
 7301 WEST 133RD ST., SUITE  
 200  
 OVERLAND PARK, KANSAS  
 66213  
 TEL: 913-381-1170  
 FAX: 913-381-1174

M/E/P:  
 PKMR ENGINEERS  
 13300 W. 98TH ST.  
 LENEXA, KANSAS 66215  
 TEL: 913-492-2400  
 FAX: 913-492-2437

STRUCTURAL:  
 BOB D. CAMPBELL & CO., INC.  
 4338 BELLEVUE AVE.  
 KANSAS CITY, MISSOURI 64111  
 TEL: 816-531-4144  
 FAX: 816-531-8372

MISSION GATEWAY

**el dorado** **OLSSON ASSOCIATES** **pkmr ENGINEERS** **BDC**

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FDP-L102  
 HARDSCAPE PLAN

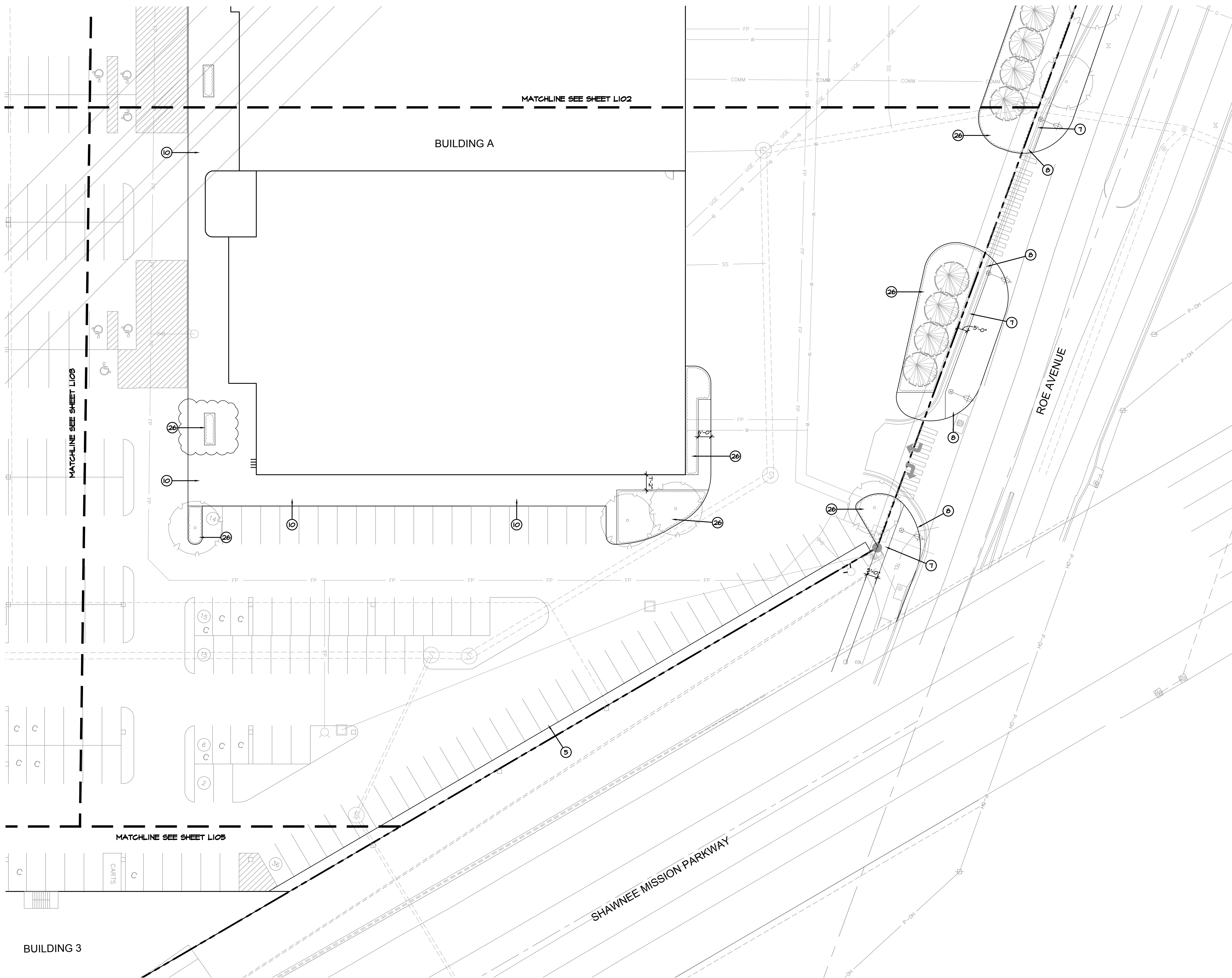
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 L\_HSC\_OVERALL\_52039  
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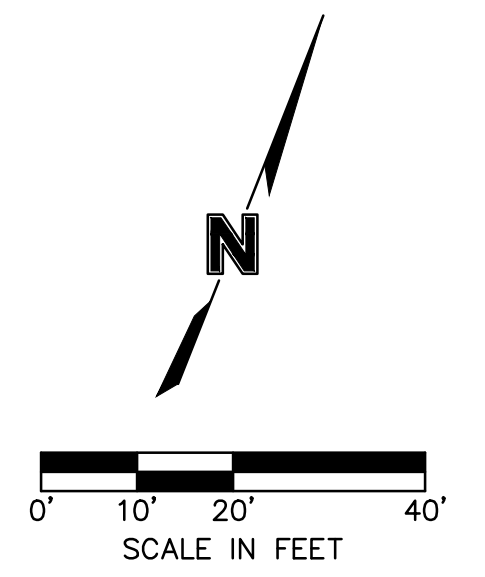


**HARDSCAPE LEGEND:**

- RIGHT OF WAY / PROPERTY LINE
- - - SHEET MATCHLINE

**HARDSCAPE PLAN NOTES:**

- 1 EXISTING WALK
- 2 BUILDING FACADE, REF: ARCHITECTURAL PLANS
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- 25 COURTYARD LED STRING LIGHTS
- 26 PLANTING BED
- 27 DIRECTIONAL SIGN, REF: L1106
- 28 MONUMENT SIGN, REF: 2/L106
- 29 MONUMENT SIGN, REF: 3/L106



01 HARDSCAPE PLAN

**FDP-L104**

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

REVISIONS:  
CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:

CIVIL / LANDSCAPE: <b>OLSSON ASSOCIATES</b> 7301 WEST 133RD ST., SUITE 200 OVERLAND PARK, KANSAS 66213 TEL: 913-381-1170 FAX: 913-381-1174	M/E/P: <b>PKMR ENGINEERS</b> 13300 W. 98TH ST LENEXA, KANSAS 66215 TEL: 913-492-2400 FAX: 913-492-2437	STRUCTURAL: <b>BOB D. CAMPBELL &amp; CO., INC.</b> 4338 BELLEVUE AVE. KANSAS CITY, MISSOURI 64111 66213 TEL: 816-531-4144 FAX: 816-531-8572
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MISSION GATEWAY

**el dorado** **OLSSON ASSOCIATES**

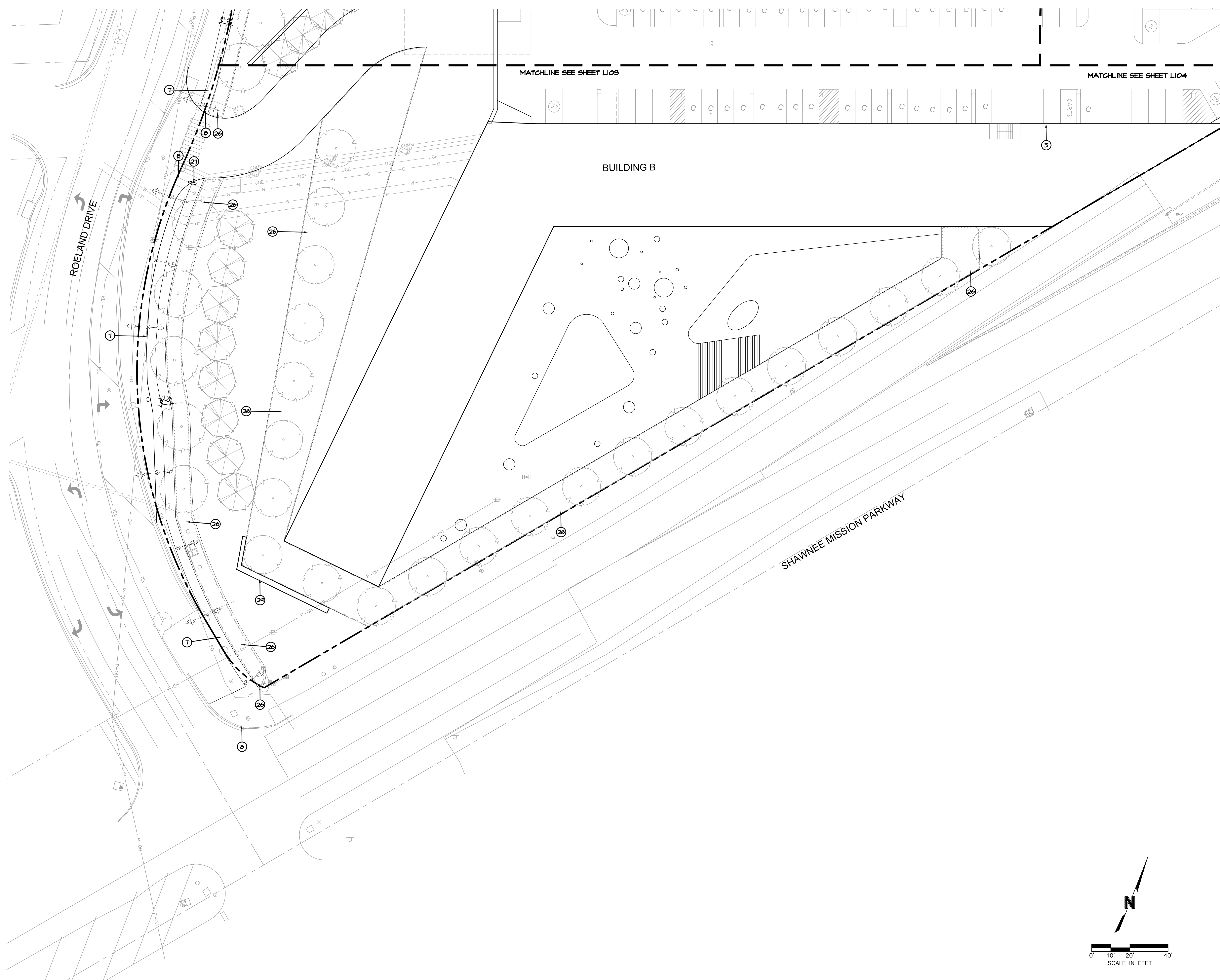
**pkmr**  
ENGINEERS

**B.D.C.**  
BOB D. CAMPBELL & CO., INC.  
Structural Engineers - Since 1952

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HARDSCAPE PLAN





**HARDSCAPE LEGEND:**

- RIGHT OF WAY / PROPERTY LINE
- - - SHEET MATCHLINE

**HARDSCAPE PLAN NOTES:**

- 1 EXISTING WALK
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- 26 PLANTING BED
- 27 DIRECTIONAL SIGN; REF. I/L106
- 28 MONUMENT SIGN; REF. 2/L106
- 29 MONUMENT SIGN; REF. 3/L106

01 HARDSCAPE PLAN

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

REVISIONS:  
CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:

<p>CIVIL / LANDSCAPE: OLSSON ASSOCIATES 7301 WEST 133RD ST., SUITE 200 OVERLAND PARK, KANSAS 66213 TEL: 913-381-1170 FAX: 913-381-1174</p>	<p>M/E/P: PKMR ENGINEERS 13300 W. 88TH ST LENEXA, KANSAS 66215 TEL: 913-492-2400 FAX: 913-492-2437</p>	<p>STRUCTURAL: BOB D. CAMPBELL &amp; CO., INC. 4338 BELLEVUE AVE. KANSAS CITY, MISSOURI 64111 TEL: 816-531-4144 FAX: 816-531-8572</p>
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MISSION GATEWAY

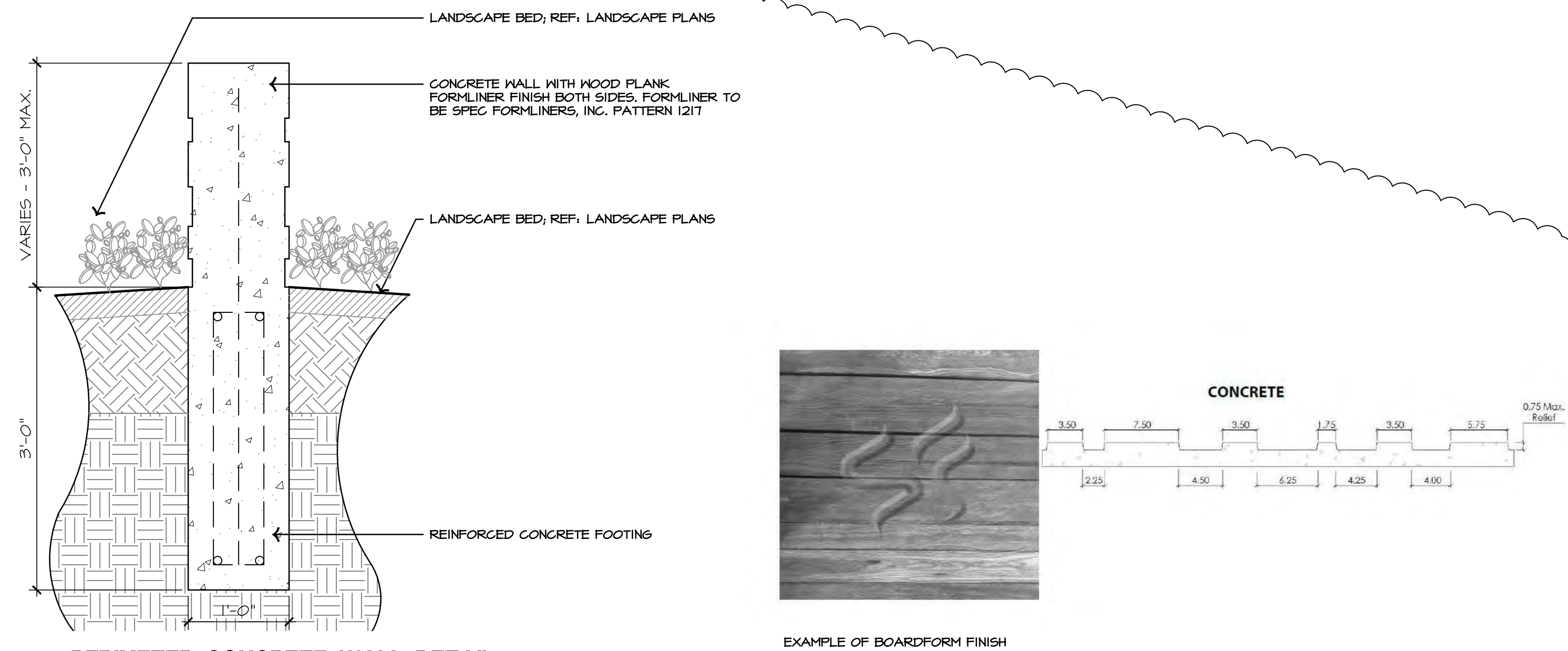
**el dorado** **OLSSON ASSOCIATES** **pkmr ENGINEERS** **B.D.C. CAMPBELL & CO., INC.**

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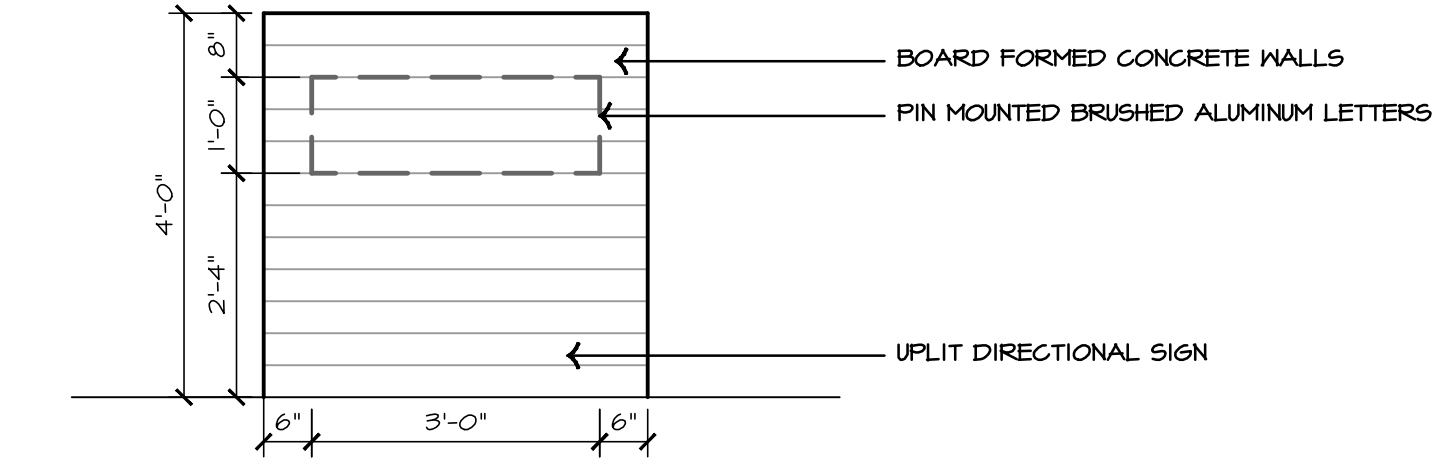
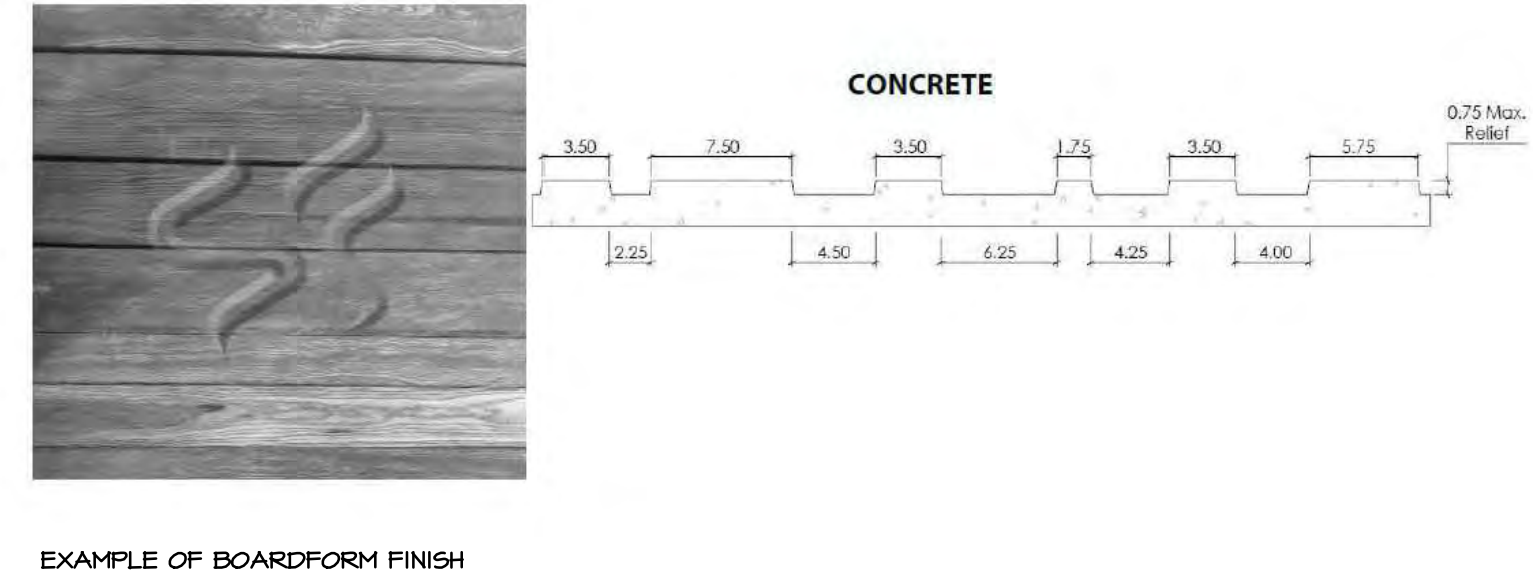
FDP-L105

HARDSCAPE PLAN

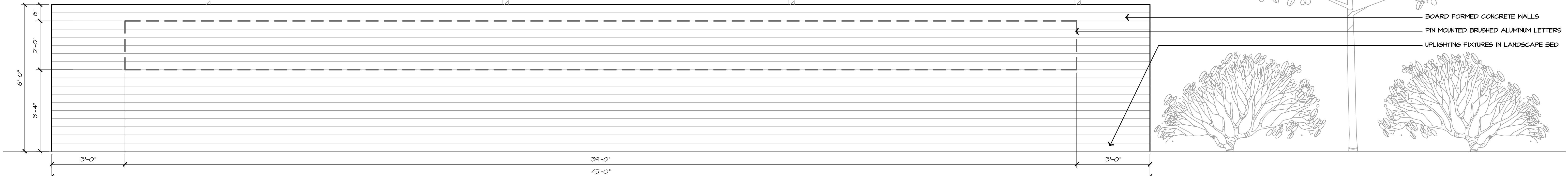
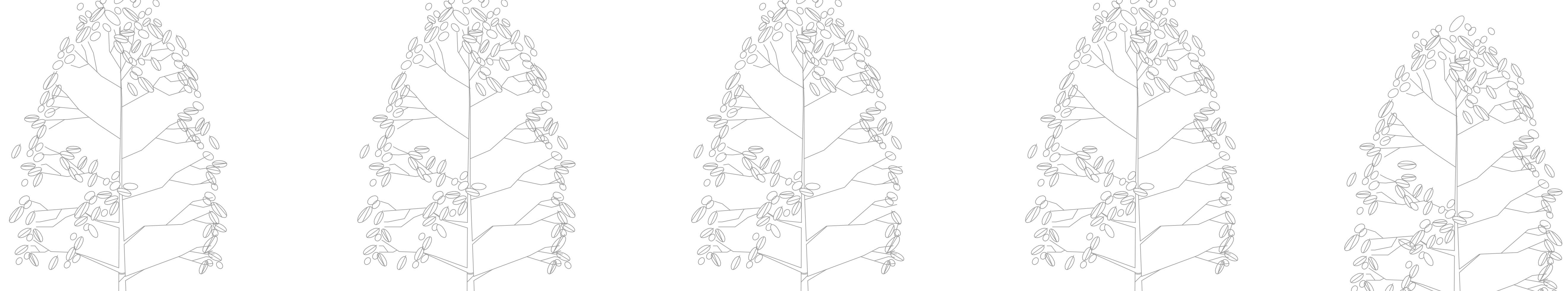




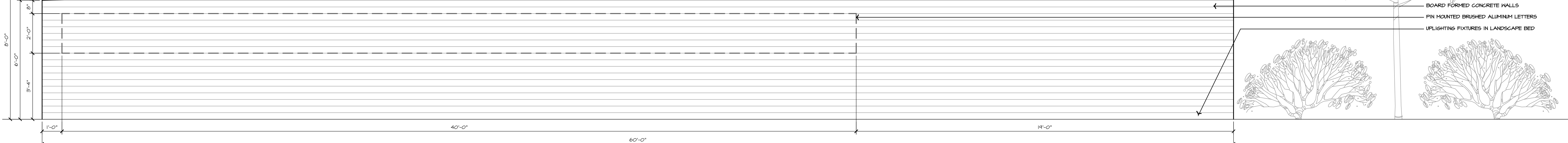
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**2 TYPICAL DIRECTIONAL SIGN**  
SCALE: N.T.S.



**2 MONUMENT SIGN AT JOHNSON DR & ROE AVE**  
SCALE: N.T.S.



**3 MONUMENT SIGN AT SHAWNEE MISSION PKWY & ROELAND AVE**  
SCALE: N.T.S.

01	HARDSCAPE DETAILS
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FINAL DEVELOPMENT PLAN  
 MARCH 17, 2017  
 REVISIONS:  
 CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:  
 CIVIL / LANDSCAPE:  
 OLSSON ASSOCIATES  
 7301 WEST 133RD ST., SUITE  
 200  
 OVERLAND PARK, KANSAS  
 66213  
 TEL: 913-381-1170  
 FAX: 913-381-1174  
 ME/P:  
 PKMR ENGINEERS  
 13300 W. 98TH ST  
 LENEXA, KANSAS 66215  
 TEL: 913-492-2420  
 FAX: 913-492-2437  
 STRUCTURAL:  
 BOB D. CAMPBELL & CO., INC.  
 4338 BELLEVUE AVE  
 KANSAS CITY, MISSOURI 64111  
 TEL: 816-531-4145  
 FAX: 816-531-8572

MISSION GATEWAY

**el dorado** **OLSSON ASSOCIATES** **pkmr ENGINEERS** **BDC**

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Structural Engineers - Since 1967

**FDP-L106**  
 HARDSCAPE DETAILS








PLAN KEY

SITE BENCH 

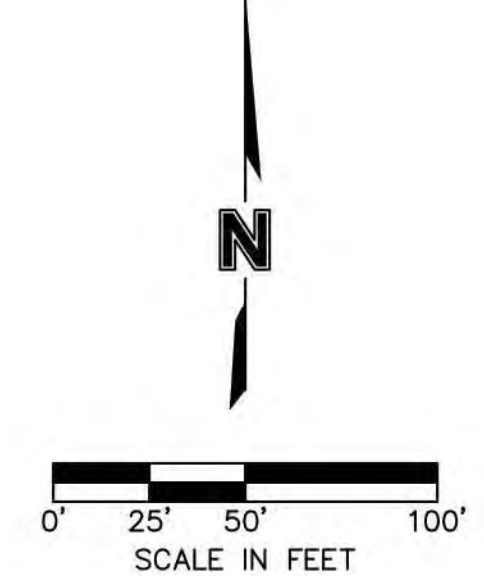
BIKE RACK 

LITTER RECEPTACLE 

BOARDWALK BENCH 

PUBLIC OPEN SPACE - 49,100 SQFT 

SEMI-PRIVATE OPEN SPACE - 54,476 SQFT 



01 OPEN SPACE & AMENITIES PLAN

FDP-L108

OPEN SPACE & AMENITIES PLAN

FINAL DEVELOPMENT PLAN  
 MARCH 17, 2017  
 REVISIONS:  
 CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:

CIVIL / LANDSCAPE: OLSSON ASSOCIATES 7301 WEST 133RD ST., SUITE 200 OVERLAND PARK, KANSAS 66213 TEL: 913-381-1170 FAX: 913-381-1174	MEP: PKMR ENGINEERS 13300 W. 98TH ST LENEXA, KANSAS 66215 TEL: 913-462-2400 FAX: 913-462-2437	STRUCTURAL: BOB D. CAMPBELL & CO., INC. 4338 BELLEVUE AVE KANSAS CITY, MISSOURI 64111 TEL: 816-831-4144 FAX: 816-831-8272
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MISSION GATEWAY

**el dorado** **OLSSON** **pkmr** **BDC**

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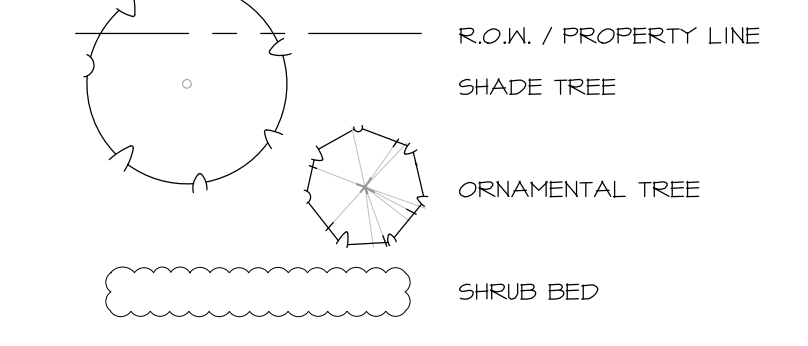
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### LANDSCAPE GENERAL NOTES:

- THE CONSTRUCTION COVERED BY THESE PLANS SHALL CONFORM TO ALL APPLICABLE STANDARDS AND SPECIFICATIONS OF THE CITY OF MISSION, KANSAS IN CURRENT USAGE. ALL STANDARDS NOT COVERED BY THE CITY SHALL BE APCA STANDARDS IN CURRENT USAGE UNLESS OTHERWISE NOTED.
- CONTRACTOR SHALL VERIFY EXACT LOCATION OF ALL EXISTING UTILITIES, DRAIN LINES AND IRRIGATION PIPING PRIOR TO COMMENCING WORK AND AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES, DRAIN LINES AND IRRIGATION PIPING.
- CONTRACTOR SHALL VERIFY AND COORDINATE ALL FINAL GRADES WITH LANDSCAPE ARCHITECT PRIOR TO COMPLETION.
- DEBRIS SHALL NOT BE ALLOWED TO ACCUMULATE AND SHALL BE REMOVED AT FREQUENT INTERVALS. AT COMPLETION OF WORK IN EACH AREA, THE CONTRACTOR SHALL GATHER AND REMOVE ALL DEBRIS, EQUIPMENT, AND EXCESS MATERIAL FROM THAT AREA. AT FINAL COMPLETION OF ALL WORK HE SHALL REMOVE ALL SUCH ITEMS FROM THE PREMISES.
- LOCATION AND PLACEMENT OF ALL PLANT MATERIAL SHALL BE COORDINATED WITH LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- THE LANDSCAPE PLANTING PLAN GRAPHICALLY ILLUSTRATES OVERALL PLANT MASSINGS. EACH PLANT SPECIES SHALL BE PLACED IN THE FIELD TO UTILIZE THE GREATEST COVERAGE OF THE GROUND PLANE. THE FOLLOWING APPLIES FOR INDIVIDUAL PLANTINGS:
  - ALL EVERGREEN SHRUBS AND CREEPING GROUNDCOVERS SHALL BE MINIMUM OF 2' FROM ANY PAVING EDGE.
  - ALL PLANTS OF THE SAME SPECIES SHALL BE EQUALLY SPACED AND SITED FOR THE BEST AESTHETIC VIEWING.
  - ALL TREES, EVERGREEN OR DECIDUOUS, SHALL BE A MINIMUM OF 4' FROM ANY PAVING EDGE.
- ANY SUBSTITUTION OF SPECIFIED PLANT MATERIAL WILL NOT BE ALLOWED WITHOUT WRITTEN AUTHORIZATION FROM LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- MULCH ALL PLANTING AREAS TO A DEPTH OF 3" DEPTH ACCORDING TO PLANS AND SPECIFICATIONS. SAMPLES SHALL BE APPROVED BY LANDSCAPE ARCHITECT.
- ALL PLANT MATERIAL WILL BE HEALTHY, VIGOROUS AND FREE OF DISEASE AND INSECTS PER AAN STANDARDS. LANDSCAPE ARCHITECT RESERVES THE RIGHT TO REJECT ANY INFERIOR OR OTHERWISE UNSUITABLE PLANT MATERIAL PROPOSED FOR USE ON THE PROJECT.
- ALL PLANTING BEDS NOT FULLY CONTAINED BY CONCRETE CURBS OR WALKS SHALL BE EDGED ACCORDING TO PLANS AND SPECIFICATIONS.
- PLANTS AND LANDSCAPE MATERIALS SHALL BE INSTALLED AS DETAILED ON PLANS.
- PLANT BACKFILL FOR TREES AND SHRUBS SHALL BE PER SPECIFICATIONS.
- ALL PLANTING BEDS SHALL BE TREATED WITH DACTHAL PRE-EMERGENT HERBICIDE AT MANUFACTURER RECOMMENDED RATES AND SHALL BE COVERED WITH SPECIFIED MULCH APPLICATION. APPLY LIGHTER APPLICATION OF DACTHAL HERBICIDE TO TOP OF MULCH LAYER.
- ALL AREAS DISTURBED DURING CONSTRUCTION THAT ARE NOT DESIGNATED AS PLANTING BEDS OR PAVEMENT AREAS SHALL BE SODED WITH A TURF-TYPE TALL FESCUE PER SPECIFICATIONS.
- ALL PLANT MATERIAL SHALL BE GUARANTEED FOR A PERIOD OF ONE YEAR AFTER OWNER'S FINAL ACCEPTANCE OF FINISHED JOB. ALL DEAD AND DAMAGED PLANT MATERIAL SHALL BE REPLACED BY LANDSCAPE CONTRACTOR AT THEIR EXPENSE. LANDSCAPE CONTRACTOR SHALL MAINTAIN PLANT MATERIAL UNTIL FINAL ACCEPTANCE.
- ALL LANDSCAPE BEDS SHALL BE MOUND AS SHOWN ON PLANS AND DETAILS.
- LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ACTUAL PLANT QUANTITIES REQUIRED TO COMPLETE THE PROJECT, AS SHOWN ON PLANS, AND BASE THEIR BID ACCORDINGLY.
- ALL LANDSCAPE AREAS WILL BE IRRIGATED WITH A FULLY AUTOMATIC IRRIGATION SYSTEM. TURF AREAS SHALL BE IRRIGATED WITH ROTOR OR SPRAY HEADS AND ALL LANDSCAPE BEDS SHALL BE IRRIGATED WITH DRIP IRRIGATION. THE SYSTEM WILL INCLUDE QUICK COUPLERS FOR MANUAL IRRIGATION WHERE NEEDED.
- PLANT KEY DESCRIPTION:
 

WP	PLANT TYPE
3	QUANTITY

### LANDSCAPE LEGEND:



### LANDSCAPE PLAN NOTES:

- PLANTING BED, REF. LANDSCAPE UNDERSTORY PLAN
- BED EDGE
- AREA TO BE SODED WITH TURF-TYPE FESCUE GRASS SOD
- TRAIL OR SIDEWALK, REF. ARCHITECTURAL PLANS
- MONUMENT SIGN

### PLANT SCHEDULE - OVERSTORY

QTY	KEY	BOTANICAL NAME	COMMON NAME	SIZE	COND.	COMMENTS
<b>SHADE TREES</b>						
13	AFA	Acer x freemanii 'Armstrong'	ARMSTRONG MAPLE	2" CAL.	B 4 B	N/A
13	GSA	Acer platanoides 'Crimson Sentry'	CRIMSON SENTRY MAPLE	2" CAL.	B 4 B	N/A
13	GBF	Quercus brissotii 'Princeton Sentry'	PRINCETON SENTRY QUERCUS	2" CAL.	B 4 B	MALE ONLY
1	LT	Liriodendron tulipifera	TULIP TREE	2" CAL.	B 4 B	N/A
6	GRS	Quercus rubra 'Princeton'	GREEN PILLAR OAK	2" CAL.	B 4 B	N/A
10	GRF	Quercus robur 'Fastigiata'	COLUMNAR ENGLISH OAK	2" CAL.	B 4 B	N/A
13	TCC	Tilia cordata 'Chancellor'	CHANCELLOR LITTLE LEAF LINDEN	2" CAL.	B 4 B	N/A
11	TDS	Toxicodendron radicans 'Shawnee Brave'	SHAWNEE BRAVE BALD CYPRESS	2" CAL.	B 4 B	N/A
8	ZSM	Zelkova serrata 'Musashino'	MUSASHINO COLUMNAR ZELKOVA	2" CAL.	B 4 B	N/A
<b>ORNAMENTAL TREES</b>						
14	AC	Amelanchier canadensis 'Shadblow'	SHADBLow SERVICEBERRY	6'-0" HT. MIN.	B 4 B	N/A
1	MGR	Magnolia stellata 'Royal Star'	ROYAL STAR MAGNOLIA	6'-0" HT. MIN.	B 4 B	N/A
15	MSS	Malus 'Spring Snow'	SPRING SNOW CRABAPPLE	6'-0" HT. MIN.	B 4 B	N/A
11	PCG	Prunus cerasifera 'Crimson Pointe'	CRIMSON POINTE FLUM	6'-0" HT. MIN.	B 4 B	N/A
<b>EVERGREEN TREES</b>						
1	FFV	Ficus flexilis 'Vanderhoff's Pyramid'	VANDERHOLF PINE	6'-0" HT. MIN.	B 4 B	N/A
1	FRG	Picea pungens 'Glauca'	COLORADO BLUE SPRUCE	6'-0" HT. MIN.	B 4 B	N/A

### LANDSCAPING REQUIREMENTS:

CITY OF MISSION, KANSAS  
 ZONING ORDINANCE CHAPTER 415 ARTICLE III  
 CURRENT SITE ZONED: MXD  
 TOTAL PROPERTY: 16.2 ACRES (105,612 SQ FT)

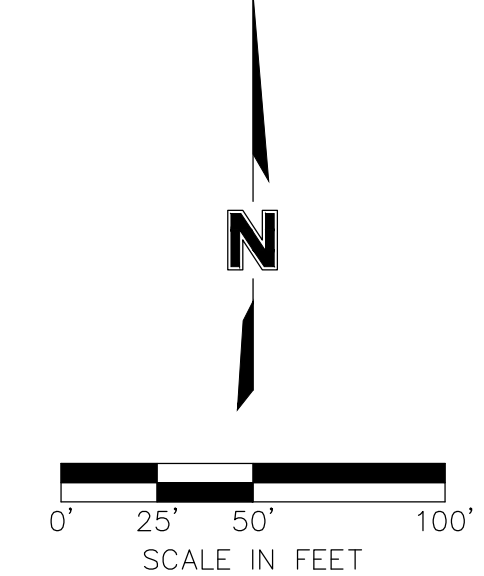
### MINIMUM TREE REQUIREMENTS:

REQUIRED PLANTS	SHOWN PLANTS	REQUIRED SQ FT	SHOWN SQ FT
ONE (1) TREE FOR EACH FIFTY (50) FEET OF STREET FRONTAGE.			
TOTAL FRONTAGE JOHNSON DRIVE = 1,039 LN FT TREES (1,039 / 50 = 20.8)	21 TREES	21 TREES	
TOTAL FRONTAGE ROE AVENUE = 504 LN FT TREES (504 / 50 = 10.1)	10 TREES	10 TREES	
TOTAL FRONTAGE SHAWNEE MISSION PARKWAY = 405 LN FT TREES (405 / 50 = 8.1)	10 TREES	10 TREES	
TOTAL FRONTAGE ROELAND DRIVE = 1,050 LN FT TREES (1,050 / 50 = 21)	21 TREES	21 TREES	
ONE (1) TREE FOR EVERY THREE THOUSAND (3,000) SQUARE FEET OF LANDSCAPED OPEN SPACE.			
TOTAL LANDSCAPED OPEN SPACE = 74,341 SQ FT TREES (104,100 / 3,000 = 34.7)	35 TREES	70 TREES	
ONE (1) TREE FOR EACH TWENTY (20) CARS OF PARKING AREA.			
TOTAL PARKING SPACES BUILDING A = 124 (UNCOVERED STALLS) TREES (124 / 20 = 6.2)	6 TREES	4 TREES	
TOTAL PARKING SPACES BUILDINGS D & C = 96 TREES (96 / 20 = 4.8)	5 TREES	7 TREES	
TOTAL PARKING SPACES BUILDING E = 30 (ON-STREET)	0 TREES	0 TREES	

### PLANTING WITHIN PARKING AREAS REQUIREMENTS:

NOT LESS THAN SIX PERCENT (6%) OF THE INTERIOR OF A PARKING LOT SHALL BE LANDSCAPED.

MINIMUM LANDSCAPE AREA (24' x 210' = 5,040 SQ FT)	TOTAL UNCOVERED PARKING SPACES BUILDING A = 124	2,008 SQ FT	1,841 SQ FT
TOTAL PARKING SPACES BUILDINGS D & C = 96 <td>1,511 SQ FT <td>3,542 SQ FT <td></td> </td></td>	1,511 SQ FT <td>3,542 SQ FT <td></td> </td>	3,542 SQ FT <td></td>	
MINIMUM LANDSCAPE AREA (41' x 210' = 8,610 SQ FT) <td></td> <td></td> <td></td>			
<b>LANDSCAPE TOTALS</b>	<b>116 TREES</b>	<b>151 TREES</b>	<b>3,514 SQ FT</b> <b>5,484 SQ FT</b>



01 OVERSTORY PLAN

FINAL DEVELOPMENT PLAN  
 MARCH 17, 2017  
 REVISIONS:  
 CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:  
 CIVIL / LANDSCAPE:  
 OLSSON ASSOCIATES  
 7301 WEST 133RD ST., SUITE 200  
 OVERLAND PARK, KANSAS 66213  
 TEL: 913-381-1170  
 FAX: 913-381-1174  
 M/E/P:  
 PKMR ENGINEERS  
 13300 W. 98TH ST  
 LENEXA, KANSAS 66215  
 TEL: 913-492-2400  
 FAX: 913-492-2437  
 STRUCTURAL:  
 BOB D. CAMPBELL & CO., INC.  
 4338 BELLEVUE AVE  
 KANSAS CITY, MISSOURI 64111  
 TEL: 816-531-4444  
 FAX: 816-531-8872

MISSION GATEWAY  
**el dorado** **Olsson** **pkmr** **BDC**  
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 Structural Engineers - Since 1922

# FDP-L201

## LANDSCAPE OVERSTORY PLAN





















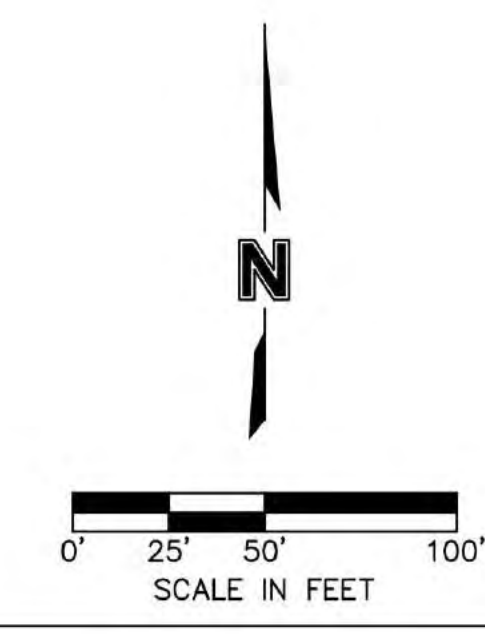


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01 LANDSCAPE PLAN

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017  
REVISIONS:  
CITY REVIEW COMMENTS 03.17.2017



CONSULTANTS:  
CIVIL / LANDSCAPE:  
OLSSON ASSOCIATES  
7301 WEST 113RD ST., SUITE  
200  
OVERLAND PARK, KANSAS  
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TEL: 913-381-1170  
FAX: 913-381-1174  
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PKMR ENGINEERS  
1309 W. 98TH ST.  
LENEXA, KANSAS 66215  
TEL: 913-492-2400  
FAX: 913-492-2437  
STRUCTURAL:  
BOB O'CAMPIELL & CO., INC.  
4338 BELLEVUE AVE.  
KANSAS CITY, MISSOURI 64111  
TEL: 816-531-4144  
FAX: 816-531-8572

MISSION GATEWAY

**el dorado** **OLSSON ASSOCIATES** **pkmr** **B&C**

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FDP-L300  
LANDSCAPE PLAN



PLANT MATERIALS

DECIDUOUS TREES



TULIP TREE



PRINCETON SENTRY GINKGO



FASTIGIATA ENGLISH OAK



ARMSTRONG MAPLE



CHANCELLOR LITTLE LEAF LINDEN

ORNAMENTAL TREES



SPRING SNOW CRABAPPLE



ROYAL STAR MAGNOLIA



SERVICEBERRY



GREEN PILLAR OAK



CRIMSON SENTRY MAPLE



SHAWNEE BRAVE BALD CYPRESS



MUSASHINO ZELKOVA



CRIMSON POINTE PLUM

EVERGREEN TREES



VANDERWOLF PINE



COLORADO BLUE SPRUCE

DECIDUOUS SHRUBS



ALLEGHENY VIBURNUM



KOREANSPICE VIBURNUM



KELSEY DOGWOOD

PERENNIALS



SPIKE GAYFEATHER



ZAGREB COREOPSIS



EVERGREEN AND DECIDUOUS SEDUM MIX

GRASSES



BIG BLUESTEM



HEAVY METAL SWITCH GRASS



ROTSTROHLBUSCH SWITCH GRASS



ADAGIO MAIDEN GRASS



HAMELN DWARF FOUNTAIN GRASS

01 LANDSCAPE MATERIALS

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017  
REVISIONS:  
CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:  
CIVIL / LANDSCAPE: OLSSON ASSOCIATES, 7301 WEST 133RD ST., SUITE 200, OVERLAND PARK, KANSAS 66213, TEL: 913-381-1170, FAX: 913-381-1174  
MEP: PKMR ENGINEERS, 13300 W. 96TH ST, LENEXA, KANSAS 66215, TEL: 913-462-2400, FAX: 913-462-2407  
STRUCTURAL: BOB D. CAMPBELL & CO., INC., 4338 BELLEVUE AVE, KANSAS CITY, MISSOURI 64111, TEL: 816-531-4144, FAX: 816-531-6072

MISSION GATEWAY



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FDP-L301

LANDSCAPE MATERIALS



HARDSCAPE MATERIALS

LIGHTING



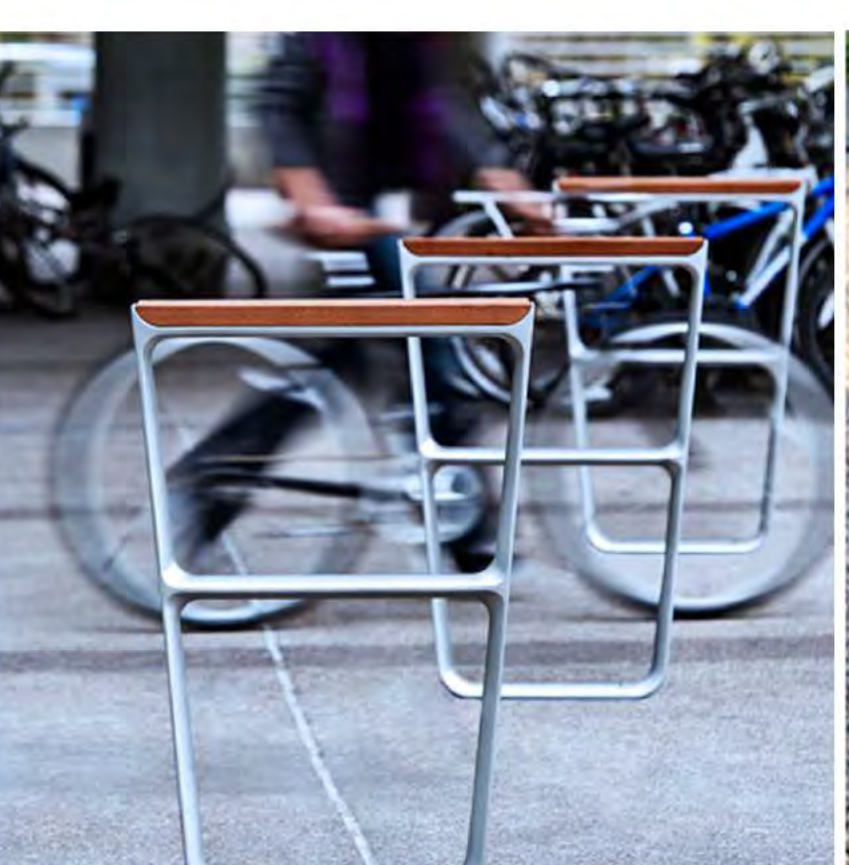
FESTOON LIGHTING FOR COURTYARD



AMENITIES



Landscape Forms - 'Multiplicity' Bench and Bike Rack  
BENCHES, BIKE RACKS, AND LITTER RECEPTACLES



Landscape Forms - 'Collect' Litter Receptacle



Landscape Forms - 'Scarborough' Bench and Litter Receptacle



Landscape Forms - 'Ring' Bike Rack

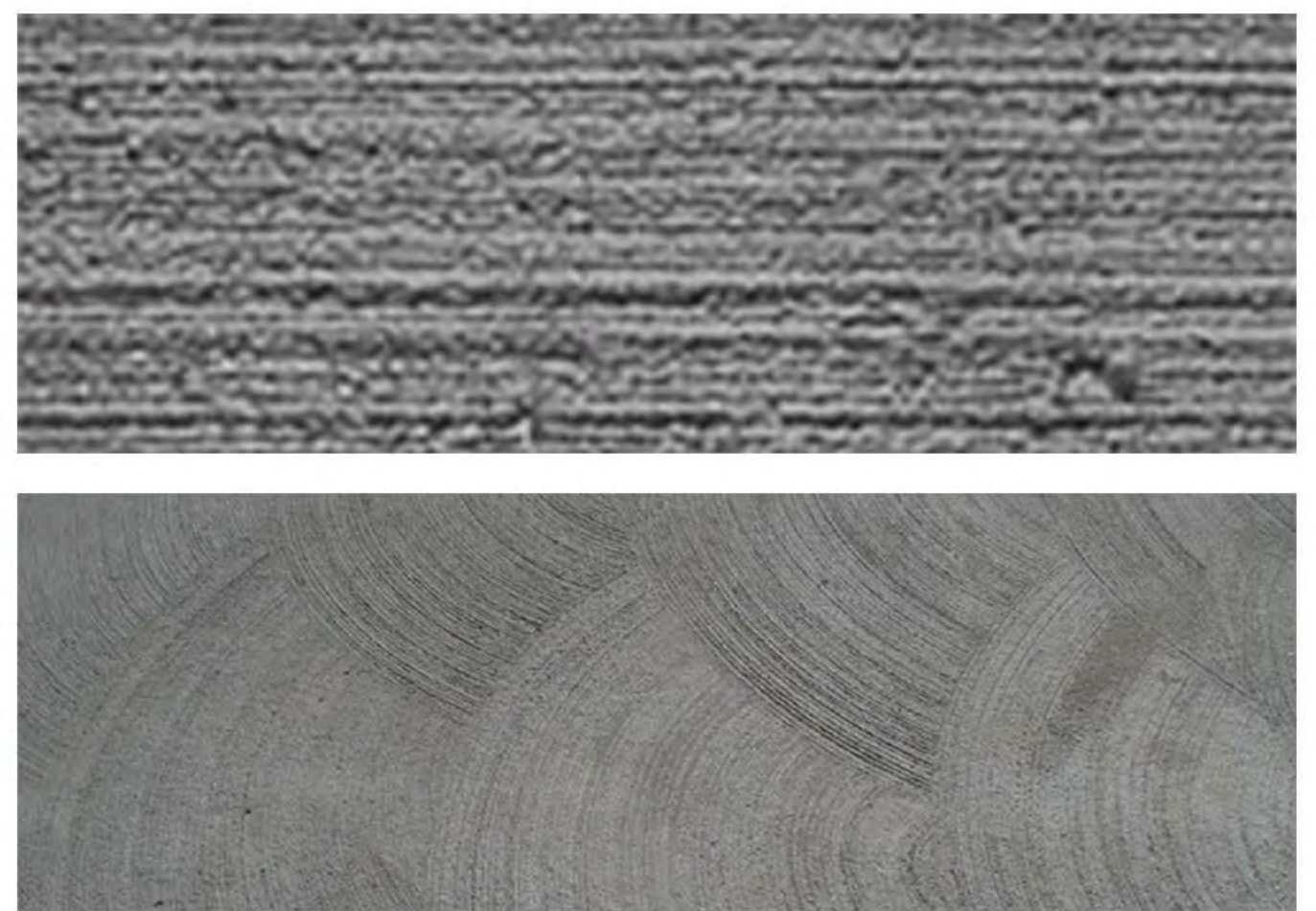
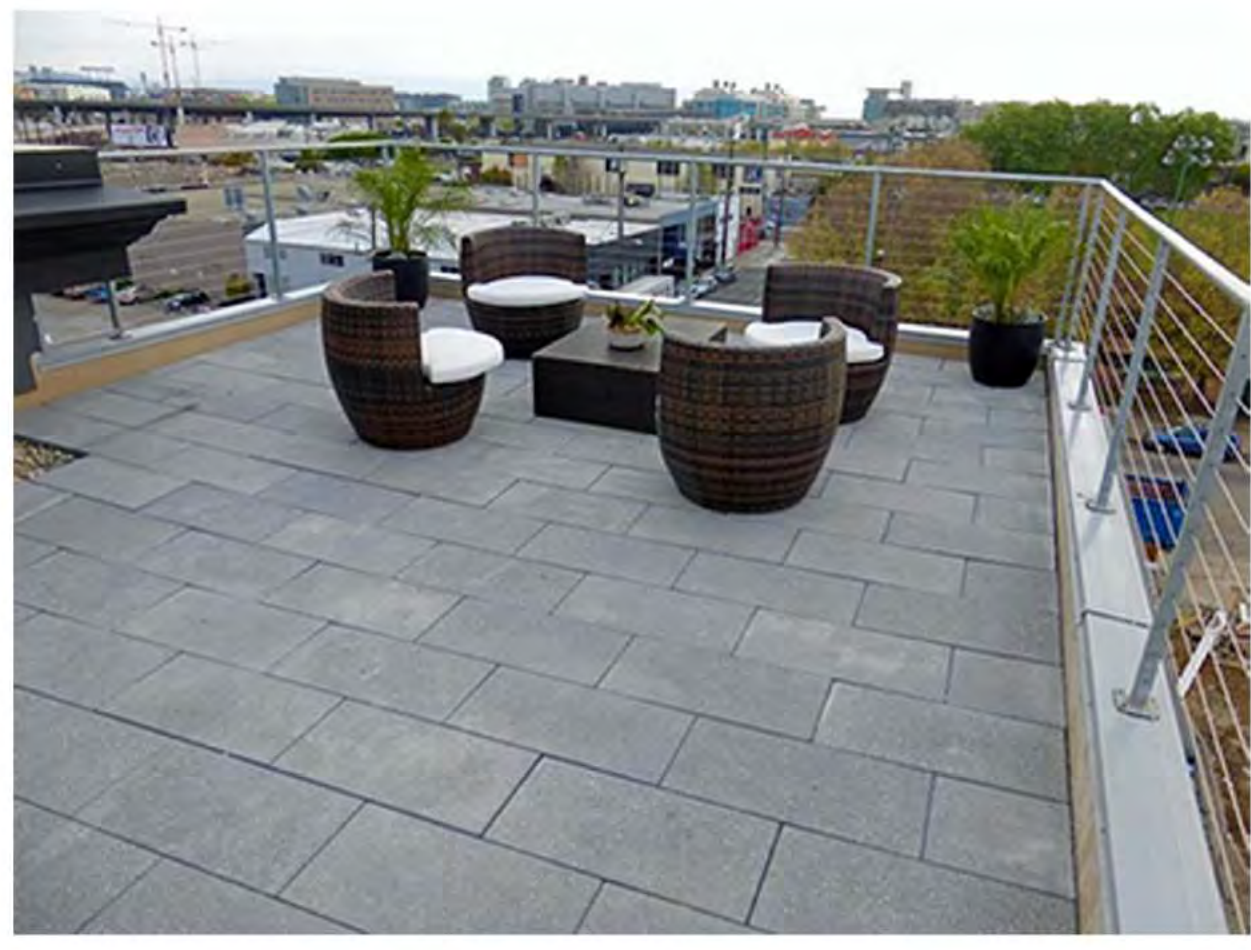
GROUND PLANE MATERIALS



DARK STAINED HARDWOOD PEDESTRIAN DECKING



HOTEL DECK PAVER SYSTEMS



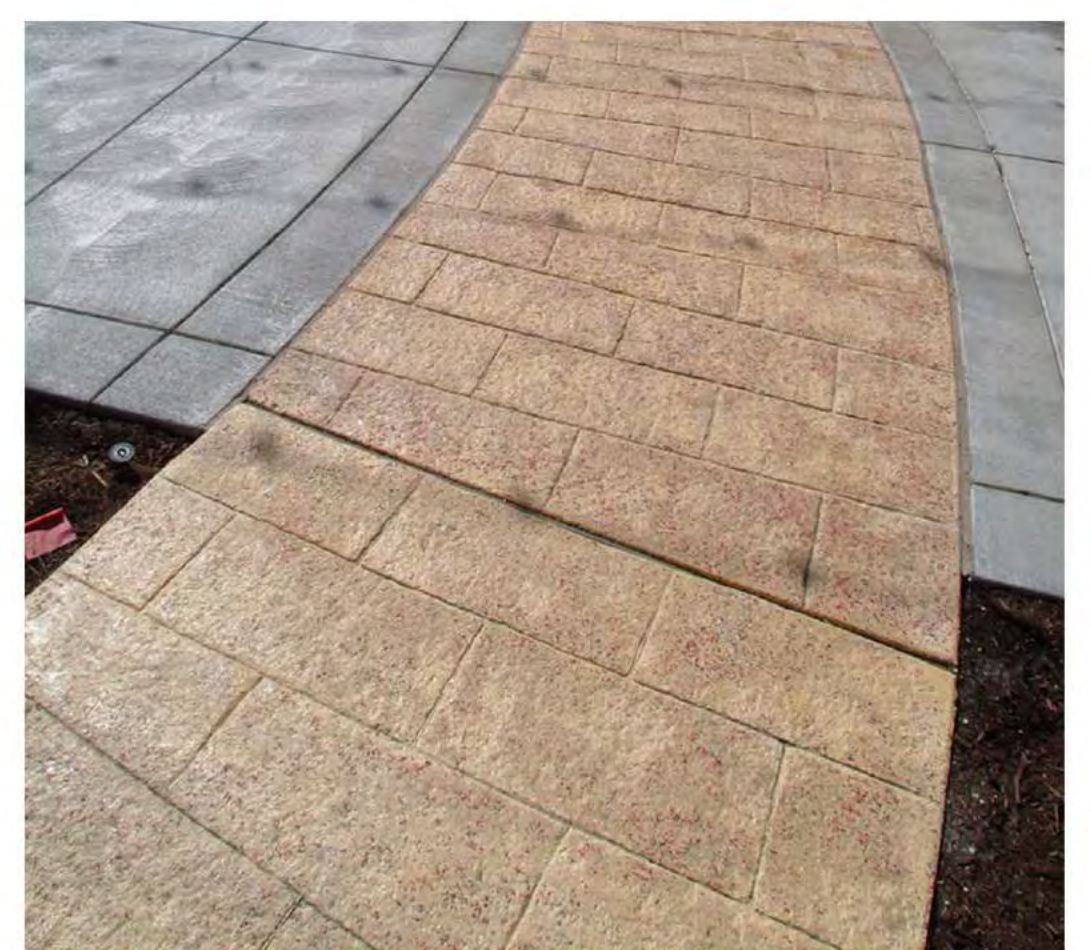
CONCRETE - GRAY



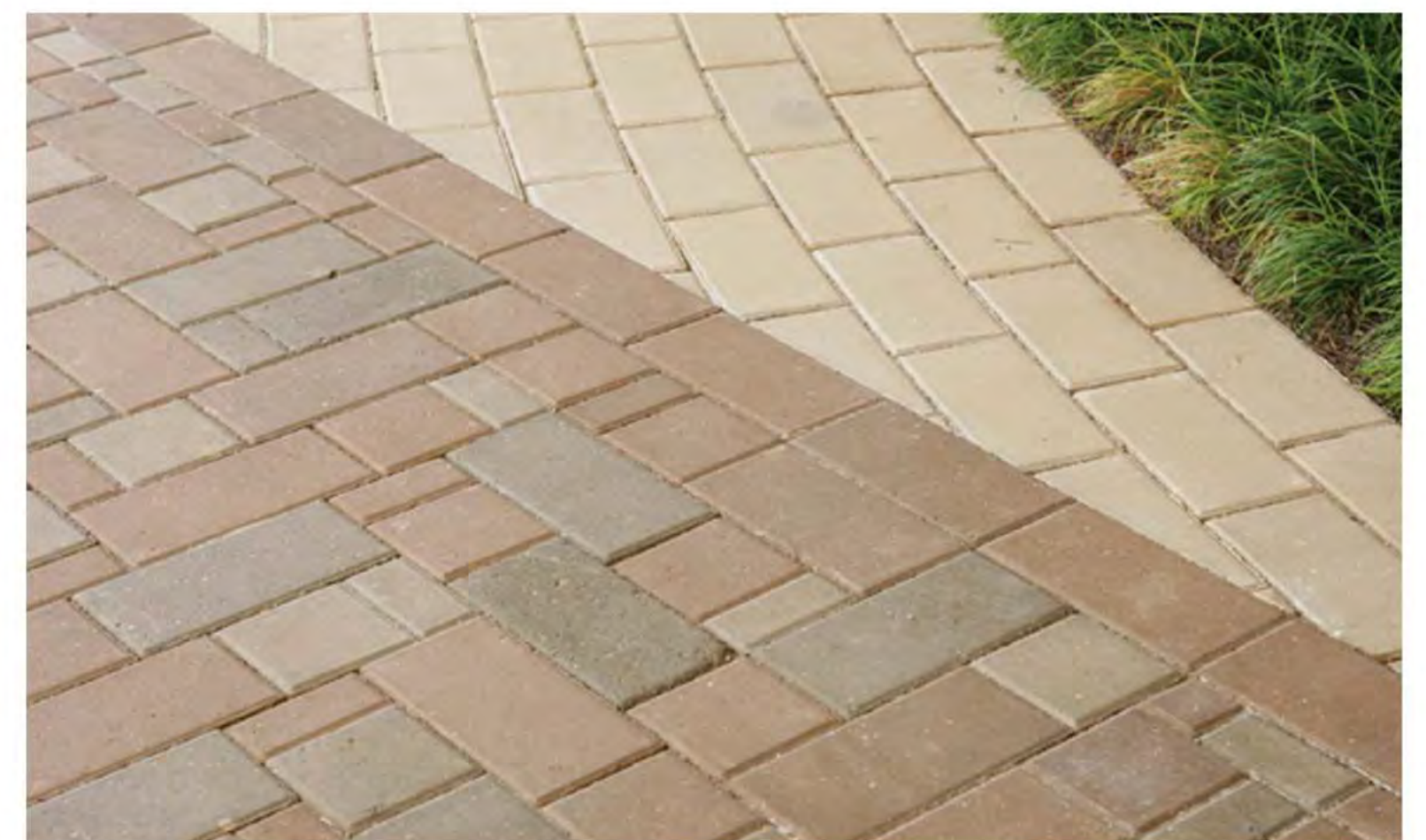
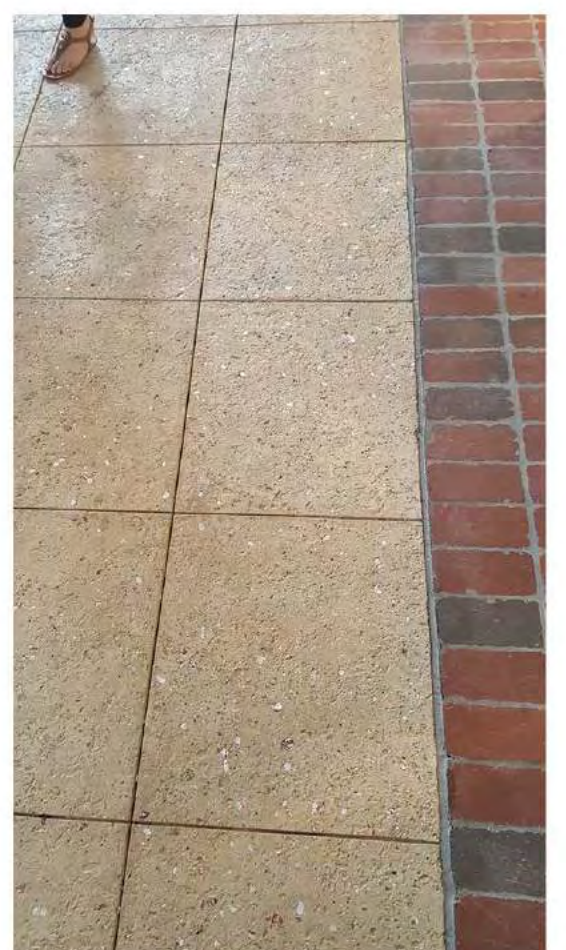
CONCRETE - COLOR



CONCRETE - STAMPED COLOR



CONCRETE - COLOR EXPOSED



CONCRETE - PAVERS

WALLS













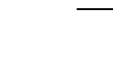


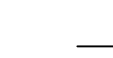
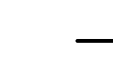




BOARD FORMED CONCRETE WALLS

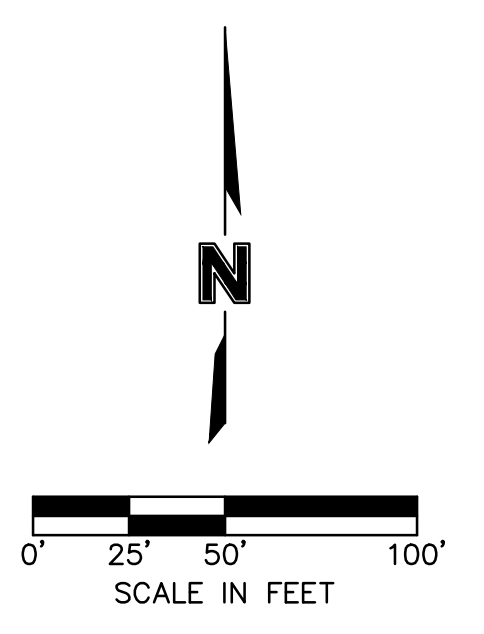
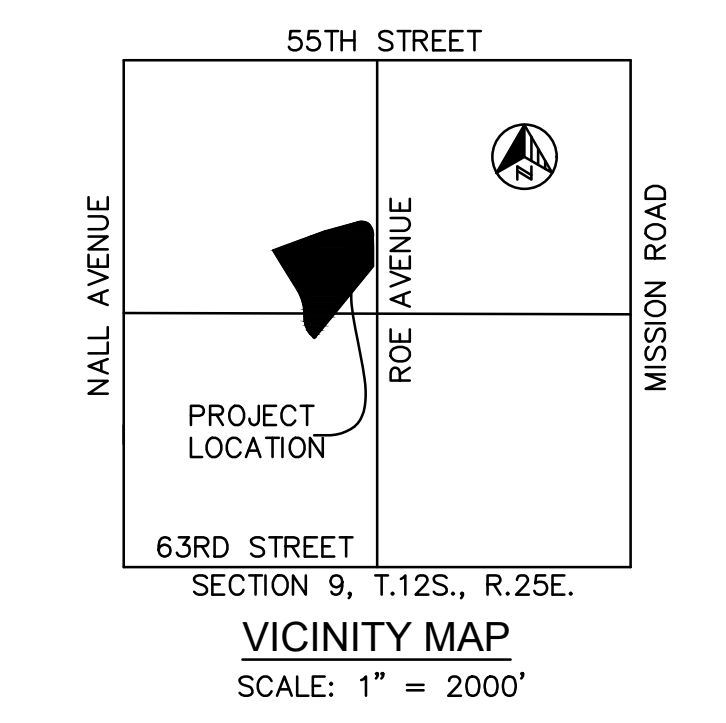
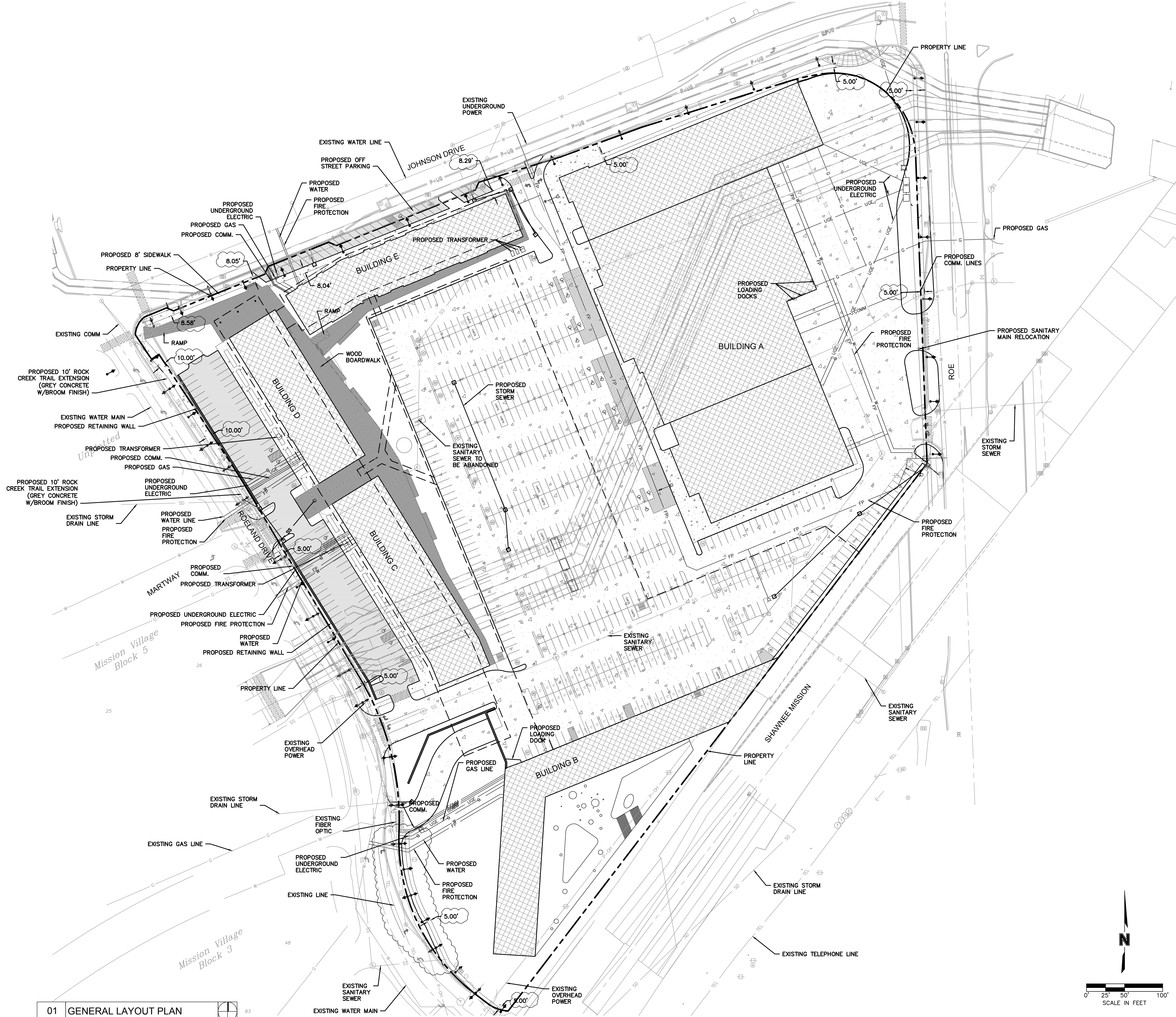




**LEGEND**

-  P-OH EXISTING OVERHEAD POWER LINE
-  P-UG EXISTING UNDERGROUND POWER LINE
-  TEL EXISTING TELEPHONE LINE
-  COMM EXISTING COMMUNICATION LINE
-  FO EXISTING FIBER OPTIC LINE
-  G EXISTING GAS LINE
-  W EXISTING WATER LINE
-  SS EXISTING SANITARY SEWER
-  SD EXISTING STORM DRAIN
-  SS EXISTING STORM SEWER
-  ——— PROPERTY BOUNDARY
-  - - - - - EASEMENT LINE
-  UGE PROPOSED UNDERGROUND ELECTRIC LINE
-  COMM PROPOSED COMMUNICATION LINE
-  W PROPOSED WATER LINE
-  G PROPOSED GAS LINE
-  FP PROPOSED FIRE PROTECTION LINE
-  SS PROPOSED SANITARY SEWER
-  C COMPACT PARKING STALLS

**NOTE:**  
ALL SIDEWALK IMPROVEMENTS SHALL MEET ADA AND MUTCD CRITERIA.



01 GENERAL LAYOUT PLAN

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017  
REVISIONS:  
CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:  
CIVIL / LANDSCAPE: OLSSON ASSOCIATES  
7301 WEST 133RD ST., SUITE 202  
OVERLAND PARK, KANSAS 66213  
TEL: 913-381-1170  
FAX: 913-381-1174  
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LENEXA, KANSAS 66215  
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FAX: 913-492-2437  
STRUCTURAL: BOB D. CAMPBELL & CO., INC.  
4338 BELLEVUE AVE  
KANSAS CITY, MISSOURI 64111  
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FAX: 816-531-8072

MISSION GATEWAY  
**el dorado** **OLSSON ASSOCIATES** **pkmr ENGINEERS** **B.D.C. BOB D. CAMPBELL & CO., INC.**  
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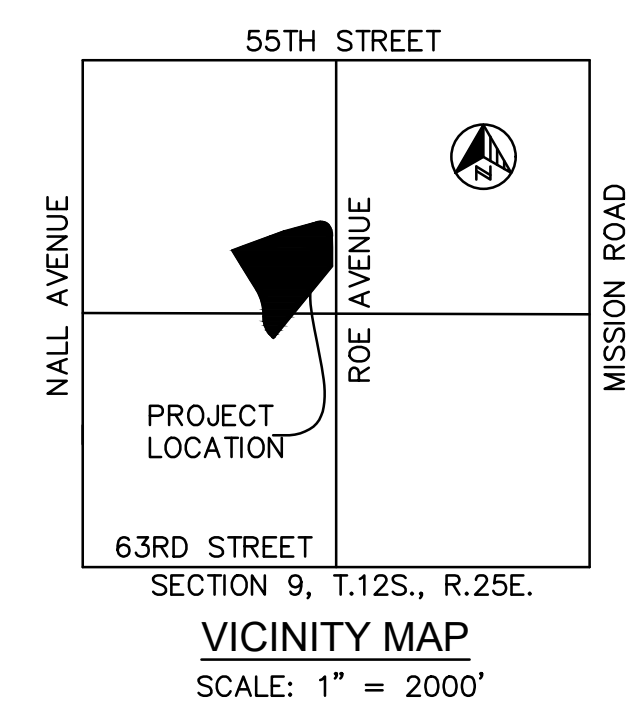
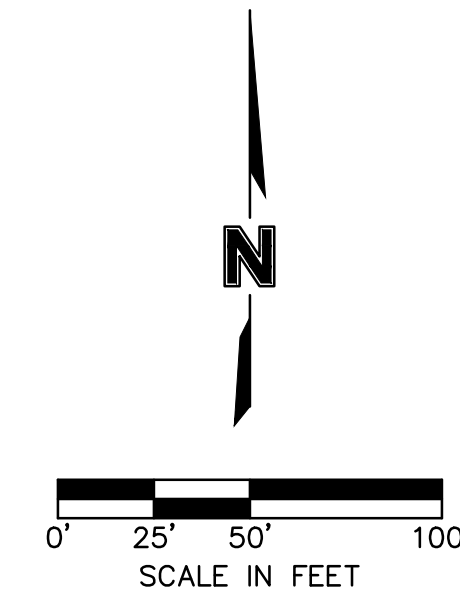
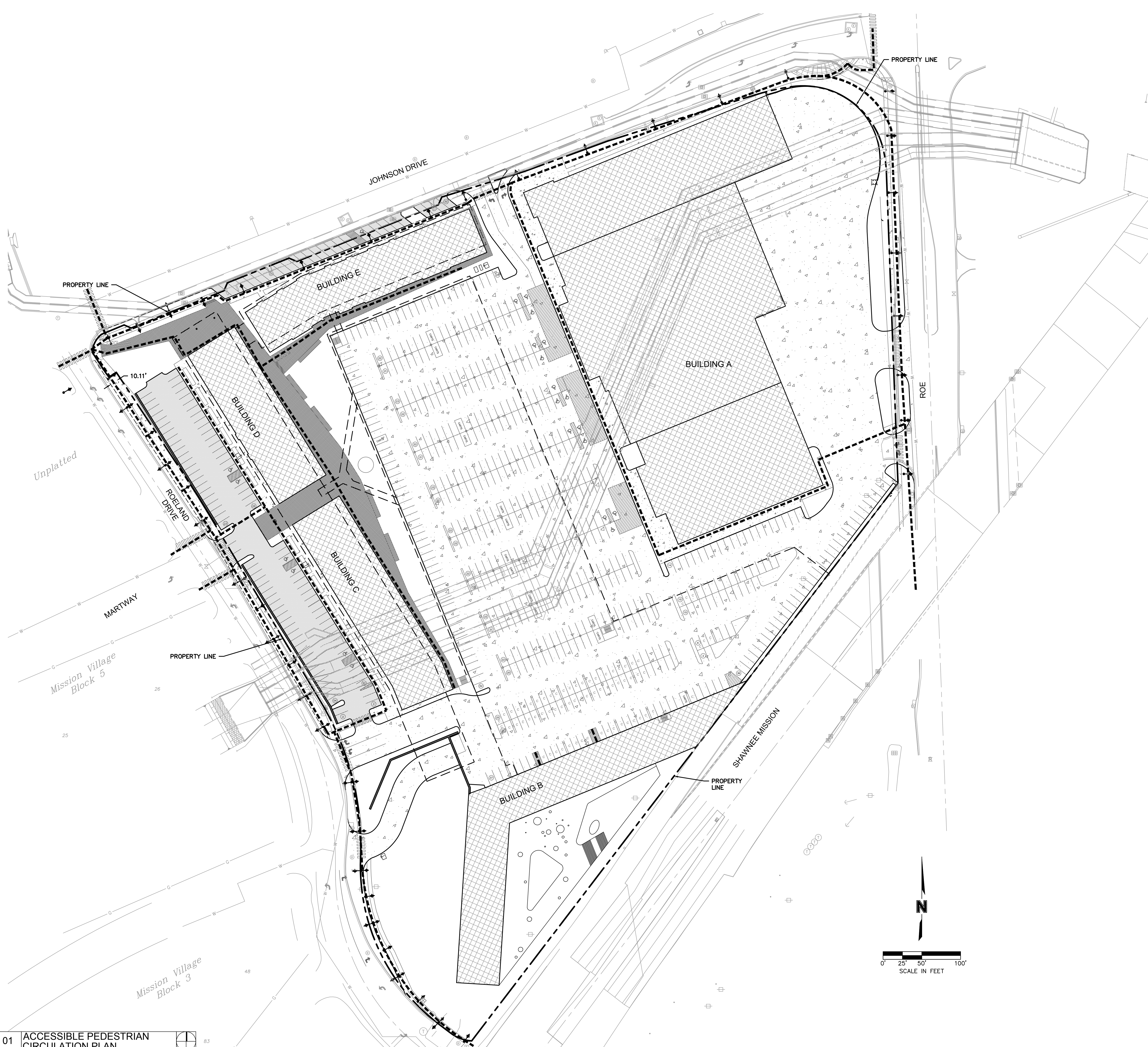
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GENERAL LAYOUT PLAN

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**LEGEND**

- PROPERTY BOUNDARY
- - - - - ADA ACCESSIBLE ROUTE IN ACCORDANCE WITH SECTION 18.120.015



**FDP-C1.1**

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01 ACCESSIBLE PEDESTRIAN CIRCULATION PLAN

FINAL DEVELOPMENT PLAN  
 MARCH 17, 2017  
 REVISIONS:  
 CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:

CIVIL / LANDSCAPE: OLSSON ASSOCIATES 7301 WEST 133RD ST., SUITE 202 OVERLAND PARK, KANSAS 66213 TEL: 913-381-1170 FAX: 913-381-1174	M/E/P: PKMR ENGINEERS 13300 W. 98TH ST LENEXA, KANSAS 66215 TEL: 913-492-2400 FAX: 913-492-2437	STRUCTURAL: BOB D. CAMPBELL & CO., INC. 4338 BELLEVUE AVE KANSAS CITY, MISSOURI 64111 TEL: 816-531-4444 FAX: 816-531-8872
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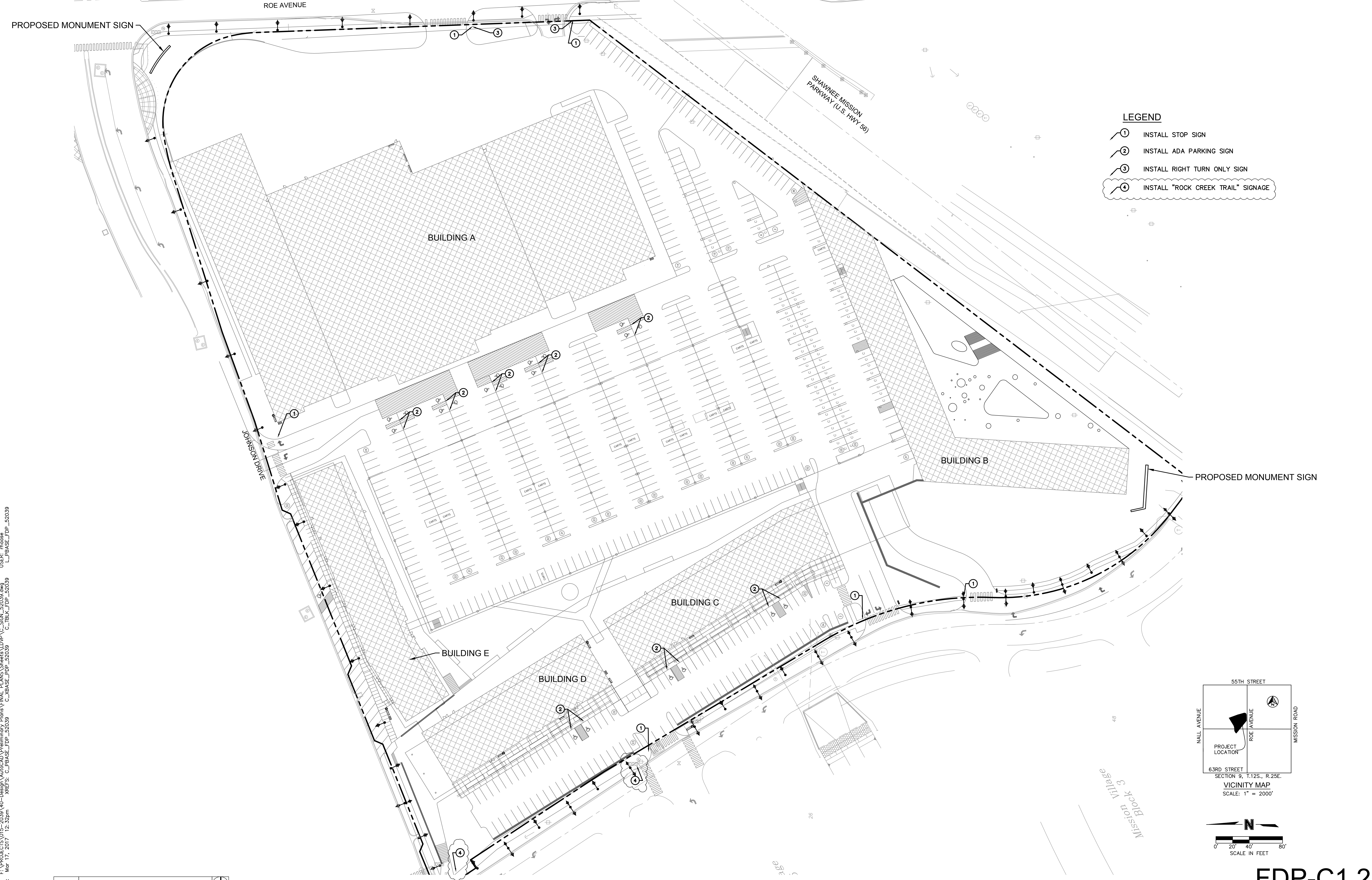
**ACCESSIBLE PEDESTRIAN CIRCULATION PLAN**

MISSION GATEWAY

**el dorado** **OLSSON ASSOCIATES** **pkmr ENGINEERS** **B.D.C. BOB D. CAMPBELL & CO., INC.**

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01 SITE SIGNAGE PLAN

FINAL DEVELOPMENT PLAN  
 MARCH 17, 2017  
 REVISIONS:  
 CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:

CIVIL / LANDSCAPE: OLSSON ASSOCIATES 7301 WEST 133RD ST., SUITE 200 OVERLAND PARK, KANSAS 66213 TEL: 913-381-1170 FAX: 913-381-1174	M/E/P: PKMR ENGINEERS 13300 W. 98TH ST LENEXA, KANSAS 66215 TEL: 913-492-2400 FAX: 913-492-2437	STRUCTURAL: BOB D. CAMPBELL & CO., INC. 4338 BELLEVUE AVE KANSAS CITY, MISSOURI 64111 TEL: 816-531-4444 FAX: 816-531-8872
--	--	--

MISSION GATEWAY

**el dorado** **OLSSON ASSOCIATES** **pkmr ENGINEERS** **B.D.C. ASSOCIATES**

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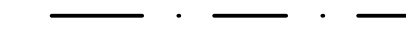
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SITE SIGNAGE PLAN

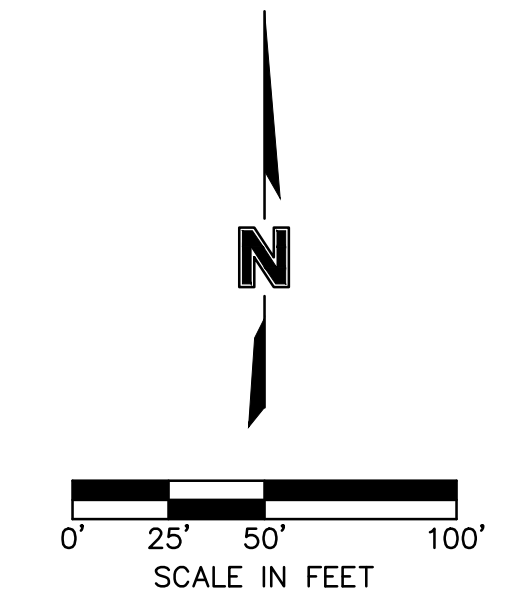
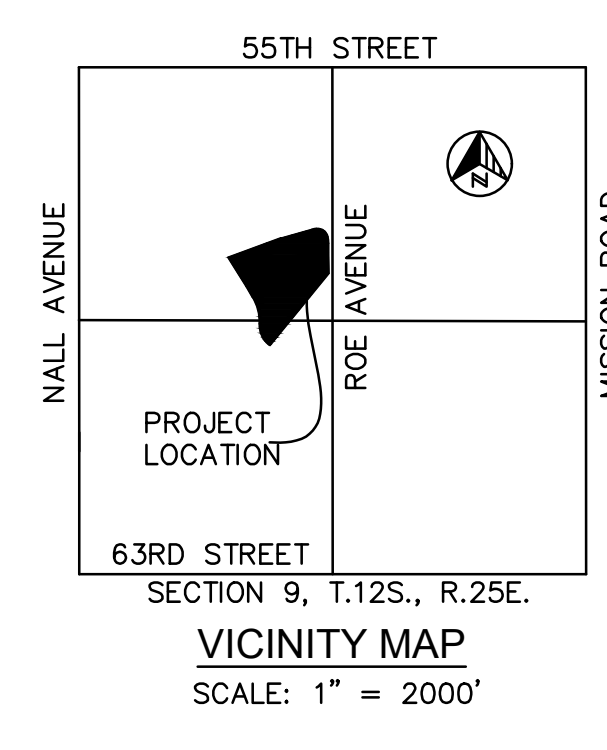
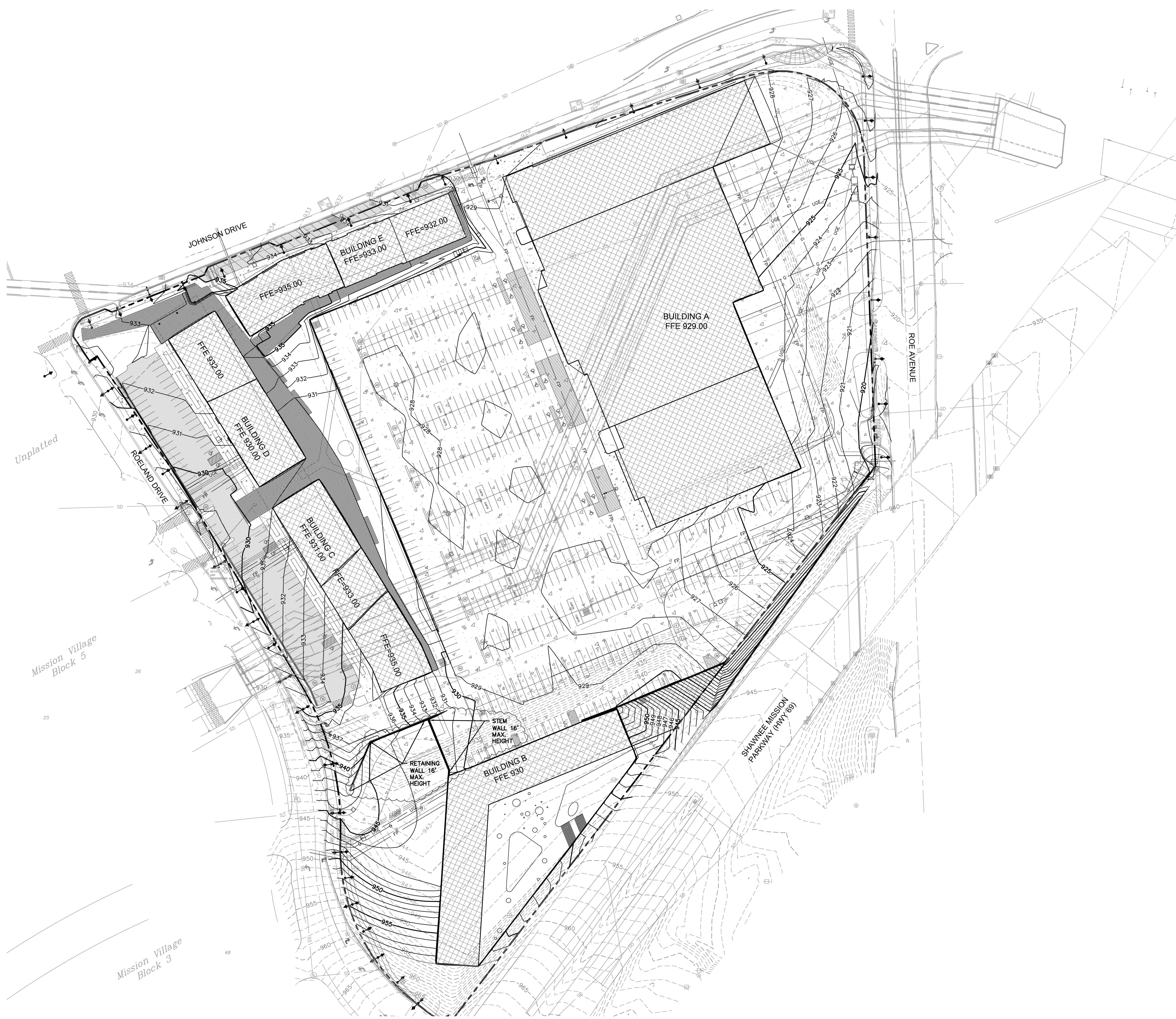






**LEGEND**

-  PROPERTY BOUNDARY
-  EXISTING GRADE CONTOUR
-  FINISHED GRADE CONTOUR
-  DITCH CENTERLINE
-  PROPOSED STORM SEWER



01 GRADING PLAN

**FINAL DEVELOPMENT PLAN**

MARCH 17, 2017

REVISIONS:  
CITY REVIEW COMMENTS 03.17.2017

**CONSULTANTS:**

CIVIL / LANDSCAPE:  
OLSSON ASSOCIATES  
7301 WEST 133RD ST., SUITE  
200  
OVERLAND PARK, KANSAS  
66213  
TEL: 913-381-1170  
FAX: 913-381-1174

M/E/P:  
PKMR ENGINEERS  
13300 W. 98TH ST  
LENEXA, KANSAS 66215  
TEL: 913-492-2400  
FAX: 913-492-2437

STRUCTURAL:  
BOB D. CAMPBELL & CO., INC.  
4338 BELLEVUE AVE  
KANSAS CITY, MISSOURI 64111  
TEL: 816-531-4444  
FAX: 816-531-8872

**MISSION GATEWAY**

**el dorado**

**OLSSON ASSOCIATES**

**pkmr ENGINEERS**

**B.D.C. BOB D. CAMPBELL & CO., INC.**

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**FDP-C2.0**

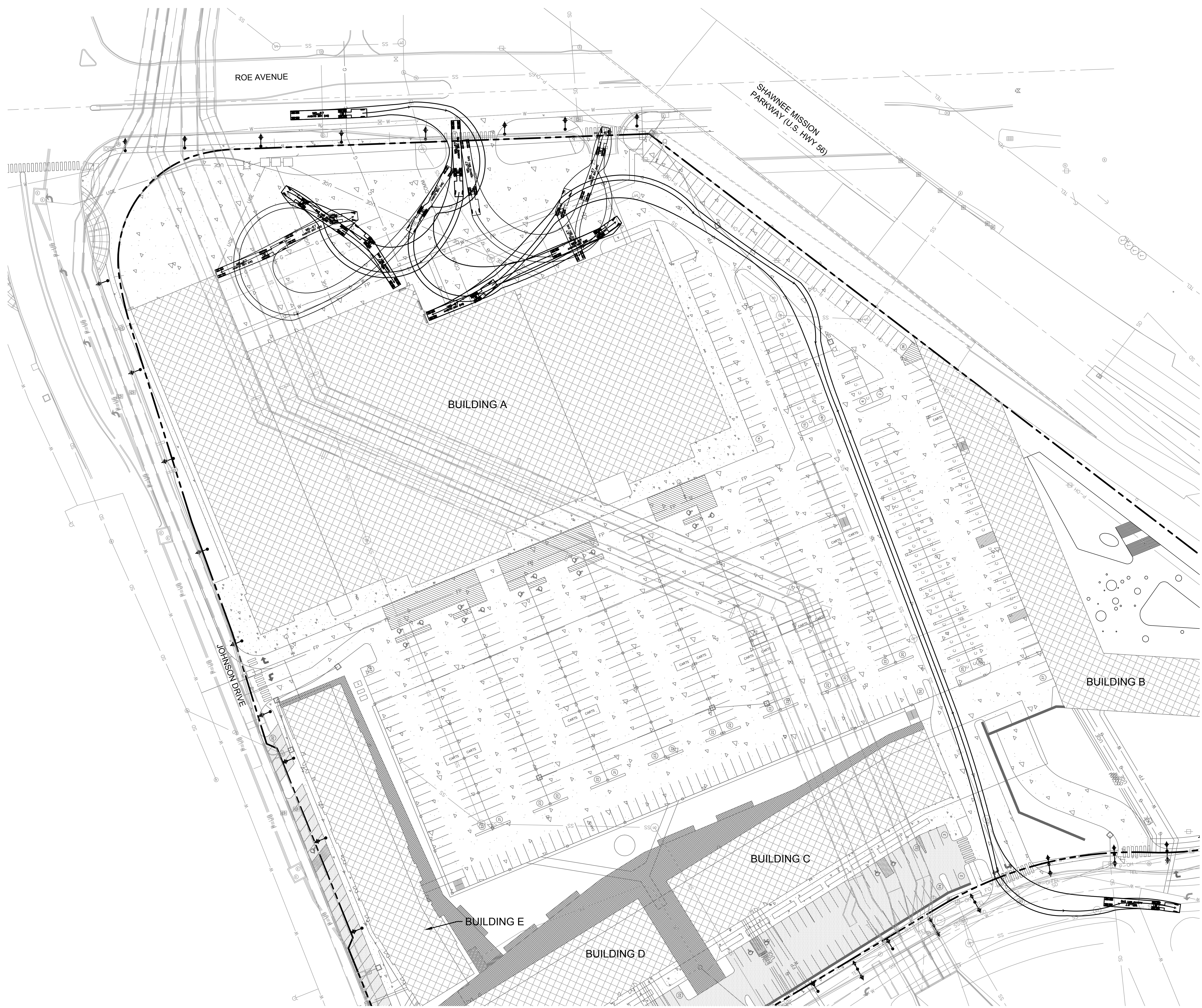
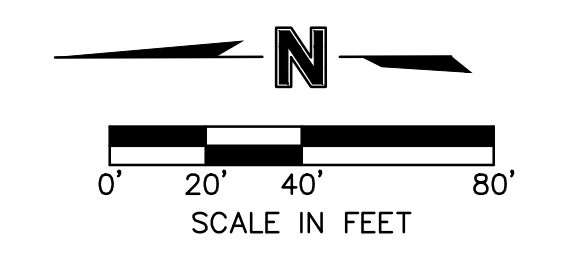
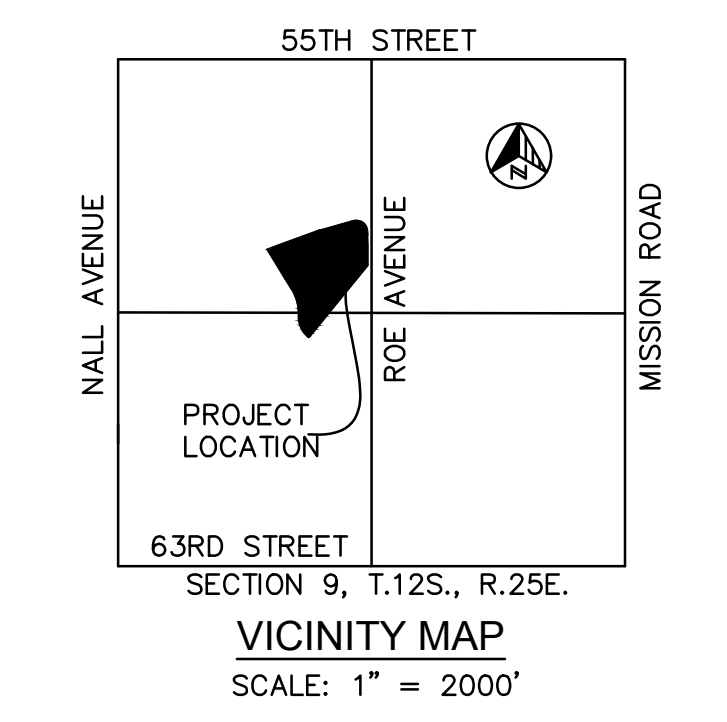
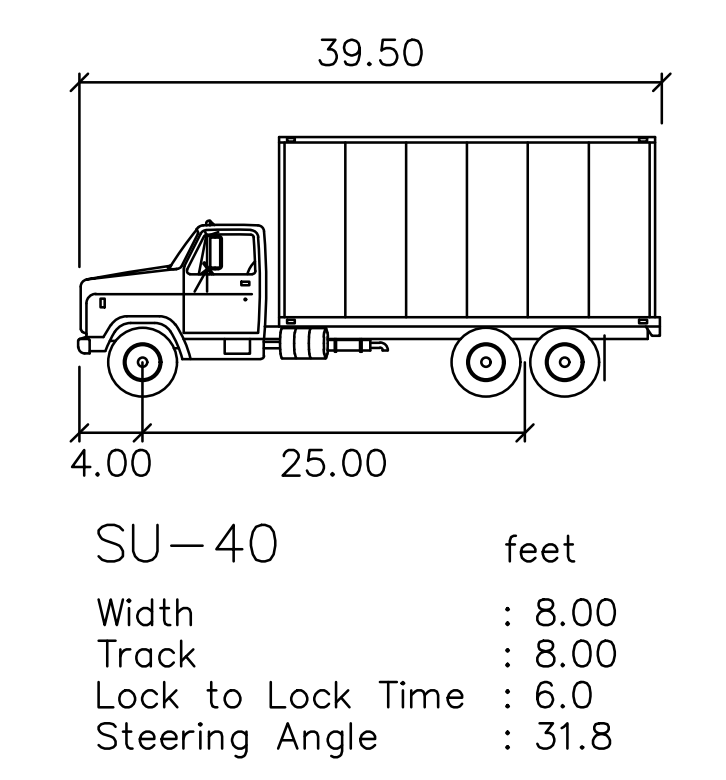
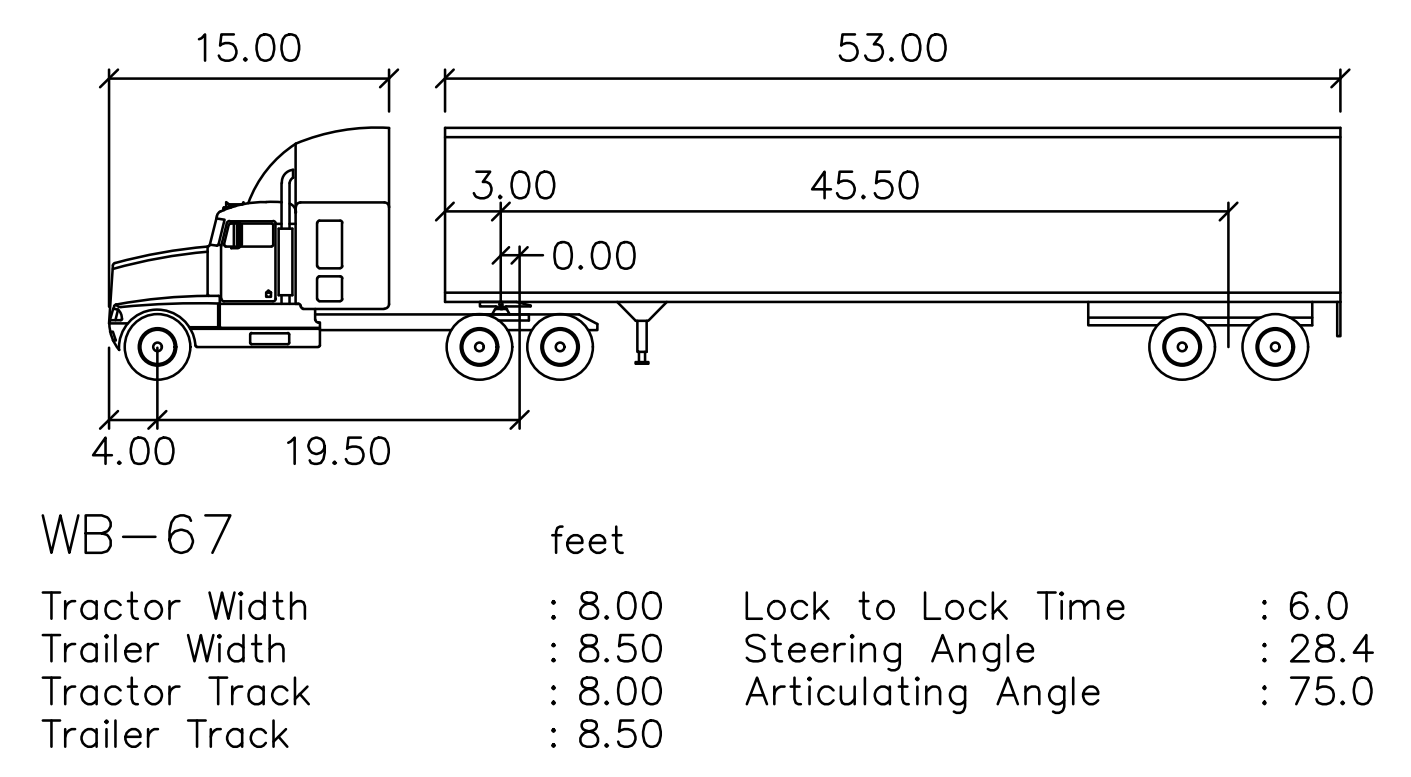
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**LEGEND**

- P-OH — OVERHEAD POWER LINE
- P-UG — UNDERGROUND POWER LINE
- E — ELECTRIC LINE
- TEL — TELEPHONE LINE
- COMM — COMMUNICATION LINE
- FO — FIBER OPTIC LINE
- CATV — CABLE TELEVISION SERVICE
- G — GAS LINE
- W — WATER LINE
- SS — EXISTING SANITARY SEWER
- SS — EXISTING STORM SEWER
- LOT LINE
- PROPERTY BOUNDARY
- EASEMENT LINE



**TRUCK MOVEMENT**

01 TURNING TEMPLATE

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FINAL DEVELOPMENT PLAN  
 MARCH 17, 2017  
 REVISIONS:  
 CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:

CIVIL / LANDSCAPE: OLSSON ASSOCIATES 7301 WEST 133RD ST., SUITE 200 OVERLAND PARK, KANSAS 66213 TEL: 913-381-1170 FAX: 913-381-1174	M/E/P: PKMR ENGINEERS 13300 W. 98TH ST LENEXA, KANSAS 66215 TEL: 913-492-2400 FAX: 913-492-2437	STRUCTURAL: BOB D. CAMPBELL & CO., INC. 4338 BELLEVUE AVE KANSAS CITY, MISSOURI 64111 TEL: 816-531-4444 FAX: 816-531-8872
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MISSION GATEWAY

**el dorado** **OLSSON ASSOCIATES**

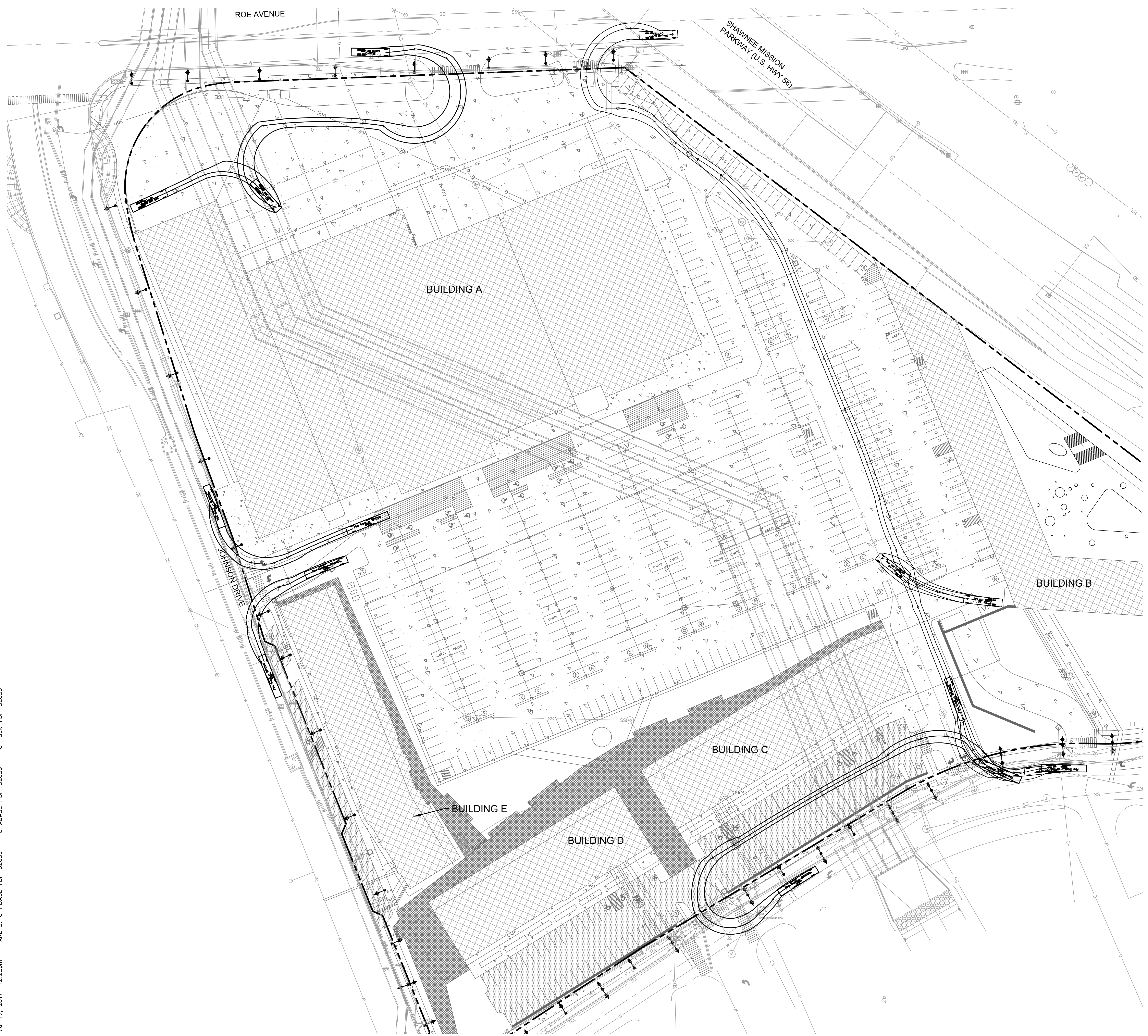
**pkmr** ENGINEERS

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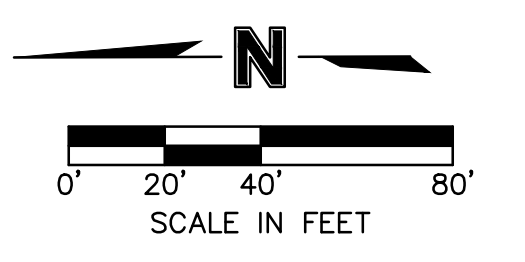
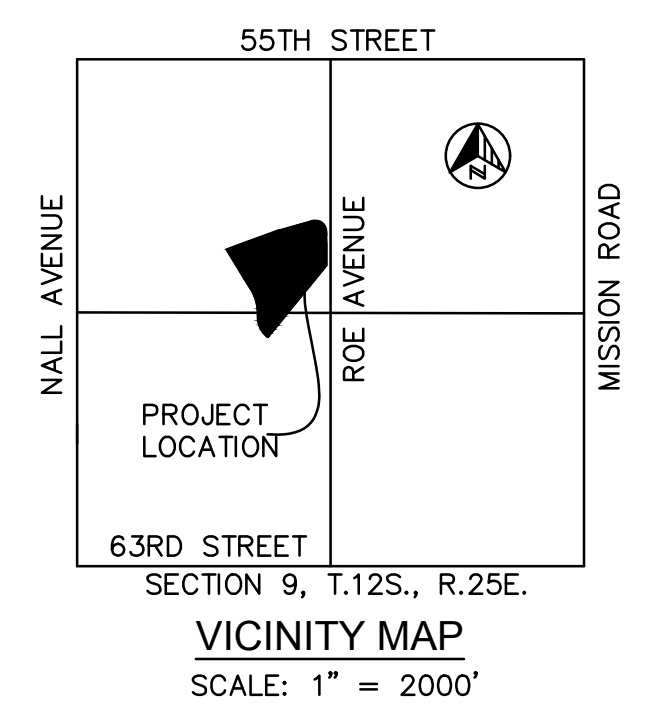
**FDP-C3.0**  
TURNING TEMPLATE





**LEGEND**

— P-OH	OVERHEAD POWER LINE
— P-UG	UNDERGROUND POWER LINE
— E	ELECTRIC LINE
— TEL	TELEPHONE LINE
— COMM	COMMUNICATION LINE
— FO	FIBER OPTIC LINE
— CATV	CABLE TELEVISION SERVICE
— G	GAS LINE
— W	WATER LINE
— SS	EXISTING SANITARY SEWER
—	EXISTING STORM SEWER
—	LOT LINE
—	PROPERTY BOUNDARY
—	EASEMENT LINE



01 TURNING TEMPLATE

TRUCK MOVEMENT

FINAL DEVELOPMENT PLAN  
 MARCH 17, 2017  
 REVISIONS:  
 CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:

CIVIL / LANDSCAPE: OLSSON ASSOCIATES 7301 WEST 133RD ST., SUITE 200 OVERLAND PARK, KANSAS 66213 TEL: 913-381-1170 FAX: 913-381-1174	M/E/P: PKMR ENGINEERS 13300 W. 98TH ST LENEXA, KANSAS 66215 TEL: 913-492-2400 FAX: 913-492-2437	STRUCTURAL: BOB D. CAMPBELL & CO., INC. 4338 BELLEVUE AVE KANSAS CITY, MISSOURI 64111 TEL: 816-531-4444 FAX: 816-531-8872
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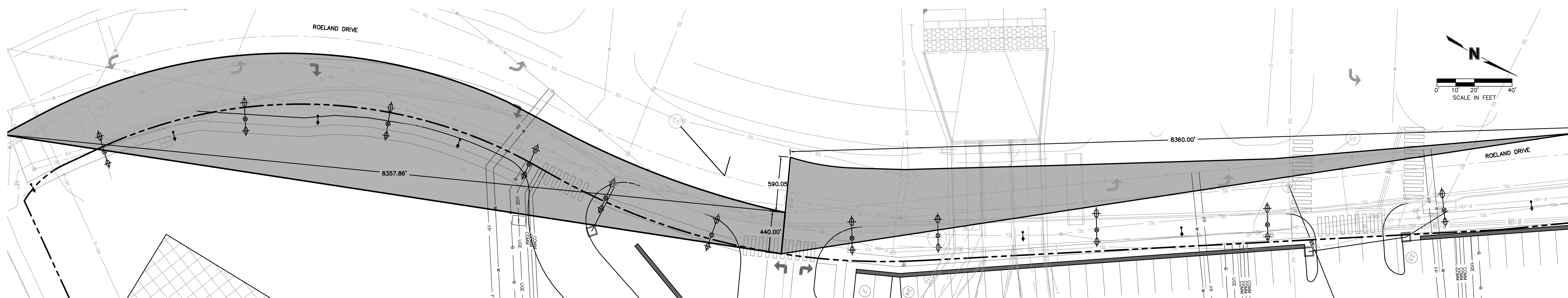
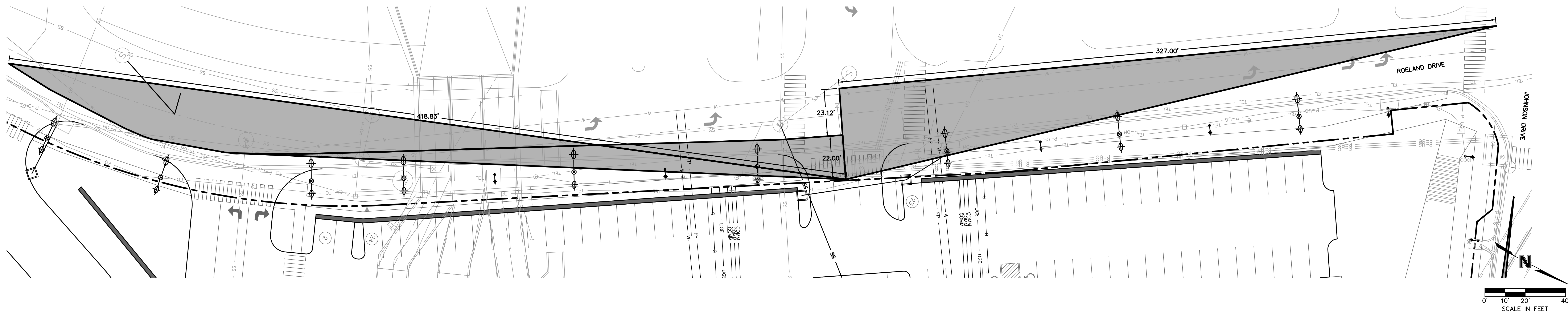
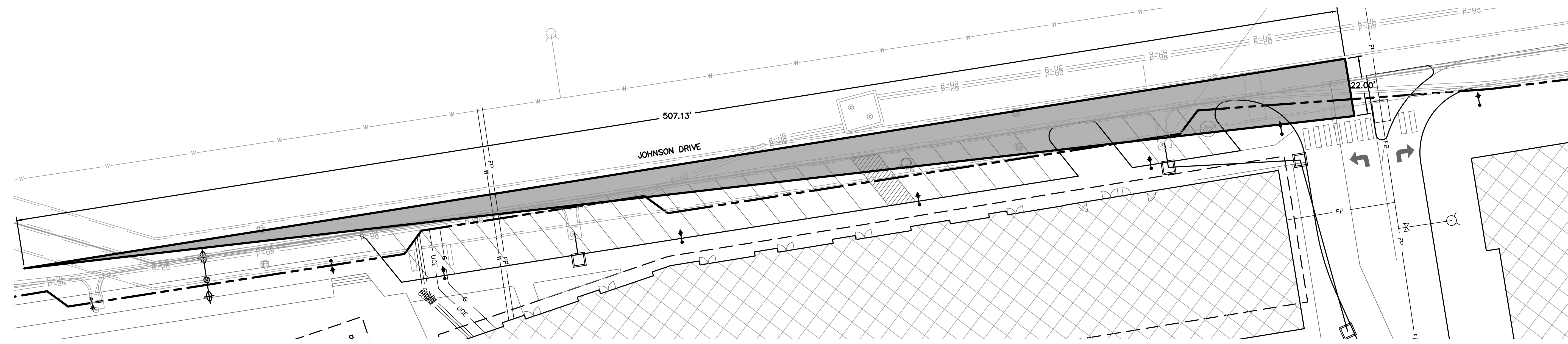
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TURNING TEMPLATE

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 USER: bboyd









01 SIGHT TRIANGLES

**EXH1**  
SIGHT TRIANGLES

FINAL DEVELOPMENT PLAN

MARCH 17, 2017

REVISIONS:  
CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:

CIVIL / LANDSCAPE:  
OLSSON ASSOCIATES  
7301 WEST 133RD ST., SUITE  
202  
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66213  
TEL: 913-381-1170  
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PKMR ENGINEERS  
13300 W. 88TH ST  
LENEXA, KANSAS 66215  
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FAX: 913-492-2437

STRUCTURAL:  
BOB D. CAMPBELL & CO., INC.  
4338 BELLEVUE AVE  
KANSAS CITY, MISSOURI 64111  
TEL: 816-531-4444  
FAX: 816-531-8872

MISSION GATEWAY

**el dorado**

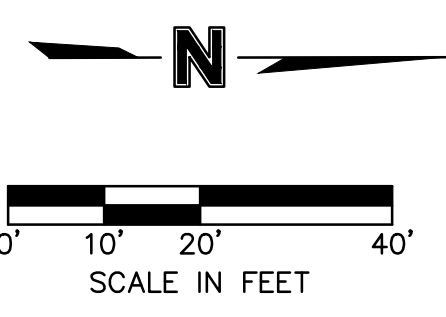
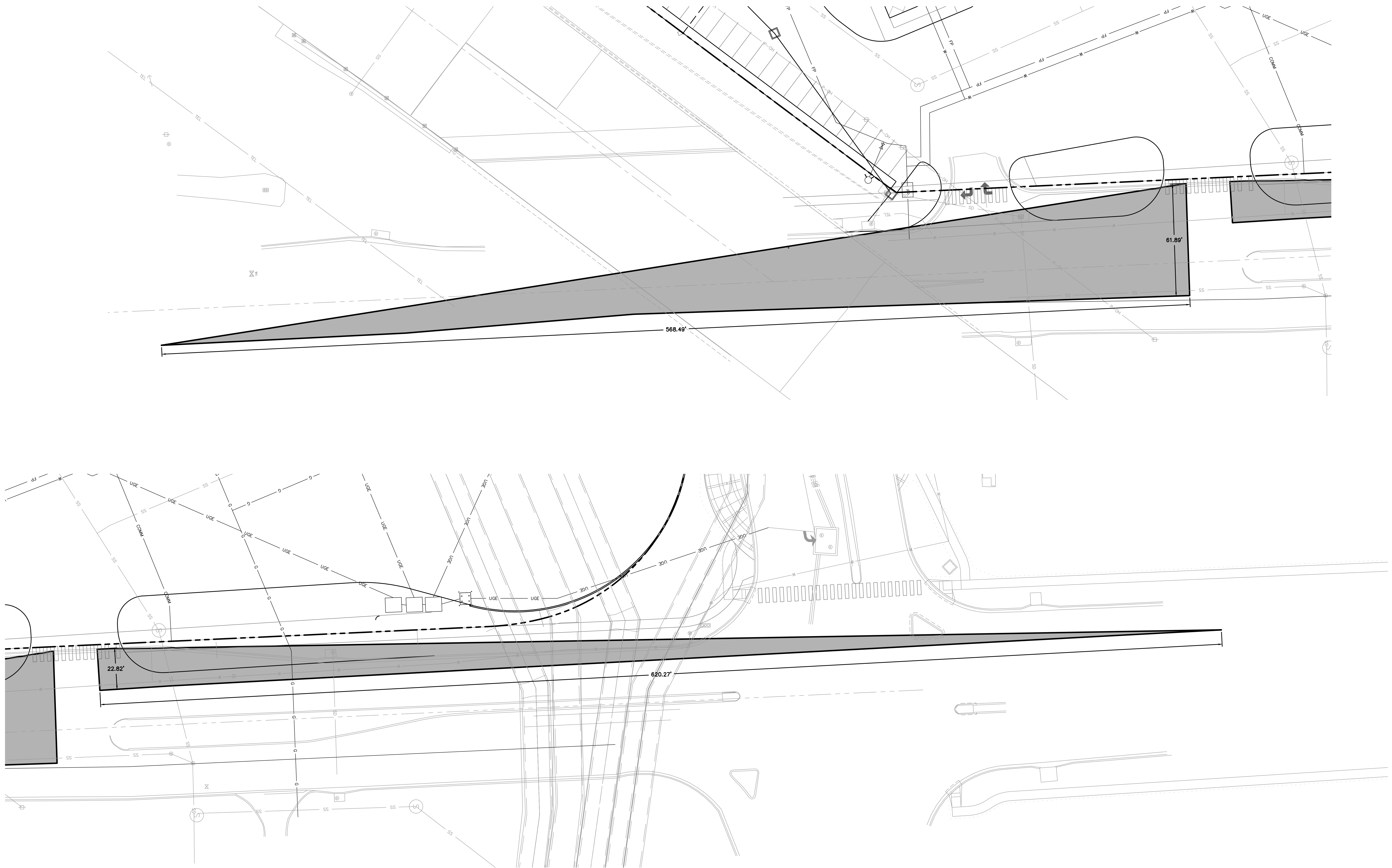
**OLSSON ASSOCIATES**

**pkmr**  
ENGINEERS

**B.D.C.**  
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01 SIGHT TRIANGLES

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

REVISIONS:  
CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:

CIVIL / LANDSCAPE:  
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MISSION GATEWAY

**el dorado**

**OLSSON ASSOCIATES**

**pkmr ENGINEERS**

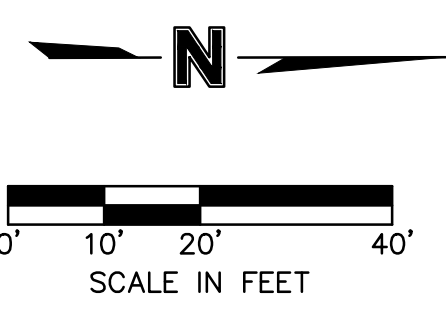
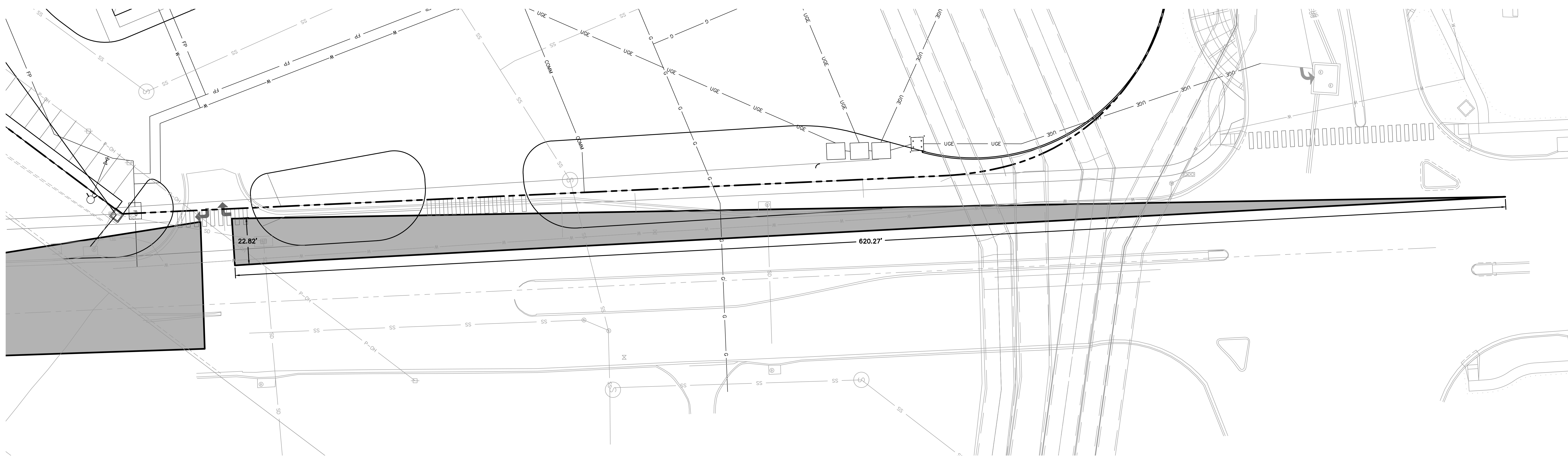
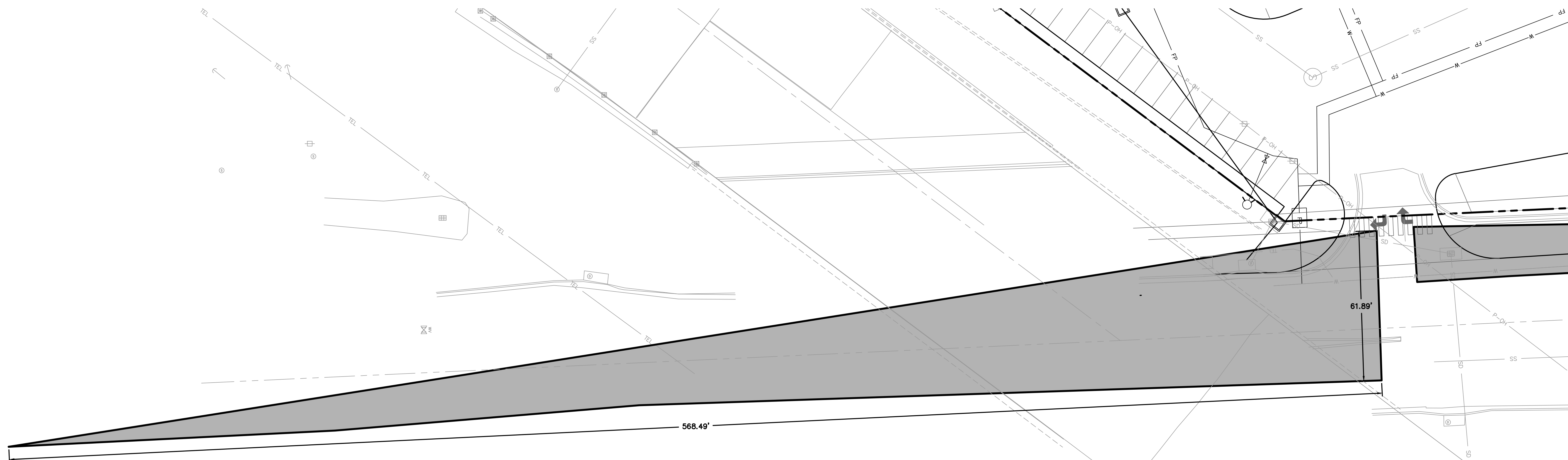
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**EXH2**

SIGHT TRIANGLES





01 SIGHT TRIANGLES

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

REVISIONS:  
CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:

CIVIL / LANDSCAPE:  
OLSSON ASSOCIATES  
7301 WEST 133RD ST., SUITE  
200  
OVERLAND PARK, KANSAS  
66213  
TEL: 913-381-1170  
FAX: 913-381-1174

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PKMR ENGINEERS  
13300 W. 98TH ST  
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FAX: 913-492-2437

STRUCTURAL:  
BOB D. CAMPBELL & CO., INC.  
4338 BELLEVUE AVE  
KANSAS CITY, MISSOURI 64111  
TEL: 816-531-4444  
FAX: 816-531-4872

MISSION GATEWAY

**el dorado**

**OLSSON ASSOCIATES**

**pkmr ENGINEERS**

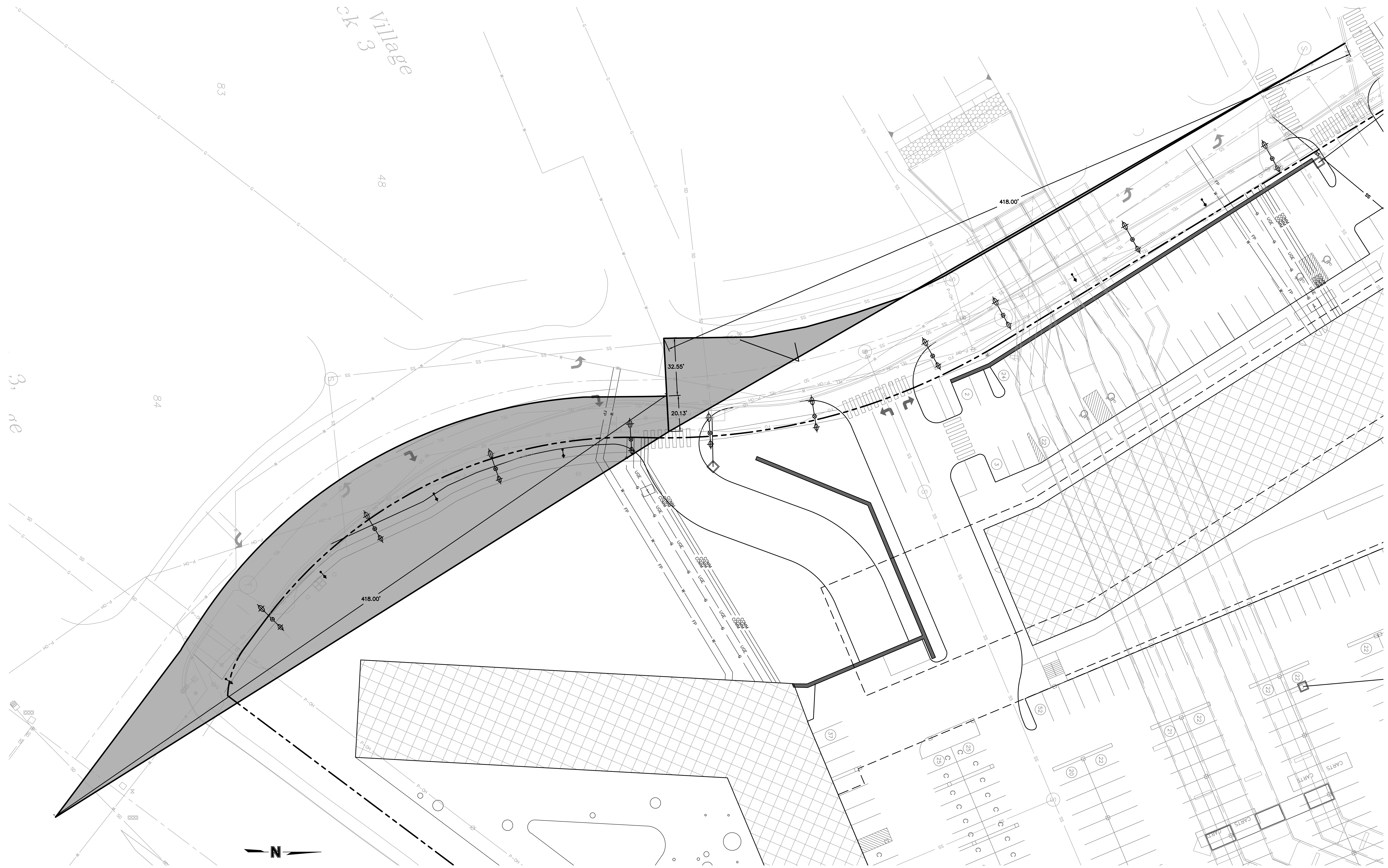
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**EXH3**

SIGHT TRIANGLES





01 SIGHT TRIANGLES

FINAL DEVELOPMENT PLAN  
MARCH 17, 2017

REVISIONS:  
CITY REVIEW COMMENTS 03.17.2017

CONSULTANTS:

CIVIL / LANDSCAPE:  
OLSSON ASSOCIATES  
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OVERLAND PARK, KANSAS  
66213  
TEL: 913-381-1170  
FAX: 913-381-1174

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PKMR ENGINEERS  
13300 W. 98TH ST  
LENEXA, KANSAS 66215  
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FAX: 913-492-2437

STRUCTURAL:  
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KANSAS CITY, MISSOURI 64111  
TEL: 816-531-4444  
FAX: 816-531-8072

MISSION GATEWAY

**el dorado**

**OLSSON ASSOCIATES**

**pkmr**  
ENGINEERS

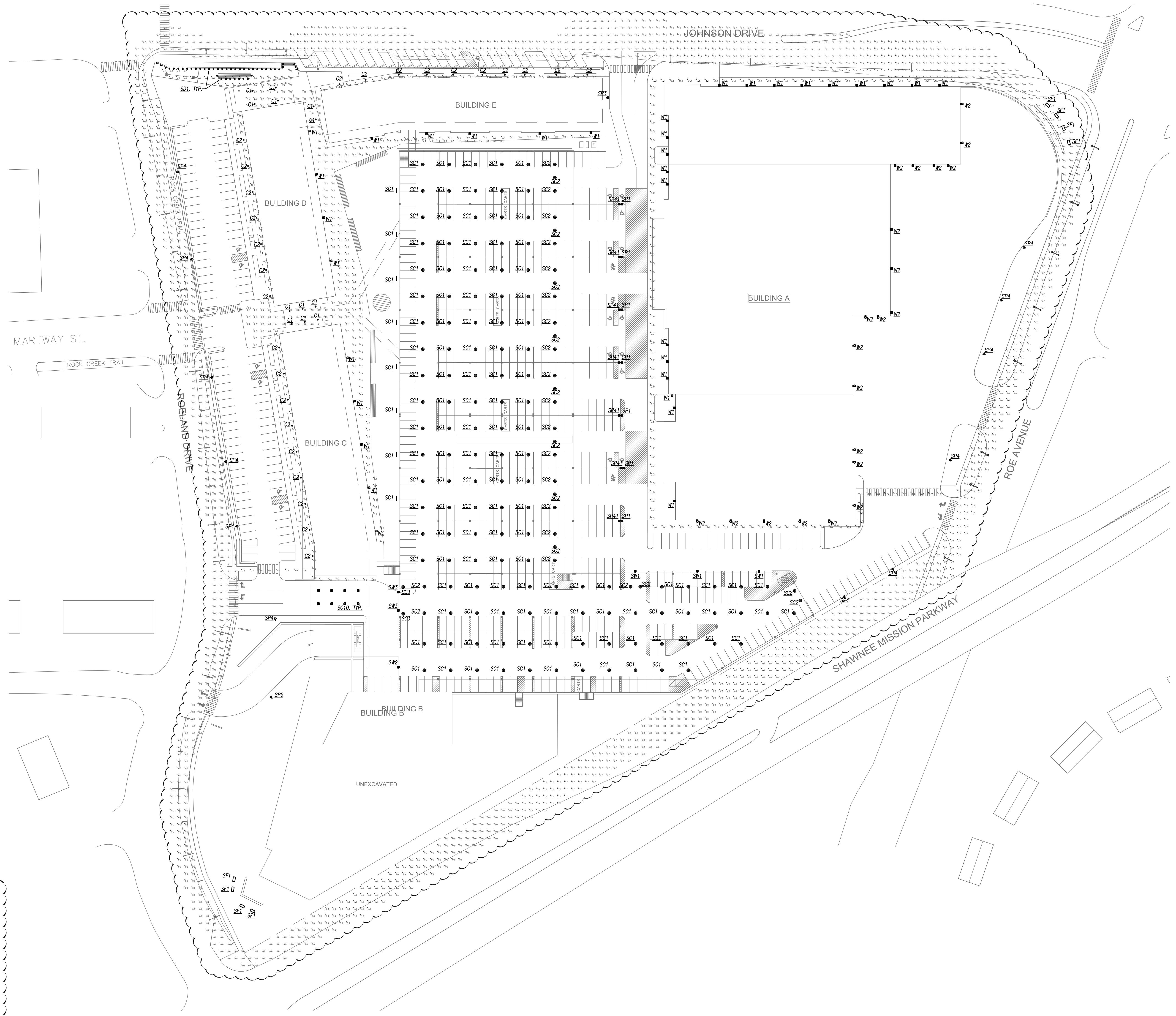
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**EXH4**

SIGHT TRIANGLES





DESCRIPTION	MAINTAINED LIGHTING LEVELS			UNIFORMITY	
	AVG. (F.C.)	MAX. (F.C.)	MIN. (F.C.)	MAX. / MIN.	AVG. / MIN.
BUILDING A WALKWAY	1.3	3.6	0.1	36.0 : 1	13.0 : 1
BUILDINGS C,D,E WALKWAYS	1.4	7.4	0.1	74.0 : 1	14.0 : 1
BOARDWALK	1.1	7.4	0.1	74.0 : 1	11.0 : 1

NOTES:  
 1. NUMBERS INDICATE FOOTCANDLE LEVELS AT GRADE.  
 2. CALCULATIONS PERFORMED USING VISUAL 2.05.  
 3. THERE SHALL BE NO DIRECT ILLUMINATION OF RESIDENTIAL PROPERTIES ADJACENT TO THIS PROPERTY OR ACROSS PUBLIC RIGHT-OF-WAY.

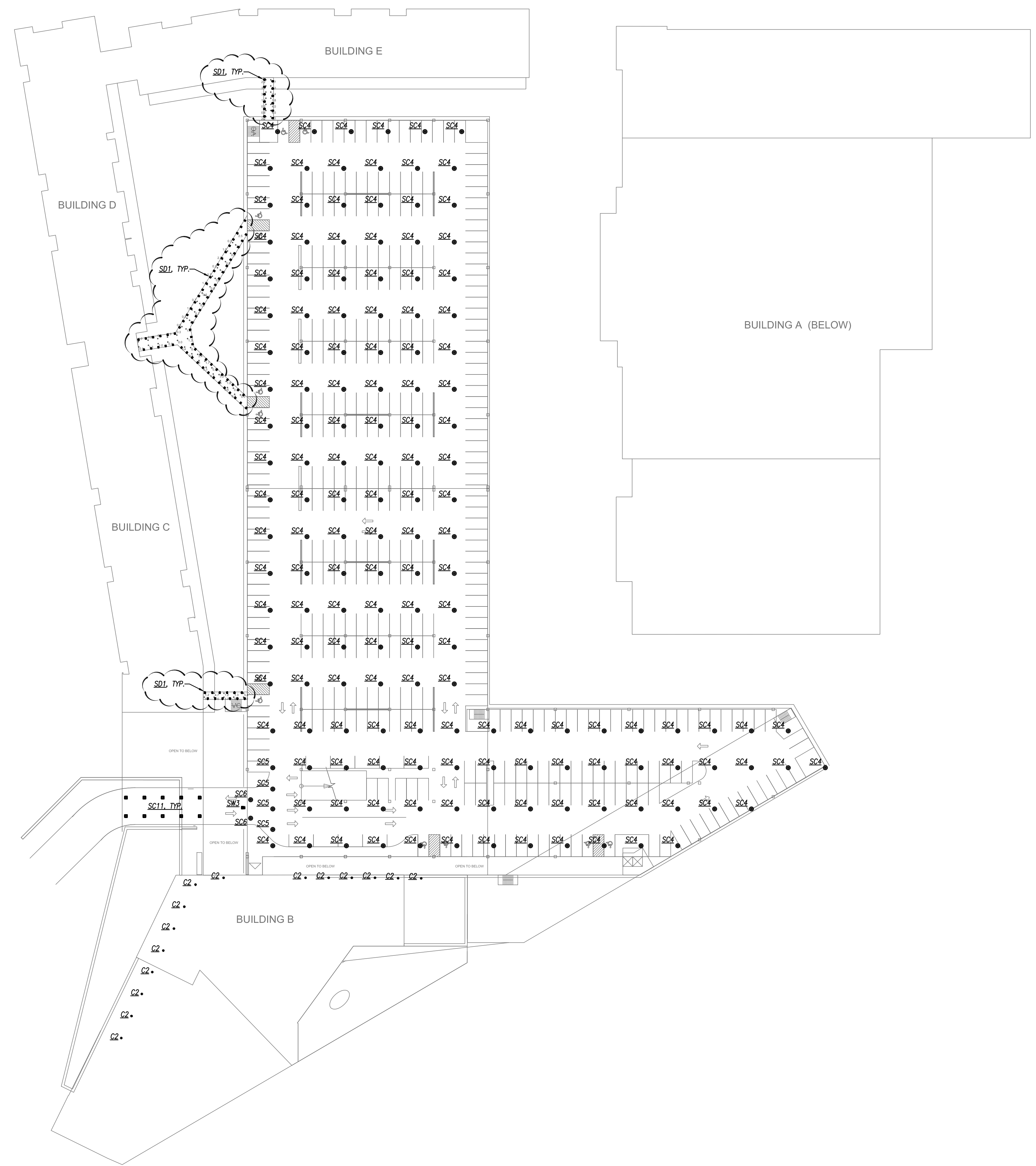


CONSULTANTS:  
 CIVIL / LANDSCAPE: OLSSON ASSOCIATES, 7301 WEST 133RD ST., SUITE 200, OVERLAND PARK, KANSAS 66213, TEL: 913-381-1170, FAX: 913-381-1174  
 M/E/P: PKMR ENGINEERS, 13300 W. 36TH ST., LENEXA, KANSAS 66215, TEL: 913-492-2400, FAX: 913-492-2437  
 STRUCTURAL: BCB & CAMPBELL & CO., INC., 4338 BELLEVUE AVE., KANSAS CITY, MISSOURI 64111, TEL: 816-531-4144, FAX: 816-531-8572

MISSION GATEWAY

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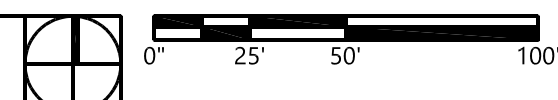
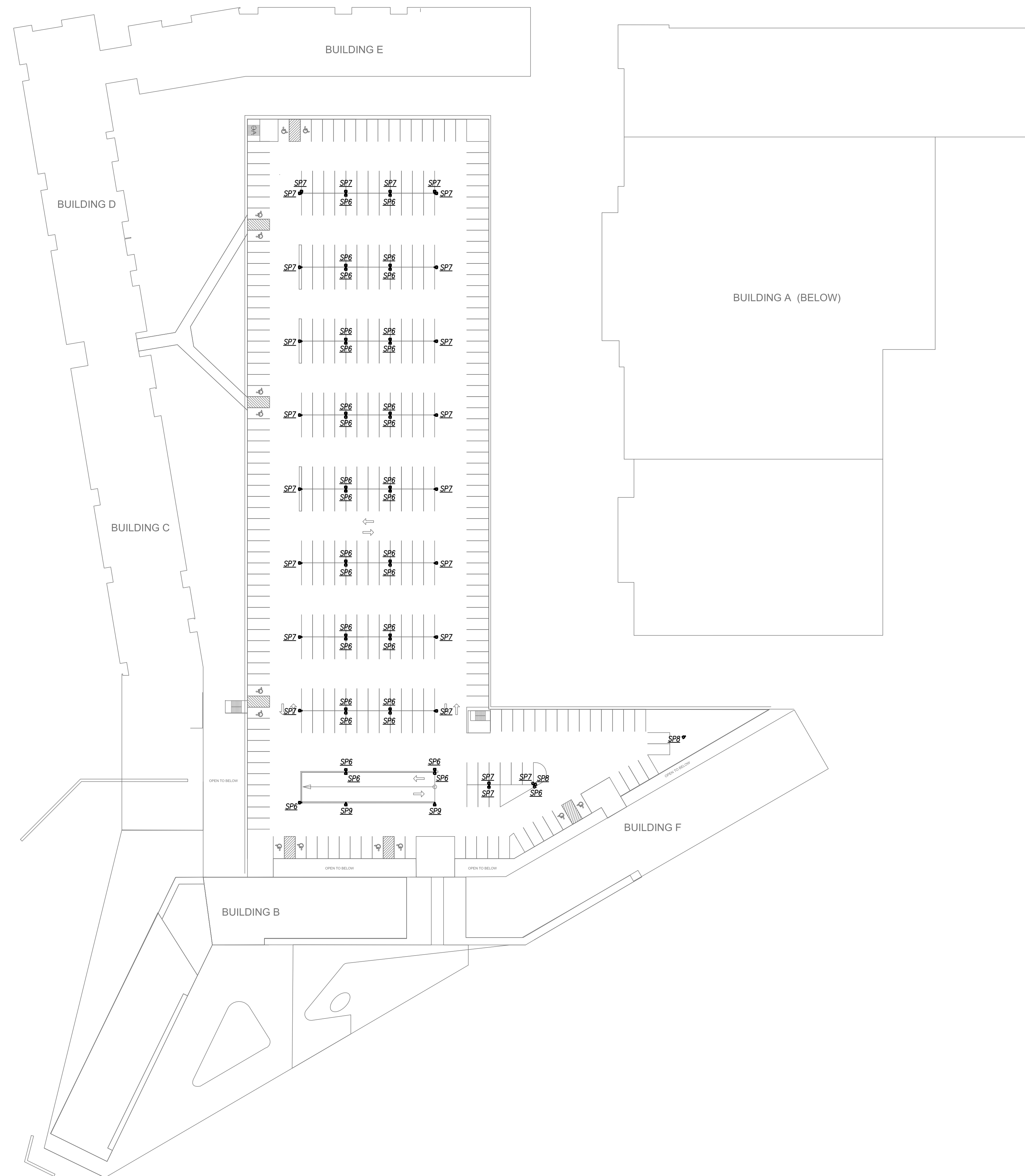


SITE STATISTICS						
DESCRIPTION	MAINTAINED LIGHTING LEVELS			UNIFORMITY		
	AVG. (F.C.)	MAX. (F.C.)	MIN. (F.C.)	MAX. / MIN.	AVG. / MIN.	
PEDESTRIAN BRIDGES	1.4	5.4	0.2	27.0 : 1	7.0 : 1	

NOTES:  
 1. NUMBERS INDICATE FOOTCANDLE LEVELS AT GRADE.  
 2. CALCULATIONS PERFORMED USING VISUAL 2.05.  
 3. THERE SHALL BE NO DIRECT ILLUMINATION OF RESIDENTIAL PROPERTIES ADJACENT TO THIS PROPERTY OR ACROSS PUBLIC RIGHT-OF-WAY.

CONSULTANTS:





CONSULTANTS:

CIVIL / LANDSCAPE:  
OLSSON ASSOCIATES  
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OVERLAND PARK, KANSAS 66213  
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M/E/P:  
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MISSION GATEWAY



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**DESCRIPTION**

The Galleon™ LED luminaire delivers exceptional performance in a highly scalable, low-profile design. Patented, high-efficiency AccuLED Optics™ system provides uniform and energy conscious illumination to walkways, parking lots, roadways, building areas and security lighting applications. IP66 rated.

**SPECIFICATION FEATURES**

**Construction**  
Extruded aluminum driver enclosure thermally isolated from Light Squares for optimal thermal performance. Heavy-wall, die-cast aluminum end caps enclose housing and die-cast aluminum heat sinks. A unique, patent pending interlocking housing and heat sink provides scalability with superior structural rigidity. 3G vibration tested. Optional tool-less hardware available for ease of entry into electrical chamber. Housing is IP66 rated.

**Optics**  
Choice of 16 patented, high-efficiency AccuLED Optics. The optics are precisely designed to shape the distribution maximizing efficiency and application spacing. AccuLED Optics create consistent distributions with the scalability to meet customized application requirements. Offered standard in 4000K (+/- 275K) CCT and minimum 70 CRI. Optional 6000K CCT and 3000K CCT. For the ultimate level of spill light control, an optional house side shield accessory can be field or factory installed. The

house side shield is designed to seamlessly integrate with the SL2, SL3, SL4 or AFL optics.

**Electrical**  
LED drivers are mounted to removable tray assembly for ease of maintenance. 120-277V 50/60Hz, 347V 60Hz or 480V 60Hz operation. 480V is compatible for use with 480V Wye systems only. Standard with 0-10V dimming. Shipped standard with Eaton proprietary circuit module designed to withstand 10kV of transient line surge. The Galleon LED luminaire is suitable for operation in -40°C to 40°C ambient environments. For applications with ambient temperatures exceeding 40°C, specify the HA (High Ambient) option. Light Squares are IP66 rated. Greater than 90% lumen maintenance expected at 60,000 hours. Available in standard 1A drive current and optional 530mA and 700mA drive currents.

**Mounting**  
Extruded aluminum arm includes internal bolt guides allowing for easy positioning of fixture during

assembly. Designed for pole or wall mounting. When mounting two or more luminaires at 90° or 120° apart, the EA extended arm may be required. Refer to the arm mounting requirement table on page 3. Round pole top adapter included. For wall mounting, specify wall mount bracket option. 3G vibration rated.

**Finish**  
Housing finished in super durable TGIC polyester powder coat paint. 2.5 mil nominal thickness for superior protection against fade and wear. Heat sink is powder coated black. Standard colors include black, bronze, grey, white, dark platinum and graphite metallic. RAL and custom color matches available. Consult the McGraw-Edison Architectural Colors brochure for the complete selection.

**Warranty**  
Five-year warranty.

**McGraw-Edison**

Catalog #	Type
Project	
Comments	Date
Prepared by	

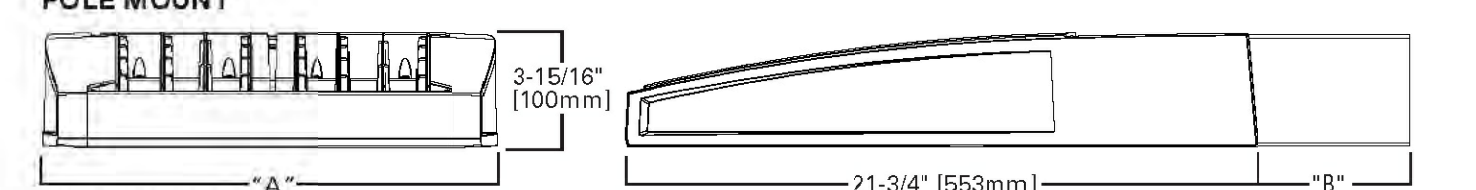


**GLEON GALLEON LED**

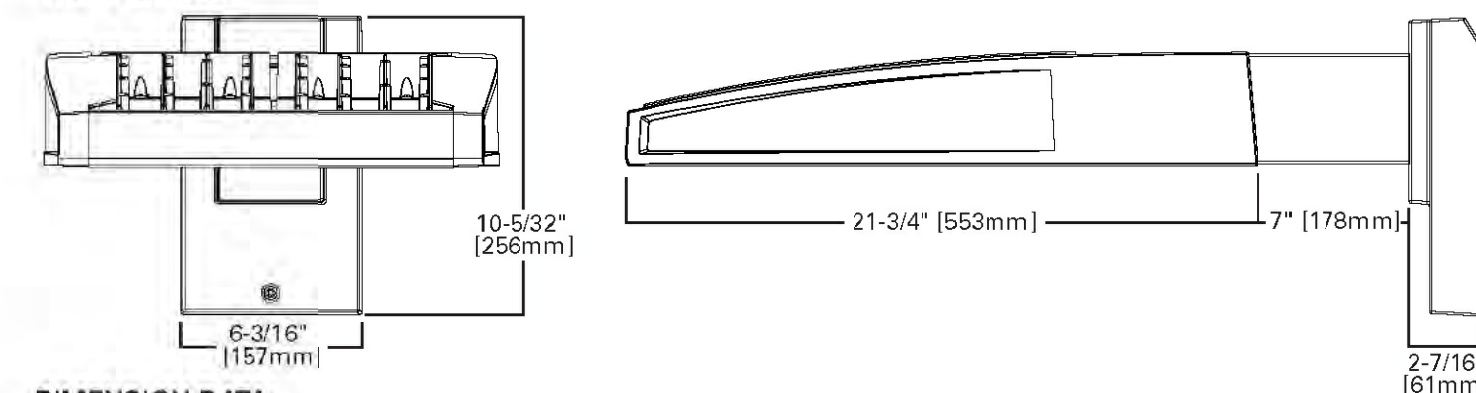
1-10 Light Squares  
Solid State LED  
AREA/SITE LUMINAIRE

**DIMENSIONS**

**POLE MOUNT**



**WALL MOUNT**



**DIMENSION DATA**

Number of Light Squares	"A" Width	"B" Standard Arm Length	"B" Optional Arm Length 1	Weight with Arm (lbs.)	EPA with Arm 2 (Sq. Ft.)
1-4	15-1/2" (394mm)	7" (178mm)	10" (254mm)	33 (15.0 kgs.)	0.96
5-6	21-5/8" (549mm)	7" (178mm)	10" (254mm)	44 (20.0 kgs.)	1.00
7-8	27-5/8" (702mm)	7" (178mm)	13" (330mm)	54 (24.5 kgs.)	1.07
9-10	33-3/4" (857mm)	7" (178mm)	16" (406mm)	63 (28.6 kgs.)	1.12

NOTES: 1 Optional arm length may be required when mounting two or more fixtures per pole at 90° or 120°. Refer to arm mounting requirement table. 2 EPA calculated with optional arm length.



\*www.designlights.org



**CERTIFICATION DATA**

ULcUL Wet Location Listed  
ISO 9001  
LM79 / LM80 Compliant  
3G Vibration Rated  
IP66 Rated  
DesignLights Consortium® Qualified\*

**ENERGY DATA**

Electronic LED Driver  
50.9 Power Factor  
<20% Total Harmonic Distortion  
120V-277V 50/60Hz  
347V & 480V 60Hz  
-40°C Min. Temperature  
40°C Max. Temperature  
50°C Max. Temperature (HA Option)



TD600020EN  
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**LIGHT FIXTURE SCHEDULE - SITE LIGHTING**

FIXTURE TYPE	MANUFACTURER	CATALOG NUMBER	DESCRIPTION	LAMP NUMBER / DESCRIPTION
C1	HALO COMMERCIAL	LD4A SERIES	4" ROUND MEDIUM BEAM RECESSED DOWNLIGHT. NEW CONSTRUCTION HOUSING. EXTRUDED ALUMINUM HEATSINK. SELF-FLANGED ALUMINUM LOWER REFLECTOR AND LENSED UPPER OPTICAL CHAMBER FOR GLARE REDUCTION. 5-11/16" OVERALL HEIGHT. UL LISTED FOR INSTALLATION IN PROTECTED WET LOCATIONS. PROVIDE WITH IP66 GASKET KIT. COORDINATE FINISH WITH ARCHITECT. 0-10V 10% DIMMING DRIVER.	1,300 LUMEN LED, 22.6 WATTS, 4000K, 80 CRI.
C2	HALO COMMERCIAL	LD4A SERIES	SAME AS FIXTURE 'C1' EXCEPT LUMEN OUTPUT.	900 LUMEN LED, 14 WATTS, 4000K, 80 CRI.
SC1	MCGRAW-EDISON	TT SERIES	ROUND PENDANT-MOUNTED LOW-BAY CANOPY/PARKING GARAGE LUMINAIRE. ONE PIECE DIE-CAST ALUMINUM HOUSING WITH SLOPED TOP AND EASY-ACCESS METAL ELECTRICAL TRAY. INJECTION-MOLDED ACRYLIC GLARE-REDUCING OPTICAL LENS. POLYESTER POWDER COAT PAINT. IP66 RATED AGAINST INGRESS OF DUST AND WATER. INTEGRAL TRANSIENT LINE SURGE SUPPRESSION UP TO 10KV. HIGH AMBIENT OPTION RATED FOR UP TO 50 DEGREES CELCIUS. WIDE DISTRIBUTION.	7,000 LUMEN LED, 58 WATTS, 4000K, 70 CRI MINIMUM.
SC2	MCGRAW-EDISON	TT SERIES	SAME AS FIXTURE 'C1' EXCEPT MEDIUM DISTRIBUTION.	7,000 LUMEN LED, 58 WATTS, 4000K, 70 CRI MINIMUM.
SC3	MCGRAW-EDISON	TT SERIES	SAME AS FIXTURE 'C1' EXCEPT CONCENTRATED DISTRIBUTION.	7,000 LUMEN LED, 58 WATTS, 4000K, 70 CRI MINIMUM.
SC4	MCGRAW-EDISON	TT SERIES	SAME AS FIXTURE 'C1' EXCEPT LUMEN OUTPUT.	5,000 LUMEN LED, 45 WATTS, 4000K, 70 CRI MINIMUM.
SC5	MCGRAW-EDISON	TT SERIES	SAME AS FIXTURE 'C1' EXCEPT LUMEN OUTPUT AND MEDIUM DISTRIBUTION.	5,000 LUMEN LED, 45 WATTS, 4000K, 70 CRI MINIMUM.
SC6	MCGRAW-EDISON	TT SERIES	SAME AS FIXTURE 'C1' EXCEPT LUMEN OUTPUT AND CONCENTRATED DISTRIBUTION.	5,000 LUMEN LED, 45 WATTS, 4000K, 70 CRI MINIMUM.
SC10	CREE	304 SERIES	RECTANGULAR RECESSED SOFFIT LUMINAIRE. STEEL MOUNTING FRAME WITH 1-1/2" APERTURE THROAT AND UNIVERSAL MOUNTING BRACKETS. DIE CAST AND EXTRUDED ALUMINUM HOUSING WITH WEATHERPROOF DRIVER CHAMBER IN CENTER TO ALLOW ACCESS FROM BELOW. COORDINATE FINISH WITH ARCHITECT. INTEGRAL SURGE SUPPRESSION. 700mA DRIVER WITH 0-10V DIMMING. SYMMETRIC PETROLEUM DISTRIBUTION.	SIXTY (60) LED ARRAY, 13,581 LUMENS, 133 WATTS, 4000K, 70 CRI MINIMUM.
SC11	CREE	304 SERIES	SAME AS TYPE 'SC10' EXCEPT DRIVE CURRENT AND LUMEN OUTPUT. 525mA DRIVER.	SIXTY (60) LED ARRAY, 9,311 LUMENS, 101 WATTS, 4000K, 70 CRI MINIMUM.
SD1	WAC LIGHTING	LEDme	HORIZONTAL RECTANGULAR DIE CAST ALUMINUM STEP LIGHT. LOW PROFILE FLUSH TO WALL AESTHETICS WITH NO VISIBLE HARDWARE. UL LISTED FOR WET LOCATIONS. FINISH PER ARCHITECT. INTEGRAL DRIVER. DIMMABLE TO 10% VIA ELV.	4 WATT LED, 3000K, 85 CRI
SF1	ACCLAIM	DYNAFLOOD SO	LED FLOOD LIGHT. DIE CAST ALUMINUM HOUSING, IP66 RATED FOR USE IN WET LOCATIONS. 40 DEGREE BEAM ANGLE. DIMMABLE VIA ELV/TRIAC.	25 WATT LED, 1176 LUMENS, 4000K
SG1	ACCLAIM	DYNAGRAZE EXTERIOR HO	OUTDOOR 4'-0" LINEAR WALL GRAZING FIXTURE. ANODIZED BRUSHED ALUMINUM HOUSING WITH POLYCARBONATE LENS. IP66 RATED FOR WET LOCATIONS. 30 DEGREE SWIVEL MOUNTING BRACKETS. INTEGRAL 3-PIN CONNECTORS ON BOTH ENDS. 10 X 60 - DEGREE BEAM OPTIC. DIMMABLE VIA TRIAC/ELV.	4,500 LUMEN LED, 80 WATTS, 4000K, 87 CRI.
SW1	MCGRAW-EDISON	GALLEON LED	WALL-MOUNTED AREA LUMINAIRE. EXTRUDED RECTANGULAR ALUMINUM HOUSING WITH DIE-CAST ALUMINUM END CAPS. EXTRUDED ALUMINUM MOUNTING ARM. 3G VIBRATION TESTED. POLYESTER POWDER COAT PAINT. IP66 RATED AGAINST INGRESS OF DUST AND WATER. LED CONFIGURATION WITH INDIVIDUAL ACRYLIC LENSES. INTEGRAL TRANSIENT LINE SURGE SUPPRESSION UP TO 10KV. HIGH AMBIENT OPTION RATED FOR UP TO 50 DEGREES CELCIUS. IES TYPE II DISTRIBUTION WITH SPILL CONTROL.	ONE (1) LED LIGHT SQUARE, 5,263 LUMENS, 56 WATTS, 4000K, 70 CRI MINIMUM.
SW2	MCGRAW-EDISON	GALLEON LED	SAME AS FIXTURE 'SW1' EXCEPT DISTRIBUTION AND LUMEN OUTPUT. IES TYPE III DISTRIBUTION WITH SPILL CONTROL.	TWO (2) LED LIGHT SQUARES, 9,976 LUMENS, 107 WATTS, 4000K, 70 CRI MINIMUM.
SW3	MCGRAW-EDISON	GALLEON LED	SAME AS FIXTURE 'SW1' EXCEPT DISTRIBUTION AND LUMEN OUTPUT. IES TYPE IV DISTRIBUTION WITH SPILL CONTROL.	FIVE (5) LED LIGHT SQUARES, 24,370 LUMENS, 264 WATTS, 4000K, 70 CRI MINIMUM.
SP1	MCGRAW-EDISON	GALLEON LED	SAME AS FIXTURE 'SW1' EXCEPT MOUNTING. POLE MOUNTED @ 15'-0" TO BOTTOM OF FIXTURE. PROVIDE WITH SQUARE STRAIGHT POLE MOUNTED ON 3'-0" TALL CONCRETE BASE. IES TYPE II DISTRIBUTION WITH SPILL CONTROL.	ONE (1) LED LIGHT SQUARE, 5,263 LUMENS, 56 WATTS, 4000K, 70 CRI MINIMUM.
SP12	MCGRAW-EDISON	GALLEON LED	SAME AS FIXTURE 'SP1' EXCEPT MOUNTING HEIGHT. POLE MOUNTED @ 20'-0" TO BOTTOM OF FIXTURE. PROVIDE WITH SQUARE STRAIGHT POLE MOUNTED ON 3'-0" TALL CONCRETE BASE.	ONE (1) LED LIGHT SQUARE, 5,263 LUMENS, 56 WATTS, 4000K, 70 CRI MINIMUM.
SP3	MCGRAW-EDISON	GALLEON LED	SAME AS FIXTURE 'SP1' EXCEPT MOUNTING HEIGHT, DISTRIBUTION, AND LUMEN OUTPUT. POLE MOUNTED @ 20'-0" TO BOTTOM OF FIXTURE. PROVIDE WITH SQUARE STRAIGHT POLE MOUNTED ON 3'-0" TALL CONCRETE BASE. IES TYPE III DISTRIBUTION WITH SPILL CONTROL.	TWO (2) LED LIGHT SQUARES, 9,976 LUMENS, 107 WATTS, 4000K, 70 CRI MINIMUM.
SP4	MCGRAW-EDISON	GALLEON LED	SAME AS FIXTURE 'SP1' EXCEPT DISTRIBUTION AND LUMEN OUTPUT. POLE MOUNTED @ 15'-0" TO BOTTOM OF FIXTURE. PROVIDE WITH SQUARE STRAIGHT POLE MOUNTED ON 3'-0" TALL CONCRETE BASE. IES TYPE IV DISTRIBUTION WITH SPILL CONTROL AND HOUSE-SIDE SHIELD.	TWO (2) LED LIGHT SQUARES, 9,976 LUMENS, 107 WATTS, 4000K, 70 CRI MINIMUM.
SP41	MCGRAW-EDISON	GALLEON LED	SAME AS FIXTURE 'SP4' EXCEPT MOUNTING HEIGHT. POLE MOUNTED @ 20'-0" TO BOTTOM OF FIXTURE. PROVIDE WITH SQUARE STRAIGHT POLE MOUNTED ON 3'-0" TALL CONCRETE BASE. IES TYPE IV DISTRIBUTION WITH SPILL CONTROL AND HOUSE-SIDE SHIELD.	TWO (2) LED LIGHT SQUARES, 9,976 LUMENS, 107 WATTS, 4000K, 70 CRI MINIMUM.
SP5	MCGRAW-EDISON	GALLEON LED	SAME AS FIXTURE 'SP1' EXCEPT DISTRIBUTION. IES TYPE III DISTRIBUTION WITH SPILL CONTROL.	ONE (1) LED LIGHT SQUARE, 5,263 LUMENS, 56 WATTS, 4000K, 70 CRI MINIMUM.
SP6	MCGRAW-EDISON	GALLEON LED	SAME AS FIXTURE 'SP1' EXCEPT MOUNTING HEIGHT AND DISTRIBUTION. POLE MOUNTED @ 13'-0" TO BOTTOM OF FIXTURE. PROVIDE WITH SQUARE STRAIGHT POLE MOUNTED ON 3'-0" TALL CONCRETE BASE. IES TYPE IV WIDE DISTRIBUTION.	ONE (1) LED LIGHT SQUARE, 5,263 LUMENS, 56 WATTS, 4000K, 70 CRI MINIMUM.
SP7	MCGRAW-EDISON	GALLEON LED	SAME AS FIXTURE 'SP6' EXCEPT IES TYPE IV FORWARD THROW DISTRIBUTION.	ONE (1) LED LIGHT SQUARE, 5,263 LUMENS, 56 WATTS, 4000K, 70 CRI MINIMUM.
SP8	MCGRAW-EDISON	GALLEON LED	SAME AS FIXTURE 'SP6' EXCEPT OUTPUT AND IES TYPE IV FORWARD THROW DISTRIBUTION.	TWO (2) LED LIGHT SQUARES, 9,976 LUMENS, 107 WATTS, 4000K, 70 CRI MINIMUM.
SP9	MCGRAW-EDISON	GALLEON LED	SAME AS FIXTURE 'SP6' EXCEPT OUTPUT.	TWO (2) LED LIGHT SQUARES, 9,976 LUMENS, 107 WATTS, 4000K, 70 CRI MINIMUM.
W1	H.E. WILLIAMS	VMPH SERIES	EXTERIOR WALL-MOUNTED FIXTURE. DIE CAST ALUMINUM HOUSING WITH INTEGRAL DIE-CAST ALUMINUM HEATSINK. INJECTION MOLDED ACRYLIC LENS. INTEGRAL SURGE SUPPRESSION. STANDARD 0-10V DIMMING DRIVER. COORDINATE FINISH WITH ARCHITECT. IES TYPE III OPTICAL DISTRIBUTION.	3,000 LUMEN LED, 36 WATTS, 4000K, 70 CRI.
W2	H.E. WILLIAMS	VMPH SERIES	SAME AS FIXTURE TYPE 'W1' EXCEPT FORWARD THROW DISTRIBUTION.	3,000 LUMEN LED, 36 WATTS, 4000K, 70 CRI.

**CONSULTANTS:**

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7301 WEST 133RD ST., SUITE 200  
OVERLAND PARK, KANSAS 66213  
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PKMR ENGINEERS  
13300 W. 36TH ST.  
LENEXA, KANSAS 66215  
TEL: 913-492-2400  
FAX: 913-492-2437

STRUCTURAL:  
BOB D. CAMPBELL & CO., INC.  
4338 BELLEVUE AVE.  
KANSAS CITY, MISSOURI 64111  
TEL: 816-531-4144  
FAX: 816-531-8572

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ACCLAIM  
LIGHTING

## DynaFlood SO



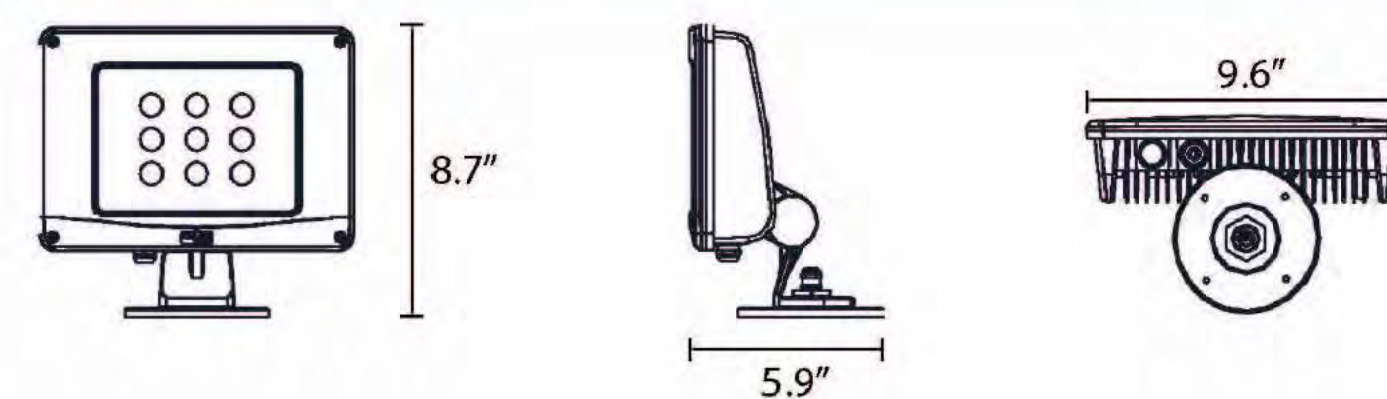
The **DynaFlood SO** is a high powered flood fixture designed for exterior illumination and outdoor signage. Its integral power supply is universal voltage and can be dimmed via ELV or TRIAC. The head pivots 180° to help you get the exact angle needed for your application.

### SPECIFICATIONS

<b>Color Temperature</b>	2700k, 3000K, 3500K, 4000k
<b>Beam Angle</b>	7° Standard, 20°, 40°, 10° x 60°, 60° via LSF Filter
<b>Drive Current</b>	500ma
<b>Power Consumption</b>	25W
<b>Total Lumens</b>	1176 @ 4000K, 7°
<b>Efficacy (lm/W)</b>	47
<b>CRI</b>	80
<b>Lumen Maintenance</b>	70% @ 50,000 hours
<b>Housing</b>	Die Cast Aluminum
<b>Operating Temperature</b>	-40°F to 122°F (-40°C to 50°C)
<b>Operating Voltage</b>	90-135VAC / 200-300VAC
<b>Dimming</b>	ELV, TRIAC
<b>Fixture Connections</b>	2 Conductor, Bare Wire Leads
<b>Rating</b>	IP 66, Wet Location
<b>Warranty</b>	5 Years
<b>Weight</b>	6 lbs (2.7 kg)
<b>Dimensions</b>	W 9.6" x H 13.9" x D 5.9" (245 mm x 221 mm x 149.5mm)

**Certifications**

### DIMENSIONS



Client:	
Project:	
Type:	
Order Code:	
Quantity:	

ACCLAIM  
LIGHTING

## DynaGraze Exterior



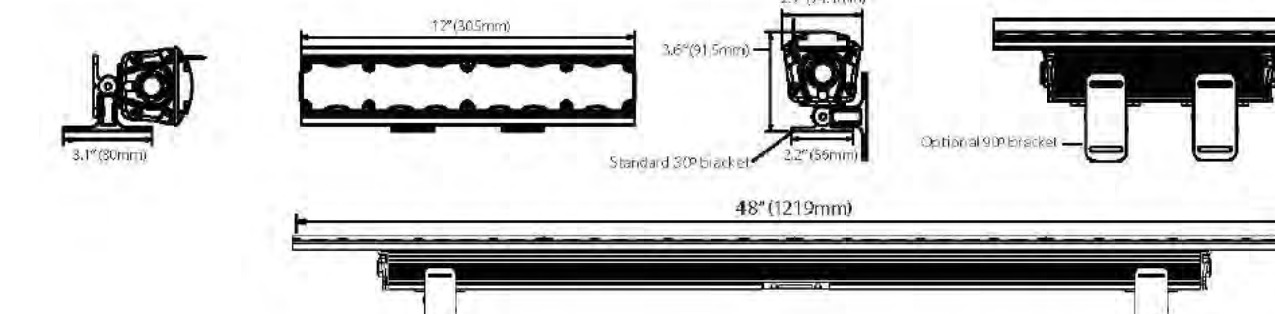
The **DynaGraze Exterior** is a high power, white LED, outdoor linear fixture. Available in 1' and 4' sections, it offers smooth linear dimming with ELV modules and 1-100% with most commercially available TRIAC dimmer. Featuring class-leading output, robust housing, LSF filter options, and a glare shield, it is the perfect option for any structure that calls for outdoor linear grazing.

### SPECIFICATIONS

<b>Color Temperature</b>	2,700K, 3000K, 3500K, 4000K
<b>Beam Angle</b>	10°, 40°, 10° x 60°, 30° x 60°, 60° (via LSF)
<b>Max Fixture Runs</b>	35' (10.6m) @ 120V / 70' (21.3m) @ 240V+
<b>Power Consumption</b>	20w (1') / 80w (4')
<b>Total Lumens</b>	834 (1') @ 2700k - (10x60) / 4,570 (4') @ 4000k - (30x60)
<b>Efficacy (lm/W)</b>	34 @ 2700k / 38 @ 3000k, 41 @ 3500k, 50 @ 4000k
<b>CRI</b>	87 @ 4000K
<b>Lumen Maintenance</b>	70% @ 50,000 Hours
<b>Finish</b>	Anodized Brushed Aluminum
<b>Housing</b>	Aluminum; Polycarbonate Lens
<b>Operating &amp; Storage Temp</b>	-40°F to 122°F (-40°C to 50°C)
<b>Operating Voltage</b>	90-135VAC, 200-300VAC
<b>Dimming</b>	ELV or TRIAC
<b>Fixture Connectors</b>	Integral, 3 Pin Male & Female Connector
<b>Rating</b>	IP 66, Wet Location
<b>Warranty</b>	5 Years
<b>Weight</b>	1' 3 lbs (1.4 kg) 4' 12 lbs (5.4 kg)
<b>Dimensions</b>	W 2.9" x H 3.6" x L 12" / 48" (74.1 mm x 91.5 mm x 305 mm / 1,220 mm)

**Certifications**

### DIMENSIONS



www.acclaimlighting.com Lumen measurements comply with IES LM-79-08 provided by a recognized, independent testing lab. © 2017 Photometry, L&L LED source, compliance documents, and EIS files are available at [www.acclaimlighting.com](http://www.acclaimlighting.com). Specification subject to change without notice.



#### CONSULTANTS:

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**Model: WL-LED100**  
LEDme® Step Light

**WAC LIGHTING**  
Responsible Lighting®



Fixture Type:

Catalog Number:

Project:

Location:

**PRODUCT DESCRIPTION**

Horizontal rectangle LEDme® Step Light. Designed for safety and style on stairways, patios, decks, balcony areas, walkways and building perimeters.

Features an architectural design. Energy efficient for long-lasting indoor and outdoor lighting solutions. Creates an attractive, romantic impression at night.

**FEATURES**

- Solid diecast brass, corrosion resistant aluminum alloy, or cast stainless steel construction
- Direct wiring, no driver needed
- Low profile, flush to wall aesthetics with no visible hardware
- 40,000 hour rated life
- Balanced lighting, free of shadows with minimum glare
- IP66 rated. Protected against high-pressure water jets
- Up to 200 fixtures can be connected in parallel
- Replaceable LED module
- 5 year WAC Lighting product warranty.

**SPECIFICATIONS**

**Construction:** Die-cast aluminum or 316 marine grade cast stainless steel

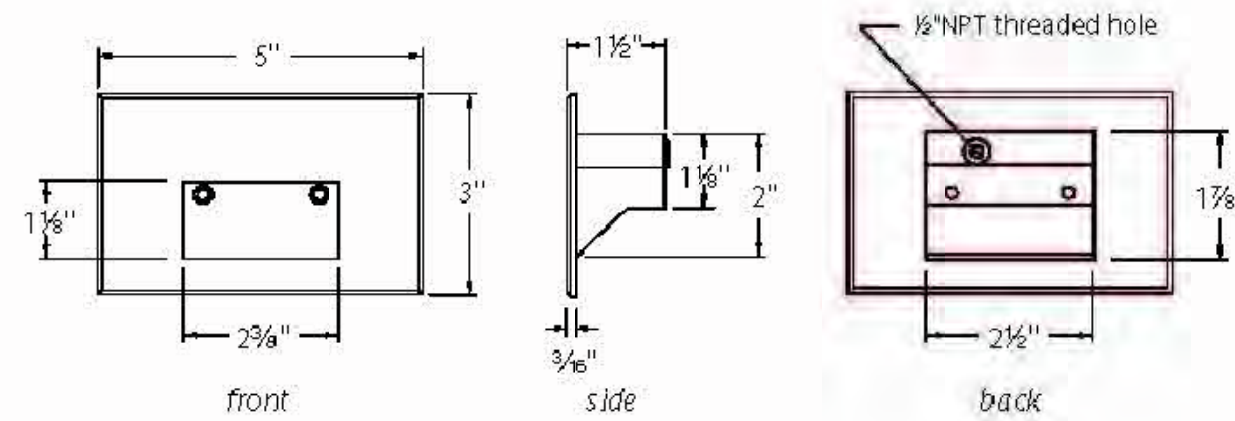
**Power:** Direct wiring, no remote driver needed. Input voltage: 120V or 277VAC 50/60Hz

**Light Source:** 3000K CCT Samsung HV-AC High Power LED, CR1-65 Optional color lenses. Total power consumption of 3.0W

**Mounting:** Fits into 2" x 4" J-Box with minimum inside dimensions of 3"L x 2"W x 2"H Includes bracket for J-Box mount

**Dimming:** Dim to 10% with electronic low voltage (ELV) dimmer Approved dimmers: Lutron Nova-T NTELV-300 & NTELV-600, Lutron Vireti VTELV-600, Lutron Diva DVELV-300P, Lutron Skylark SELV-300P, Lutron Maestro MAELV-600

**Standards:** IP66, UL & cUL Listed for wet locations



**ORDER NUMBER**

Model #	Light Color	Finish
WL-LED100	C White 3000K	SS Stainless Steel
WL-LED100F	AM Amber 610nm	BK Black on Aluminum
	RD Red 640nm	WT White on Aluminum
	BL Blue 430nm	BN <sup>®</sup> Brushed Nickel on Aluminum
		BZ Bronze on Aluminum
WL-LED100	C White 3000K	BBR Bronze on Brass
	AM Amber 610nm	

\*Brushed Nickel Finish is for interior use only



Example: WL-LED100F-BL-SS

wacighting.com Phone (800) 526-2588 Fax (800) 526-2585  
 Headquarters/Eastern Distribution Center 44 Harbor Park Drive Port Washington, NY 11050  
 Central Distribution Center 1600 Distribution Ct Lithia Springs, GA 30122  
 Western Distribution Center 1750 Archibald Avenue Ontario, CA 91760

WAC Lighting retains the right to modify the design of our products at any time as part of the company's continuous improvement program. AUG 2016

**304 Series™**

LED Recessed Soffit Luminaire

**Product Description**

High performance energy efficient LED down light, designed for use in drop ceilings with 20-24" (508-610mm) on center building construction. Heavy gauge steel recessed mounting frame features 1-1/2" (38mm) deep aperture throat to accommodate most standard ceiling thicknesses, and universal mounting brackets that accept 1/2" (13mm) EMT conduit, C-channel mounting bars or flat bar hangers (all by others) with 5" (127mm) vertical adjustment from either above or below the ceiling. Luminaire is constructed from rugged die cast and extruded aluminum components. LED driver is mounted in a sealed weather-tight center chamber that allows for access from below the luminaire. High performance heat sinks specifically designed for LED recessed soffit applications.

**Applications:** Petroleum stations, convenience stores, drive-thru banks and restaurants, retail and grocery

**Performance Summary**

Patented NanoOptic® Product Technology

Made in the U.S.A. of U.S. and imported parts

CRI: Minimum 70 CRI

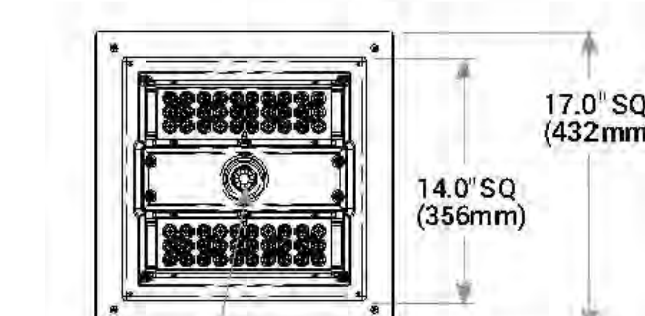
CCT: 4000K (+/- 300K), 5700K (+/- 500K) standard

Limited Warranty\*: 10 years on luminaire/10 years on Colorfast DeltaGuard® finish

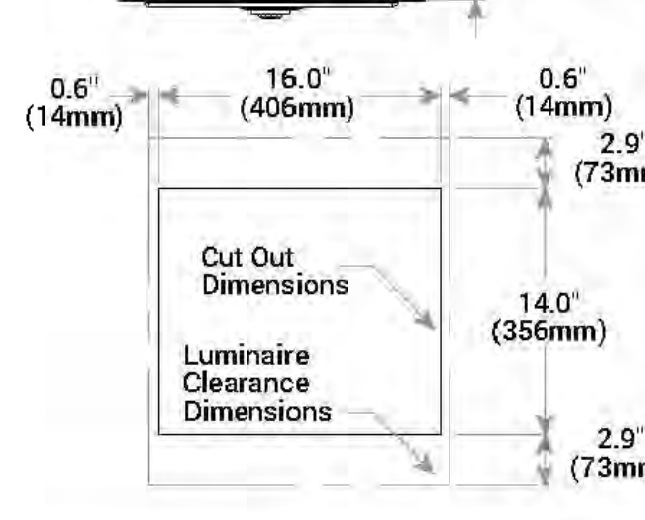
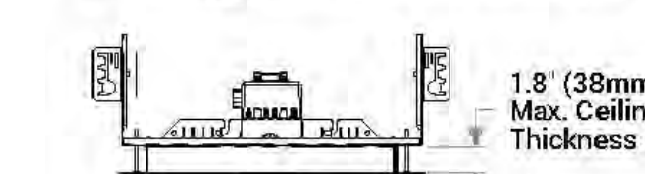
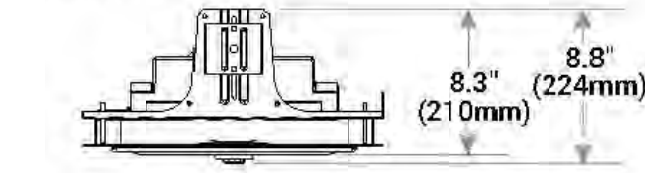
\*See www.cree.com/lighting/products/warranty for warranty terms

**Accessories**

Field-Installed	Mounting C-Channel
Hand-Held Remote XA-SERIES888 For successful implementation of this programmable multi-level option, a minimum of one hand-held remote is required	XA-AC14 Pair of 14" (356mm) rigid 3/4" x 1/2" (19mm x 13mm) galvanized C-Channel bars
NA-AC22 Pair of 22" (559mm) rigid 3/4" x 1/2" (19mm x 13mm) galvanized C-Channel bars	XA-AC22 Pair of 22" (559mm) rigid 3/4" x 1/2" (19mm x 13mm) galvanized C-Channel bars
NA-AC30 Pair of 30" (762mm) rigid 3/4" x 1/2" (19mm x 13mm) galvanized C-Channel bars	XA-AC30 Pair of 30" (762mm) rigid 3/4" x 1/2" (19mm x 13mm) galvanized C-Channel bars
Mounting Brackets XA-AB14 Kit contains 4	



Multi-Level Sensor Location (ordered as an option)



LED Count (x10)	Weight
D1	22.5 lbs. (10.2kg)
D5	22.5 lbs. (10.2kg)

**Ordering Information**

Example: SFT304-EM-RW-04-E-UL-WH-350

SFT-304	RM	E	UL	Color Options	Drive Current	Options		
Product	Optic	Mounting	LED Count (x10)	Series	Voltage	Color Options	Drive Current	Options
SFT-304	SM Type V Medium SS Type V Short PS Petroleum Symmetric & Sprock Petroleum	RM Recessed	D4 D5 - Available on Non-C rated luminaires only	E	UL Universal 120-277V - For use with IC option only UL Universal 04-480V - For use with IC option only 12 120V - Available on Non-C rated luminaires only 27 277V - Available on Non-C rated luminaires only	BK Black S2S Silver WV White	350 350mA 525 525mA 700 700mA	DIM 0-10V Dimming - Control by others - Refer to dimming spec sheet for details - Can't exceed specified drive current F Fuse - When code dictates fusing, use time delay fuse - Refer to ML spec sheet for availability with ML options IC Type IC Rated - Available with UL, 12 and 27 voltages only - Must specify UL or LH voltage ML Multi-Level - Refer to ML spec sheet for details PML Programmable Multi-Level - Refer to PML spec sheet for details 40K 4000K Color Temperature - Minimum 70 CRI - Color temperature per luminaire



Rev. Date: V1 08/03/2015



US: www.cree.com/lighting T (800) 236-6800 F (262) 504-5415

Canada: www.cree.com/canada T (800) 473-1234 F (800) 890-7507



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LENEXA, KANSAS 66215  
TEL: 913-492-2400  
FAX: 913-492-2437

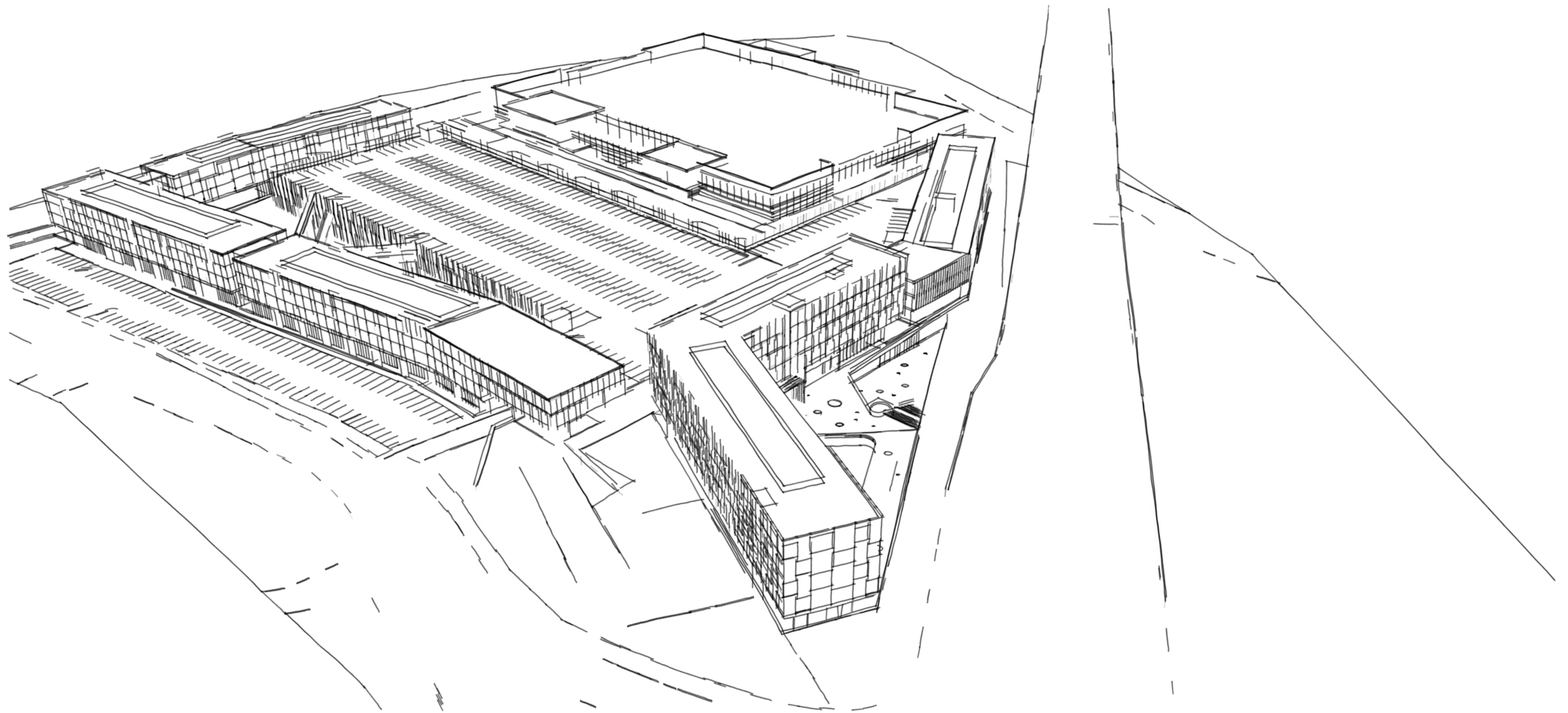
STRUCTURAL  
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4338 BELLEVUE AVE.  
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TEL: 816-531-4144  
FAX: 816-531-8572

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# MISSION GATEWAY

A MULTI-USE RETAIL DEVELOPMENT  
JOHNSON DRIVE AND ROE AVENUE, MISSION, KS

PRELIMINARY DEVELOPMENT PLAN  
AUGUST 17, 2015

CONSULTANTS:

CIVIL / LANDSCAPE:  
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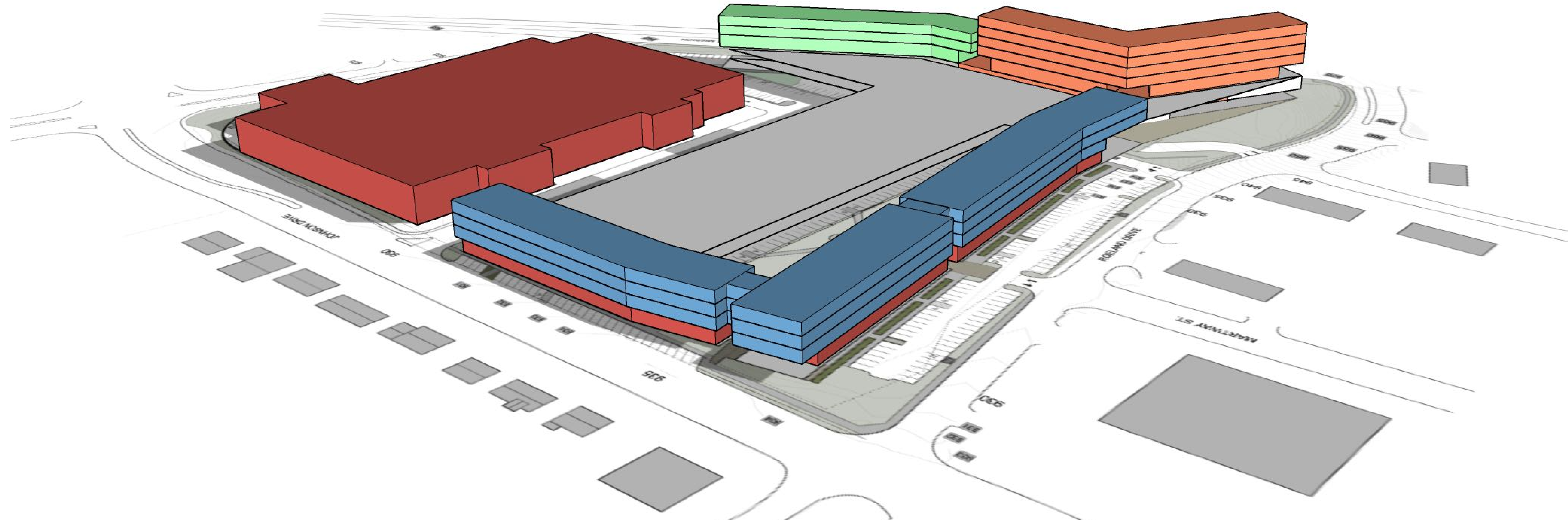
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ASSOCIATES

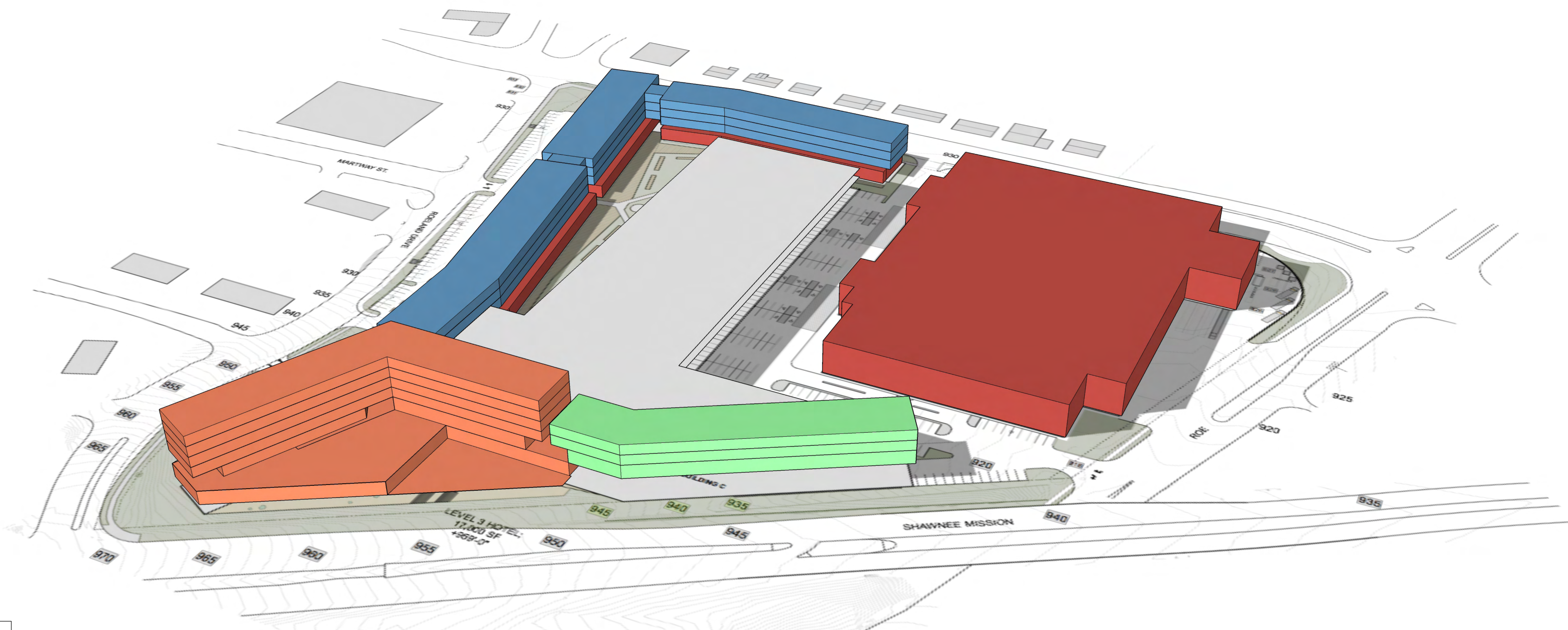
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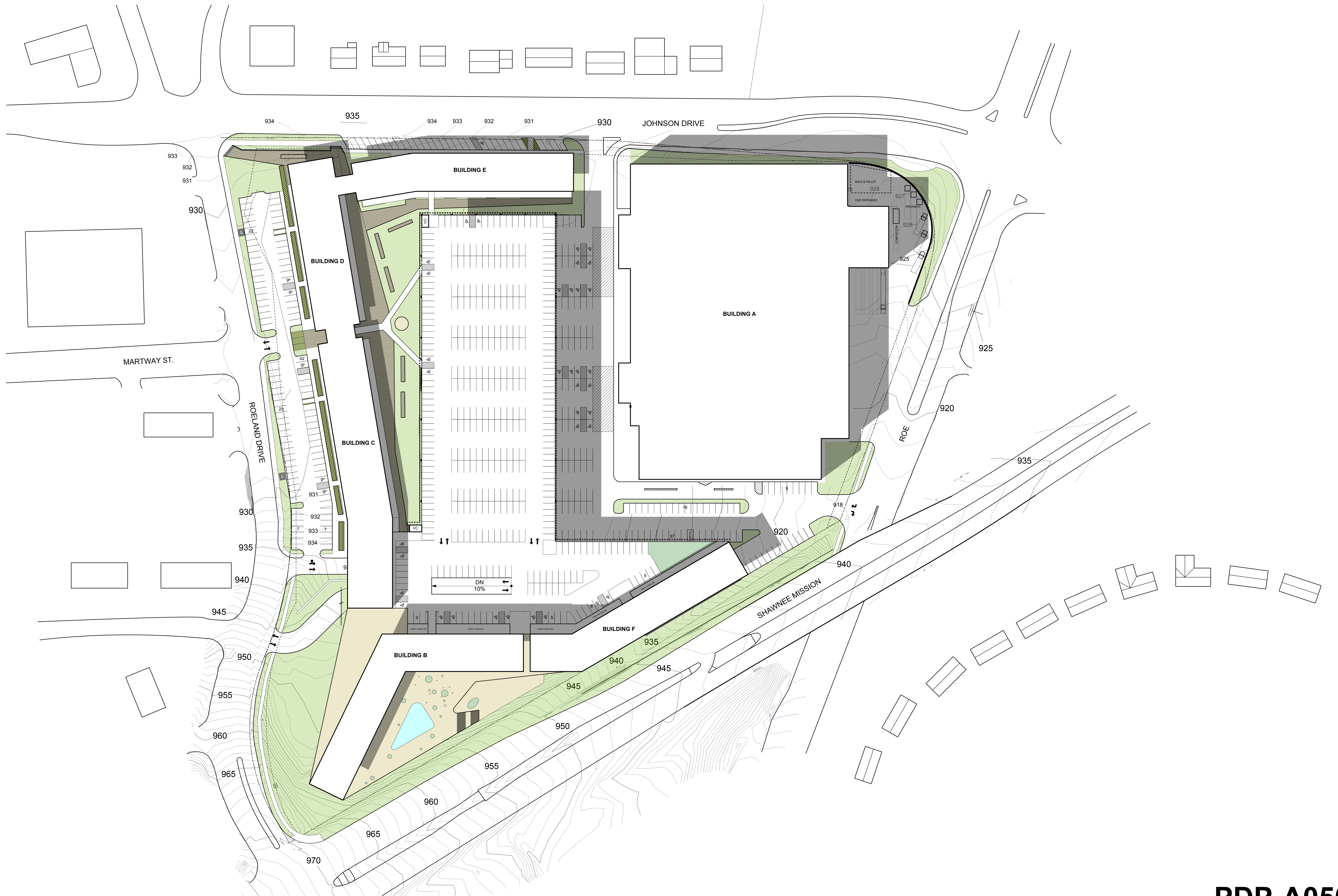
02 STACKING DIAGRAM



- RETAIL
- RESIDENTIAL
- HOTEL
- OFFICE

01 STACKING DIAGRAM





01 SITE PLAN

PRELIMINARY DEVELOPMENT PLAN  
AUGUST 17, 2015

**PDP-A050**  
SITE PLAN

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ASSOCIATES

**pkmr**

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PARKING TOTALS	
LEVEL	QUANTITY
LEVEL 1	751
LEVEL 2	420
LEVEL 3	380
<b>TOTALS</b>	<b>1551</b>

01 SITE PLAN - LEVEL 1

PRELIMINARY DEVELOPMENT PLAN  
AUGUST 17, 2015

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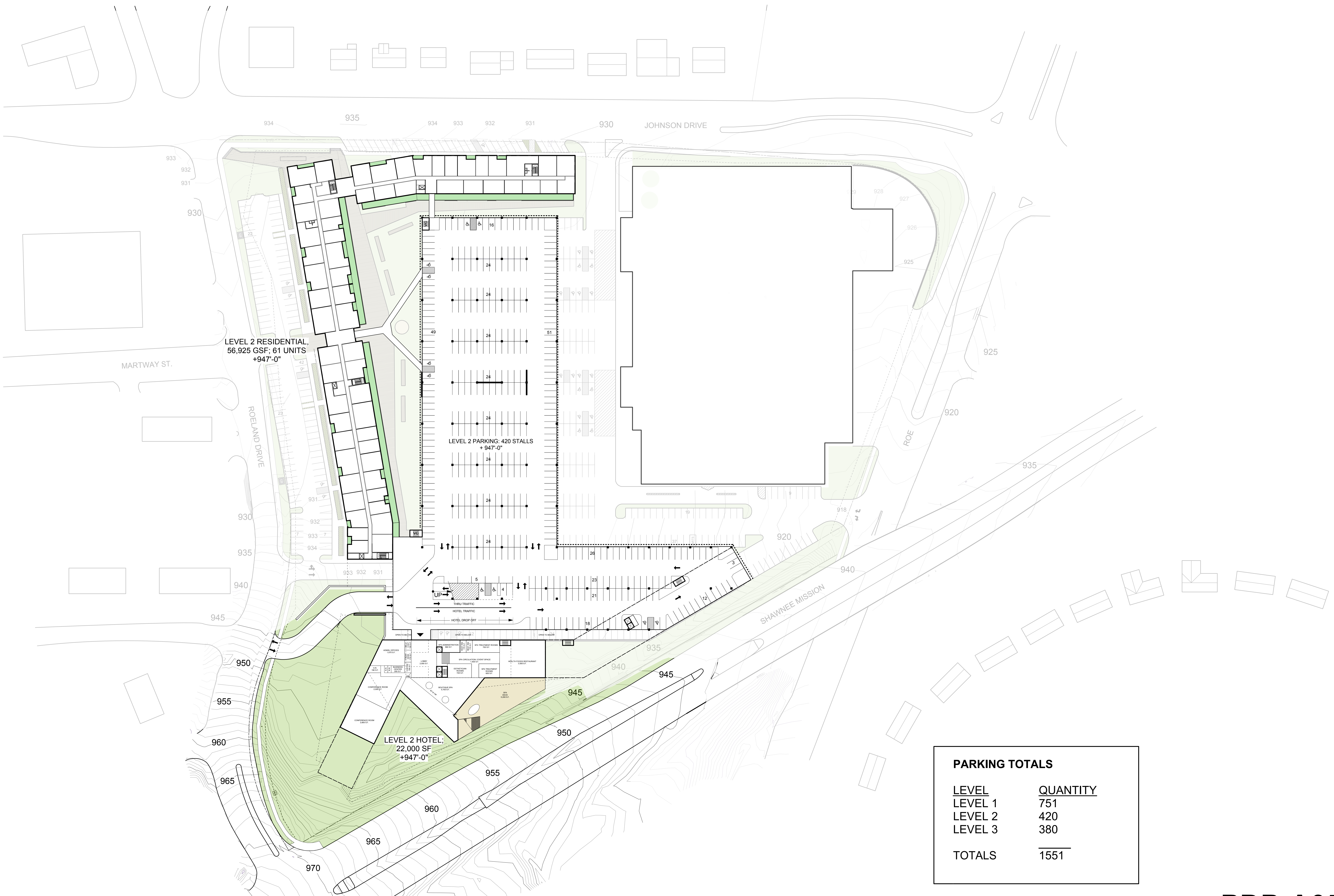
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**PDP-A051**

SITE PLAN (LEVEL 1)





LEVEL 2 RESIDENTIAL,  
56,925 GSF; 61 UNITS  
+947'-0"

LEVEL 2 PARKING: 420 STALLS  
+947'-0"

LEVEL 2 HOTEL:  
22,000 SF  
+947'-0"

PARKING TOTALS	
LEVEL	QUANTITY
LEVEL 1	751
LEVEL 2	420
LEVEL 3	380
<b>TOTALS</b>	<b>1551</b>

01 SITE PLAN - LEVEL 2

PRELIMINARY DEVELOPMENT PLAN  
AUGUST 17, 2015

CONSULTANTS:  
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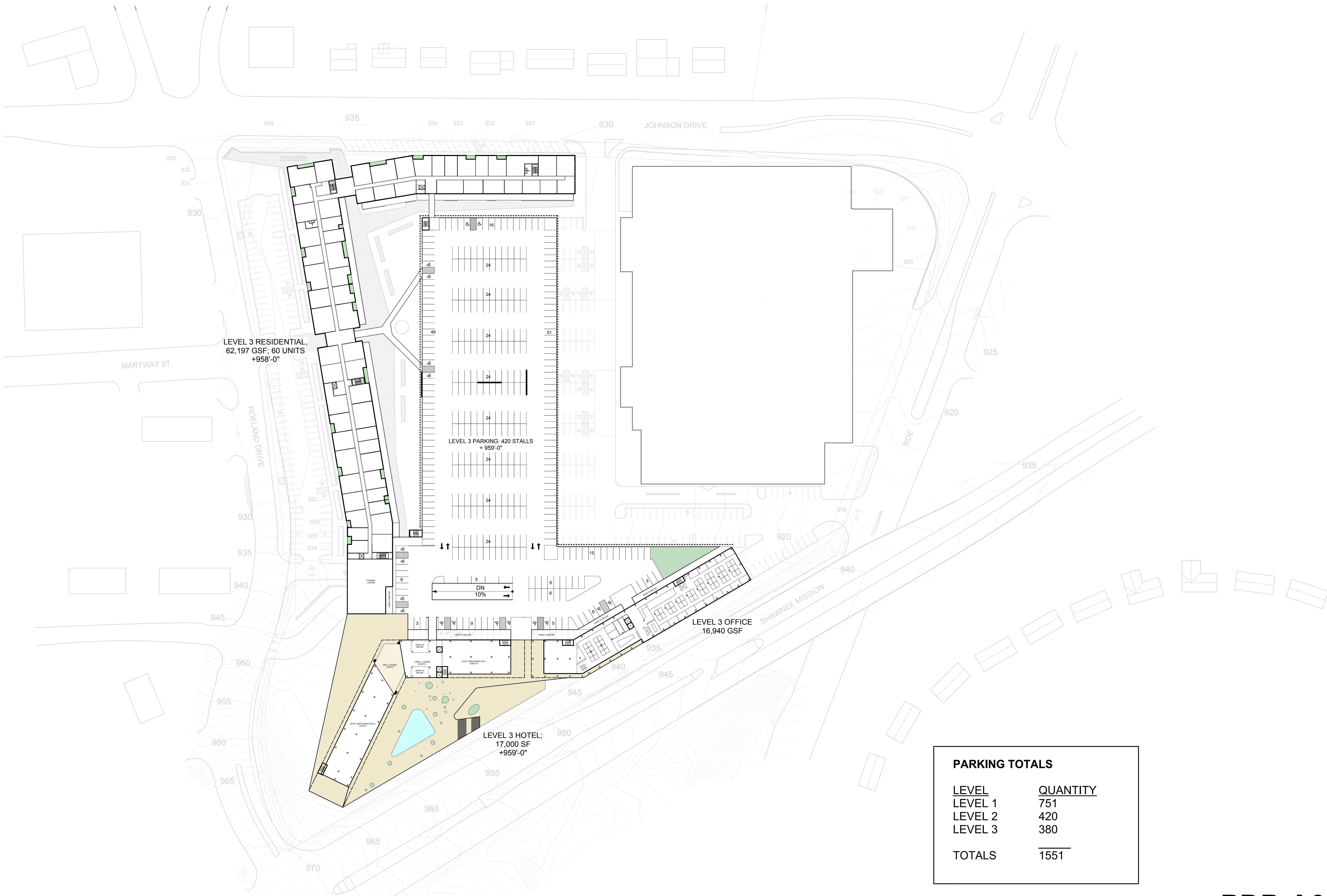
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**PDP-A052**

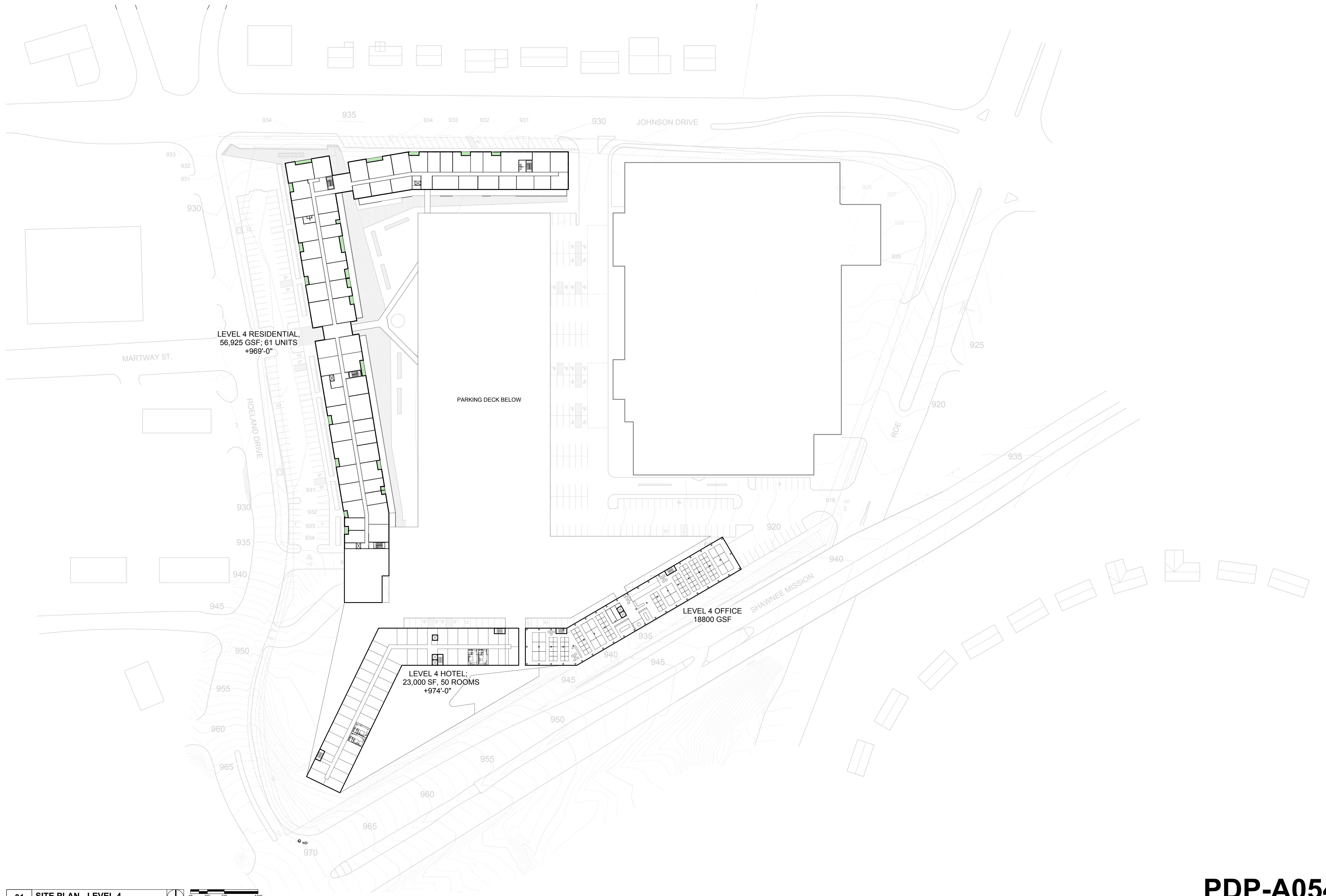
SITE PLAN (LEVEL 2)



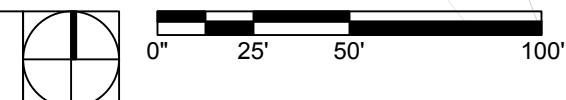


PARKING TOTALS	
LEVEL	QUANTITY
LEVEL 1	751
LEVEL 2	420
LEVEL 3	380
<b>TOTALS</b>	<b>1551</b>





01 SITE PLAN - LEVEL 4



PRELIMINARY DEVELOPMENT PLAN  
AUGUST 17, 2015

CONSULTANTS:

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MEP/  
PKMR ENGINEERS  
13300 W. 98TH ST.  
LENEKA, KANSAS 66215  
TEL: 913-492-2400  
FAX: 913-492-2437

STRUCTURAL:  
BOB D. CAMPBELL & CO., INC.  
4338 BELLEVUE AVE.  
KANSAS CITY, MISSOURI 64111  
TEL: 816-531-4144  
FAX: 816-531-8572

MISSION GATEWAY

**el dorado**

510 avenida cesar e chavez kansas city missouri 64108 p 816 474 3838 f 816 474 0836 www.eldoradoarchitects.com

**OLSSON**  
ASSOCIATES

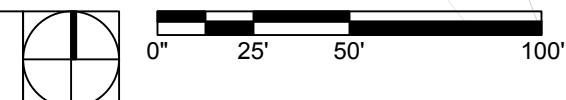
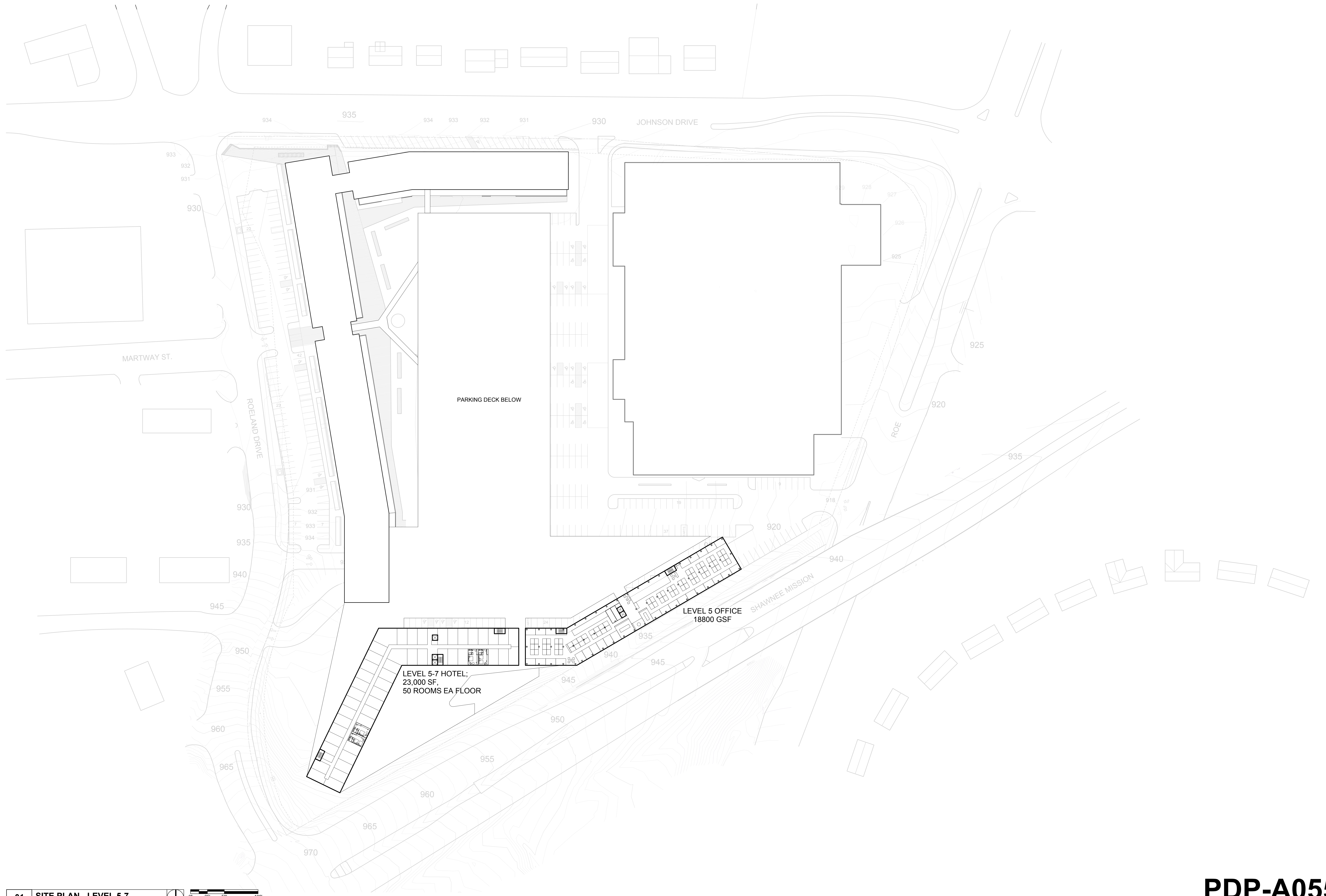
**pkmr**

**BDC**  
BOB D. CAMPBELL & CO., INC.  
Structural Engineers - Since 1957

**PDP-A054**

SITE PLAN (LEVEL 4)





CONSULTANTS:

CIVIL / LANDSCAPE  
OLSSON ASSOCIATES  
7301 WEST 133RD ST. SUITE 200  
OVERLAND PARK, KANSAS 66213  
TEL: 913-381-1170  
FAX: 913-381-1174

ME/P  
PKMR ENGINEERS  
13300 W. 98TH ST  
LENEKA, KANSAS 66215  
TEL: 913-492-2400  
FAX: 913-492-2437

STRUCTURAL  
BOB D. CAMPBELL & CO., INC.  
4338 BELLEVUE AVE.  
KANSAS CITY, MISSOURI 64111  
TEL: 816-531-4144  
FAX: 816-531-8572

MISSION GATEWAY

**el dorado**

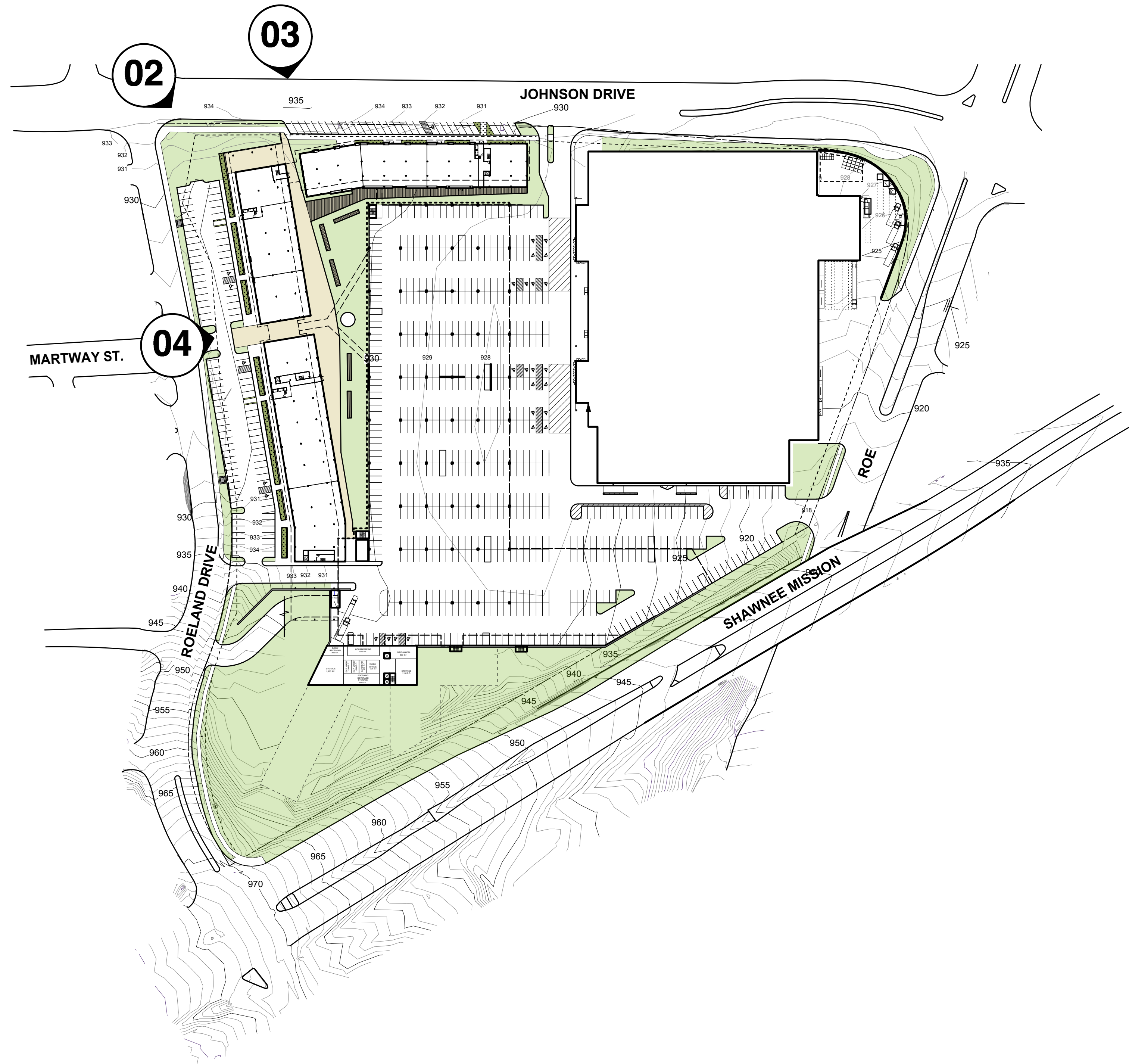
510 avenida cesar e chavez kansas city missouri 64108 p 816 474 3838 f 816 474 0836 www.eldoradoarchitects.com

**OLSSON**  
ASSOCIATES

**pkmr**

**BDC**  
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Structural Engineers - Since 1957





02 ARCHITECTURAL VIGNETTE - RETAIL / RESIDENTIAL

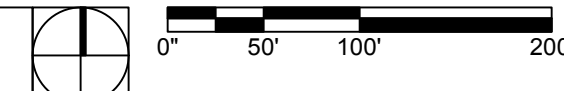


03 ARCHITECTURAL VIGNETTE - RETAIL / RESIDENTIAL



04 ARCHITECTURAL VIGNETTE - RETAIL / RESIDENTIAL

01 SITE PLAN



PRELIMINARY DEVELOPMENT PLAN  
AUGUST 17, 2015

CONSULTANTS:

CIVIL / LANDSCAPE:  
OLSSON ASSOCIATES  
7301 WEST 133RD ST., SUITE 200  
OVERLAND PARK, KANSAS 66213  
TEL: 913-381-1170  
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M/E/P:  
PKMR ENGINEERS  
13300 W. 99TH ST  
LENEXA, KANSAS 66215  
TEL: 913-492-2400  
FAX: 913-492-2437

STRUCTURAL:  
BOB D. CAMPBELL & CO., INC.  
4508 BELLEVIEW AVE.  
KANSAS CITY, MISSOURI 64111  
TEL: 816-531-4144  
FAX: 816-531-8872

04.29.15

MISSION GATEWAY

**el dorado**

510 avenida cesar e chavez kansas city missouri 64108 p 816 474 3838 f 816 474 0836 www.eldoradoarchitects.com

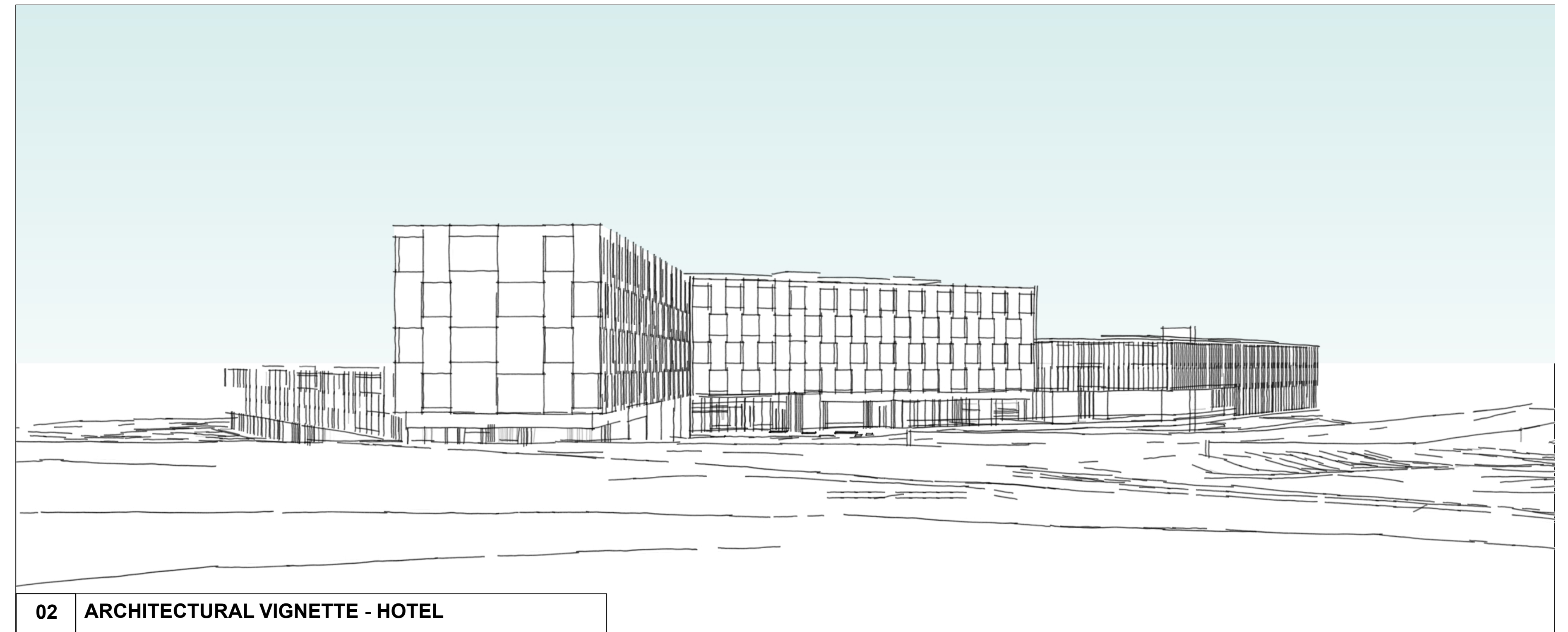
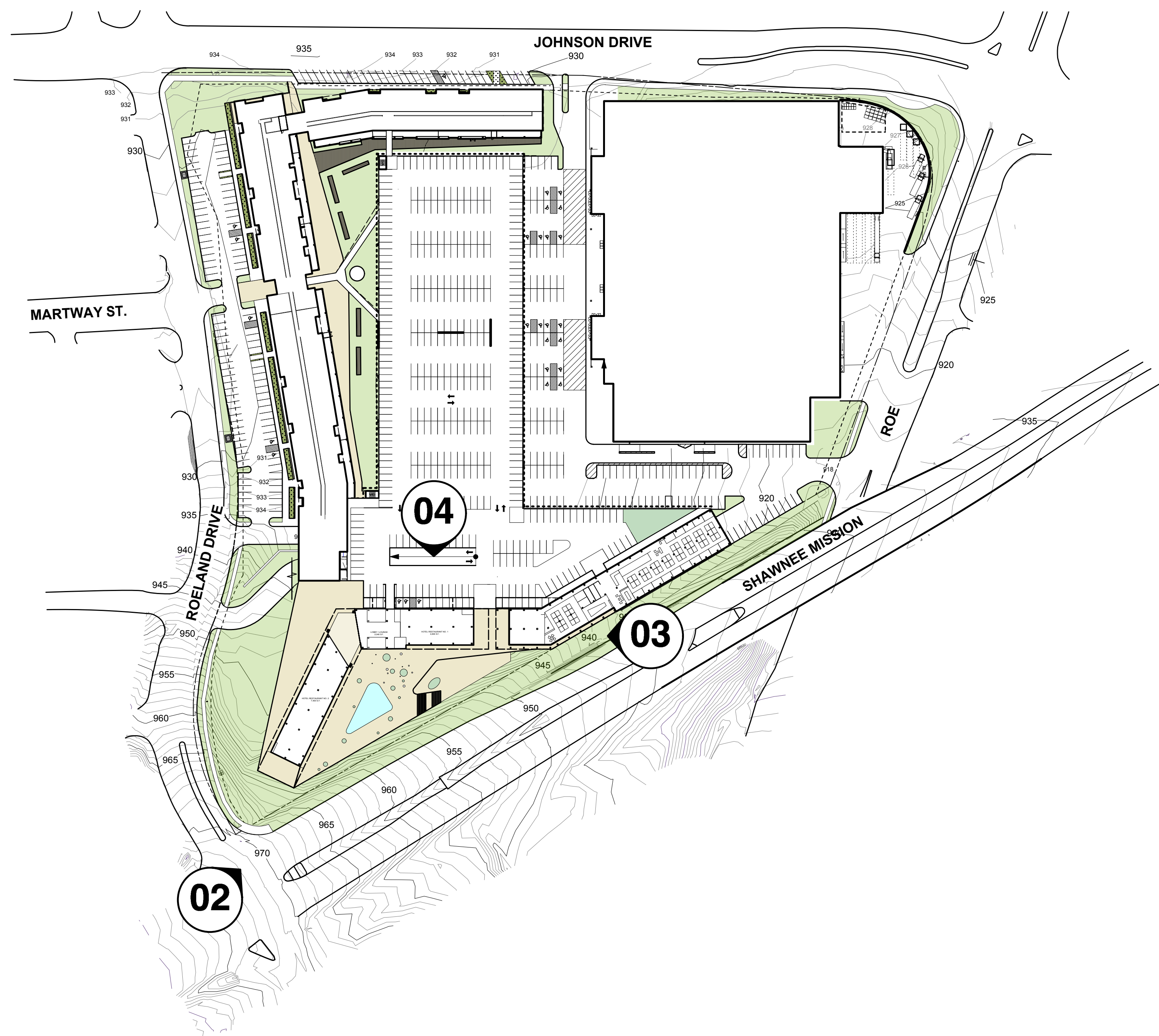
**OLSSON**  
ASSOCIATES

**pkmr**

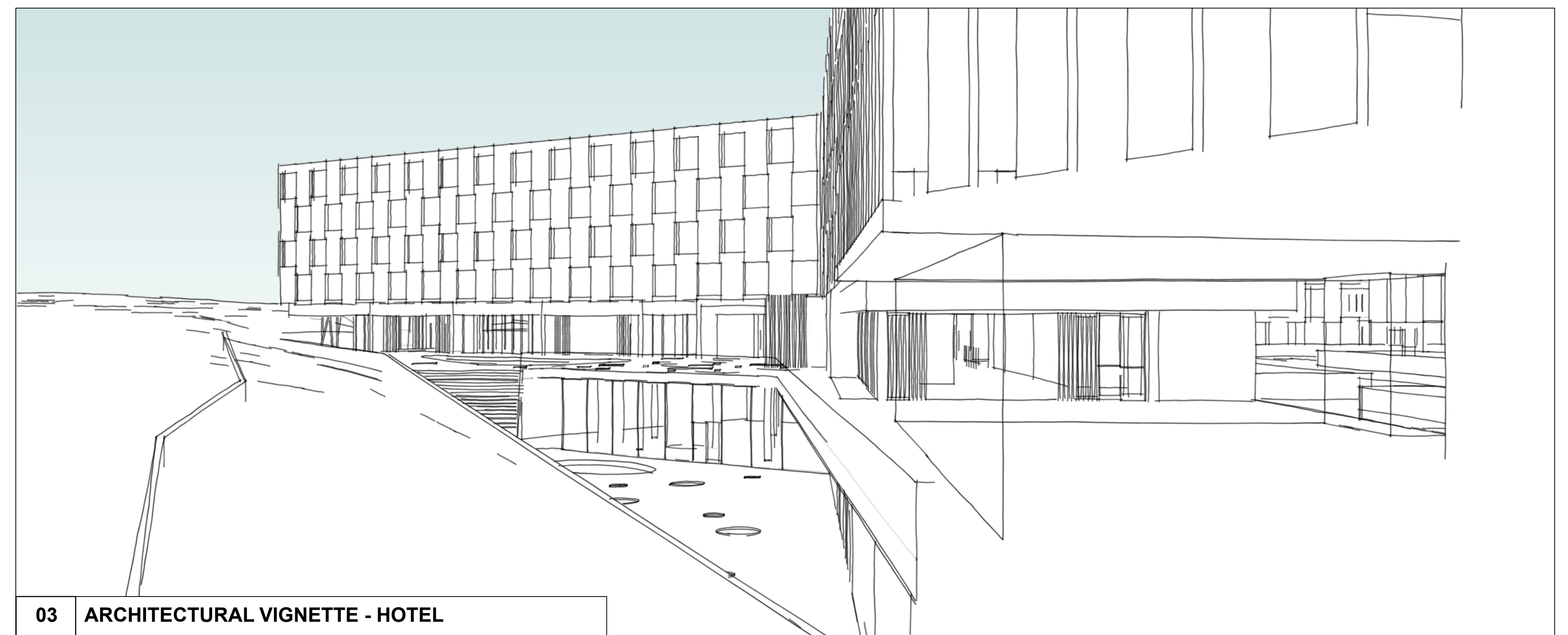
**BDC**  
BOB D. CAMPBELL & CO., INC.  
Structural Engineers - Since 1987

**PDP-A010**  
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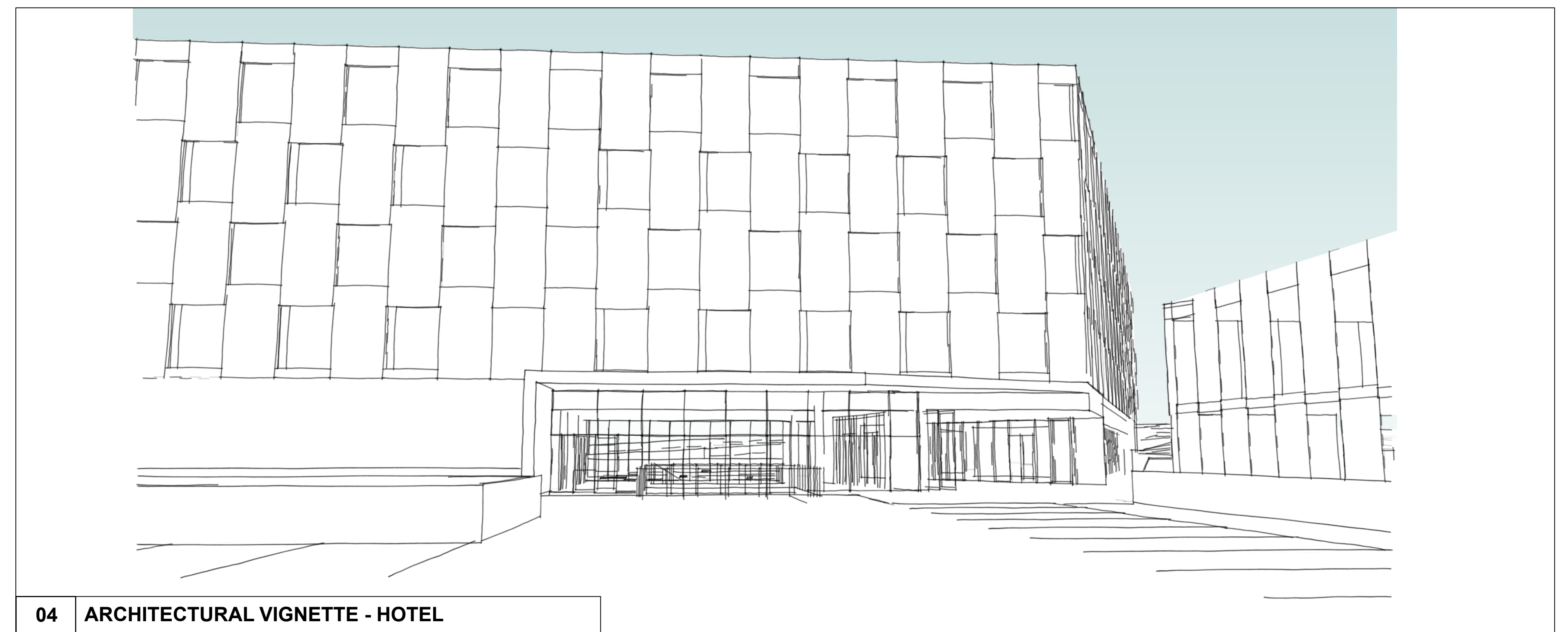




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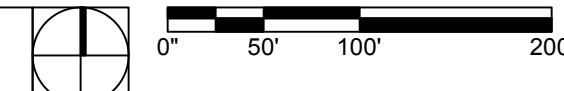


03 ARCHITECTURAL VIGNETTE - HOTEL



04 ARCHITECTURAL VIGNETTE - HOTEL

01 SITE PLAN



PRELIMINARY DEVELOPMENT PLAN  
AUGUST 17, 2015

**PDP-A011**

ARCHITECTURAL VIGNETTES

CONSULTANTS:

CIVIL / LANDSCAPE:  
OLSSON ASSOCIATES  
7301 WEST 133RD ST., SUITE 200  
OVERLAND PARK, KANSAS 66213  
TEL: 913-381-1170  
FAX: 913-381-1174

M/E/P:  
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13300 W. 99TH ST  
LENEXA, KANSAS 66215  
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KANSAS CITY, MISSOURI 64111  
TEL: 816-531-4144  
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04.29.15

MISSION GATEWAY

**el dorado**

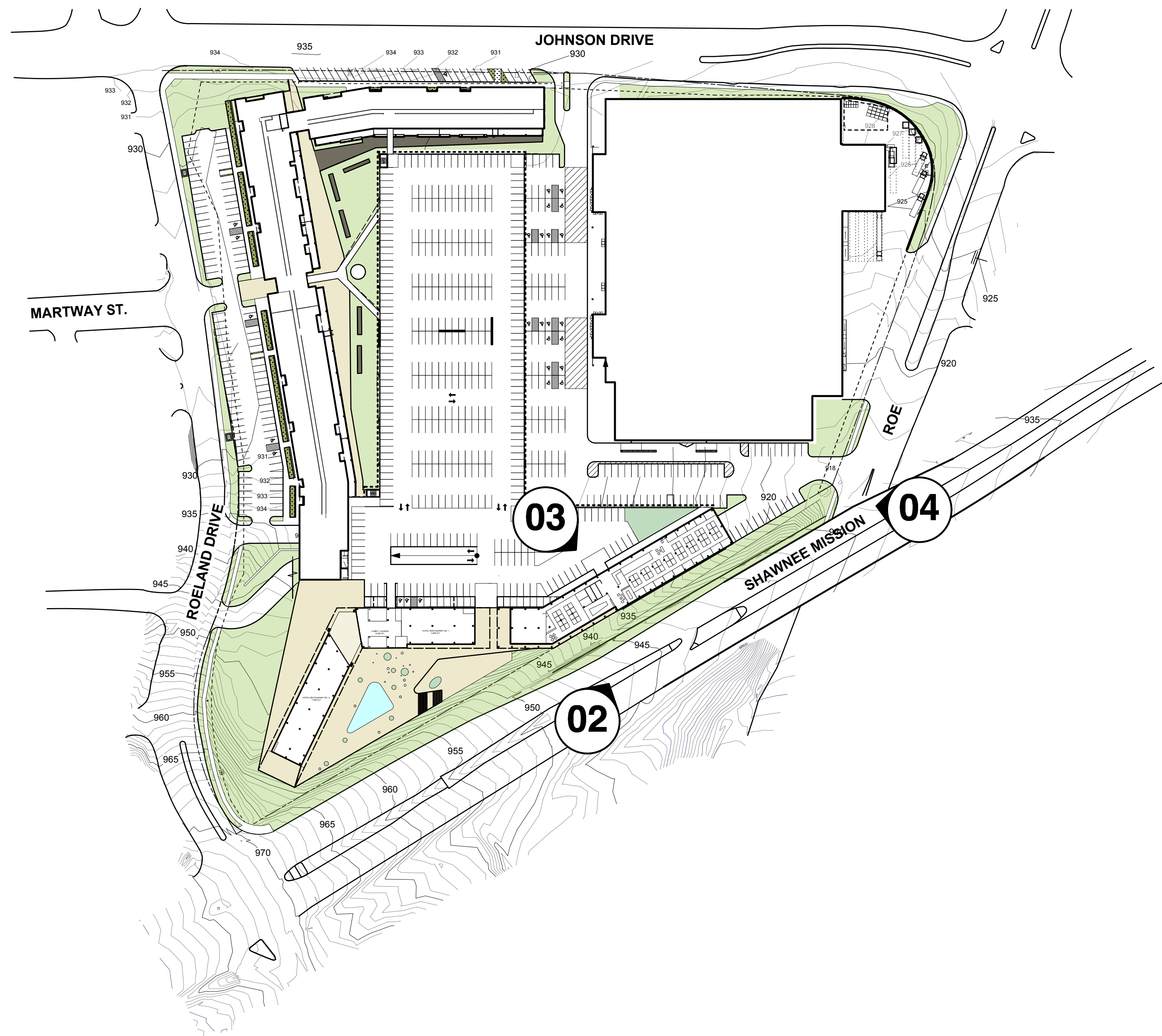
510 avenida cesar e chavez kansas city missouri 64108 p 816 474 3838 f 816 474 0836 www.eldoradoarchitects.com

**OLSSON**  
ASSOCIATES

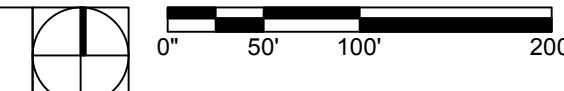
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**BDC**  
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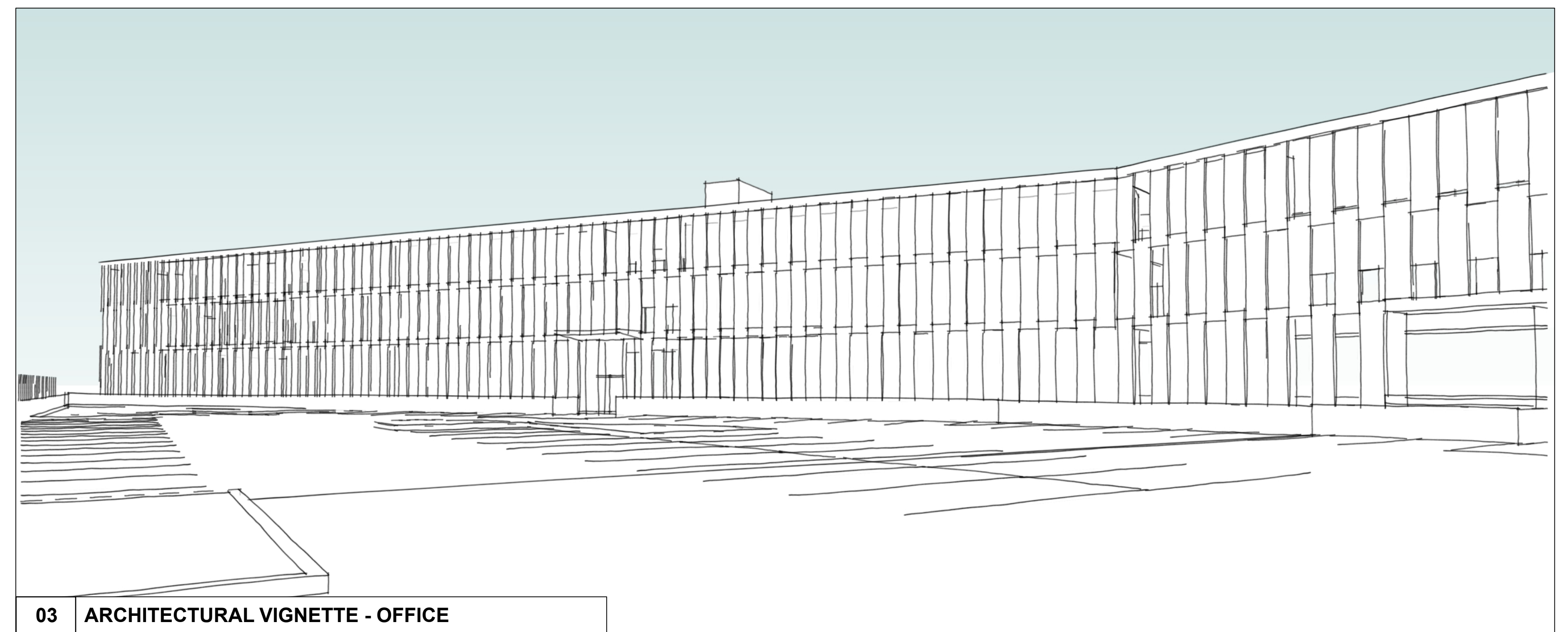
01 SITE PLAN



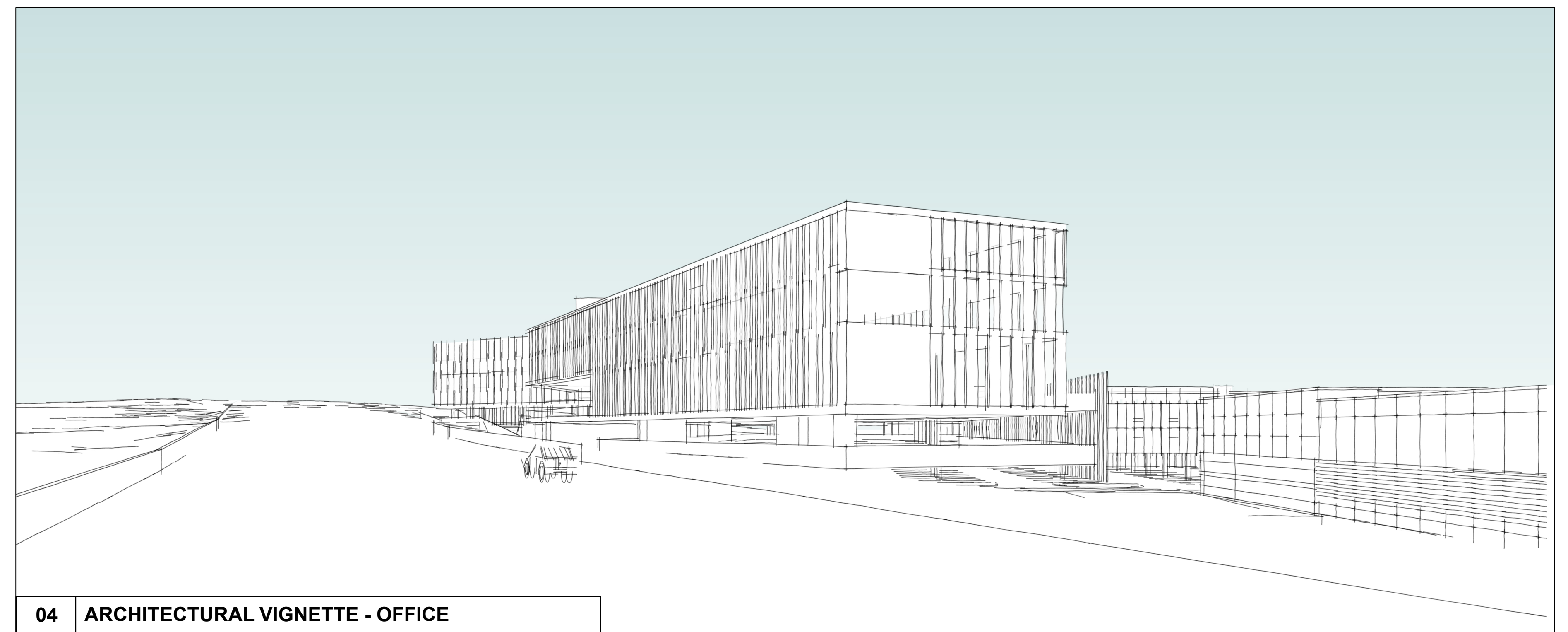
PRELIMINARY DEVELOPMENT PLAN  
AUGUST 17, 2015



02 ARCHITECTURAL VIGNETTE - OFFICE



03 ARCHITECTURAL VIGNETTE - OFFICE



04 ARCHITECTURAL VIGNETTE - OFFICE

# PDP-A012

BUILDING ELEVATIONS

CONSULTANTS:

CIVIL / LANDSCAPE:  
OLSSON ASSOCIATES  
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OVERLAND PARK, KANSAS 66213  
TEL: 913-381-1170  
FAX: 913-381-1174

M/E/P:  
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LENEXA, KANSAS 66215  
TEL: 913-492-2400  
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STRUCTURAL:  
BOB D. CAMPBELL & CO., INC.  
4508 BELLEVIEW AVE.  
KANSAS CITY, MISSOURI 64111  
TEL: 816-531-4144  
FAX: 816-531-8872

04.29.15

MISSION GATEWAY

**el dorado**

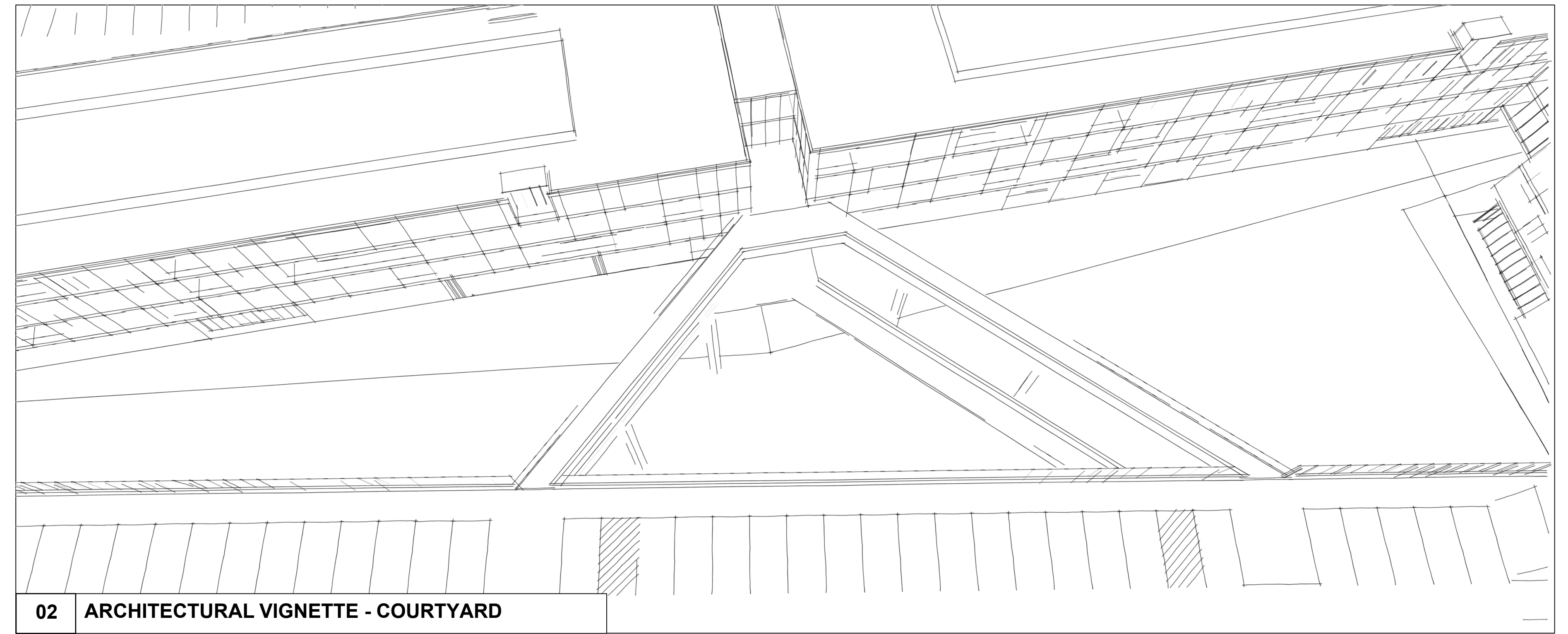
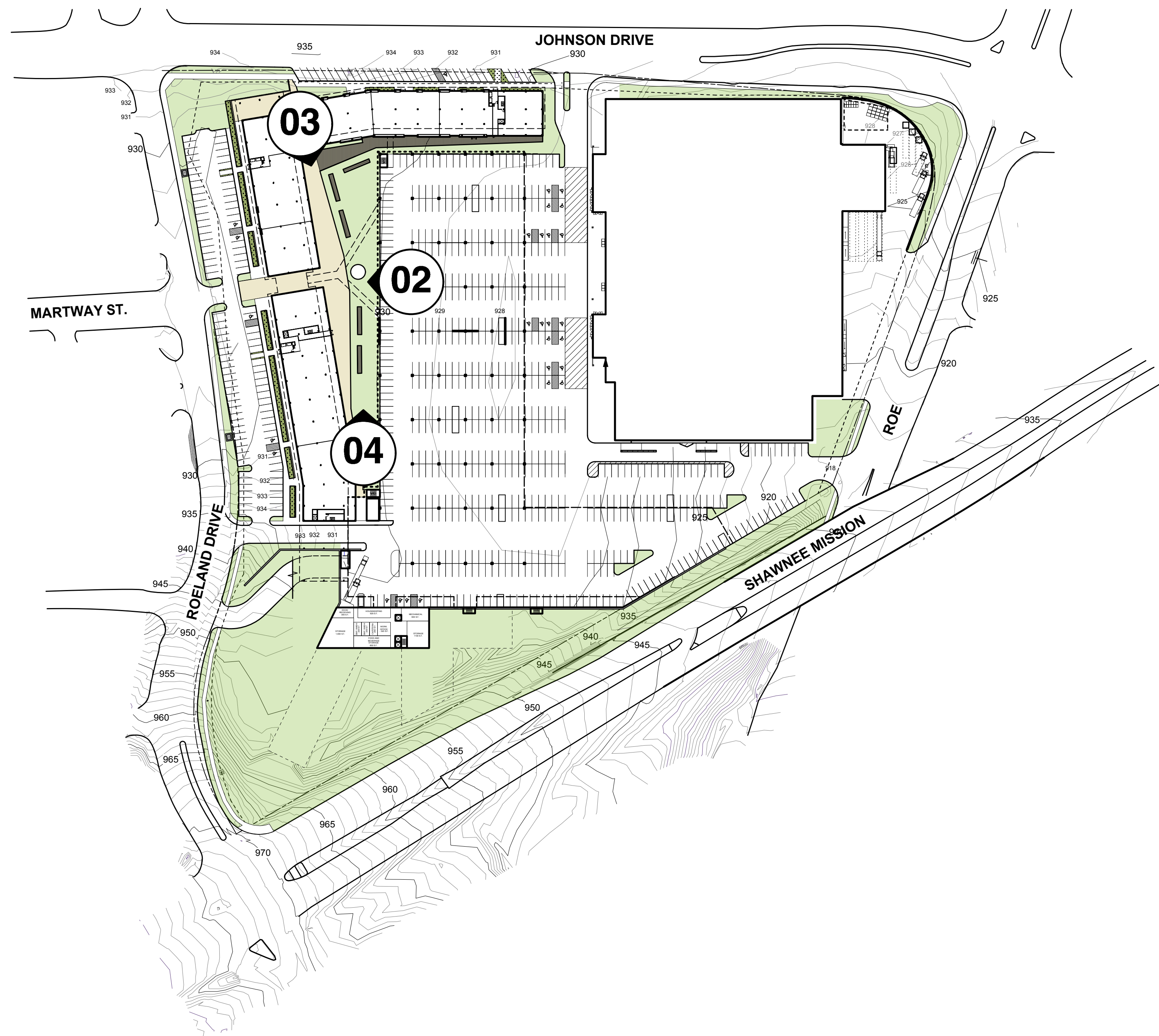
**OLSSON**  
ASSOCIATES

**pkmr**

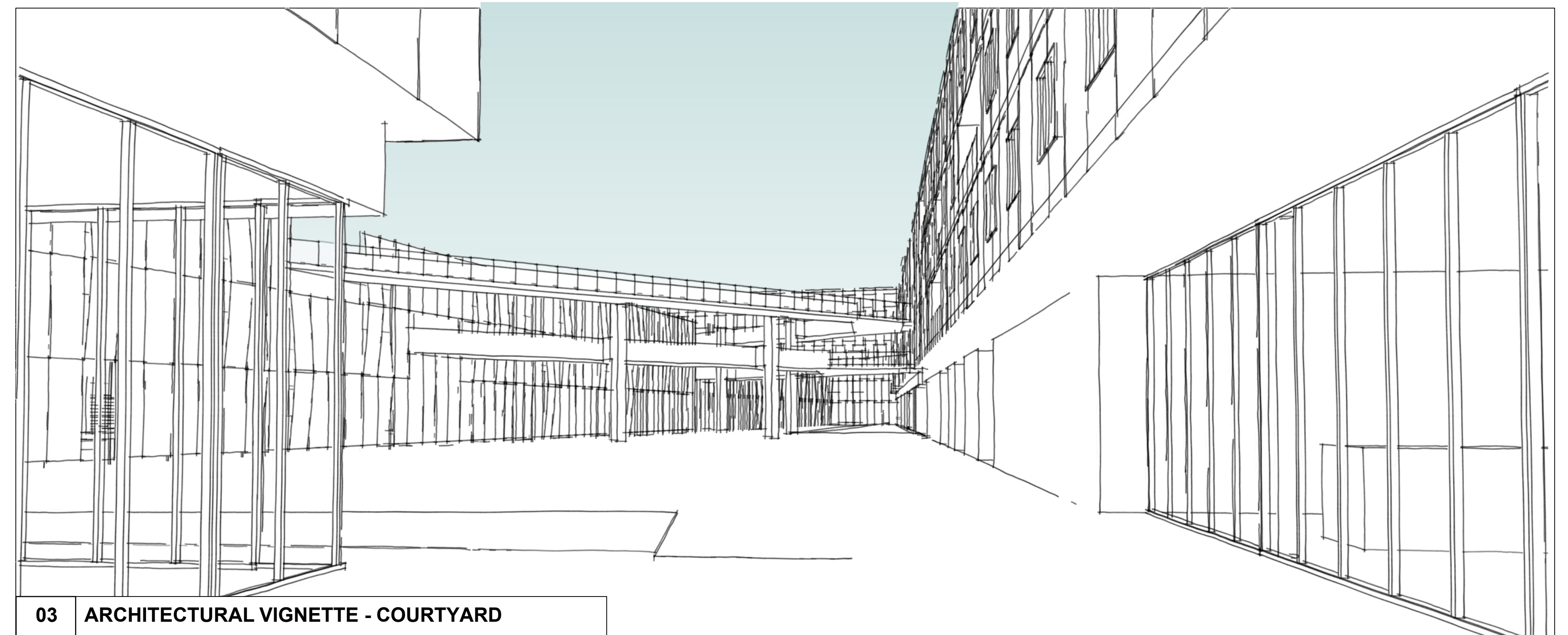
**BDC**  
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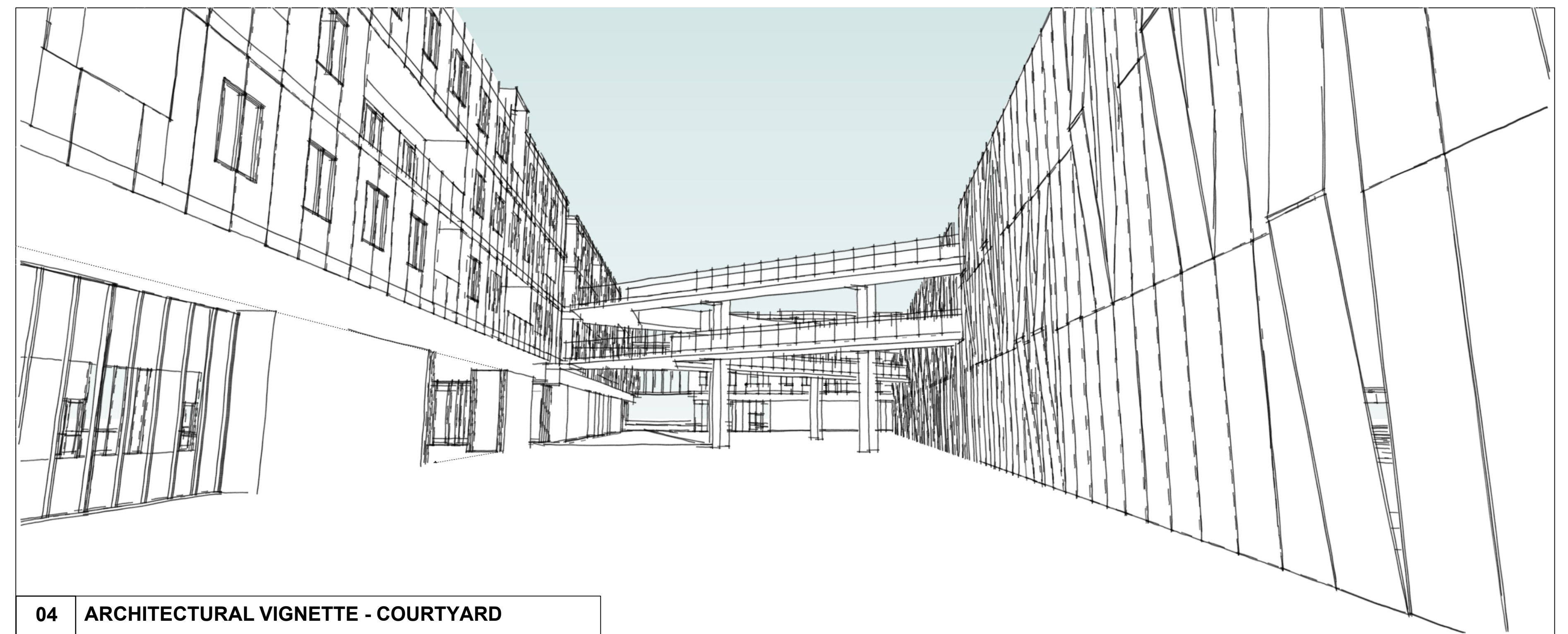




02 ARCHITECTURAL VIGNETTE - COURTYARD



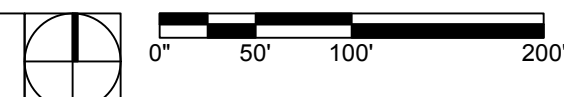
03 ARCHITECTURAL VIGNETTE - COURTYARD



04 ARCHITECTURAL VIGNETTE - COURTYARD

01 SITE PLAN

PRELIMINARY DEVELOPMENT PLAN  
AUGUST 17, 2015



**PDP-A013**  
BUILDING ELEVATIONS

CONSULTANTS:

CIVIL / LANDSCAPE: OLSSON ASSOCIATES 7301 WEST 133RD ST., SUITE 200 OVERLAND PARK, KANSAS 66213 TEL: 913-381-1170 FAX: 913-381-1174	M/E/P: PKMR ENGINEERS 13300 W. 98TH ST LENEXA, KANSAS 66215 TEL: 913-492-2400 FAX: 913-492-2437	STRUCTURAL: BOB D. CAMPBELL & CO., INC. 4508 BELLEVIEW AVE. KANSAS CITY, MISSOURI 64111 TEL: 816-531-4144 FAX: 816-531-8872	04.29.15
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MISSION GATEWAY

**el dorado**

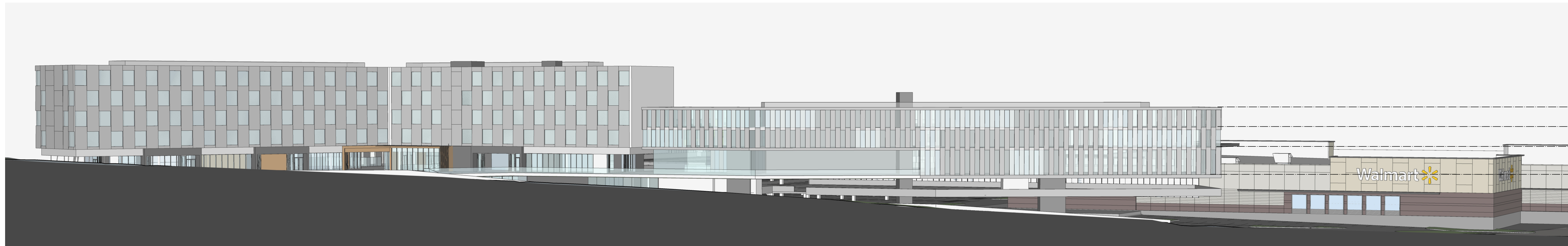
**OLSSON**  
ASSOCIATES

**pkmr**

**BDC**  
BOB D. CAMPBELL & CO., INC.  
Structural Engineers - Since 1987

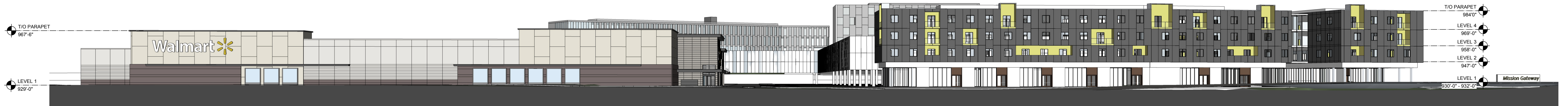
510 avenida cesar e chavez kansas city missouri 64108 p.816.474.3838 f.816.474.0836 www.eldoradoarchitects.com





FROM SHAWNEE MISSION

**03 SITE ELEVATION**



FROM JOHNSON DRIVE

**02 NORTH SITE ELEVATION**



FROM ROELAND DRIVE

**01 WEST SITE ELEVATION**

**PRELIMINARY DEVELOPMENT PLAN**  
AUGUST 17, 2015

CONSULTANTS:

CIVIL / LANDSCAPE:  
OLSSON ASSOCIATES  
7301 WEST 133RD ST., SUITE 200  
OVERLAND PARK, KANSAS 66213  
TEL: 913-381-1170  
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PKMR ENGINEERS  
13300 W. 98TH ST.  
LENEXA, KANSAS 66215  
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FAX: 913-492-2437

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04.29.15

MISSION GATEWAY

**el dorado**

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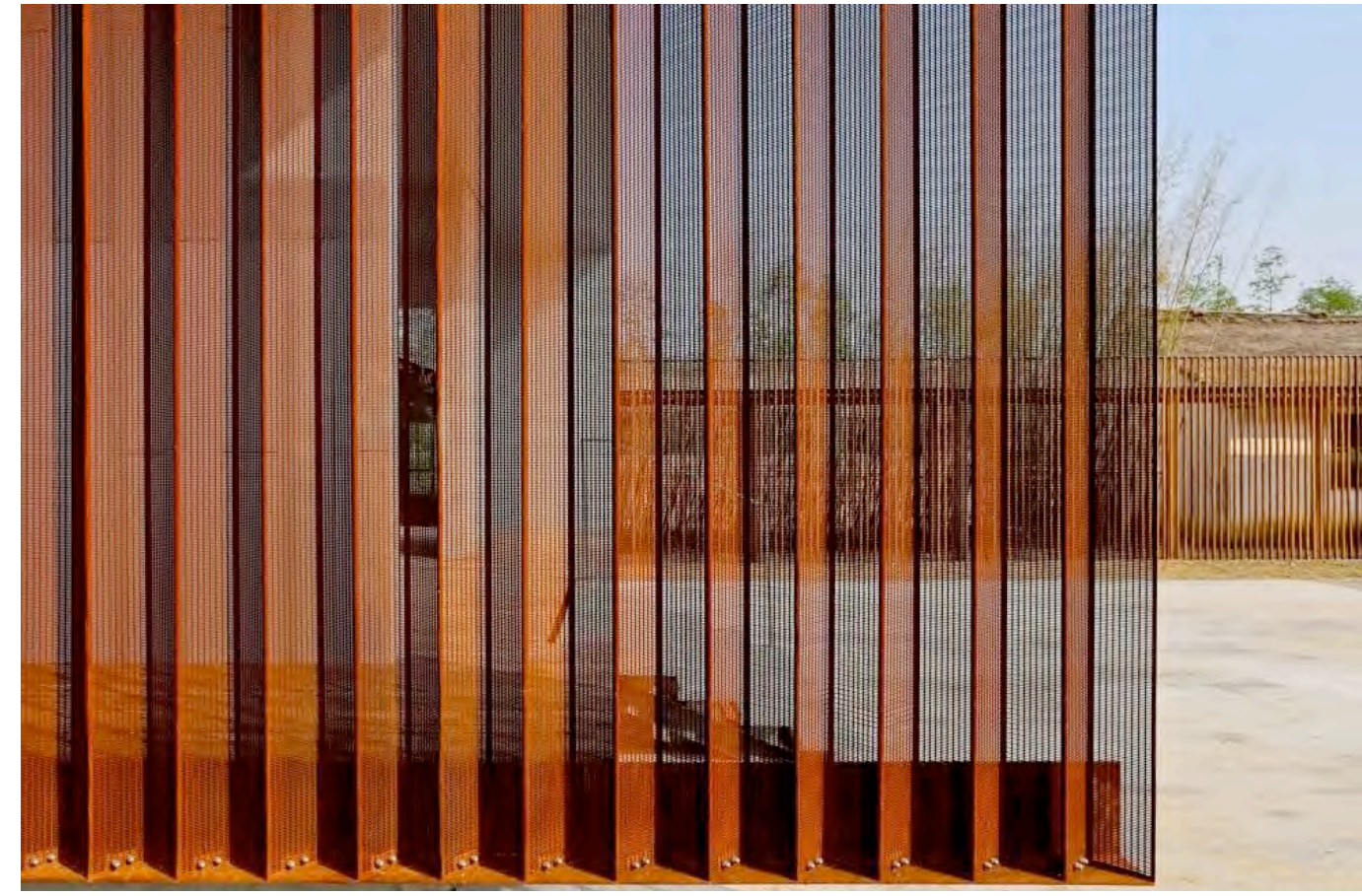
**OLSSON**  
ASSOCIATES

**pkmr**

**BDC**  
BOB D. CAMPBELL & CO., INC.  
Structural Engineers - Since 1987

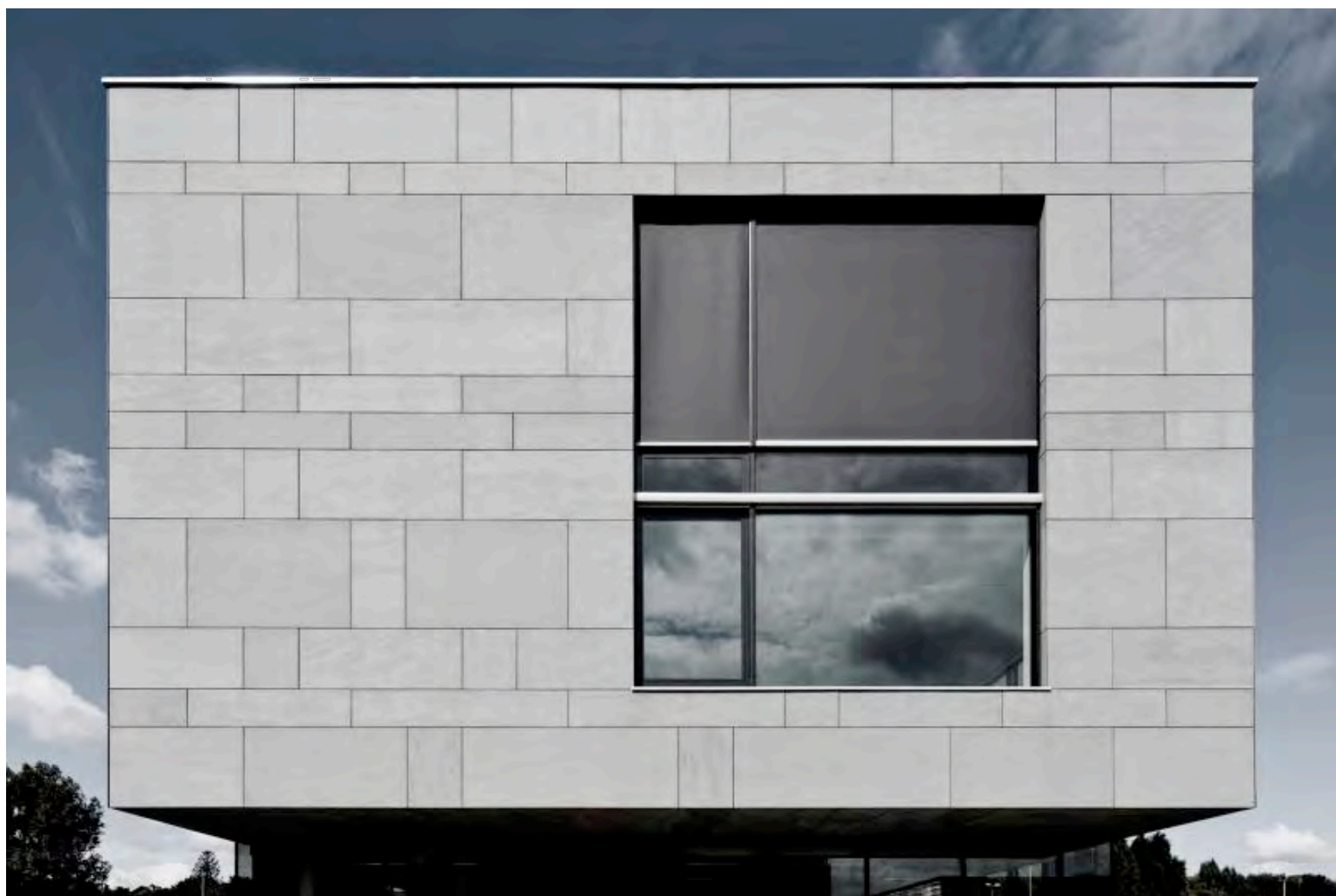
**PDP-A200**  
BUILDING ELEVATIONS



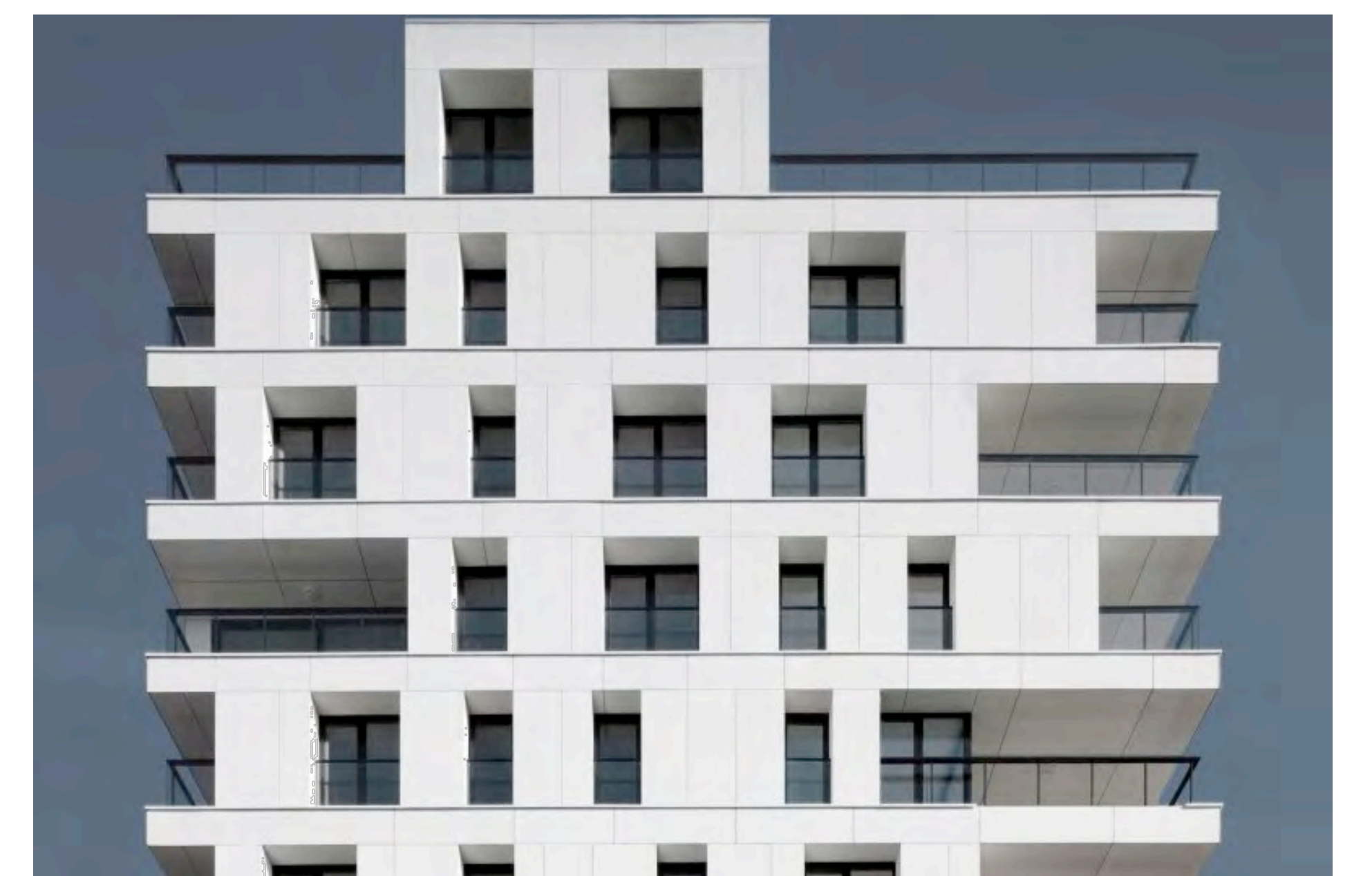


PERFORATED METAL SCREEN

02 MATERIAL STUDIES - PARKING GARAGE



FIBER CEMENT BOARD CLADDING



FIBER CEMENT BOARD CLADDING

01 MATERIAL STUDIES - RESIDENTIAL / HOTEL / OFFICE

PRELIMINARY DEVELOPMENT PLAN  
AUGUST 17, 2015

CONSULTANTS:

CIVIL / LANDSCAPE:  
OLSSON ASSOCIATES  
7301 WEST 133RD ST., SUITE 200  
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PKMR ENGINEERS  
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TEL: 913-492-2400  
FAX: 913-492-2437

STRUCTURAL:  
BOB D. CAMPBELL & CO., INC.  
4308 BELLEVUE AVE.  
KANSAS CITY, MISSOURI 64111  
TEL: 816-531-4144  
FAX: 816-531-8872

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**OLSSON ASSOCIATES**

**pkmr**

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Structural Engineers - Since 1987

**PDP-A800**

MATERIAL STUDIES

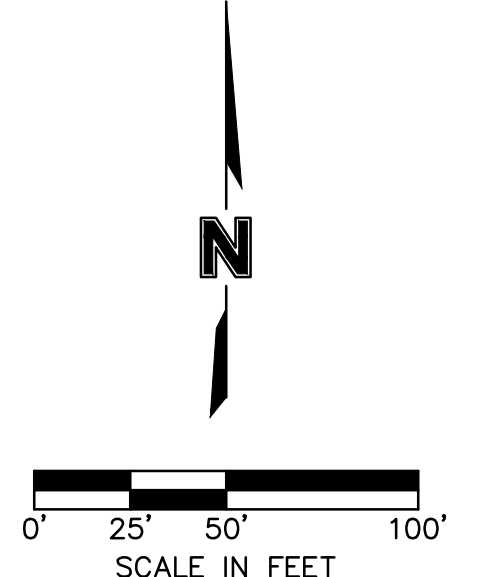
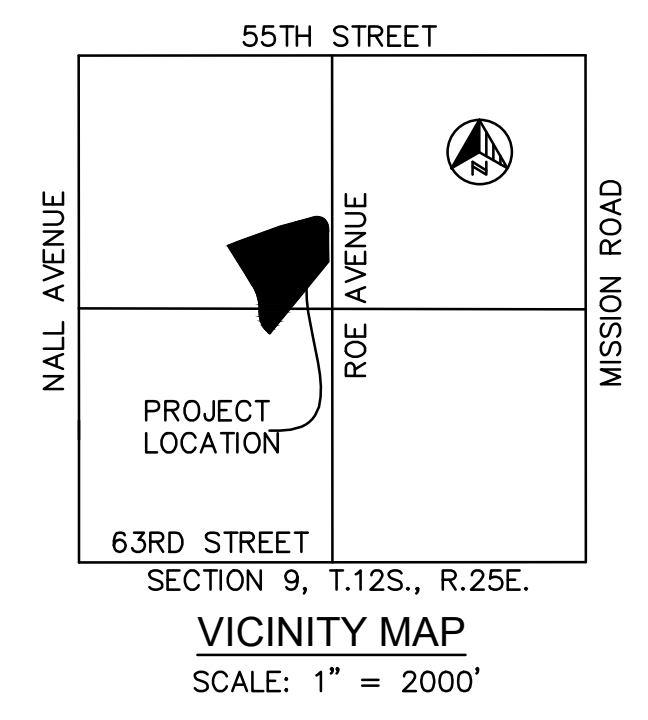
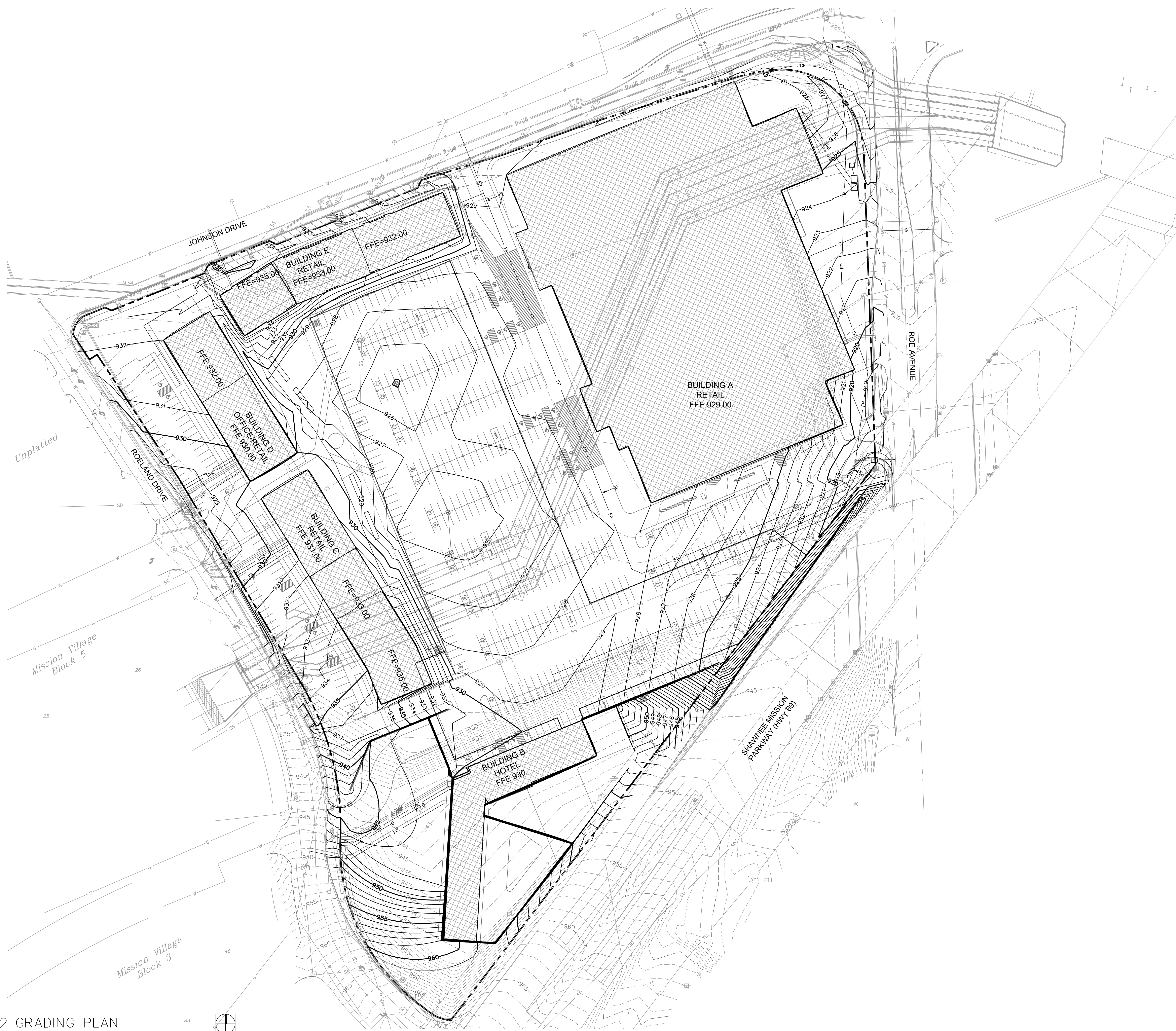






**LEGEND**

- P-OH — OVERHEAD POWER LINE
- P-UG — UNDERGROUND POWER LINE
- E — ELECTRIC LINE
- TEL — TELEPHONE LINE
- COMM — COMMUNICATION LINE
- FO — FIBER OPTIC LINE
- CATV — CABLE TELEVISION SERVICE
- G — GAS LINE
- W — WATER LINE
- SS — EXISTING SANITARY SEWER
- — EXISTING STORM SEWER
- — LOT LINE
- — PROPERTY BOUNDARY
- — EASEMENT LINE
- - - - EXISTING GRADE CONTOUR
- - - - FINISHED GRADE CONTOUR
- - - - DITCH CENTERLINE



02 GRADING PLAN

PRELIMINARY DEVELOPMENT PLAN  
AUGUST 17, 2015

CONSULTANTS:  
 CIVIL / LANDSCAPE: OLSSON ASSOCIATES  
 7301 WEST 133RD ST., SUITE 200  
 OVERLAND PARK, KANSAS 66213  
 TEL: 913-381-1170  
 FAX: 913-381-1174  
 M/E/P: PKMR ENGINEERS  
 13300 W. 98TH ST.  
 LENEXA, KANSAS 66215  
 TEL: 913-492-2400  
 FAX: 913-492-2437  
 STRUCTURAL: BOB D. CAMPBELL & CO., INC.  
 4336 BELLEVUE AVE.  
 KANSAS CITY, MISSOURI 64111  
 TEL: 816-531-4144  
 FAX: 816-531-8572

MISSION GATEWAY  
**el dorado**

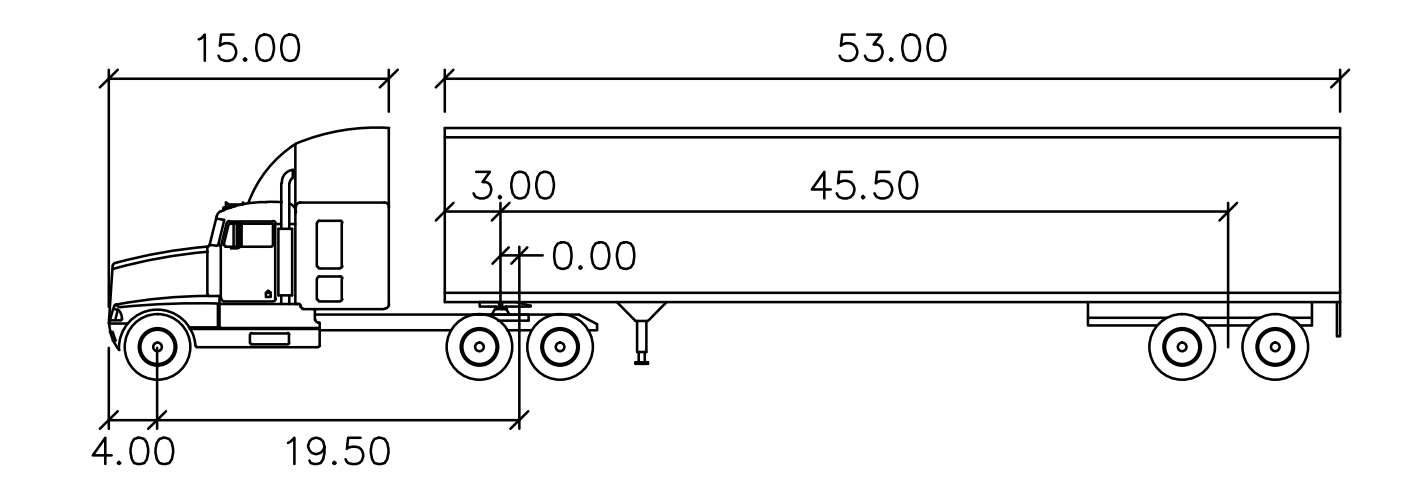
**OLSSON ASSOCIATES**  
**pkmr ENGINEERS**  
**B.D.C. BOB D. CAMPBELL & CO., INC.**  
 Structural Engineers - Since 1957

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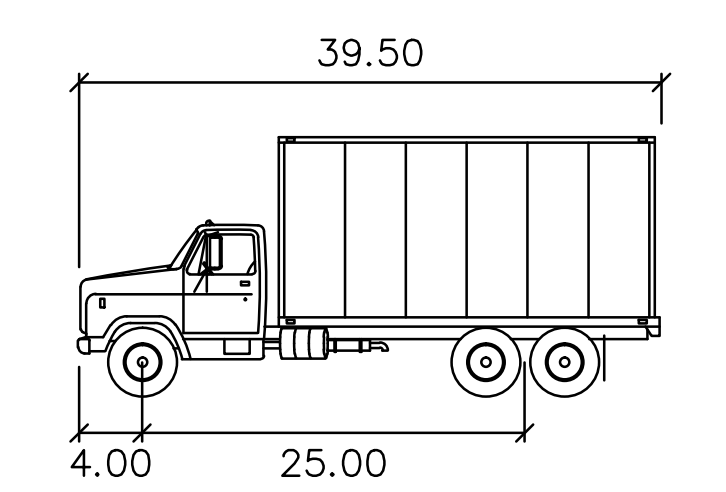
**LEGEND**

- P-OH — OVERHEAD POWER LINE
- P-UG — UNDERGROUND POWER LINE
- E — ELECTRIC LINE
- TEL — TELEPHONE LINE
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- ES — EXISTING STORM SEWER
- LOT LINE
- PROPERTY BOUNDARY
- EASEMENT LINE



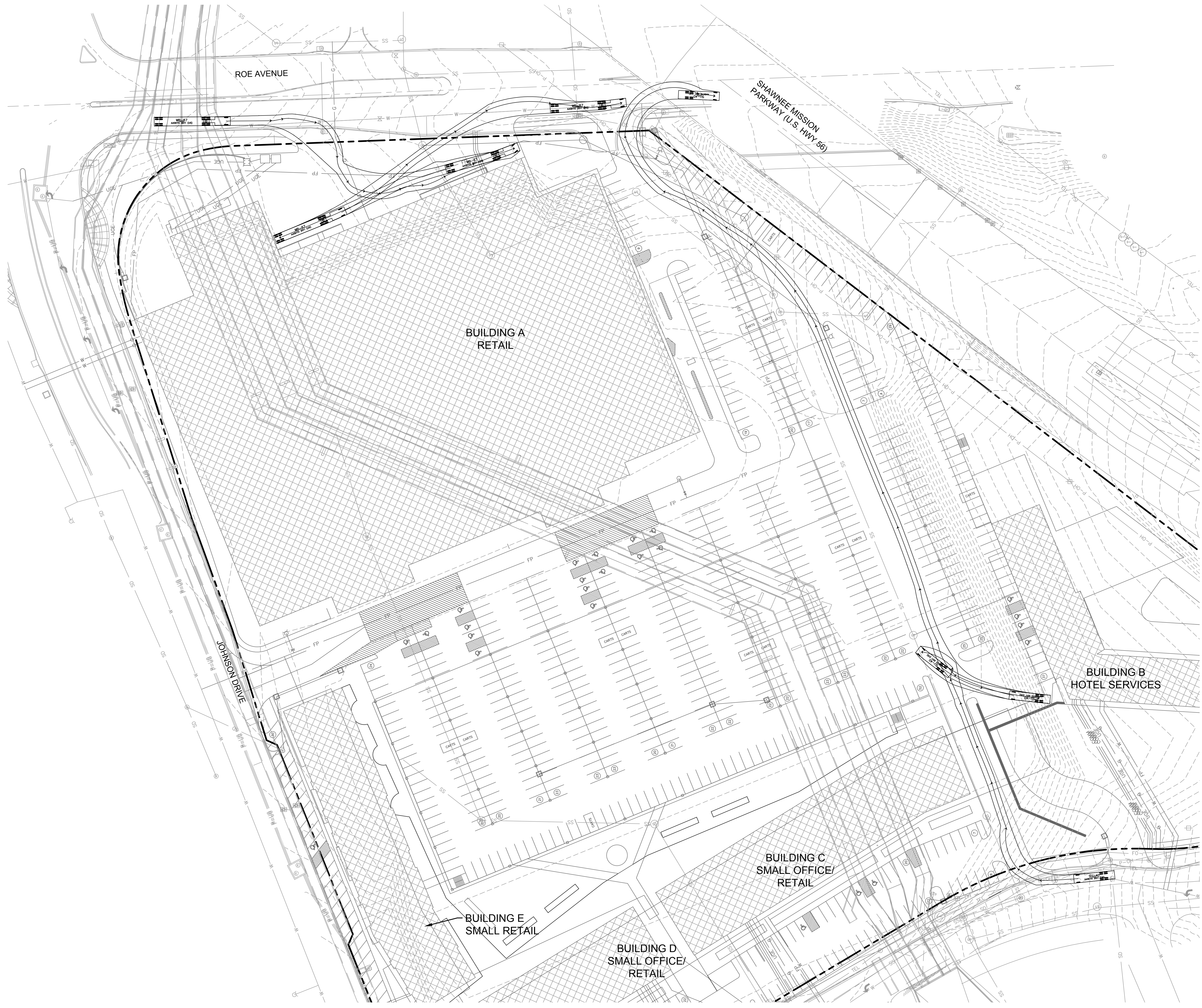
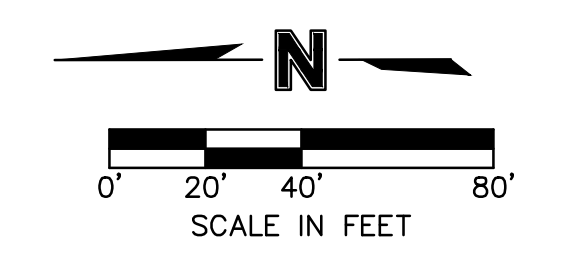
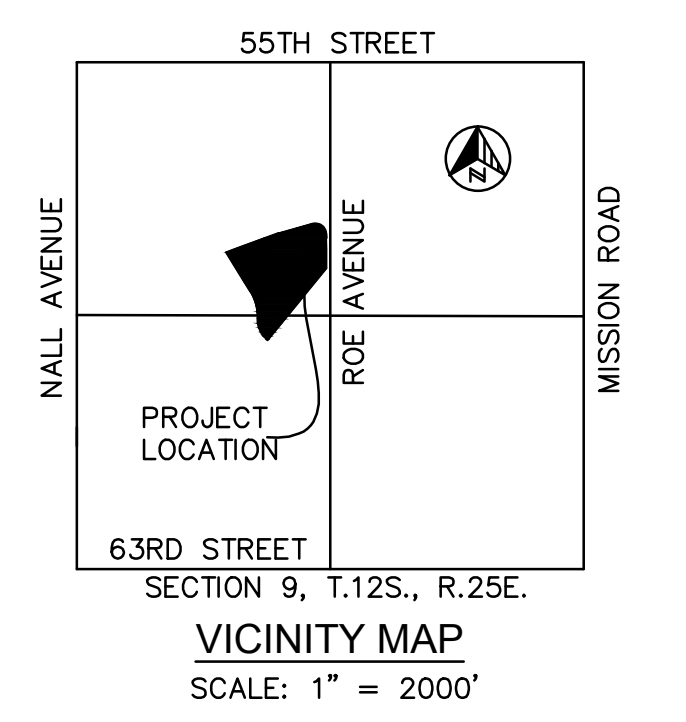
**WB-67**                      feet

Tractor Width	: 8.00	Lock to Lock Time	: 6.0
Trailer Width	: 8.50	Steering Angle	: 28.4
Tractor Track	: 8.00	Articulating Angle	: 75.0
Trailer Track	: 8.50		



**SU-40**                      feet

Width	: 8.00
Track	: 8.00
Lock to Lock Time	: 6.0
Steering Angle	: 31.8



**TRUCK MOVEMENT**

**03 TURNING TEMPLATE**

PRELIMINARY DEVELOPMENT PLAN  
AUGUST 17, 2015

**CONSULTANTS:**  
**CIVIL / LANDSCAPE:**  
 OLSSON ASSOCIATES  
 7301 WEST 133RD ST., SUITE 200  
 OVERLAND PARK, KANSAS 66213  
 TEL: 913-381-1170  
 FAX: 913-381-1174  
**M/E/P:**  
 PKMR ENGINEERS  
 13300 W. 98TH ST.  
 LENEXA, KANSAS 66215  
 TEL: 913-492-2400  
 FAX: 913-492-2437  
**STRUCTURAL:**  
 BOB D. CAMPBELL & CO., INC.  
 4336 BELLEVUE AVE.  
 KANSAS CITY, MISSOURI 64111  
 TEL: 816-531-4144  
 FAX: 816-531-8572

MISSION GATEWAY  
**el dorado**

**OLSSON ASSOCIATES**      **pkmr**      **B.D.C.**  
 BOB D. CAMPBELL & CO., INC.  
 Structural Engineers - Since 1967

USER: lcast  
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 DATE: Aug 17, 2015 12:39pm  
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To: Mission Planning Commission  
From: Danielle Sitzman, City Planner  
Date: March 27, 2017  
Re: Case # 16-07 Sign Code Changes

---

As a follow up to the discussion and public hearings conducted over the past year staff has prepared the attached sign code changes for consideration and recommendation by the Planning Commission to the City Council. The City's land use attorney has reviewed the proposed changes.

#### Analysis of Proposed Sign Code Changes

Attached is a redline copy of the proposed code changes as well as a table showing the impact by zoning district and sign type. The proposed changes are intended to address only a few targeted issues, not overhaul the existing sign code. If adopted as proposed, most businesses will continue to be allowed the same number, size and type of signs as they are now.

The most significant changes from the previous version reviewed include the following:  
*(underlined and italicized below)*

- Allow window signs anywhere in Mission with a limit on coverage of windows of 50% of the window glazing while continuing to limit glazing coverage in the Downtown District to 10%. Permits for window signs would be required.
- Require sign permits and permit fees for temporary signs. Allow for permit fees to be waived for temporary signs in the R-1, R-2 and R-3 residential zones, applications from churches, schools, community centers, libraries and other charitable non-profit entities. Sign permit fees to be established by Resolution.
- Limit the posting of temporary signs to no more than 90 days per calendar year per business. The allowable lengths of posting as follows: Consecutive days 3,7,15,30, or 60; Non-Consecutive Days 30 or 60.
- Prohibit costumed characters as attention attracting devices. Comments received regarding regulating this activity as commercial messaging were neutral to somewhat opposed. Therefore, staff has removed any proposed changes to costumed characters from consideration.
- Eliminate all pole signs by December 31, 2023 through an amortization process. Comments received were largely in opposition or merely neutral on this action. Therefore, staff has removed any amortization proceedings from consideration.



- Continue to regulate/prohibit pole signs. The definitions of pole and monument signs have been edited to clarify the desired sign characteristics. The previous exemption for pole signs meeting certain criteria within 1000' of the intersection of Metcalf Avenue and Johnson Drive has also been removed. Any detached sign which does not meet the proposed definition of monument sign will become a legal non-conforming sign upon adoption of these code changes. Based on a review of the 2015 sign inventory, an additional 17 detached signs will be added to this list. Legal non-conforming signs are regulated under Section 420.220 and may be used and maintained but not refaced to serve a different establishment or business.
- Based on comments received, changes have been added to the definition of sign maintenance to include corporate merger, consolidation or other legal name change as allowable reasons to change copy or logo without being considered sign refacing.

Other changes proposed which have not been altered since they were last presented include:

- Allow marquee signs by right in three additional commercial zoning districts
- Allow monument signs by right in the Main Street District 1 and Pedestrian Oriented Business District zoning districts under certain conditions
- Allow electronic signs for the display of prices for fuel
- Streamline the approval process for electronic time/temperature instruments
- Standardize the processes of placing liens against properties that have been abated by the City

#### Next Steps

The Planning Commission is required to make a recommendation to the City Council whenever changes to the text of the zoning code are proposed.

#### Staff Recommendation

Staff recommends approval of the proposed changes to the the code as presented in the draft ordinance. Text shown as strikethrough will be removed in final version to be adopted by the City Council.



**Proposed City of Mission Sign Regulations  
March 2017**

Definition	Sign Permit Required?	Single-Family Residential	Multi-Family Residential	Civic Buildings (Schools, Churches, etc.)	C1 - CP-1 (Light Commercial)	C-2A, MS1, MS2 (Downtown)	C-O / CP-O (Office)	All Others (Heavy Commercial / Industrial)	
<b>Major Types</b>									
<b>Wall Signs</b>	Any sign attached to and erected parallel to and within one (1) foot of the face or wall of a building, including signs painted on the walls of buildings. Awnings which are backlighted shall be considered wall signs.	Yes	No	No	Per tenant: 2 signs allowed Limit 1 per façade 32 sqft max area	Per tenant: 3 signs allowed 1 per façade not more than 5% of area of each façade	Per tenant: 3 signs allowed 1 per façade not more than 10% of area of each façade	Per building: 3 signs allowed 1 per façade not more than 5% of area of each façade	Per tenant: 3 signs allowed 1 per façade not more than 10% of area of each façade Allowed 1 additional non-illuminated wall sign, max. 9sqft in area at each major entrance to multi-tenant
<b>Monument Signs</b>	A detached sign where the width of the base of the sign is a minimum of one-half (½) the width of the widest part of the sign face or where the base consists of two (2) or more supports so that the top edge of the sign face of a monument sign is ten (10) feet or less above the ground. The materials of the base of a monument sign shall be one (1) of the following: masonry, wood, anodized metal, stone or	Yes	1 at each entrance to a subdivision / project.  Max 6' height Max. 50 sqft per sign.	1 at each entrance to a subdivision / project.  Max 6' height Max. 50 sqft per sign.	1 allowed in lieu of a wall sign  Max. 6' to 8' height based on a setback formula	1 allowed in lieu of a wall sign  6' to 10' height based on a setback formula  Shopping centers do not need to surrender a wall sign	No <b>(YES-where minimum distance from the right-of-way line to the closest building is equal or greater to 15 feet)</b>	1 allowed in lieu of a wall sign  Max. 6' to 10' height based on a setback formula	1 allowed in lieu of a wall sign  Mac. 6' to 10' height based on a setback formula  Shopping centers do not need to surrender a wall sign
<b>Projecting Signs</b>	Any sign, including a time and/or temperature instrument located on the face of the building, extending more than one (1) foot from the face of the building to which it is attached.	Yes	No	No	No	1 allowed in lieu of a wall sign, subject to same area restrictions;  Min. 7' above ground Max. 3' from wall	1 allowed in lieu of a wall sign, subject to same area restrictions;  Min. 7' above ground	No  Min. 7' above ground	
<b>Under Canopy (Pedestrian)</b>		Yes	No	No	No	One at each exterior ground floor entry beneath awnings, canopies, etc.  Max. 3sqft per sign.	One at each exterior ground floor entry beneath awnings, canopies, etc.  Max. 3sqft per sign. Min. 7' above ground	No  Max. 3sqft per sign.	



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<p align="center"><b>Window Signs</b></p>	<p>Not Defined A non-illuminated sign of a permanent nature applied directly on either the interior or exterior surface of building's glazed surface including but not limited to windows and doors.</p>	<p align="center"><b>Yes</b></p>	<p>No except in the Downtown District: <b>(YES) Limited to covering 50% of glazing</b></p>	<p>No except in the Downtown District: <b>(YES) Limited to covering 50% of glazing</b></p>	<p>No except in the Downtown District: <b>(YES) Limited to covering 50% of glazing</b></p>	<p>No except in the Downtown District: <b>(YES) Limited to covering 50% of glazing</b></p>	<p>No except in the Downtown District: <b>(YES)</b> Limited to covering 10% of glazing</p>	<p>No except in the Downtown District: <b>(YES) Limited to covering 50% of glazing</b></p>	<p>No except in the Downtown District: <b>(YES) Limited to covering 50% of glazing</b></p>
<p align="center"><b>Temporary Signs</b></p>	<p>A sign that by its nature or content is intended for posting for a short period of time, is typically constructed from non-durable materials such as paper, cardboard, cloth, plastic and/or wallboard, is not permanently installed, and does not constitute a structure subject to the City's Building Code or Zoning Code provisions.</p>	<p align="center"><b>No</b> <b>(YES-Excluding temp signs in R-1, R-2, R-3 residential zones from having to get a permit and charitable institutions and entities from permit fees)</b></p>	<p align="center">No limitation on number; Aggregate square footage of 48 sqft, with no sign exceeding 16sqft Sign may be up for 60 days, then sign must be removed for 60 days (Limit posting to no more than 90 days per calendar year per business. The allowable lengths of posting are as follows: Consecutive Days (3,7,15,30,or 60) Non-Consecutive Days (30 or 60) Maximum Sign Height 6', required setback of 3' from the property line. Real estate signs (property for sale or lease) excluded from limitations on duration.  For multi-family only: One 50sqft banner sign for 60 days displaying "now leasing", "now renting", etc. for two months every three years</p>						



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	<b>Definition</b>		<b>Single-Family Residential</b>	<b>Multi-Family Residential</b>	<b>Civic Buildings (Schools, Churches, etc.)</b>	<b>C1 - CP-1 (Light Commercial)</b>	<b>C-2A, MS1, MS2 (Downtown)</b>	<b>C-O / CP-O (Office)</b>	<b>All Others (Heavy Commercial / Industrial)</b>
<b>Minor Types</b>									
<b>ATM</b>	Not Defined	<b>No</b>	No	No	No	Where ATMs are allowed: 3 signs, 1 per face, 8 sqft per sign. 1 additional non-illuminated sign, 4sqft.			
<b>Construction Identification</b>	Signs at construction sites that are either: a. Posted at the entrance to the job site explaining rules for contractors and others entering the site; or b. Signs posted on construction site or painted onto construction trailers actively being used on the site, provided such signs are limited to the name, logo, address and phone number of a contractor working at the site. This provision is not intended to allow any signs that advertise a particular project, developer, owner or other business, nor is it intended to allow any sales, leasing or hiring information to be displayed.	<b>No</b>	Excluded From Regulation						
<b>Directional</b>	Not Defined	<b>No</b>	1 sign allowed for each entrance to a building site; each key intersection; each entrance to drive-through. Ground-mounted: Max. 5' height, Max. 4sqft area per sign. Wall-mounted: Max. 2sqft area per sign. Max. 25% area used to display owner / tenant / etc. No sign permits required.						
<b>Entrance Directory</b>	Signs on or adjacent to doors of commercial or industrial buildings displaying information such as the names and address of the occupant, the hours of operation or instructions for deliveries. Such signs shall not exceed four (4) square feet in area, shall not contain letters exceeding six (6) inches in height and shall not be illuminated. Where multiple tenants share the same door, the sign may display the names and address of each tenant.	<b>No</b>	No	No	No	Excluded From Regulation			
<b>Interior Neon</b>	Not Defined	<b>No</b>	No	No	No	1 sign. Max 10sqft. Signs below 3sqft do not require a sign permit.			
<b>Marquee</b>	Any sign attached flat against or under the marquee or permanent sidewalk canopy of a building but not on the upper surface of a marquee or canopy.	<b>Yes</b>	No	No	No	Yes	<b>No (YES)</b>	No	Optional instead of wall signs



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<b>Menu Board</b>	Not Defined	<b>Yes</b>	No	No	No	Where drive-through services are allowed: 2 signs, Max. 8' height, Max. 32sqft area per sign Drive-in establishments: 1 sign per stall, 8sqft area per drive-in sign, 32sqft area per walk-up sign, , Max. 6" lettering height.			
<b>Project Identification</b>	One (1) construction site identification sign of professional quality may be permitted during the development of any project. Such sign may identify the project, the owner or developer, future tenants, the architects, engineers, realtors, contractors and subcontractors involved, funding sources and may contain related information including, but not limited to, sale or leasing information. [...] All such signs shall be removed prior to issuance of any certificate of occupancy, temporary or final, for the last building in each phase of the project. Sign permits shall not be required for such signs.	<b>No</b>	1 sign. Max 6" height, Max. 32sqft area per sign, 10' setback from right-of-way						
<b>Residential Signs</b>	Signs not exceeding four (4) square feet in area that are customarily associated with residential use, limited to signs giving property identification names or numbers, names of residents, signs on mailboxes or newspaper tubes, signs posted on private property relating to private parking or signs warning the public against trespassing or danger from animals.	<b>No</b>	Excluded From Regulation	Excluded From Regulation	Excluded From Regulation	No	No	No	No
<b>Vehicle Signs</b>	Signs painted on or otherwise permanently attached to currently licensed motor vehicles, which vehicles are not primarily used as signs.	<b>No</b>	Excluded From Regulation						



**Proposed City of Mission Sign Regulations  
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	<b>Definition</b>		<b>Single-Family Residential</b>	<b>Multi-Family Residential</b>	<b>Civic Buildings (Schools, Churches, etc.)</b>	<b>C1 - CP-1 (Light Commercial)</b>	<b>C-2A, MS1, MS2 (Downtown)</b>	<b>C-O / CP-O (Office)</b>	<b>All Others (Heavy Commercial / Industrial)</b>
<b>Prohibited Types</b>									
<b>Attention-Attracting</b>	Any device with flashing, blinking, rotating or moving action or any banner, searchlight or balloons designed or intended to attract the attention of the public to an establishment or to a sign.	<b>No</b>	No	No	No	No	No	No	No
<b>Electronic Message</b>	Not defined; prohibited except for "time and temperature instruments" (Adding Fuel Prices)	<b>No</b>	No	No	No	No	No	No	No
<b>Off-Premise</b>	A sign which displays any message directing attention to a business, product, service, profession, commodity, activity, event, person, institution or any other commercial message, which is generally conducted, sold, manufactured, produced, offered or occurs elsewhere than on the premises where such sign is located.	<b>No</b>	No	No	No	No	No	No	No
<b>Outdoor Advertising</b>	A sign, generally known as outdoor advertising, mounted on a semi-permanent structure and depicting information not directly related to the property upon which it is placed. Except where a specific distinction is drawn, the term "billboard" includes "poster panel".	<b>Yes</b>	Special Use Permit	Special Use Permit	Special Use Permit	Special Use Permit	Special Use Permit	Special Use Permit	Special Use Permit
<b>Pole</b>	A sign which is supported by one (1) or more poles, uprights or braces in the ground so that the bottom edge of the sign face is more than ten (10) feet above the ground. (Remove exemption w.i. 1000' of Metcalf Ave & Johnson Drive)	<b>No</b>	No	No	No	No	No	No	No
<b>Roof</b>	Any sign erected, constructed and maintained wholly upon or over the roof line.	<b>No</b>	No	No	No	No	No	No	No



**Proposed City of Mission Sign Regulations  
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<b>Snipe</b>	Any sign of a material such as cardboard, paper, pressed wood, plastic or metal which is attached to a fence, window, tree, utility pole or temporary structure or any sign which is not securely fastened to a building or firmly anchored to the ground.	<b>No</b>	No	No	No	No	No	No	No
<b>Other Prohibitions</b>			Signs in the public right of way; abandoned or obsolete (old tenant / business) signs; signs that are demonstrably false or misleading (i.e. zoning)						



## Pole Sign Index (Non-Conforming Detached Signs)

#	Name/DBA	Address (approx)	Sign Type Under Proposed Code Changes
1	AccuServ	4811 Lamar Ave	Pole
2	Advanced Auto Parts	5360 Johnson Dr	Pole
3	Affiliated Management Services	5651 Broadmoor St	Pole
4	American Stroke Foundation	5916 Dearborn St	Pole
5	Arby's	6780 Johnson Dr	Pole
6	Auman Mack/Agenda USA	5300 Foxridge Dr	Pole
7	BP Gas Station	5500 Johnson Dr	Pole
8	Bridges at Foxridge Apartments	5250 Foxridge Dr	Pole
9	Fast Eddy's	5112 Johnson Drive	Pole
10	First Baptist Church of Mission	5641 Outlook St	Pole
11	Goodyear	5200 Johnson Drive	Pole
12	Hy-Vee	6655 Martway St	Pole
13	Johnny's Bar B-Q*	5959 Broadmoor St	Pole
14	Lamar Professional Building	5940 Lamar Avenue	Pole
15	Little Lambs Montessori School	5837 Riggs Rd	Pole
16	Mattress Furniture Outlet	5930 Broadmoor Dr	Pole
17	McDonalds	6751 Johnson Dr	Pole
18	Mission Bank at Corp Center	5800 Foxridge Dr	Pole
19	Mission Bank Johnson Dr	5201 Johnson Dr	Pole
20	Mission Gardens Apartments	5905 West 58th St	Pole
21	Mission Heights Apartments	5801 Woodson St	Pole
22	Mission West Shopping Center	6500 Martway St	Pole
23	Mission Bank Martway	6040 Martway St	Pole
24	Outlook Apartments	5939 Outlook St	Pole
25	Popeyes*	6821 Johnson Dr	Pole
26	Pride Cleaners	5924 Broadmoor St	Pole
27	Public Storage	6560 Foxridge Dr	Pole
28	Retail/Tenant Change	5945 Woodson St	Pole
29	Sonic	6665 Martway St	Pole
30	Target	6100 Broadmoor St	Pole
31	The Hair Studio	5923 Woodson St	Pole
32	The Maples Apartments	5811 Maple Dr	Pole
33	Town Topic	6018 Johnson Dr	Pole
34	Trinity Lutheran Church	5601 W 62nd St	Pole
35	Uncommon Supply	5860 Beverly Ave	Pole
36	US Properties Building	5845 Horton St	Pole
37	Village Inn*	5800 Broadmoor	Pole
38	Wendy's	5900 Roeland Dr	Pole

Updated to show signs identified as monument signs which will become pole signs under adopted code changes based on the 2015 sign inventory data. Additional signs may be affected. \* indicates pole signs currently allowed w.i. 1000' of Johnson Drive, existing since 2003, having an existing valid sign permit.











## Chapter 430. Signs

### Section 430.010. Statement of Intent.

[Ord. No. 1091 §§1 – 3(App. A §430.010), 9-24-2003]

- A. The intent of this Chapter is to create the framework for a comprehensive and balanced system of sign regulations to facilitate an easy and pleasant communication between people and their environment and to avoid the visual clutter that is potentially harmful to traffic and pedestrian safety, property values, business opportunities and community appearance. With these purposes in mind, it is the intent of this Chapter to authorize the use of signs which are:
1. Compatible with their surroundings,
  2. Appropriate to the activity that displays them,
  3. Expressive of the identity of individual activities and the community as a whole, and
  4. Legible in the circumstances in which they are seen.

### Section 430.020. Sign Definitions.

[Ord. No.1091 §§1 – 3(App. A §430.020), 9-24-2003; Ord. No. 1142 §17, 12-8-2004; Ord. No. 1259 §2, 5-21-2008; Ord. No. 1303 §1, 9-23-2009]

As used in this Chapter, the following terms shall have these prescribed meanings:

#### **ATTENTION-ATTRACTING DEVICE**

Any devices with flashing, blinking, rotating or moving action, ~~flags, streamers, spinners, pennants, or any banner, lights-searchlight, or balloons, inflatable, air-activated, or similar devices or ornamentations~~ designed or intended to attract the attention of the public to an establishment or to a sign.

#### **~~BACKLIGHTED SIGN~~**

~~Any sign which displays direct or indirect light from the back of such sign through a transparent, translucent or open material.~~

#### **~~BUILDING LOT~~**

~~Any pieces or parcel of land or a portion of a subdivision, the boundaries of which have been established by some legal instrument of record, that is recognized and intended as a unit for the purposes of transfer of ownership.~~

#### **COMMERCIAL SIGN MESSAGE**

Any sign wording, logo or other representation that, directly or indirectly, names, advertises or calls attention to a business, product, service or other commercial activity. A sign which identifies, advertises or directs attention to a business or is intended to



induce the purchase of goods, property or service including, without limitation, any sign naming a brand of goods or service and real estate signs, as further defined below.

**DETACHED SIGN**

Any sign located on the ground or on a structure located on the ground and not attached to a building.

**DIRECTLY ILLUMINATED SIGN**

Any sign where the source of illumination of the sign is exposed to the human eye such as, but not limited to, an incandescent bulb or florescent tube.

**INDIRECTLY ILLUMINATED SIGN**

Any sign which is partially or completely illuminated at any time by a light source which is so shielded as to not be visible at eye level.

**MARQUEE SIGN**

Any sign attached flat against or under the marquee or permanent sidewalk canopy of a building but not on the upper surface of a marquee or canopy.

**MONUMENT SIGN**

A detached sign where the width of the base of the sign is a minimum of one-half (1/2) the width of the widest part of the sign face and/or where the base consists of two (2) or more supports so that the top edge of the sign face of a monument sign is ten (10) feet or less above the ground. The materials of the base of a monument sign shall be one (1) of the following: masonry, wood, anodized metal, stone or concrete.

**DIGITAL BILLBOARD**

A billboard which has a computer controlled board that displays an image through the use of light emitting diode display (LED) or similar technology.

**FACADE**

All wall planes of a building which are visible from one side or perspective,

**FUEL PRICING DISPLAY**

A sign or portion of a sign displaying only fuel pricing information with no additional advertising or content.

**MARQUEE**

A permanent roof like structure projecting beyond a building over an entrance often to a hotel, theater, or other building.

**MURAL**

Any mosaic, painting, or graphic art or combination thereof which is professionally applied to a building. A figure, painting or design professionally applied to and made

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~~integral with a wall or ceiling surface and which does not convey any commercial a commercial message information.~~

**NEON TUBE**

~~A directly illuminated sign which is a free formed surface that creates words, numerals, figures, devices, designs, trademarks or logos and for which the light source is luminescent gas.~~

**NON-CONFORMING SIGN SUPPORT**

~~The supporting members of the sign such as frame, poles, brackets or structure by which a sign is mounted on or affixed to a building or the ground which do not comply with the standards established in the Building Code.~~

**OBSOLETE SIGN**

~~An off site or on site sign for a business or activity which is no longer operated on the premises or in the City.~~

**OFF-SITE SIGN**

~~A sign which displays any message directing attention to a business, product, service, profession, commodity, activity, event, person, institution or any other commercial message, which is generally conducted, sold, manufactured, produced, offered or occurs elsewhere than on the premises where such sign is located.~~

**ON-SITE SIGN**

~~A sign that is other than an off site sign.~~

**POLE SIGN**

~~A sign which is supported by one (1) or more poles, uprights or braces in the ground so that the bottom edge of the sign face is more than ten (10) feet above the ground.~~

**NON-COMMERCIAL MESSAGE**

Any sign, wording, logo, or other representation that is not defined as a commercial message.

**POST**

To erect, attach or affix in any manner including, without limitation, nailing, tacking, tying, gluing, pasting, painting, stacking, marking or writing.

**POSTER PANEL OR BILLBOARD**

~~A sign, generally known as outdoor advertising, mounted on a semi permanent structure and depicting information not directly related to the property upon which it is placed. Except where a specific distinction is drawn, the term “billboard” includes “poster panel”.~~

**PROJECT IDENTIFICATION SIGN**

~~A sign identifying only a project and developer.~~



**PROJECTING SIGN**

Any sign, including a time and/or temperature instrument located on the face of the building, extending more than one (1) foot from the face of the building to which it is attached.

**REAL ESTATE SIGN**

A sign indicating the availability for sale, rent or lease of the specific lot, building or portion of a building upon which the sign is posted.

**ROOF SIGN**

Any sign erected, constructed and maintained upon the roof or wholly or partially above a vertical wall of a building, wholly upon or over the roof line.

**SEMI-ILLUMINATED SIGN**

Any sign located on a building face which is uniformly illuminated over its entire area, including the area of the sign, by use of electricity or other artificial light.

**PROJECT**

Multi-family housing development or a single development on identifiable parcel(s) of land.

**SIGHT DISTANCE TRIANGLE**

The triangular area of land at the intersection of streets or a street and driveway formed by a diagonal line connection two points located on intersecting street curb lines the short leg of which shall be twenty (20) feet and the long leg of which shall be one hundred forty (140) feet. Such area shall remain free of any sign related obstruction to vision more than two (2) feet in height measured from the roadway.

**SIGN**

Any framed, bracketed, free-formed, painted or engraved surface which is fabricated to create words, numerals, figures, devices, designs, trademarks or logos, which is mounted on or affixed to a building or the ground and which is sufficiently visible to persons not located on the lot where such device is located to attract the attention of such persons or to communicate information to them. "Sign" includes sign supports. Murals shall not be considered to be signs.

**SIGN, ABANDONED**

A sign, other than a billboard, advertising a business, service, commodity, accommodation, attraction or other enterprise or activity that is no longer operating or being offered or conducted.

**SIGN, ATTACHED**

A sign that is mounted, painted, or otherwise applied to a building.

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**SIGN, BACKLIGHTED**

Any sign whose light source is located in the interior of the sign so that the rays go through the face of the sign.

**SIGN, BANNER**

A sign made of fabric or other non-rigid material with no enclosing framework.

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**SIGN, DETACHED**

Any sign located on the ground or on a structure located on the ground and not attached to a building.

**SIGN, DIRECTLY ILLUMINATED**

Any sign that is illuminated by an external or internal light source that is visible to a person standing on the ground.

**SIGN, DIRECTIONAL**

A sign providing directions necessary or convenient for visitors or clients coming onto a premises.

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**SIGN, ELECTRONIC MESSAGE**

A sign with a fixed or changing display or message composed of a series of lights that may be changed through electronic means.

**SIGN, ILLUMINATED**

A sign illuminated by or exposed to artificial lighting either by lights on or in the sign or directed toward the sign.

**SIGN, INDIRECTLY ILLUMINATED**

Any sign that is completely or partially illuminated at any time by an external light source that is so shielded as to not be visible at eye level.

**SIGN, MARQUEE**

Any sign attached flat against, under, or above the marquee of a building.

**SIGN, MENU BOARD**

A permanently mounted sign displaying the bill of fare for a drive-through or drive-in restaurant.

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**SIGN, MONUMENT**

A detached sign where the width of the base of the sign is a minimum of one-half (1/2) the width of the widest part of the sign face. The materials of the base of a monument sign shall be one (1) of the following: masonry, wood, anodized metal, stone or concrete.



**SIGN, NEON TUBE**

A directly illuminated sign which is a free-formed surface that creates words, numerals, figures, devices, designs, trademarks or logos and for which the light source is luminescent gas.

**SIGN, NON-CONFORMING**

A sign that on the effective date of this Article does not conform to one (1) or more of the regulations set forth in this Article.

**SIGN, OBSOLETE**

An off-site or on-site sign for a business or activity which is no longer operated on the premises or in the City.

**SIGN, OFF-SITE**

A sign which displays any message directing attention to a business, product, service, profession, commodity, activity, event, person, institution or any other commercial message, which is generally conducted, sold, manufactured, produced, offered or occurs elsewhere than on the premises where such sign is located.

**SIGN, ON-SITE**

A sign that is other than an off-site sign.

**SIGN, PEDESTRIAN ORIENTED**

A sign near street or sidewalk level, oriented and scaled to the pedestrian rather than the motorist.

**SIGN, PERMANENT**

A sign attached to a building or structure, or to the ground in a manner that enables the sign to resist environmental loads, such as wind, and that precludes ready removal or movement of the sign and whose intended use appears to be indefinite.

**SIGN, POLE**

A detached sign which is supported by one (1) or more poles, uprights or braces in the ground.

**SIGN, PORTABLE**

A sign whose principal supporting structure is intended, by design and construction, to be used by resting upon the ground for support and may be easily moved or relocated for reuse.

**SIGN, POSTER PANEL OR BILLBOARD**

A sign, generally known as outdoor advertising, mounted on a permanent or semi-permanent structure and depicting information not directly related to the property upon



which it is placed. Except where a specific distinction is drawn, the term “billboard” includes “poster panel” and “digital billboard”.

**SIGN, PROJECT IDENTIFICATION**

A sign identifying only a project or developer.

**SIGN, PROJECTING**

Any attached sign, including a time and/or temperature instrument located on the face of the building, extending more than one (1) foot from the face of the building.

**SIGN, REAL ESTATE**

A temporary sign indicating the availability for sale, rent or lease of the specific lot, building or portion of a building upon which the sign is posted.

**SIGN, ROOF**

Any sign erected, constructed and maintained upon the roof or wholly or partially above a vertical wall of a building.

**SIGN, SNIPE**

Any sign of a material such as cardboard, paper, pressed wood, plastic or metal which is attached to a fence, window, tree, utility pole or temporary structure or any sign which is not permanently fastened to a building or firmly anchored to the ground.

**SIGN, TEMPORARY**

A sign that by its nature or content is intended for posting for a short period of time, is typically constructed from non-durable materials such as paper, cardboard, cloth, plastic and/or wallboard, is not permanently installed, and does not constitute a structure subject to the City’s Building Code or Zoning Code provisions.

**SIGN, WALL**

Any sign attached to and erected parallel to and within one (1) foot of the face or wall of a building, including signs painted on the walls of buildings. Awnings which are backlighted shall be considered wall signs.

**SIGN, WINDOW**

A non-illuminated sign of a permanent nature applied directly on either the interior or exterior surface of a building’s glazed surface including but not limited to windows and doors.

**SIGN ALTERATION**

The replacement, enlargement, reduction, reshaping or adding to a sign, frame, pole, brackets or any supporting member.

**SIGN AREA**

The entire face of a sign including any framing, trim, or molding, but not including the supporting structure.



### **SIGN HEIGHT**

The vertical distance measured from the average ground level to the highest point of the sign or sign structure.

### **SIGN MAINTENANCE**

The normal care and minor repair that is necessary to retain a safe, attractive and finished structure, frame, ~~supportspole~~, brackets or surface. Changing copy or logo without increasing sign dimensions shall be considered maintenance if the information, product or service depicted remains the same and if the sign is to serve the identical establishment using the same business firm name as before the change. Maintenance may include a change in name on the sign due to corporate merger, consolidation or other legal name change.

### **SIGN REFACING**

Changing or replacing the words, numerals or other surface of the sign to serve a different establishment or business or to create a substantially different visual effect without altering, moving or replacing the structure, frame, ~~pole~~ supports, or bracket supporting the sign.

### **SIGN SUPPORT**

The supporting members of the sign such as frame, poles, brackets or structures by which a sign is mounted on or affixed to a building or the ground.

### **~~SNIFE SIGN~~**

~~Any sign of a material such as cardboard, paper, pressed wood, plastic or metal which is attached to a fence, window, tree, utility pole or temporary structure or any sign which is not permanently/securely fastened to a building or firmly anchored to the ground.~~

### **~~TEMPORARY SIGN~~**

~~A sign that by its nature or content is intended for posting for a temporary period of time/posting on public or private property, is typically constructed from non-durable materials such as paper, cardboard, cloth, plastic and/or wallboard and does not constitute a structure subject to the City's Building Code or Zoning Code provisions.~~

### **TIME AND/OR TEMPERATURE INSTRUMENT**

A sign displaying only time and/or temperature information with no additional advertising or comments.

### **~~TWO-FACED SIGN~~**

~~A sign with two (2) sign faces where the angle of separation of the faces is not greater than ninety percent (90%).~~

### **~~WALL SIGN~~**

~~Any sign attached to and erected parallel to and within one (1) foot of the face or wall of a building, including signs painted on the walls of buildings. Awnings which are backlighted shall be considered wall signs.~~



**WINDOW SIGN**

~~A non-illuminated sign of a permanent nature applied directly on glass. A window sign does not include placards or poster board panels leaning in windows, which are prohibited, except for open/closed signs that shall be no larger than one (1) square foot.~~

**WINDOW DISPLAY**

A window, storefront, or opening in the exterior wall of any portion of a building through which merchandise, services, or business are displayed or advertised.

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Section 430.030. Generally.

[Ord. No. 1091 §§1 – 3(App. A §430.030), 9-24-2003; Ord. No. 1259 §3, 5-21-2008]

Other than lawful non-conforming signs, no signs shall be permitted in any district except in accordance with the provisions of this Chapter. Signs placed in the public right-of-way or on publicly owned land and which are not in compliance with the regulations of this Chapter are subject to removal by the City without notice or compensation. Signs placed on private property that are not in compliance with the regulations of this Chapter ~~are shall be removed~~ subject to removal by the property owner or the property owner’s agent without notice or compensation. In addition, any sign that is not in compliance with the regulations of this Chapter is subject to the penalty provisions of Section **400.100**. These provisions may be applied to the person or business placing such sign, business or organization advertised in such sign or the owner of the property or vehicle on which such sign is placed.

Section 430.040. Permit Required.

[Ord. No. 1091 §§1 – 3(App. A §430.040), 9-24-2003]

~~B.A.~~ Except as otherwise provided in this Chapter, no sign shall be installed, erected or set in place until a sign permit has been issued by the ~~City Public Works Department~~ in accordance with Section **103.040(C)**.

~~C.B.~~ Applications for sign permits shall be filed upon forms prescribed by the City, accompanied by ~~two-one (12)~~ one (1) sets of ~~plans plats~~ drawn to scale indicating the sign size, location, method of illumination, content, colors, materials of the sign and structure, and method of attachment.

~~D.~~ All signs hereafter installed shall have permanently affixed thereto a label clearly visible at all times indicating the number of the sign permit issued therefor.

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~~E.C.~~ All signs are to be designed, fabricated and installed in a professional manner by a qualified and City licensed sign professional, except that temporary ~~signs banners~~ may also be installed by a business or property owner.

~~F.D.~~ With the exception of temporary signs, all materials and finishes are expected to be of high quality with an anticipated life span of ten (10) years or more. All electrical signs must have a UL label.

### Section 430.050. Signs Excluded From Regulation.

[Ord. No. 1091 §§1 – 3(App. A §430.050), 9-24-2003; Ord. No.1259 §4, 5-21-2008]

A. Except for the provisions of Subsections **(A)**, **(B)**, **(C)** and **(D)** of Section 430.110, ~~and Subsection (E) of Section 430.090,~~ the following signs are excluded from regulation under this Chapter:

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1. Signs not exceeding four (4) square feet in area that are customarily associated with residential use, limited to signs giving property identification names or numbers, names of residents, signs on mailboxes or newspaper tubes, signs posted on private property relating to private parking or signs warning the public against trespassing or danger from animals.
2. Signs erected by or on behalf of or pursuant to the authorization of a governmental body, including legal notices, identification and informational signs and traffic, directional or regulatory signs.
3. Official signs of a non-commercial nature erected by public utility companies.
4. Flags, pennants or insignia of any governmental body when not displayed in connection with a commercial promotion or as an advertising device and provided that not more than three (3) governmental flags, pennants or insignia shall be displayed on any property.
5. Integral decorative or architectural features of buildings or works of art, so long as such features or works do not contain letters, trademarks, moving parts or lights.
6. Signs on or adjacent to doors of commercial or industrial buildings displaying information such as the names and address of the occupant, the hours of operation or instructions for deliveries. Such signs shall not exceed four (4) square feet in area, shall not contain letters exceeding six (6) inches in height, ~~and shall not be illuminated.~~ Where multiple tenants share the same door, the sign may display the names and address of each tenant.
7. Signs painted on or otherwise permanently attached to currently licensed motor vehicles, which vehicles are operable and not primarily used as signs.



~~7. Window Temporary signs.~~

8. Signs at construction sites that are either:

- a. Posted at the entrance to the job site explaining rules for contractors and others entering the site; or
- b. Signs posted on construction site or painted onto construction trailers actively being used on the site, provided such signs are limited to the name, logo, address and phone number of a contractor working at the site. This provision is not intended to allow any signs that advertise a particular project, developer, owner or other business, nor is it intended to allow any sales, leasing or hiring information to be displayed.

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Section 430.060. Prohibited Signs.

[Ord. No.1091 §§1 – 3(App. A §430.060), 9-24-2003; Ord. No. 1142 §18, 12-8-2004; Ord. No.1259 §5, 5-21-2008; Ord. No. 1303 §2, 9-23-2009]

- A. The following signs are prohibited except as stated below or as otherwise specifically provided hereinafter:
  - 1. Outdoor advertising (such as poster panels, billboards and off-site promotional signs), except where a special use permit has been obtained for such sign.
  - 2. Attention-attracting devices, ~~pennants and streamers.~~
  - 3. Snipe signs, portable signs, placards or poster board panels leaning in windows, or similar signs that are not permanently affixed to a building, structure or the ground, other than temporary signs as specifically permitted by this Chapter.
  - 4. Directly illuminated signs, except when used with fixtures specifically designed to illuminate signs or with marquee signs. All direct lighting must be shielded and confined to the sign surface and must not be a nuisance to neighboring residential properties.
  - 5. Electronic message signs, other than time and/or temperature instruments or fuel pricing displays.
  - 6. Pole signs, ~~except that an existing business as of 2003 within one thousand (1,000) feet of the intersection of Johnson Drive and Metcalf Avenue are permitted to maintain, reface or replace existing pole signs for which a valid sign permit has previously been issued. New signs that replace existing pole signs must reduce the non-conforming height, area or setback of the previously issued sign permit.~~
  - 7. Roof signs.



8. Signs in the public right-of-way; except for governmental signs, traffic signs, temporary signs as allowed in this Chapter and signs specifically allowed in the right-of-way by the provisions of this Chapter.
9. Obsolete signs.
10. Obscene signs. "Obscene" is defined as any material that: (a) whether the average person applying contemporary community standards would find that the work, taken as a whole, appeals to the prurient interest; (b) whether the work depicts or describes, in a patently offensive way, sexual conduct specifically defined by the applicable State law; and (c) whether the work, taken as a whole, lacks serious ~~literary~~ literary, artistic, political or scientific value.
11. Abandoned ~~non-conforming~~ signs.
12. Signs containing false or misleading advertising.
13. Signs falsely stating that property is zoned for specified land uses or signs stating that property will be zoned for specified land uses in the future.
14. Signs declaring that property is reserved for or is being held for a future land use which is inconsistent with the then existing zoning classification for the land.

#### Section 430.070. Signs Permitted in All Districts.

[Ord. No. 1091 §§1 – 3(App. A §430.070), 9-24-2003; Ord. No. 1142 §19, 12-8-2004; Ord. No. 1259 §6, 5-21-2008]

- A. Churches, schools, libraries, community centers or other public/semi-public facilities shall be allowed two (2) wall signs with not more than one (1) on a facade. No such sign shall have an overall area exceeding thirty-two (32) square feet. In lieu of one (1) of the wall signs, one (1) ~~detached~~ monument sign shall be permitted. Such monument sign shall be located on the premises and not less than ten (10) feet from the street right-of-way and each sign face shall not exceed fifty (50) square feet in area. The height of such sign shall not exceed six (6) feet above the average grade or six (6) feet above the nearest adjacent street grade, whichever is greater; provided that for each two (2) foot setback from the street right-of-way in excess of ten (10) feet, an additional foot may be added to the height of the sign to a maximum of eight (8) feet. These institutions shall be exempt from the requirement of payment of fees as indicated in Section **103.040(C)**.
- B. One (1) construction site identification sign of ~~good~~ professional quality may be permitted during the development of any project. Such sign may identify the project, the owner or developer, future tenants, the architects, engineers, realtors, contractors and subcontractors involved, funding sources and may contain related information including, but not limited to, sale or leasing information. Such sign shall not exceed six (6) feet in height or thirty-two feet (32) square feet per face with a maximum of two (2) faces and shall be set back at least ten (10) feet from the street right-of-way. All such signs shall be



removed prior to issuance of any certificate of occupancy, temporary or final, for the last building in each phase of the project. Sign permits shall not be required for such signs.

- C. One (1) ~~parking lot~~ directional sign shall be permitted at each entrance to a building site, at key intersections within the site and at each entrance to a drive-through facility. Ground-mounted signs shall not exceed five (5) feet in height, shall not exceed four (4) square feet of sign area per face and may be single- or double-faced. Wall-mounted signs shall not exceed two (2) square feet of sign area and shall be single-faced. Such signs may indicate entrances, exits, addresses, direction of traffic flow and the location of loading docks, parking areas, leasing offices, ATMs, delivery doors, drive-through lanes and similar facilities. Up to twenty-five percent (25%) of the area of the sign may be used to display the name or logo of the developer, building or principal tenant. Sign permits shall not be required for such signs.
- D. *Substitution Of Messages.* Subject to the land owner's consent, a non-commercial message of any type may be substituted for any duly permitted or allowed commercial message or any duly permitted or allowed non-commercial message, provided that the sign or sign structure is legal without consideration of message content. This substitution of message may be made without any additional approval or permitting. The purpose of this provision is to prevent any inadvertent favoring of commercial speech over non-commercial speech or favoring of any particular non-commercial message over any other non-commercial message. This provision does not create a right to increase the total amount of signage on a parcel or land use, nor does it affect the requirement that a sign structure or mounting device be properly permitted. This provision does not allow for the substitution of an off-site commercial message in place of an on-site commercial message.
- ~~E.~~ *Variance.* Any person seeking minor adjustments to the strict application of this Section to the posting of a temporary sign shall file an "application for variance" in accordance with Section ~~440.310~~ of the Zoning Code.
- ~~F-E.~~ *Penalties.* Violations of the provisions of this Section shall be a civil infraction and punishable by a minimum fine of fifty dollars (\$50.00) and up to five hundred dollars (\$500.00). Each day of a continued violation shall constitute a separate violation for the purposes of applying the penalty provision of this Section.
- ~~G-F.~~ *Severability.* Severability is intended throughout and within the provisions of this ~~Chapter~~Section. If any Section, Subsection, sentence, clause, phrase or portion of this ~~Chapter~~Section is held to be invalid or unconstitutional by a court of competent jurisdiction, then that decision shall not affect the validity of the remaining portions of this ~~Chapter~~ Section.

#### Section 430.080. Signs Permitted in Residential Districts.

[Ord. No. 1091 §§1 – 3(App. A §430.080), 9-24-2003; Ord. No. 1259 §7, 5-21-2008]



- A. In lieu of any sign permitted under ~~this Section, Subsection (A)~~, a structure being used as a residential real estate sales or leasing office may be permitted one (1) non-illuminated, detached sign. Such sign shall be not more than twelve (12) square feet in area with a maximum height of six (6) feet above grade and shall be set back a minimum of ten (10) feet from the street right-of-way. Sign permits shall not be required for such signs.
- B. In addition to construction site identification signs, each multi-family project may have one (1) banner-type sign not exceeding fifty (50) square feet. Such banners may be located only on the roof or walls of the project clubhouse or on one (1) of the individual multi-family buildings within the project. Said banner shall be in place for a maximum time period of sixty (60) days from the issuance of the sign permit and may not be renewed except that each complex may obtain additional permits for the above described banners after a three (3) year time period has passed from the issuance of previous sign permits for a banner for the subject multi-family project. Any such banners may only contain the words “now renting” or “now leasing” or “for sale”, along with the name of the complex, address or location, phone numbers, development company or owner and directional symbols.
- ~~C. Project identification signs for a subdivision or residential project may be permitted at each entrance to the subdivision or project. All project identification signs shall be monument signs and, except as hereinafter provided, shall be located on the premises. Such signs shall not exceed six (6) feet in height above the average grade and the sign face shall not exceed fifty (50) square feet in area per face. If not sitting within the landscaped setback, the sign base shall be located within a curbed landscaped area extending a minimum of three (3) feet on all sides of the sign base. Where a project is situated on both sides of a public or private street, one (1) project identification sign may be located on each side of the street or, alternatively, one (1) project identification sign may be located in a landscaped median with the approval of the Planning Commission. Where a project identification sign is to be located in the public right-of-way, plans shall be submitted to the Planning Commission indicating the location, size and design of the project identification sign as well as a copy of the bylaws or other documentation of the association which will have permanent responsibility for maintenance of the sign and required landscaping.~~ ~~In addition to project identification signs permitted at entrances to the subdivision or project, project identification signs may be permitted on the premises adjacent to the intersection of two (2) thoroughfares or the intersection of a thoroughfare and a collector street.~~
- ~~D. Window signs covering no more than 50% of the glazing on any façade of a building.~~

**Section 430.090. Signs Permitted in Commercial and Industrial Districts.**

[Ord. No. 1091 §§1 – 3(App. A §430.090), 9-24-2003; Ord. No. 1142 §20, 12-8-2004; Ord. No. 1259 §8, 5-21-2008]

- A. To the extent that the residential land use is permitted in a commercial or industrial district, signs for a residential land use shall be permitted as in the applicable residential district.

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B. *Districts “C-O” And “CP-O”.*

1. In Districts “C-O” and “CP-O”, wall signs are permitted on three (3) facades, The total area of signs on each facade shall not exceed five percent (5%) of the area of the wall upon which it is mounted.

~~2.~~ In lieu of one (1) of the wall signs, one (1) ~~detached~~ monument sign for each building shall be permitted. Such sign shall not exceed six (6) feet in height above the average grade and the sign face shall not exceed fifty (50) square feet in area per face if located at least ten (10) feet from the public street right-of-way or private street curb line. For each additional two (2) foot setback from the public street right-of-way or private street curb line over ten (10) feet, one ~~EO~~ additional foot may be added to the height of the sign to a maximum of ten (10) feet. If not sitting within the landscaped setback, the sign base shall be located within a curbed landscaped area extending a minimum of three (3) feet on all sides of the sign base.

~~2-3.~~ Window signs covering no more than 50% of the glazing on any façade of a building.

C. *Districts “C-1” And “CP-1”.*

1. Each business or commercial establishment shall be permitted not more than three (3) wall signs, not more than one (1) on each business facade. The area of such sign shall not exceed five percent (5%) of the total area of the business facade on which it is placed. Alternatively, multi-story buildings may be permitted three (3) wall signs per building, not more than one (1) sign per building facade, as indicated above for Districts “C-O” and “CP-O”. Such signs shall not extend above the height of the wall upon which they are mounted. In addition, one (1) non-illuminated wall sign, not more than nine (9) square feet in area, may be placed at each major entrance to a multi-tenant building.

2. In lieu of one (1) wall sign, one (1) projecting sign of the same area as the wall sign replaced shall be permitted, provided that no projecting sign shall extend more than three (3) feet from the face of the building. The lowest point of such sign must be a minimum of seven (7) feet above the ground or sidewalk.

3. Except in the case of a shopping center designed as one (1) ~~project development unified entity~~ and consisting of one (1) or several buildings, either attached or freestanding, one (1) ~~detached~~ monument sign shall be permitted in lieu of one (1) wall sign for each commercial building, which sign shall conform to the height, size and setback requirements applicable to monument signs in Districts “C-O” and “C P-O”.

4. In the case of a shopping center designed as one (1) ~~project development unified entity~~ and consisting of one (1) or several buildings, either attached or freestanding, one (1) additional ~~detached~~ monument sign may be permitted identifying the entire center. Such sign shall conform to the height, size and

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setback requirements applicable to monument signs in Districts “C-O” and “CP-O”.

5. For buildings with a non-retractable awning, canopy or covered walkway along one (1) or more sides of the building, a sign may be hung or mounted under the canopy or covered walkway identifying each business with an exterior ground floor entrance. Such signs shall be located at or near the business entrance, shall be oriented toward pedestrians on the walkway and shall not exceed three (3) square feet in sign area per face. One (1) sign shall be permitted per business entry.

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5-6. Window signs covering no more than 50% of the glazing on any façade of a building.

D. *Districts “C-2, “CP-2”, “C-2B”, “CP-2B” And Industrial Districts.*

1. Each business or commercial establishment shall be permitted not more than three (3) wall ~~or marquee~~ signs, not more than one (1) on each business facade, the area of which sign shall not exceed ten percent (10%) of the total area of the facade upon which it is placed. Such sign shall not extend above the height of the wall ~~or marquee~~ on which it is mounted. In addition, one (1) non-illuminated wall sign, not more than nine (9) square feet in area, may be placed at each major entrance to a multi-tenant building. Alternatively, multi-story buildings may be permitted three (3) wall signs per building, not more than one (1) sign per building facade, as indicated above for Districts “C-O” and “CP-O”.

2. In lieu of any or all of the wall signs, up to three (3) marquee signs, not more than one (1) on each marquee façade, shall be permitted for each establishment. Marquee signs may incorporate direct illumination into their design.

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2-3. In lieu of one (1) of the attached signs, one (1) projecting sign shall be permitted for each establishment, provided that the area of such projecting sign shall not exceed ten percent (10%) of the total area of the facade upon which it is attached and shall not extend above the roof level of the building where the sign is located. The lowest point of such sign must be a minimum of seven (7) feet above the ground or sidewalk.

3-4. Except in the case of a shopping center designed as one (1) project development unified entity and consisting of one (1) or several buildings, either attached or freestanding, one (1) ~~detached~~ monument sign shall be permitted for each commercial building in lieu of one (1) wall sign or projecting sign. Such sign shall conform to the height, size and setback requirements applicable to monument signs in Districts “C-O” and “CP-O”



~~4.5.~~ In the case of a shopping center designed as one (1) ~~project-development unified entity~~ and consisting of one (1) or several buildings, either attached or freestanding, one (1) additional ~~monument detached~~ sign may be permitted identifying the entire center. A gasoline service station, designed as part of the center, may also have a ~~detached-monument~~ sign. All other signs in the center shall consist of wall, projecting or marquee signs. The ~~detached-monument~~ sign shall conform to the height, size and setback requirements applicable to monument signs in Districts “C-O” and “CP-O”.

~~5.6.~~ Drive-through restaurants and car washes may have two (2) menu boards located in conjunction with each drive-through lane. Such sign shall not exceed eight (8) feet in height or thirty-two (32) square feet of sign area per face. Drive-in restaurants or food service establishments with walk-up service windows may have one (1) menu board per order station. Menu boards for drive-in service shall not exceed eight (8) square feet of sign area per face and menu boards for walk-up service shall not exceed thirty-two (32) square feet of sign area per face. All menu boards shall be oriented toward the location of the vehicle or pedestrian being served and shall not contain lettering that exceeds six (6) inches in height.

7. For buildings with a non-retractable awning, canopy or covered walkway along one (1) or more sides of the building, a sign may be hung or mounted under the canopy or covered walkway identifying each business with an exterior ground-floor entrance. Such signs shall be located at or near the business entrance, shall be oriented toward pedestrians on the walkway and shall not exceed three (3) square feet in sign area per face. One (1) sign shall be permitted per business entry.

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~~6.8.~~ Window signs covering no more than 50% of the glazing on any façade of a building.

E. Districts “C-2A”, MSI” And “MS2”.

~~1.~~ ~~Each business or commercial establishment shall be permitted signage as follows:~~

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~~a.1.~~ ~~Each business or commercial establishment shall be permitted three (3) wall signs, not more than one (1) on a facade, provided that the area of each sign shall not exceed ten percent (10%) of the total area of the facade upon which it is placed. Such signs shall not extend above the height of the wall upon which they are mounted.~~

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~~b.2.~~ In lieu of one (1) of the wall signs, one (1) projecting sign shall be permitted for each establishment, provided that the area of such projecting sign does not exceed ten percent (10%) of the total area of the facade upon which it is attached or ten (10) square feet, whichever is more, and does not extend above the roof level of the building where the sign is located. The lowest point of such sign must be a minimum of seven (7) feet above the ground or sidewalk.



3. A combination of a wall sign and projecting sign shall also be allowed, in which case the total area of both signs shall not exceed ten percent (10%) of the total area of the facade upon which the signs are attached. The projecting sign shall not be more than ten (10) square feet.

4. In lieu of any or all of the wall signs, up to three (3) marquee signs, not more than one (1) on each marquee façade, shall be permitted for each establishment. Marquee signs may incorporate direct illumination into their design

e. Except in the case of a shopping center designed as one (1) project and consisting of one (1) or several buildings, either attached or freestanding, one (1) monument sign shall be permitted in lieu of one (1) wall sign for each commercial building, which sign shall conform to the height, size and setback requirements applicable to monument signs in Districts “C-O” and “C P-O” and be subject to the following restriction:

6. The minimum distance from the right-of-way line to the closest building or buildings on the portion of the property where the sign is to be located must be equal to or greater than fifteen (15) feet.

7. In the case of a shopping center designed as one (1) project and consisting of one (1) or several buildings, either attached or freestanding, one (1) additional monument sign may be permitted identifying the entire center. Such sign shall conform to the height, size and setback requirements applicable to monument signs in Districts “C-O” and “CP-O”.

2-8. In addition to the permitted wall signs, one (1) pedestrian oriented sign per business entry shall be permitted to be placed beneath a non-retractable awning and/or canopy that extends from the building. Such sign shall be limited to three (3) square feet in area and may be internally illuminated. The lowest point of such sign must be a minimum of seven (7) feet above the ground or sidewalk. In lieu of said pedestrian oriented sign, one (1) non-illuminated identification sign, not more than three (3) square feet in area, may be placed on the wall at each major entrance to the building. The highest point of the sign may be no higher than seven (7) feet above the sidewalk.

3-9. Project identification signs for a residential project may be permitted at each entrance to the project. All project identification signs shall be monument signs and, except as hereinafter provided, shall be located on the premises. Such signs shall not exceed six (6) feet in height above the average grade and the sign face shall not exceed fifty (50) square feet in area per face. If not sitting within the landscaped setback, the sign base shall be located within a curbed landscaped area extending a minimum of three (3) feet on all sides of the sign base. Where a project is situated on both sides of a public or private street, one (1) project identification sign may be located on each side of the street or, alternatively, one (1) project identification sign may be located in a landscaped median with the approval of the Planning Commission. Where a project identification sign is to be

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located in the public right-of-way, plans shall be submitted to the Planning Commission indicating the location, size and design of the project identification sign as well as a copy of the bylaws or other documentation of the association which will have permanent responsibility for maintenance of the sign and required landscaping.

~~4. On property zoned "MS2", monument signs are permitted subject to the following restrictions;~~

~~a. One (1) detached monument sign shall be permitted for each building in lieu of one (1) wall sign or projecting sign; provided however, that in the case of a shopping center designed as one (1) unified entity and consisting of one (1) or several buildings, either attached or freestanding, one (1) detached monument sign shall be permitted for the entire center.~~

~~b. The minimum setback from the right-of-way line to the closest building or buildings on the lot must be equal to or greater than fifteen (15) feet.~~

~~e. Such sign shall not exceed six (6) feet in height above average grade and the sign face shall not exceed fifty (50) square feet in area per face. If not sitting within the landscaped setback, the sign base shall be located within a curbed landscaped area which is equal to or greater than two hundred (200) square feet.~~

~~d. The minimum setback for the monument sign is ten (10) feet.~~

~~5. Monument signs are not permitted in the "C-2A" and "MS1" Districts unless a minimum sufficient ten (10) foot setback exists for such a sign.~~

~~6-10. Window signs covering no more than 50% of the glazing of any façade of a building except in the area defined in Section 410.330 as the Downtown District, the following requirements shall apply:~~

~~a. Window signs are geared towards pedestrians and therefore should be no larger than ten percent (10%) of the storefront glazing. Signs should be of a permanent nature applied directly on the glass. No placards or poster board panels leaning in windows are allowed, except for open/closed signs that shall be no larger than one (1) square foot.~~

~~b. No canopies, panels or awnings which span through two (2) or more storefronts shall be allowed. Backlit awnings are permitted only if the awning is opaque. If a portion of a backlit awning is not opaque, then that portion shall be considered a "sign".~~

F. *Districts "C-1", "CP-1", "C-2", "CP-2", "C-2A", "C-2B", "CP-2B", "MS1" And "MS2". Where one (1) retail establishment (the "subtenant") leases space and conducts business within another retail establishment (the "primary tenant") but does not have an*



exterior business facade and an exterior door leading directly to the subtenant space, one ~~EO~~ exterior wall sign may be permitted if all of the following conditions are met:

1. The subtenant's business establishment occupies at least one hundred (100) square feet of floor area and is staffed and open for business during predetermined hours.
2. The primary tenant's business establishment occupies at least twenty-five thousand (25,000) square feet of floor area.
3. The subtenant's business is a separate legal entity from the primary tenant's business, as opposed to a department, division or subsidiary of the primary tenant's business. The application may be required to include a copy of the lease agreement between the subtenant and the primary tenant or other documentation acceptable to the City Attorney to establish that the two (2) businesses are separate legal entities.
4. A private sign criteria for the building or shopping center has been submitted to and approved by the Planning Commission which specifically provides for subtenant signage, including standards for the sign location, size, style, color and content. Such sign criteria shall include scale drawings of the facades of all primary tenants where subtenant signs are authorized showing the permitted locations for subtenant signs.
5. The total area for all signs on the same facade does not exceed the allowable signage area for that district. The sign permit application for a subtenant sign shall include a scale drawing of the primary tenant's facade showing the location and size of the subtenant sign and all other signage on that facade. ~~The application shall also include a copy of the lease agreement between the subtenant and the primary tenant or other documentation acceptable to the City Attorney that the two (2) businesses are separate legal entities.~~ The provisions of this Section for subtenant signs shall not apply to businesses within an enclosed shopping mall or to businesses that are conducted primarily by automated machines.

- G. ~~Interior n~~ Neon signs or tubes ~~as defined in this Subsection~~ shall only be allowed when they are placed inside a building and be allowed only in Districts "C-1", "CP-1", "C-2", "CP-2", "C-2A", "MS1", "MS2", "MP" and "M-1". If such signs or tubes are within forty-eight (48) inches of any window or door and visible from outside the building, then such sign or tubing shall not occupy an area greater than ten (10) square feet and only one (1) such neon sign area shall be allowed per business; provided however, that such signs under three (3) square feet shall not be required to have a sign permit. All such neon signs or tubes shall not blink, flash or otherwise be used to display intermittent lighting sequences or to simulate motion. Permitted neon tubes or signs shall be installed, wired and inspected in accordance with the adopted building codes of the City of Mission. Neon signs shall not be oriented addressed towards residential areas.
- H. Any neon sign or tubes removed for other than repair shall not be replaced with any other sign or surface which is not in compliance with existing laws and requirements.



- I. In districts where automated teller machines (ATMs) are permitted, up to three (3) signs are permitted, no more than one (1) per machine/canopy face, identifying the ATM or the bank, financial institution or ATM network associated with that machine. Such signs shall not exceed eight (8) square feet in area. In addition, one (1) non-illuminated sign is permitted per ATM to identify the types of ATM cards accepted or special instructions to ATM users. Such sign shall not exceed four (4) square feet in area, shall be mounted flush to the side of the machine and shall not require a sign permit.

J. Only the portion of an awning which is backlit and transparent shall be considered a sign.

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### Section 430.100. Signs Permitted in Conjunction With Special Use Permits.

[Ord. No. 1091 §§1 – 3(App. A §430.100), 9-24-2003]

- A. In the case of signs permitted by the issuance of a special use permit ~~uses~~, all ~~wall and detached~~ signs shall be approved by City Council after recommendation from the Planning Commission, except where private sign criteria have been previously approved for the project development.
- B. The special use permit for signage shall be processed as required in Sections **440.050** through **440.140**.
- C. Where appropriate, the sign regulations of the underlying zoning district or the most analogous zoning district ~~shall~~may be followed.

### Section 430.110. Additional Regulations Applicable To All Districts.

[Ord. No. 1091 §§1 – 3(App. A §430.110), 9-24-2003; Ord. No. 1142 §21, 12-8-2004; Ord. No. 1259 §9, 5-21-2008]

- A. No sign may be located such that it substantially interferes with the view necessary for motorists to proceed safely through intersections or to enter onto or exit from public or private streets or driveways. No detached sign ~~ground-mounted sign with a height of more than thirty six (36) inches above the road level~~ shall be located in the sight distance triangle on a corner lot, ~~the short leg of which shall be twenty (20) feet and the long leg of which shall be one hundred forty (140) feet measured along the curb line or edge of pavement.~~
- B. No sign may be erected such that by its location, color, size or shape, it would tend to obstruct the view of or be confused with traffic signals or other signs erected by governmental agencies.
- C. All signs shall be of sound structural quality, be maintained in good repair and have a clean and neat appearance. Land adjacent to such signs shall be kept free from debris, weeds and trash. If signs are not being maintained as described and the City Director of Public Works deems them a public hazard or nuisance, such signs shall be ordered repaired or removed.



D. No part of any sign shall be located closer than ten (10) feet from any side or rear property line.

~~E.~~ Time and/or temperature instruments may be permitted provided they are designed in harmony with the building and are architecturally compatible. ~~Such signs shall be subject to approval by the approved by City Council.~~

~~F.E.~~ The dimensions of a backlighted sign, canopy or panel shall be measured by constructing a rectangle from the highest, lowest and widest points of such object, display or surface which displays a light source, except that the opaque surfaces immediately related to or a part of that same sign or panel shall also be calculated in the dimensions of the sign. Provided further, in calculating the dimensions of a continuous panel which spans at least eighty percent (80%) of any one (1) facade of a single or common structure with multiple tenants, the continuous opaque areas between, over and below the lighted areas for such independent operations shall not be calculated to determine the dimensions of the signs, canopies or panels.

~~F.~~ The dimensions of a ~~wall~~ sign shall be measured by constructing a rectangle from the highest, lowest and widest points of such object, display or surface.

~~H.~~ ~~Any pole sign existing on January 1, 2016 shall be removed no later than December 31, 2023, it being the legislative finding of the Governing Body that a period of seven (7) years is a reasonable time in which to require removal.~~

#### Section 430.120. Private Sign Criteria .

[Ord. No. 1091 §§1 – 3(App. A §430.120), 9-24-2003]

All hotels and motor hotels and shopping centers, business parks, office parks, ~~or~~ industrial parks and MXD developments shall be required to prepare a set of sign criteria governing all exterior signs in the development to assure harmony and visual quality throughout the development. As an alternative to the specific sign requirements of any commercial or industrial district, the owner may seek approval of an alternate set of sign criteria. Such criteria shall be binding upon all subsequent purchasers or lessees within the development. The size, colors, materials, styles of lettering, appearance of logos, types of illumination and location of signs shall be set out in such criteria. ~~In all respects, the criteria shall be within the regulations set out in this Code and shall be for the purpose of assuring harmony and visual quality throughout the development.~~ Final development plans ~~(in the case of a planned zoning district) or building permits (in the case of a conventional zoning district)~~ shall not be approved until the Planning Commission has approved the sign criteria. No sign permit shall be issued for a sign that does not conform to the criteria. For purposes of this Section, the terms “*shopping centers, business parks, office parks, ~~or~~ industrial parks, or MXD developments*” shall mean a project of one (1) or more buildings that has been planned as an integrated unit or cluster on property under unified control or ownership at the time that zoning was approved by the City. The sale, subdivision or other partition of the site after zoning approval does not exempt the project or portions thereof from complying with these regulations relative to the number of detached signs, harmony and visual quality of signs to be installed. Where the ownership of the development is divided among two (2) or more owners,



proposed amendments to the sign criteria must have written support from all owners or be specifically designed to affect on the property owned or represented by the applicant. Provided, however, that approved sign criteria shall in no event include prohibited signs.

### Section 430.130. (Reserved)

[1] *Editor's Note – Ord. no. 1259 §10, adopted May 21, 2008, repealed section 430.130 "political signs" in its entirety. Former section 430.130 derived from ord. no. 1091 §§7 – 3(App. A §430.730), 9-24-03. At the editor's discretion, this section has been reserved for the city's future use.*

### Section 430.140. Removal of Obsolete or Abandoned Signs.

[Ord. No. 1091 §§1 – 3(App. A §430.140), 9-24-2003; Ord. No. 1259 §ii, 5-21-2008]

- A. If a building, structure or premise is vacated for a six (6) month period of time, any off-site or on-site signs in connection therewith shall be deemed obsolete and abandoned. ~~The~~ owner of said property shall be responsible for removing any ~~such off-site or on-site~~ signs with the exception of advertisements dealing with the sale or leasing of the facility. In addition, the owner shall be responsible for restoring the facade of the building, structure or premise.
- B. If the time period set forth in Subsection (A) has elapsed and the sign or signs have not been removed, the ~~Code Enforcement Officer~~ City shall send written notification by certified mail, return receipt requested, to the property owner of record demanding indicating that the sign shall be removed. If the sign has not been removed within thirty (30) days after the receipt of the notice, the City may have the sign removed and the cost assessed to the property owner.
- C.   Where a sign has been removed by the City pursuant to Subsection (B), the City ~~Clerk~~ shall mail a statement of the cost of removal of said sign or signs to the last known address of the owner of record or person in charge of such property. If such costs are not paid within ten (10) days from the mailing of such notice, a record of the costs of abatement shall be certified to the City Clerk who shall cause such costs to be assessed against the particular lot or piece of land on which such sign or signs were so removed. The City Clerk shall certify the assessment to the County Clerk at the time other special assessments are certified for spread on the tax rolls of the County. the Governing Body shall proceed to pass an ordinance levying a special assessment for such cost against the lot or piece of land and the City Clerk shall certify such assessment to the County Clerk for collection and payment the same as other assessments and taxes are collected and paid to the City.

### Section 430.150. Temporary Signs.

[Ord. No. 1259 §12, 5-21-2008]

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A. *Purpose And Findings.* The City of Mission is enacting this Section to establish reasonable regulations for the posting of temporary signs on public and private property. Temporary signs left completely unregulated can become a threat to public safety as a traffic hazard and detriment to property values as an aesthetic nuisance. By implementing these regulations, the City intends to:

1. Balance the rights of individuals to convey their messages through temporary signs and the right of the public to be protected against the unrestricted proliferation of signs;
2. Further the objectives of the City's Comprehensive Plan;
3. Protect the public health, safety and welfare;
4. Reduce traffic and pedestrian hazards;
5. Protect property values by minimizing the possible adverse effects and visual blight caused by signs;
6. Promote economic development; and
7. Ensure the fair and consistent enforcement of the temporary sign regulations specified in the following provisions.

B. *Temporary Signs Permitted In All Zones.*

~~1.~~ Temporary signs may be posted on property in all zones of the City, subject to the following requirements and those applicable provisions stated elsewhere in the City's sign ordinance. ▲

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~~+2.~~ Permits for temporary signs are not required in the R-1, R-2 or R-3 Districts.

~~3.~~ The total square footage for temporary signs in any district per business or commercial establishment, in the aggregate, shall not exceed forty-eight (48) square feet, with no individual sign exceeding sixteen (16) square feet. The total square footage of a sign is measured to include all of the visible display area of only one (1) side of the sign and only the area of one (1) side of a double sign is included in the aggregate calculation. ▲

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~~4.~~ Such signs shall not exceed six (6) feet in height. ▲

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~~2-5.~~ Such signs shall maintain a setback at least three (3) feet from the property line.

~~3-6.~~ No temporary sign shall obstruct or impair access to a public sidewalk, public or private street or driveway, traffic control sign, bus stop, fire hydrant or any other type of street improvements furniture or otherwise create a hazard, including a tripping hazard.



4-7. No temporary sign shall be illuminated or painted with a light-reflecting paint.

5-8. A temporary sign shall only be posted with the consent of the property owner or applicant.

9. Temporary signs may be posted for the following periods of time not to exceed a total of 90 days per calendar year.

a. Up to 3, 7, 15, 30, or 60 consecutive days; or

b. Up to 30 or 60 non-consecutive days. A temporary sign may be posted for a period of up to sixty (60) days, at which time the sign shall be removed for a period of at least sixty (60) days;

6-c. Except for real estate signs advertising property for sale or lease, which may remain until the property is sold or leased.

C. *Authorization Required For Posting Temporary Signs In The Public Rights-Of-Way.*

Temporary signs shall not be posted in the public rights-of-way without obtaining a permit for such posting from the ~~Director of Community Development or the Director of Community Development's designee~~ City and pursuant to the ~~Department of Community Development's~~ City policy regarding the posting of signs in the public rights-of-way. This restriction includes the posting of temporary signs on trees, utility poles and other structures within the rights-of-way.

D. *Removal Or Replacement Of Temporary Signs.*

1. The person who has posted or directed the posting of a temporary sign is responsible for the removal or replacement of that sign in accordance with this Section.
2. If that person does not remove or replace the temporary sign in accordance with this Section, then the property owner or occupant of the building lot where the sign is posted is responsible for the sign's removal or replacement.
3. If the ~~Director Of Community Development or the Director of Community Development's designee~~ City finds that any sign is posted in violation of these regulations on private property, ~~then(s)he shall give~~ written notice ~~shall be given~~ to the person who has posted or directed the posting of the sign. If that person fails to remove or replace the sign so as to comply with the standards herein set forth within seventy-two (72) hours after such notice, ~~an action may be commenced under Section 430.070(E), such sign may be removed by the Director.~~
4. The ~~Director of Community Development or the Director of Community Development's designee~~ City may immediately remove temporary signs posted on public property or rights-of-way in violation of this Section and ~~commence an action under Section 430.070(E), file a civil complaint against the person who posted the sign to recover the cost of removing the sign.~~

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# Chapter 420. Accessory Uses, Prohibited Uses and Non-Conforming Uses

## Article IV. Non-Conforming Situations and Vested Rights

### Section 420.220 Non-Conforming Signs.

[Ord. No. 1142 §14, 12-8-2004; Ord. No. 1259 §1, 5-21-2008]

- A. Subject to the remaining restrictions of this Section and the provisions of Section **420.200**, non-conforming signs that were otherwise lawful on the effective date of this Article may be continued.
- B. No person may engage in any activity that causes an increase in the extent of non-conformity of a non-conforming sign. Without limiting the generality of the foregoing, no non-conforming sign may be enlarged or altered in such a manner as to aggravate the non-conforming condition, nor may illumination be added to any non-conforming sign.
- C. A non-conforming sign may not be moved or replaced and the message may not be changed, except to bring the sign into complete conformity with this Article, except for "sign maintenance" as defined in Section **430.020**. In conducting "sign maintenance" to a non-conforming sign, a new sign permit is required any time a sign is ~~altered~~refaced.
- D. Subject to the other provisions of this Section, non-conforming signs may be maintained and repaired so long as the cost of such work within any twelve (12) month period does not exceed fifty percent (50%) of the value (tax value if listed for tax purposes) of such sign. No such work shall be done without the person proposing to do such work first submitting such information as may be required to satisfy the Public Works Director that the cost of such work would not exceed fifty percent (50%) of the value of the sign.
- E. If a non-conforming sign other than a billboard advertises a business, service, commodity, accommodation, attraction or other enterprise or activity that is no longer operating or being offered or conducted, that sign shall be deemed abandoned and shall be removed (including support structure) within thirty (30) days after such abandonment by the sign owner, owner of the property where the sign is located or other person having control over such sign.
- F. If a non-conforming billboard remains blank for a continuous period of one hundred eighty (180) days, that billboard shall be deemed abandoned and shall, within thirty (30) days after such abandonment, be altered to comply with this Article or be removed by the sign owner, owner of the property where the sign is located or other person having control over such sign. For purposes of this Section, a sign is "blank" if:
  - 1. It advertises a business, service, commodity, accommodation, attraction or other enterprise or activity that is no longer operating or being offered or conducted; or
  - 2. The advertising message it displays becomes illegible in whole or substantial part; or
  - 3. The advertising copy paid for by a person other than the sign owner or promoting an interest other than the rental of the sign has been removed.
- G. Sales or leasing signs existing on the effective date of this Article which do not conform to the requirements of Section **430.080(C)** or not previously in compliance with applicable regulations shall be removed by July 1, 2005.



# Chapter 445. Building Permits, Special Use Permits and Certificate of Occupancy

## Article III. Special Use Permits

### Section 445.180. Designated Use.

[Ord. No. 1007 §16-220.01, 1-24-2001; Ord. No. 1013 §1, 6-27-2001; Ord. No. 1045 §16-220.01, 8-14-2002; Ord. No. 1091 §§1 — 3(App. A §445.180), 9-24-2003; Ord. No. 1124 §26, 8-25-2004]

A. The following uses may be located in any district with the issuance of a Special Use Permit, except in those districts listed after each use, after a public hearing and upon recommendation of the Planning Commission and approval of the City Council and subject to such regulations, restrictions and conditions within the district and any other conditions of zoning or use so stipulated or imposed:

Use	Use to be Excluded from following districts
Automobile parking lot properly landscaped, surfaced, drained and screened	Residential districts
<u>Billboard Signs</u>	<u>Residential districts</u>
Cemeteries	C-2A, MS1, MS2, DND
Dog kennel — veterinary	None
Electric power substations	None
Discount store or superstore	Residential districts
Hospital	Residential districts, C-2A, MS1, MS2
Motels and hotels	Residential districts
Nurseries	Residential districts
Nursing homes	None
Radio, television and microwave towers and antennae, and in cases where such devices cannot conform to height, area or other standards of Chapter 415	None
Clubs, private where alcoholic beverages are consumed on the premises. These include taverns, beer halls, discotheques, dance clubs and the like	Residential districts
Refuse dump or land fill	Residential districts, C-2A, MS1, MS2
Reservoirs — underground	None
Sewage pumping stations	None
Water tanks	None

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B. In addition, temporary use of land or building for a use that is not otherwise permitted for that premises provided such temporary use shall conform to the following conditions:

1. The use is temporary in nature and will not require major change or addition to an existing building or structure.



2. The use will not be offensive or damaging to surrounding properties by way of noise, traffic generation, commotion, storage of materials or equipment, or visual effects which are in contrast to and not in harmony with the surrounding neighborhood.

3. The use can readily be terminated and any equipment, appurtenances and materials removed immediately upon expiration or repeal of the special use permit.

4. No special use permit for a temporary use shall be valid for more than six (6) months but may be renewed by the same process as the original special use permit.

C. The Planning Commission and City Council may designate such other uses as appropriate for a special use permit upon a finding that the use is appropriate in a certain location but is not listed as allowed in any district or is only allowed in a district which contains other uses inappropriate in this subject location.



RESOLUTION NO. \_\_\_\_

A RESOLUTION ESTABLISHING A SIGN PERMIT FEE SCHEDULE

WHEREAS, the City deems it necessary and prudent to require sign permits and other permits related to construction of signs to ensure the compliance with adopted codes and standards and;

WHEREAS, the City deems it necessary and prudent to require fees in conjunction with the filing of applications for permits to further ensure the compliance with adopted codes and standards and;

WHEREAS, the City requires a fee to paid for such permits.

NOW, THEREFORE, BE IT RESOLVED BY THE COUNCIL OF THE CITY OF MISSION, KANSAS, as follows:

SIGN PERMIT FEES

1. Temporary Signs:

Consecutive Days

- 3 consecutive days- \$20
- 7 consecutive days- \$25
- 15 consecutive days- \$40
- 30 consecutive days- \$75
- 60 consecutive days-\$150

Non-Consecutive Days

- 30 non-consecutive days- \$100
- 60 non-consecutive days- \$200

- 2. Charitable institutions and entities are exempt from temporary permit fees.
- 3. Permanent Signs: \$25.00, plus \$1.50 per square foot or fraction thereof of projected sign surface area.
- 4. Sign Refacing: \$25.00, plus \$.50 per square foot or fraction thereof of projected sign surface area.
- 5. On signs with more than one (1) face the fee shall be based upon the sum of the areas of all sign faces.

Sign Professional/Contractor Occupational License Fees  
Sign professionals/contractor occupational license fee: \$50.00



PASSED AND APPROVED BY THE CITY COUNCIL this \_\_\_ day of \_\_\_\_, 2017.

SIGNED AND APPROVED BY THE MAYOR this \_\_\_ day of \_\_\_\_, 2017.

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Steve Schowengerdt, Mayor

ATTEST:

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Martha Sumrall, City Clerk





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To: Mission Planning Commission  
From: Danielle Murray, City Planner  
Date: November 21, 2016  
Re: Case # 16-07 Sign Code Changes

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### **Background**

Over time, staff has received various comments and complaints about signs in the commercial districts of the City. Sometimes these complaints have been about the maintenance of permanent signs or awnings, but more often they have involved the location and appearance of temporary signs and/or window signs. In 2012, in an attempt to encourage voluntary compliance with the existing sign code, staff began providing reminders of sign code requirements with the annual business license renewal process. In early 2015, in response to increased feedback by the community and City Council, staff proposed a process to **conduct an inventory of existing signs, examine the current sign code regulations**, and to **propose possible changes** before beginning an **expanded sign code enforcement** effort.

### **Sign Code Regulation**

Mission regulates signs through Sign Code Ordinances in Chapter 430 of the Land Use Regulations or Zoning Code of the Municipal Code. The Sign Code defines the various types of signs allowed and explains the physical conditions for their installation including:

- 1) Where they may be installed
- 2) Limitations on size and number
- 3) How long they may be in place
- 4) If a sign permit is required prior to installation

Signs are a form of speech and expression and, as such, are protected by the First Amendment to the Constitution. Various court cases across the country have affirmed a City's power to regulate signs only in a content-neutral fashion. Generally, that means cities may make rules about the **"time, place and manner"** of signs as long as those rules don't make any distinction based on what the sign says. The First Amendment does not protect false claims or otherwise unlawful activity.

As a component of the zoning code, state statute requires that changes to the sign code be reviewed by the Planning Commission and receive a public hearing before being referred on to the City Council for final adoption. The City's sign code is developed in consultation with legal counsel and has been reviewed by the City's land use attorney.

### **Sign Code Enforcement**

In Mission, sign permits are required for most but not every sign type. When they are required, permit applications are reviewed and issued by the City Planner. Historically, sign



code enforcement has also been conducted by the planner, primarily on a complaint driven basis. Occasional special sweeps to address seasonal sign issues (elections, tax preparation, post storm event repairs, etc) have also been conducted. Recently, the role of Neighborhood Services staff has been expanded to include a more direct role in building and zoning issues. This realignment will allow for an increase in the capacity to provide regular, proactive sign code enforcement.

As with any enforcement activity, staff attempts to gain voluntary compliance first through informal communication and education. If voluntary compliance is not possible, the more formal process of issuing citations to appear in court can be implemented under the current code. In some specific circumstances signs can be removed by staff or abated by a professional contractor. Penalties such as fines can only be assigned by the municipal judge, and multiple court hearings are often required. Involuntary enforcement generally takes more staff resources and may not be as effective in resolving a problem in the long term.

### **Sign Code Change Process**

In consultation with the City Council staff developed the following steps to assess and document the current situation, educate local business and property owners, and evaluate the strengths and weaknesses of our existing sign code. The goal is to create a sign code enforcement program that is fair and predictable for Mission residents and merchants and that can be sustained with available staff resources.

- Conduct a sign audit to document what signs are already “on the ground” and in permit files. Visit every commercial property in the city, including apartment complexes and churches, to inventory the existing signs. To do this the City acquired a temporary software subscription that could be accessed from mobile devices by the Neighborhood Services Officers.
- Create a comprehensive inventory database of the audit information.
- Analyze audit data for patterns and trends. Determine how many possible sign code violations exist and what general categories they fall into. (i.e. signs without permits, signs in wrong locations, sign duration violations, sign numbers, sign size)
- Solicit input regarding the best ways to address issues identified. Are sign code changes needed to achieve the preferred outcomes? Staff has communicated with the City Council as audits, analysis, and proposed code changes were developed. A letter summarizing the proposed changes and inviting comment on them was sent to every business currently holding an occupation license. The entire text of the proposed changes and a brief memo describing them was also posted to the City’s website. Staff held two meetings in November with business and property owners to explain the proposed changes and solicit feedback. Comments have been summarized and are included in the packet for Planning Commission and City Council review.
- Communicate intentions to the community. Continue education efforts for businesses and local sign companies through various media and personalized correspondence.



- Design a system of continuous sign code enforcement that can be consistently implemented across the entire city.

**Sign Inventory and Code Analysis**

The sign inventory documented **817 signs** throughout Mission. The four most common sign types were: wall signs (315), window signs (199), temporary signs (91), and monument signs (87). Full descriptions of each sign type are presented in the attached handout and specifically discussed in the following pages. In commercially zoned areas, the typical business has only **two or three signs**.

Number of Signs by Zoning District	Signs	
	#	%
Residential Districts ( <i>R-x, RP-x, DND</i> )	52	6%
Light Commercial Districts ( <i>C-1, CP-1</i> )	32	4%
Downtown Districts ( <i>MS1, MS2</i> )	444	54%
Office Districts ( <i>C-O, CP-O</i> )	55	7%
Heavy Commercial / Industrial Districts ( <i>All other districts</i> )	234	29%
<b>TOTAL</b>	<b>817 signs</b>	



## **Pole Signs**

There are currently **23** pole signs within the City. The installation and replacement of pole signs has been prohibited since September 2003. Existing pole signs are regulated as non-conforming uses and investment is limited to maintenance only, with the intent being the eventual elimination of all pole signs.

### ***POLE SIGN***

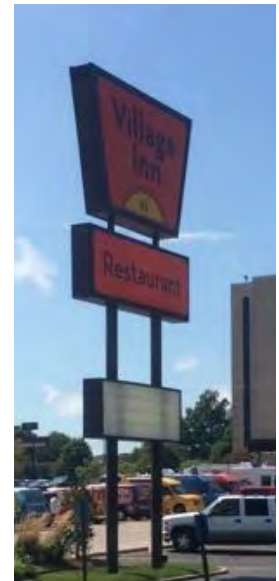
*A sign which is supported by one (1) or more poles, uprights or braces in the ground so that the bottom edge of the sign face is more than ten (10) feet above the ground.*

Since 2009, nine (9) existing pole signs have been removed throughout the City. This was accomplished through a combination of voluntary compliance with zoning code enforcement notices, City abatement, or normal business practices. Pole sign regulations were last amended in 2009 to allow existing pole signs in the vicinity of Johnson Drive and Metcalf Avenue under certain conditions. There are three signs that continue to meet these conditions (Village Inn, Johnny's BBQ, and Popeye's).

At the Community Development Committee's December 2015 meeting, the Committee communicated a preference to continue the prohibition on pole signs and to explore implementation of ordinance changes that would establish a deadline for property owners to actively remove all pole signs within the City. Additional discussion surrounding pole sign code revisions asked the following questions.

### ***Decision Points (Time, Place, Manner)***

- Does the City Council still desire to implement these changes in the near future, or include them in a larger discussion of sign code revisions?
- Should the City require by ordinance that pole signs be removed by a certain date (an amortization policy)? If so, how long should businesses have to remove these signs?





**Temporary Signs**

The results of the sign inventory show **91** temporary commercial signs were in place throughout Mission during the sign inventory, with about one-quarter (23) of those being used for purposes related to real estate and project development (purposes treated differently under our sign code).



Number of Temporary Signs by Zoning District	Signs	
	#	%
<b>Residential Districts</b> <i>(R-x, RP-x, DND)</i>	10	11%
<b>Light Commercial Districts</b> <i>(C-1, CP-1)</i>	40	44%
<b>Downtown Districts</b> <i>(MS1, MS2)</i>	7	8%
<b>Office Districts</b> <i>(C-O, CP-O)</i>	4	4%
<b>Heavy Commercial / Industrial Districts</b> <i>(All other districts)</i>	30	33%
<b>TOTAL</b>	<b>91 signs</b>	

Temporary signs can be used for any purpose under our code, but often are used to fulfill one of the following needs:

- Political signs
- Contractor projects
- Special events
- Garage sales
- Sales and promotions



The current sign code does not require a sign permit for temporary signs, allows an unlimited number of signs but restricts the overall square footages, and limits any one sign's duration to 60 days before the sign must be removed for at least 60 days. Signs which advertise property for sale are limited in duration to the period of time for which the property is on the market for sale.



While these are reasonable restrictions, they are very difficult to enforce. Staff would have to proactively monitor properties for the appearance of temporary signs, document them when seen, and spend time comparing current signs to historical signs or documenting that a sign has been in place for longer than 60 days. Proving that someone is using more than the gross allowable square footage would require staff to locate and measure each individual sign. The typical policy solution to these type of concerns is to require permits - but that may be seen as too burdensome for these signs.



Beyond issues with these restrictions, temporary signs are being used throughout the City in ways that were not intended when the code was authored. It would be difficult for enforcement alone to resolve concerns about temporary signage within the current code.

For example, in the City's business districts, many of the signs inventoried were used purely to identify or draw attention to a business - rather than to recognize a sale or some other special or temporary event. While our code should not make any distinctions between these uses, these needs would be more appropriately satisfied by permanent signage. However, temporary signs often come at a lower cost to produce, provide greater flexibility to the business, and allow for more sign space than would otherwise be allowed to an individual business.



Similarly, the sign code allows signs advertising a property for sale or lease to remain until that property is leased or sold. However, in the case of larger multi-family developments, there is virtually always *some* level of vacancy that triggers this exception in our code. Additional discussion surrounding temporary signs asked the following questions.

**Decision Points (Time, Place, Manner)**

- Should the City require a permit for temporary signs?
- How long should signs be allowed to stay out? Should the City impose any other form of time restriction, i.e. limiting temporary signs to certain events each year?
- Is the quality of these signs an issue that the code should address?
- Should we change how we count or limit signs? Should we focus on the number of signs per tenant or per parcel, rather than the aggregate size?





## Window Signs

The current sign code only allows window signs in the Downtown District - generally, the Johnson Drive corridor from Lamar Avenue to Nall Avenue. Such signs are intended to communicate with pedestrians walking through the district, at a slower speed than vehicle traffic. Accordingly, they are limited in size to 10% of the window glass of the facade. The current code also specifies that they should be affixed to the glass in a permanent manner rather than as copy that is changed frequently. A sign permit is required for a window sign. Window displays and lettering that identifies a business, address, or hours of operation are exempt from the sign code.

However, a large number of properties have window signs without permits and or are located outside of the downtown and are technically out of conformance with the current code. For many businesses, permanent or rotating window signage is a common business practice. Many surrounding/peer communities either limit window signs to a certain area percentage (often 50%) or exclude them from sign regulation entirely. Additional discussion surrounding window signs asked the following questions.

### **Decision Points (Time, Place, Manner)**

- Should we allow and provide specific criteria for these signs?
- Where should these signs be allowed? Should the Downtown District be treated differently than other areas of the City?
- Should the code prescribe any “quality” requirements for these signs, such as that they must be printed on durable materials?







### **New / Emerging Sign Practices to Regulate**

Mission currently does not have a significant number of vehicular signs or people hired to act as sign holders in the public right of way. In the past, mobile, dedicated sign trucks have circulated town on public streets and parked in underused or vacant parking lots visible to major thoroughfares. While regulating these types of advertising are difficult, many communities and people view them as a nuisance.

### **Decision Points (Time, Place, Manner)**

- Should the city be proactive in enacting regulations to prohibit these signs?

### **Other Legal Concerns**

In June of 2015, the U.S. Supreme Court issued a decision in *Reed v. Town of Gilbert* that rules unconstitutional many of the categories used by cities to regulate signs. These concerns are not unique to the City of Mission; indeed, they affect most cities in the United States.

In addition, the Kansas Legislature recently prevented cities from regulating the placement or number of *political* signs posted for the period of time surrounding an election. The position of the League of Kansas Municipalities is that this law is unconstitutional under *Reed* as a content-based discrimination in speech; however, this has not yet been tested before a court.

The City considered the proposed sign code revisions in consultation with our city attorney to address these conflicts with case law and state law. Significant interpretation or testing of the impacts of the *Reed* decision is not yet available, however our land use attorney is comfortable with the proposed changes.

### **Proposed Sign Code Changes**

In February 2016 Staff presented final analysis of the inventory and asked for direction to prepare code changes to respond to identified problems. Council's direction to staff at the worksession and subsequent committee meeting was to:

- Continue to prohibit pole signs throughout the City
- Require pole signs to be removed within a certain number of years
- Establish stricter regulations for temporary signage



- Expand the allowance of window signs by right, subject to reasonable restrictions

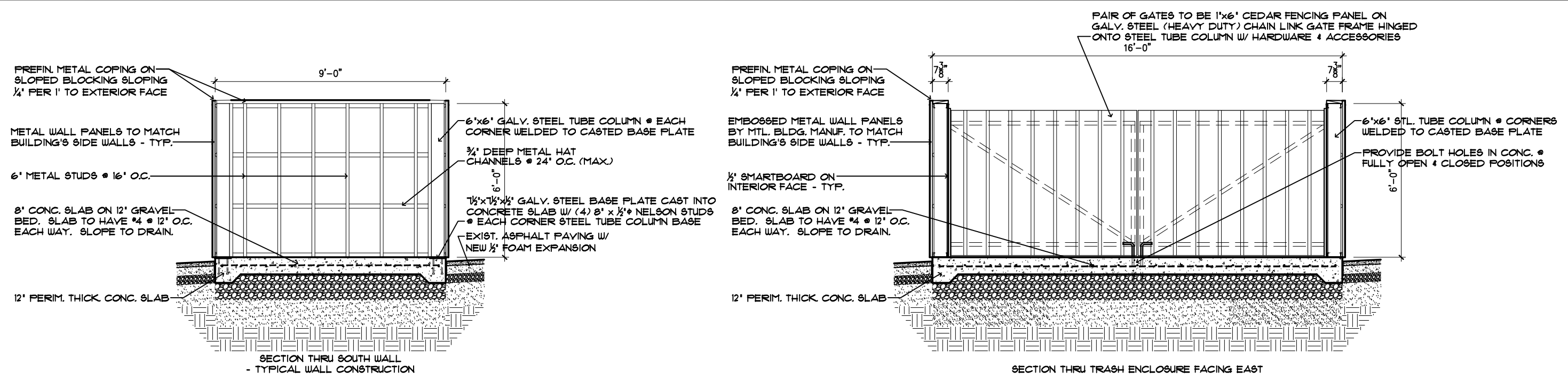
In addition, staff also considered any other necessary changes to the code to improve enforcement, to address other issues identified through the inventory, or to comply with recent case law. Attached is a redline copy of the proposed code changes as well as a table showing the impact by zoning district and sign type. If adopted as proposed, very little of the existing sign code standards will change. Most businesses will continue to be allowed the same number, size and type of signs as they are now. The changes in the proposed code that are most likely to impact business are as follows:

- Allow marquee signs by right in three additional commercial zoning districts
- Allow monument signs by right in the Main Street District 1 and Pedestrian Oriented Business District zoning districts under certain conditions
- Allow window signs anywhere in Mission with a limit on coverage of windows in the downtown district of 50% of the window glazing
- Allow electronic signs for the display of prices for fuel
- Require sign permits for temporary signs
- Limit the posting of temporary signs to no more than three 60-day periods per year per business
- Prohibit costumed characters as attention attracting devices
- Eliminate all pole signs by December 31, 2023 through an amortization process.
- Streamline the approval process for electronic time/temperature instruments
- Standardize the processes of placing liens against properties that have been abated by the City

### **Staff Recommendation**

The Planning Commission should consider the proposed final draft of code changes and open the required public hearing for any comment. At the conclusion of the meeting, the Commission should continue the public hearing to their January 23, 2017 meeting to accommodate any changes to the proposed code the Commission may recommend and to allow for additional public comment after the holidays. Staff recommends adoption of the code changes as currently proposed.





**3 TRASH ENCLOSURE ELEVATIONS**  
NO SCALE

**SITE DATA:**  
ADDRESS: 6101 JOHNSON DRIVE, MISSION, KS 66202  
ZONING: P18  
LOT SIZE: 25,349 SF. (0.58 ACRES)  
EXISTING PERVIOUS SITE AREA: 6,704 SF. (26.4%)  
EXISTING IMPERVIOUS SITE AREA: 18,645 SF. (73.6%)  
PROPOSED PERVIOUS SITE AREA: 3,220 SF. (12.7%)  
PROPOSED IMPERVIOUS SITE AREA: 22,119 SF. (87.3%)

**LEGAL DESCRIPTION:**  
S-12-25 N 210' W 140' E S25' NW/4 SE1/4 EX 20' AC IN S7 4 SUBJ TO N 60' IN HWY 679 AC5 M/L MIC 83

**BUILDING DATA:**  
BUILDING AREA: 3,343 SF  
BUILDING HEIGHT: 16'-8" - 1 STORY  
BUILDING USE: EXISTING RESTAURANT & BAR  
CONSTRUCTION TYPE: VB (SPRINKLERED)  
OCCUPANCY TYPE: A-2 ASSEMBLY  
OCCUPANT LOAD: 192 OCCUPANTS  
COMPLIES W/ ADA GUIDELINES  
EXITS: 3

**PARKING DATA:**  
EXISTING: 25 PARKING SPACES  
(INCLUDING 2 VAN-ACCESSIBLE H.C. SPACES)  
NEW: 19 PARKING SPACES  
TOTAL: 44 PARKING SPACES

**PROPERTY DEVELOPER INFO:**  
RH. JOHNSON & CO.  
4520 MADISON AVENUE  
KANSAS CITY, MO 64111

**ARCHITECT INFO:**  
SULLIVAN PALMER ARCHITECTS  
8621 JOHNSON DRIVE  
MERRIAM, KS 66202

- GENERAL CIVIL NOTES:**
- The contractor shall be responsible for obtaining all permits, bonds and insurance, as required by the governing agencies including local, state and federal authorities having jurisdiction over the work proposed by these construction drawings. All costs associated with these restrictions shall be the responsibility of the contractor, and shall be included in the bid.
  - All construction within the public right-of-way or existing easements shall conform to the requirements of the standard specification and design criteria, as adopted by the City of Mission, KS.
  - The contractor shall be responsible for damage to existing structures, utilities, fences and/or paving not designated for removal and shall repair the same at his own expense.
  - The demolition of existing pavement, curb, storm drainage structures, underground electrical power lines, and other related utilities as designated on the drawings, or as deemed necessary to construct the proposed improvements, will be the contractor's responsibility. All debris and waste materials removed during construction shall be disposed of by the contractor off the project site, in accordance with local laws and ordinances. All costs associated with the disposal of waste materials shall be at the contractor's expense.
  - The contractor shall be responsible for the materials, tools, equipment, and labor as necessary for construction of temporary erosion control during construction operations on this project.
  - The site contractor is responsible for coordination of all work required for the installation of the proposed underground utilities which are to be placed under the pavement or other hard surfacing.
  - The contractor shall be responsible for all signage, barricades, lighting, etc. as required for temporary traffic control. A traffic control plan shall be submitted to the engineer & KDOT. The traffic control plan shall be submitted a minimum of 2 weeks prior to the start of work.
  - Testing and inspection services provided during construction operations will be provided by an independent testing laboratory selected by the owner. All costs associated with construction testing will be paid for by the owner, except where construction tests fail to meet the required specifications. The cost of all retesting, if required, will be the contractor's responsibility.
  - The contractor agrees that by the use of these drawings, he shall be solely responsible for the safety of the construction workers and the general public.

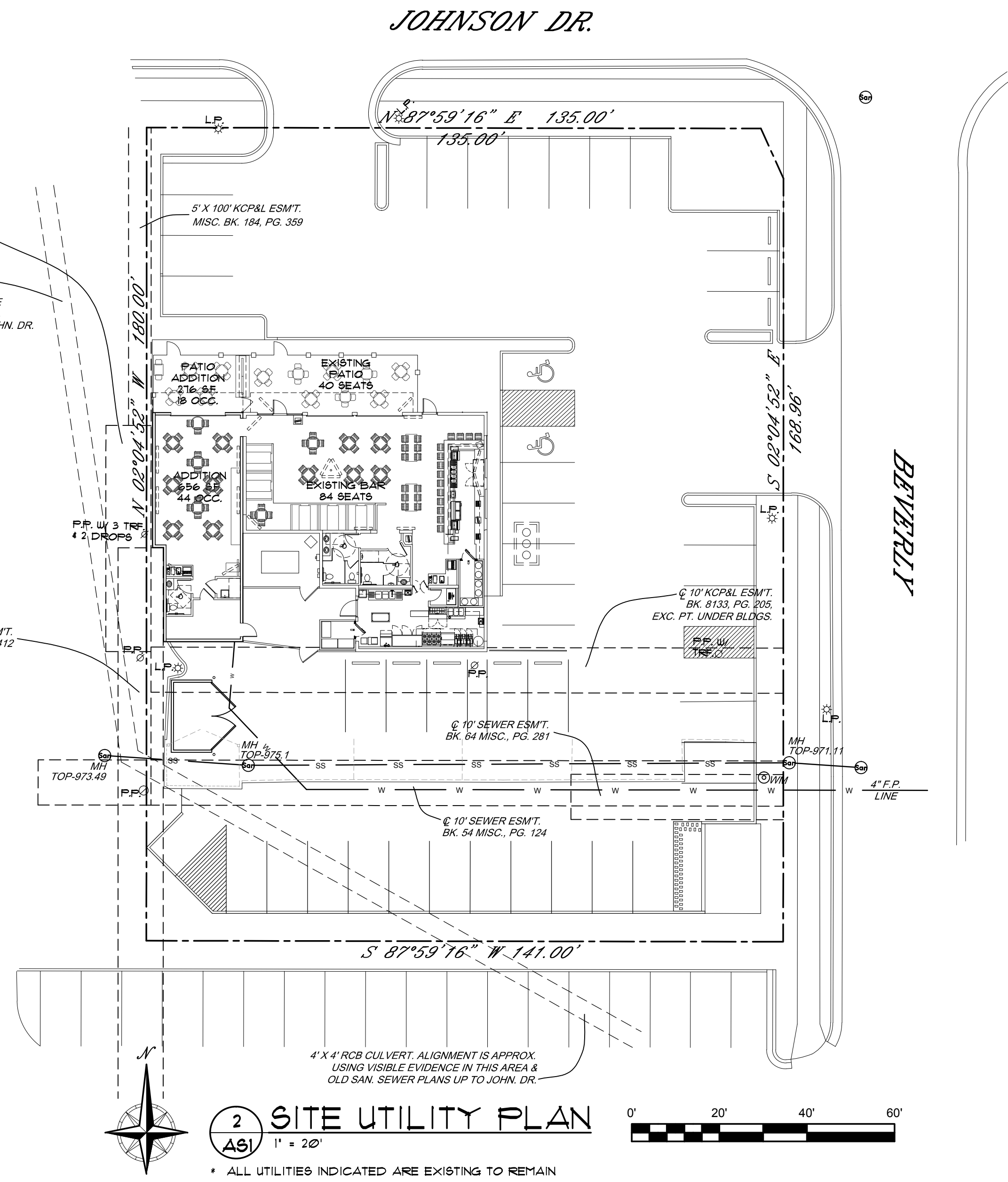
**LEGEND**

AC	= AIR CONDITIONER
ASPH	= ASPHALT
BF	= BOARD FENCE
BLDG	= BUILDING
BF	= BOARD FENCE
CL	= CURB/INLET
CLF	= CHAIN LINK FENCE
CMWH	= CORRUGATED METAL WINDOW WELL
CONC	= CONCRETE
CS	= CONCRETE CURB STOP
DS	= DOWNSPOUT
EM	= ELECTRIC METER
ESMT	= EASEMENT
FH	= FIRE HYDRANT
FNDR	= FOUNDATION
GI	= GRATE/INLET
GL	= GLOBE LIGHT
GM	= GAS METER
HCPV	= HANDICAPPED PARKING SIGN
HCVPS	= HANDICAPPED VAN PARKING SIGN
MH	= MANHOLE
OH	= OVERHANG
P.O.B.	= POINT OF BEGINNING
PE	= PEDESTRIAN (DOOR) OR PEDESTAL
PP	= POWER POLE
PVC	= POLYVINYL CHLORIDE
RCB	= REINFORCED CONCRETE BOX
R/W	= RIGHT-OF-WAY
SAN	= SANITARY
SECT	= SECTIONALIZER
SVC	= SERVICE
STLT	= STREETLIGHT
STMH	= STORM MANHOLE
SWB	= SOUTHWESTERN BELL
T.C.E.	= TEMPORARY CONSTRUCTION EASEMENT
TRF	= TRANSFORMER
TIP	= TELEVISION PEDESTAL
TY	= TYPICAL DETAIL
UE	= UTILITY EASEMENT
UGT	= UNDERGROUND TELEPHONE
WM	= WATER METER
WV	= WATER VALVE
○	= FOUND 1/2" BAR WITH CAP #886, UNLESS NOTED
●	= SET 1/2" BAR WITH CAP #886
□	= FOUND JO. COUNTY MONUMENT IN BOX

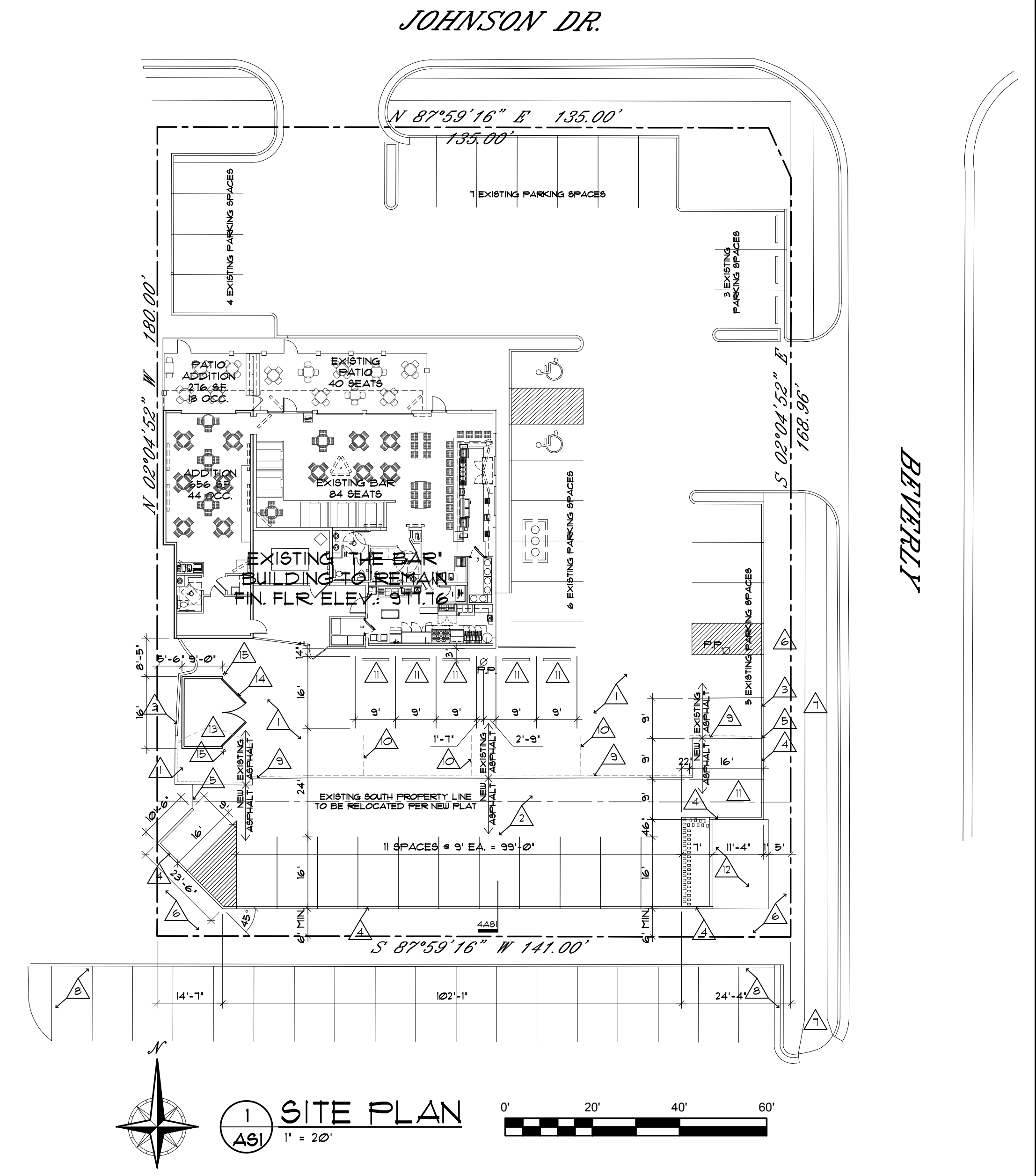
  

—E—	= OVERHEAD ELECTRIC
—G—	= GAS
—SLS—	= UNDERGROUND STREETLIGHT SERVICE
—SS—	= SANITARY SEWER
—ST—	= STORM SEWER
—T—	= OVERHEAD TELEPHONE
—UE—	= UNDERGROUND ELECTRIC
—UGT—	= UNDERGROUND TELEPHONE
—UGTV—	= UNDERGROUND TELEVISION
—W—	= WATER
—X—	= FENCE

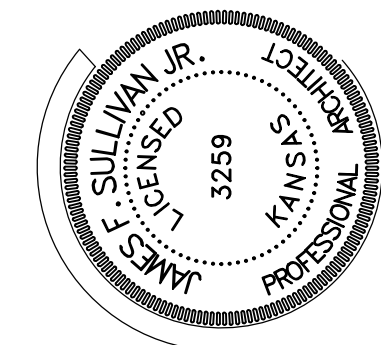
- SCOPE OF WORK AND SITE PLAN NOTES:**
- EXISTING ASPHALT PAVEMENT TO REMAIN.
  - NEW PAVEMENT TO BE 2" ASPHALTIC CONCRETE SURFACE COURSE ON 6" ASPHALTIC CONC. BASE COURSE (4" ASPHALTIC CONC. BASE COURSE AT PARKING SPACES) ON 4" CLEAN CRUSHED STONE LEVELING COURSE. RE: GRADING PLAN IAS2.
  - EXISTING CONCRETE CURB TO REMAIN.
  - NEW CONCRETE CURB TO BE 6" HIGH ABOVE NEW PAVEMENT. RE: GRADING PLAN IAS2.
  - PROVIDE 1/2" FOAM EXPANSION JOINT BETWEEN NEW LANDSCAPE WALL AND EXISTING CURB/NEW PAVEMENT.
  - EXISTING GRASS AREA TO REMAIN. REFER TO LANDSCAPE PLAN 2AS2.
  - EXISTING SIDEWALK TO REMAIN.
  - EXISTING ADJACENT PARKING LOT PAVEMENT W/ EXISTING CURBS TO REMAIN.
  - DEMO EXISTING CURB FOR NEW PARKING LOT EXPANSION.
  - REMOVE EXISTING PARALLEL PARKING LOT STRIPING.
  - PROVIDE CONCRETE WHEEL STOPS AS INDICATED.
  - PROVIDE ST. JOHN'S ENERGY DISSIPATOR & INFILTRATION TRENCH. REFER TO SHEET AS3 STORMWATER DESIGN.
  - DEMO EXISTING TRASH ENCLOSURE.
  - NEW TRASH ENCLOSURE, EXTERIOR TO MATCH EXISTING. REFER TO ELEVATION 3AS1.
  - INSTALL CONCRETE STEEL BOLLARD AS INDICATED.



**2 SITE UTILITY PLAN**  
1" = 20'



**1 SITE PLAN**  
1" = 20'



**THE BAR**  
**PARKING LOT EXPANSION**  
6101 JOHNSON DR., MISSION, KS 66202

**SULLIVAN PALMER ARCHITECTS**  
1401 W. 10th St., Mission, Kansas, 66202  
Phone: (913) 886-0122  
Fax: (913) 886-0122

Date: 12-8-16

Revision:

1-13-17
1-31-17

Drawn by: NCE  
Sheet: **AS1**

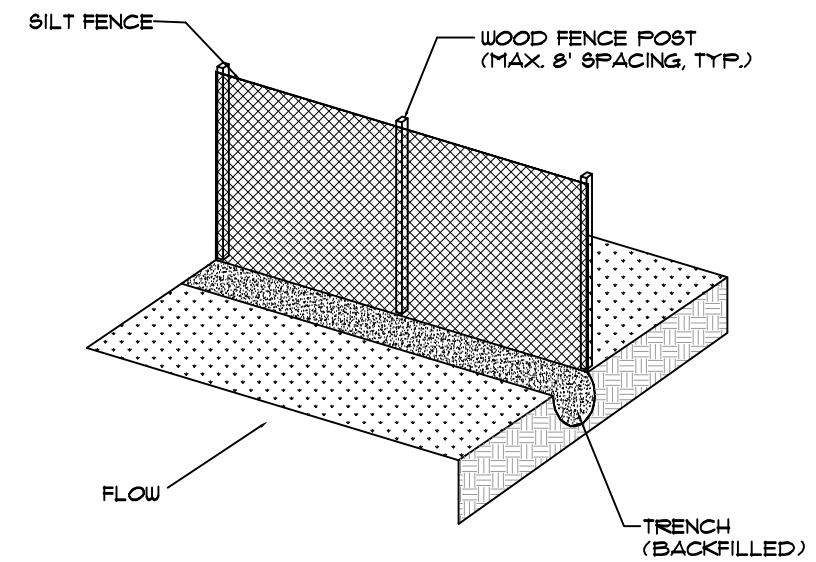


GENERAL LANDSCAPE NOTES:

1. PROVIDE NURSERY'S STANDARD ONE YEAR WARRANTY ON ALL MATERIALS.
2. ALL TREE WELLS & SHRUB BEDS TO RECEIVE AN INITIAL PRE-EMERGENT, AND SHALL RECEIVE 3" OF SHREDDED HARDWOOD (DARK BROWN) MULCH.
3. (DARK BROWN) MULCH TO BE 3" THICKNESS. ELEVATION OF (DARK BROWN) MULCH SHALL BE 1/2" BELOW CONCRETE CURBS.
4. ANY TREES INSTALLED WITHIN 6' OF A SIDEWALK OR CURB SHALL INCLUDE ROOT BARRIER MATERIALS INSTALLED AT THE TIME OF TREE PLANTING.
5. SEPERATE TURF AND MULCH AREAS WITH STANDARD COMMERCIAL STEEL EDGING AS APPROVED BY ARCHITECT.
6. CONTRACTOR SHALL IMMEDIATELY WATER ALL PLANT MATERIALS FOLLOWING PLANTING, AND CONTINUE WATERING & MAINTENANCE UNTIL OWNER ACCEPTANCE.
7. ALL PLANTING MATERIALS SHALL BE OF EXCELLENT QUALITY, FREE OF DISEASE & INFESTATION, AND TRUE TO THE TYPE, CULTIVAR, SIZE, COLOR AS SPECIFIED. SUBSTITUTIONS SHALL BE APPROVED BY ARCHITECT. INSTALLATION SHALL FOLLOW SOUND PROFESSIONAL PLANTING PROCEDURES, LAID OUT IN A UNIFORM AND CONSISTENT PATTERN, AND TRUE TO THE LANDSCAPE PLAN.

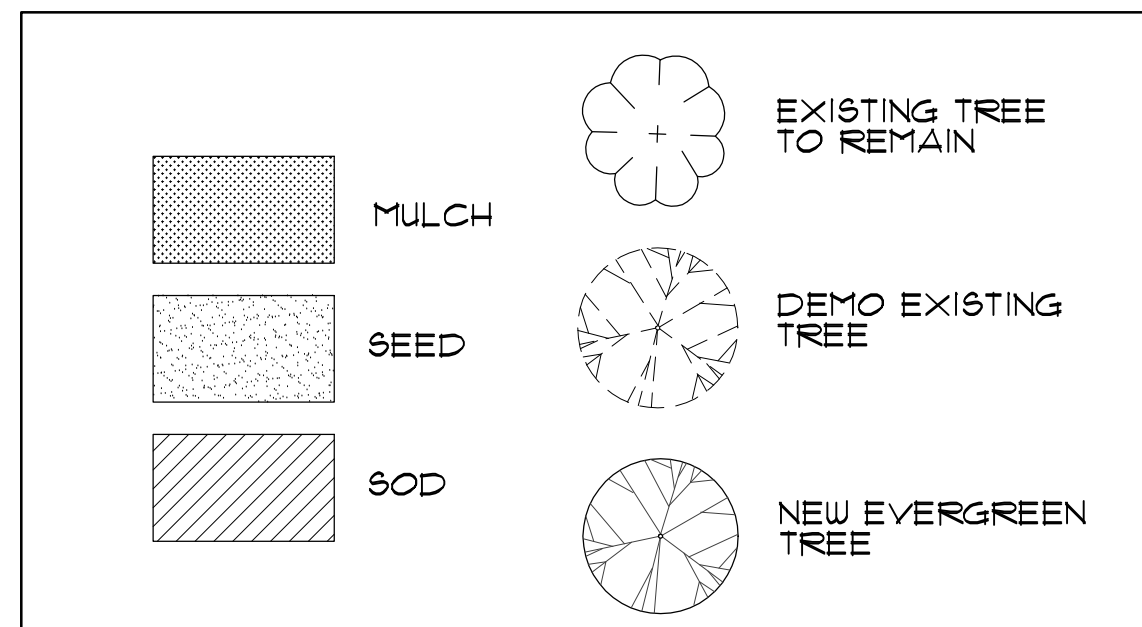
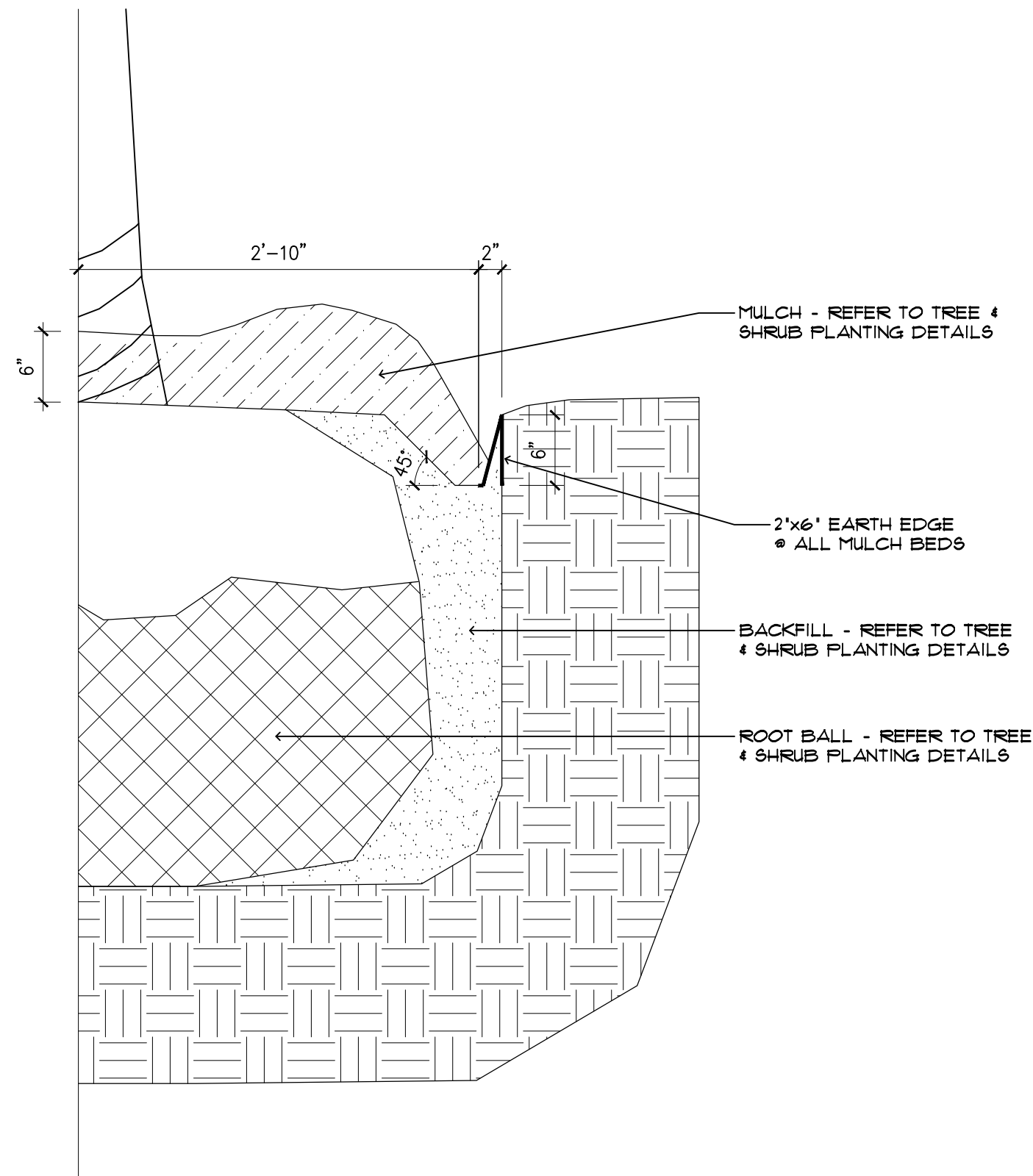
GENERAL GRADING NOTES:

1. All contours and elevations shown on drawings are referenced to City of Mission, KS datum.
2. Existing and proposed contours are shown at one foot intervals. Unless otherwise noted, the proposed contours and elevations as shown represent the approximate finish grade of the asphalt, top of curb and finish grade of unpaved areas. The contractor shall adjust site grading as necessary to establish sub grade elevations. Final grade and sub grade elevations shall be established from the elevations shown on grading plans.
3. Existing utilities depicted on the plans are shown in approximate locations. It will be the responsibility of the contractor to verify in the field the exact location of these or any other utilities in or adjacent to the project. Prior to the start of site grading operations, the contractor shall be responsible for contacting all applicable utility companies to obtain assistance in determining the exact location of underground utility lines and determining required methods of protection and acceptable methods of construction near these utilities.
4. All areas where construction is proposed shall be stripped of all vegetation, organic matter and topsoil to a minimum depth of 12". The materials removed during clearing and grubbing operations shall not be incorporated into controlled fills.
5. Prior to the placement of new fill materials, the existing sub grade shall be proof rolled and inspected under the direction of the geotechnical engineer. Under no condition shall the fill material be placed until the sub grade is approved.
6. Where fill material is to be placed on existing slopes greater than 5 horizontal to 1 vertical, the existing slope shall be benched with a minimum vertical face of 3". The fill material shall then be placed in horizontal lifts and compacted in strict accordance with the recommendations of the geotechnical engineer.
7. All fill materials placed beneath pavement, floor slabs & foundations, and pavement areas shall be considered controlled fills and shall be free of organic matter and debris and placed in loose lifts, not to exceed 9". Earth materials placed beneath pavement, floor slabs, foundations shall be compacted to a minimum dry density of 95% of the materials' maximum dry density as determined by ASTM D-698.
8. The sub grade in cut sections shall meet the same compaction requirements as set forth for fill material. If unsuitable sub grade conditions are encountered, the unsuitable material shall be removed and replaced with suitable fill material.
9. It will be the contractor's responsibility to make all adjustments to existing or proposed manholes, storm drainage structures, and related structures, as required to match the finish grade of the proposed site improvements.



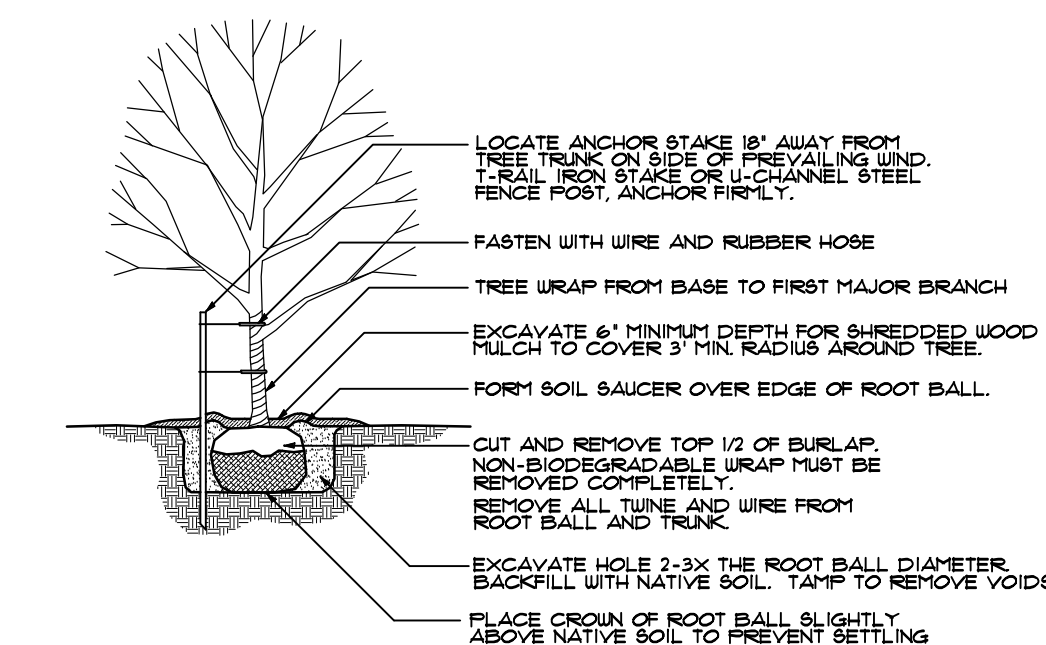
- CONSTRUCTION SPECIFICATIONS**
1. Wood posts which support the silt fence shall be installed on a slight angle toward the anticipated runoff source.
  2. Silt Fence shall be trenched in with a spade or mechanical trencher so that the downlope face of the trench is flat and perpendicular to the line of flow.
  3. The trench should be a minimum of 6" deep and 3-4" wide to allow for the silt fence to be laid in the ground and backfilled.
  4. Silt Fence should be securely fastened to each steel support post or to woven wire which is in turn attached to the silt fence posts.
  5. Inspection shall be frequent and repair or replacement shall be made promptly as needed.
  6. Silt Fence shall be removed when it has served its usefulness so as not to block or impede storm flow or drainage.
  7. Sediment trapped by the practice shall be uniformly distributed on the source area prior to topsoiling.
  8. The Erosion Control shall be Silt Fence. Additional Erosion Control provided by contractor may be Straw Bale Dike.

SILT FENCE (NOT TO SCALE)



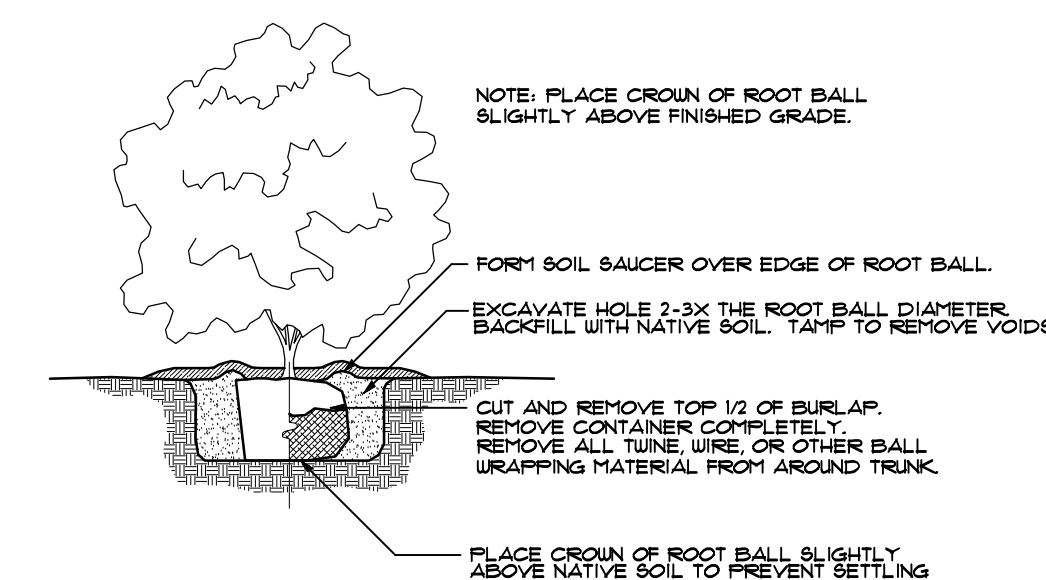
LANDSCAPE SCHEDULE-TREES/SHRUBS

KEY COMMON NAME	BOTANICAL NAME	SIZE	NO.	REMARKS
IC JAPANESE HOLLY 'STEEDS'	ILEX CRENATA	5' HT.	3	
JV EASTERN REDCEDAR	JUNIPERUS VIRGINIANA	2'	10	6" HIGH AT TIME OF PLANTING
TC JAPANESE YEW	TAXUS CUSPIDATA	5 GAL.	4	3' HIGH AT TIME OF PLANTING
ES JAPANESE YEW	TAXUS CUSPIDATA			EXISTING SHRUB
ET 'GREENSPIRE' LITTLELEAF LINDEN	TILIA CORDATA			EXISTING TREE



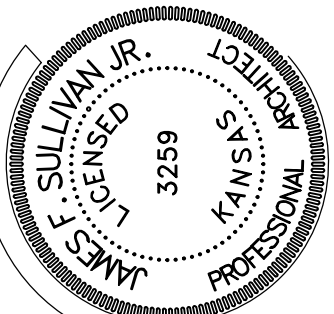
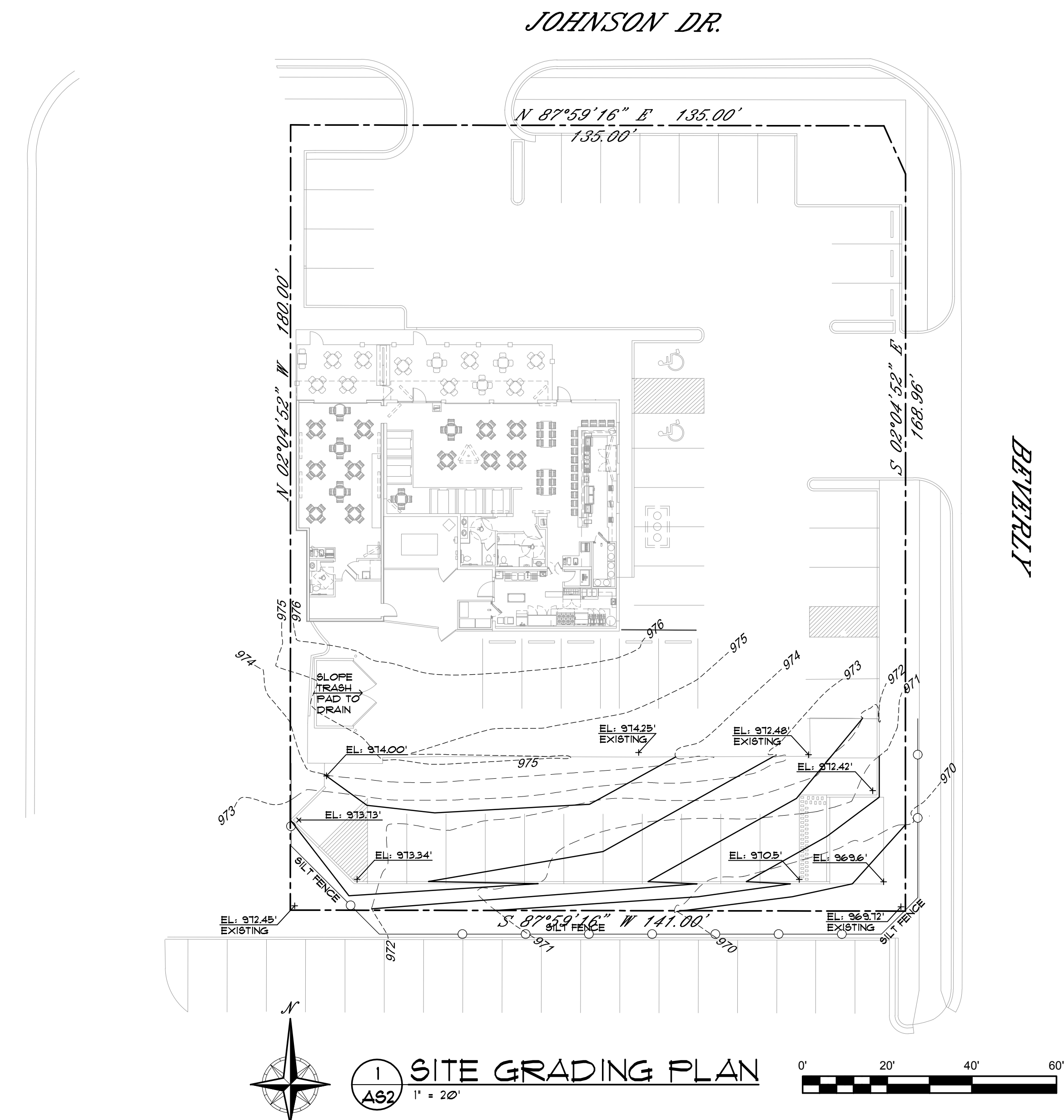
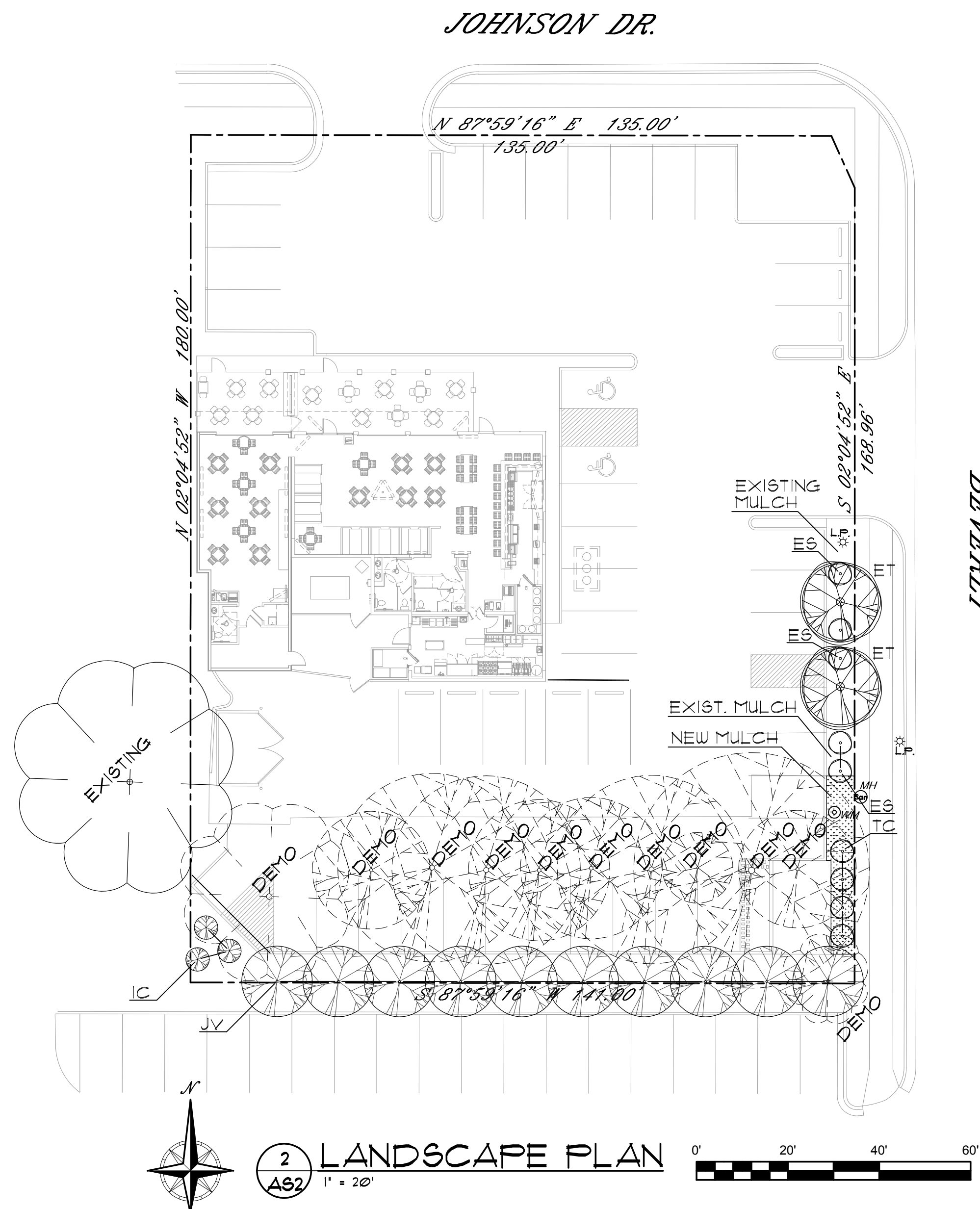
TREE PLANTING DETAIL - TYP.

NO SCALE

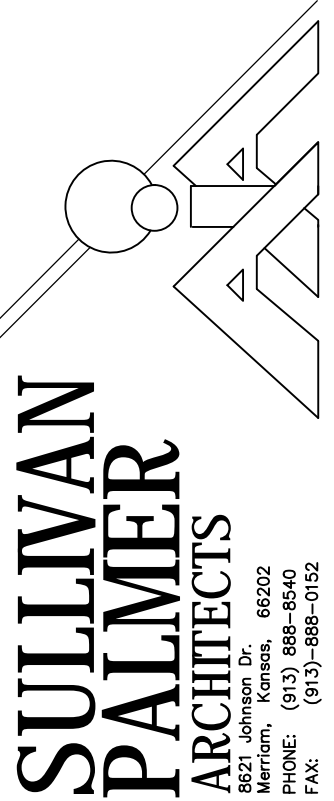


SHRUB PLANTING DETAIL - TYP.

NO SCALE



**THE BAR**  
**PARKING LOT EXPANSION**  
 6101 JOHNSON DR., MISSION, KS 66202



**SULLIVAN PALMER ARCHITECTS**  
 1001 S. 10th St., Mission, Kansas, 66202  
 PHONE: (913) 886-0122  
 FAX: (913) 886-0122

Date:  
12-8-16

Revision:  
1-13-17  
1-31-17

Drawn by:  
NCE

Sheet:

**AS2**





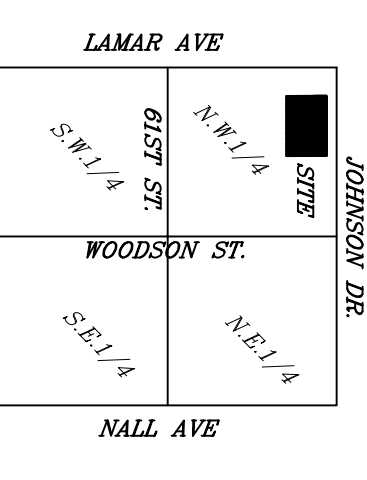
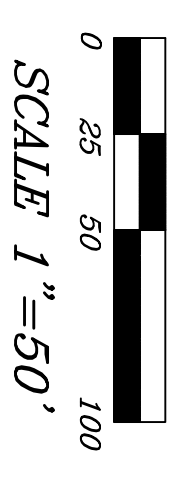
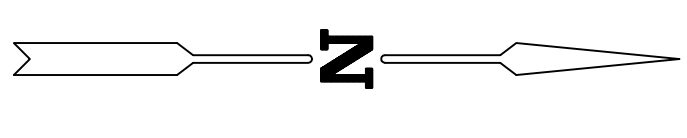






**PRELIMINARY PLAT**  
**DOWNTOWN MISSION**  
**IN THE N.W. 1/4 OF THE S.E. 1/4**  
**OF SEC. 8-12-25**  
**CITY OF MISSION, JOHNSON COUNTY, KANSAS**

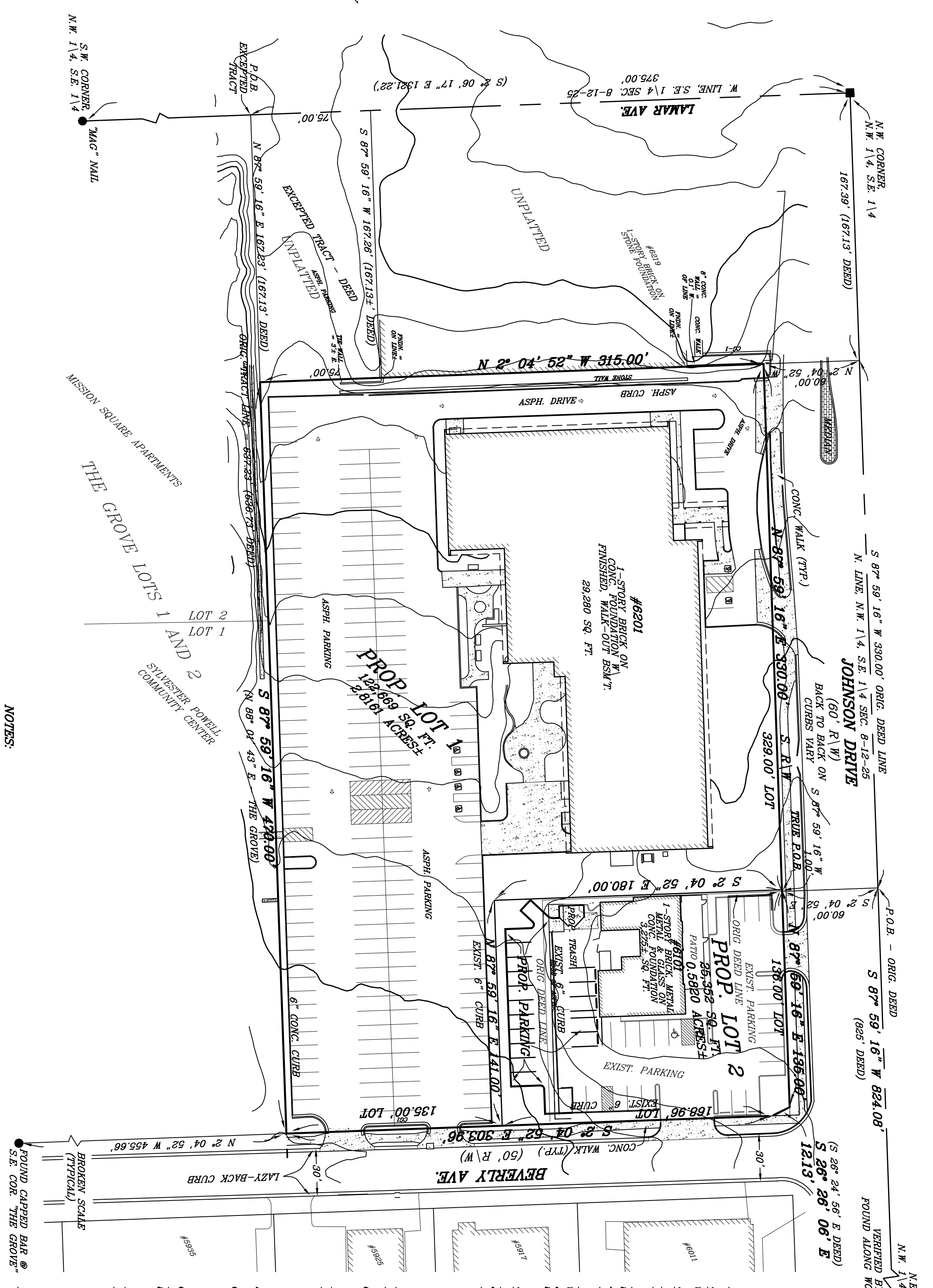
Developed by:  
 6201 JOHNSON, INC. and  
 JOHNSON 6101, LLC  
 4020 MADISON AVE., SUITE 300  
 MANASSAS CITY, VA 20108  
 810-588-2404  
 Date Prepared:  
 October 21, 2016



**VICINITY MAP**  
 S.E. 1/4 SEC. 8-12-25  
 (NO SCALE)

BEARINGS ARE BASED ON THE N. LINE OF THE S.E. 1/4 HAVING A BEARING OF N 89° 01' 49" E PER GPS SURVEY ON THE S.E. 1/4 BY TRANS-SYSTEMS CORP. CIRCA 2008 WHICH WAS USED FOR AN ALTA SURVEY IN 2008 BY THIS COMPANY

- LEGEND**
- = SET 1/2" BEHAR, 1/4" X 5/8" CAP IN CONCRETE, UNLESS NOTED
  - = FOUND BEHAR CAP NOTED
  - = FOUND BRASS DISK IN UNLESS NOTED
  - = FOUND BRASS DISK IN UNLESS NOTED
- LEGEND**
- BLDG = BUILDINGS
  - EXC = EXCAVATED
  - PERM = PERMANENT
  - F-PT = FOOT
  - ASPH = ASPHALT
  - CONC = CONCRETE
  - UTL = UTILITY
  - UTL/E = UTILITY EASEMENT



**LEGAL DESCRIPTION:**  
 ALL THAT PART OF THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER (NW 1/4, SE 1/4) OF SECTION 8, TOWNSHIP 12, RANGE 25, IN MISSION, JOHNSON COUNTY, KANSAS, DESCRIBED AS FOLLOWS:  
 BEGINNING AT A POINT ON THE NORTH LINE OF SAID 1/4, 1/4 SECTION THAT IS 5 97' 69" 14" W. OF THE POINT OF BEGINNING OF THE TRACT OF BEHARION OF THE SOUTH LINE OF JOHNSON DRIVE, AS ESTABLISHED, SAID POINT BEING THE TRUE POINT OF BEGINNING OF THIS TRACT, THENCE N 87° 59' 16" E ALONG SAID SOUTH LINE, 135.00'; THENCE S 28° 24' 56" W (DEED), 121.3' TO A POINT ON THE WEST LINE OF BEVERLY AVENUE AS ESTABLISHED, SAID POINT BEING 694.00' (686 DEED) WEST OF SAID WEST LINE OF SAID 1/4, 1/4 SECTION, THENCE S 2° 04' 52" E, PARALLEL TO SAID EAST LINE OF SAID 1/4, 1/4 SECTION, 170.00' FEET TO A POINT 167.23' (167.13' MORE OR LESS, DEED) EAST OF THE WEST LINE OF SAID 1/4, 1/4 SECTION, THENCE N 2° 04' 52" W, 315.00' TO A POINT ON THE SOUTH LINE OF SAID JOHNSON DRIVE, AS ESTABLISHED, 60.00' SOUTH OF THE NORTH LINE OF SAID 1/4, 1/4 SECTION, SAID POINT BEING ALSO 167.59' (167.13' DEED) EAST OF THE WEST LINE OF SAID 1/4, 1/4 SECTION, THENCE N 87° 59' 16" E ALONG SAID SOUTH LINE, 330.00' TO THE POINT OF BEGINNING.  
 THE ABOVE DESCRIBED TRACT CONTAINS 148,022 SQUARE FEET OR 3.3981 ACRES, MORE OR LESS.

**DEDICATIONS:**  
 The undersigned proprietor of the above described tract of land has caused the same to be subdivided in the manner shown on the accompanying plat, which hereafter shall be known as "DOWNTOWN MISSION".  
 The proprietors, successors, and assigns, of property described on this plat hereby dedicate for public use all land described on this plat as streets or easements, and does not constitute acceptance of any terms or conditions set forth in any agreement and shown on this plat.  
 In accordance with KSA 12-210b, all rights, obligations, reservations, assessments or interest and shown on this plat shall be treated as to use and as to title, severally to be indemnity the City of Mission, Kansas, of any expense incident to the relocation of any existing utility improvements herebefore installed and required to be relocated in accordance with proposed improvements described in this plat.  
 An easement is hereby granted to the City of Mission, Kansas, to enter upon, construct, maintain, use, and authorize the location of conduits for providing water, gas, cable, electric, sewers, and other utility services, including related facilities and drainage facilities, upon, over, under, and across those areas outlined and designated on this plat as "Utility Easement" or "U/E", and further, subject to administration and regulation by the City, the subordinate use of such areas by other governmental entities and utilities, furnished or authorized to do business in the City of Mission, Kansas.  
 The undersigned proprietor of the above described land hereby consents and agrees that the governing body of any special assessment district shall have the authority to assess and collect special assessments for street lighting, street cleaning, and other services therefrom from the land and effects of such special assessments and that the amount of unpaid special assessments on such land dedicated, shall become and remain a lien on the remainder of this land fronting or abutting on said dedicated road or street.

**NOTES:**  
 THE INFORMATION TAKEN FROM CHICAGO TITLE INSURANCE COMPANY TITLE NO. 20161580, RECORDED DATE OCTOBER 21, 2016 AT 6:00 AM (LOT 1) & TITLE NO. 20161581, RECORDED DATE OCTOBER 21, 2016 AT 6:00 AM (LOT 2) FILE NO. 20161580, EFFECTIVE DATE SEPTEMBER 18, 2016 AT 6:00 AM (LOT 1) & FILE NO. 20161581, EFFECTIVE DATE SEPTEMBER 18, 2016 AT 6:00 AM (LOT 2) THIS PROPERTY IS IN ZONE - X = AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN AS PER FINAL MAP 20091C02025, DATED AUGUST 3, 2009.  
 RIGHT-OF-WAY FOR JOHNSON DRIVE WAS ESTABLISHED BY SURVEY OF THE CITY OF MISSION, JOHNSON COUNTY, KANSAS, BY RAYMOND W. CAMPBELL ON FILE AT THE COUNTY ENGINEERS OFFICE, & AS PARTIALLY THERETBY BY OTHER DEEDS, PLANS, ETC.  
 ZONING = M2 - MAIN STREET DISTRICT 1, USAGE IS COMMERCIAL.  
 LIQUID WASTE IS HANDLED BY EXISTING SANITARY SEWERS.

**ORIGINAL DESCRIPTION OF RECORD - LOT 1:**  
 ALL THAT PART OF THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER (NW 1/4, SE 1/4) OF SECTION 8, TOWNSHIP 12, RANGE 25, IN MISSION, JOHNSON COUNTY, KANSAS, DESCRIBED AS FOLLOWS:  
 BEGINNING AT A POINT ON THE NORTH LINE OF SAID 1/4, 1/4 SECTION 665 FEET WEST OF THE NORTHEAST CORNER THEREOF, THENCE WEST ALONG SAID NORTH LINE 330 FEET, THENCE SOUTH PARALLEL TO THE EAST LINE OF SAID 1/4, 1/4 SECTION, 167.13 FEET, MORE OR LESS, TO THE WEST LINE OF SAID 1/4, 1/4 SECTION, THENCE SOUTH 75 FEET ALONG SAID WEST LINE, THENCE EAST PARALLEL TO SAID NORTH LINE 694.00 FEET, MORE OR LESS, TO A POINT 686 FEET NORTH OF SAID WEST LINE, 140 FEET, THENCE WEST PARALLEL TO SAID NORTH LINE 140 FEET, THENCE NORTH 210 FEET TO THE POINT OF BEGINNING, EXCEPT ANY PART OF SAID 210 FEET AND ON DEDICATED FOR ROADS OR PUBLIC RIGHT OF WAYS.  
 EXCEPT THAT PART IN JOHNSON DRIVE, AND EXCEPT THAT PART DESCRIBED AS FOLLOWS:  
 ALL THAT PART OF THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER (NW 1/4, SE 1/4) OF SECTION 8, TOWNSHIP 12, RANGE 25, IN MISSION, JOHNSON COUNTY, KANSAS, DESCRIBED AS FOLLOWS:  
 BEGINNING AT A POINT ON THE WEST LINE OF SAID QUARTER SECTION, 665 FEET WEST OF THE NORTHEAST CORNER THEREOF, THENCE EAST PARALLEL TO THE NORTH LINE OF SAID QUARTER SECTION 167.13 FEET, THENCE NORTH PARALLEL TO THE EAST LINE OF SAID QUARTER SECTION, 75 FEET, THENCE SOUTH PARALLEL TO THE WEST LINE OF SAID QUARTER SECTION, 167.13 FEET, MORE OR LESS, TO THE WEST LINE OF SAID QUARTER SECTION, THENCE SOUTH ALONG THE WEST LINE OF SAID QUARTER SECTION 75 FEET, MORE OR LESS, TO THE POINT OF BEGINNING.

**ORIGINAL DESCRIPTION OF RECORD - LOT 2:**  
 THE NORTH 210 FEET OF THE WEST 140 FEET OF THE EAST QUARTER OF SECTION 8, TOWNSHIP 12, RANGE 25, IN THE CITY OF MISSION, JOHNSON COUNTY, KANSAS, EXCEPT THAT PART IN HIGHWAY, STREET OR ROAD.

**NOTES:**  
 THE INFORMATION TAKEN FROM CHICAGO TITLE INSURANCE COMPANY TITLE NO. 20161580, RECORDED DATE OCTOBER 21, 2016 AT 6:00 AM (LOT 1) & TITLE NO. 20161581, RECORDED DATE OCTOBER 21, 2016 AT 6:00 AM (LOT 2) FILE NO. 20161580, EFFECTIVE DATE SEPTEMBER 18, 2016 AT 6:00 AM (LOT 1) & FILE NO. 20161581, EFFECTIVE DATE SEPTEMBER 18, 2016 AT 6:00 AM (LOT 2) THIS PROPERTY IS IN ZONE - X = AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN AS PER FINAL MAP 20091C02025, DATED AUGUST 3, 2009.  
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 ZONING = M2 - MAIN STREET DISTRICT 1, USAGE IS COMMERCIAL.  
 LIQUID WASTE IS HANDLED BY EXISTING SANITARY SEWERS.

**PREPARED BY:**  
**ROARK SURVEYING**  
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 LENEXA, KS. 66215  
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 RoarkSurveying@aol.com

**DOWNTOWN MISSION - PRELIM - PLAT**  
 11-11-16



**STAFF REPORT**  
**Planning Commission Meeting March 27, 2017**

**AGENDA ITEM NO.:** 3

**PROJECT # / TITLE:** Case # 16-10

**REQUEST:** Final Site Plan-Parking Lot Expansion

**LOCATION:** 6101 Johnson Drive (The Bar)  
Mission, Kansas 66202

**PROPERTY OWNER:** RH Johnson Company  
4520 Madison Ave, Ste 300  
Kansas City, MO

**APPLICANT:** Nick Ewing  
Sullivan Palmer Architects  
8621 Johnson Dr  
Merriam, KS

**ADVERTISEMENT:** NA

**PUBLIC HEARING:** NA

**STAFF CONTACT:** Danielle Sitzman



Property Information

The property is located on the southeast corner of Johnson Drive and Beverly Avenue. The property is currently zoned Main Street District 1 "MS-1". It is located in the Downtown District and subject to the Johnson Drive Design Guidelines. The Main Street District 1 "MS1" was assigned to this property at the time of the city initiated rezoning of entire downtown in 2006.



The District was designed to reinforce and encourage the existing character within the core of the downtown.

Surrounding properties are zoned as follows:

Properties surrounding the subject property are zoned “MS-1” Main Street District 1 and are used for retail, hair salon, animal care, restaurant, auto repair, and office (now vacant).

Comprehensive Plan Future Land Use Recommendation for this area:

The subject property is identified as appropriate for “Mixed Use Medium-Density” development and Parks and Pathways.

All surrounding properties are currently developed:

Surrounding properties are developed with a mix of attached and freestanding buildings for commercial uses. The property directly to the west contains an unoccupied office building.

**BACKGROUND INFORMATION:**

In 2014 the property changed ownership and underwent a renovation to convert it from a gas/service station to a bar and grill. A site plan review was conducted administratively by staff in August of 2014 as the proposed changes to the existing building were not significant in scope. Changes included the removal of the gas pump island canopy, conversion of one overhead door into storefront, and the addition of a small outdoor patio. All new materials used matched the existing ones. The applicant also resolved all staff comments regarding improvements to the site such as street trees, landscaping, equipment and trash screening. The surrounding sidewalk and streetscape had previously been reconstructed by the City as part of the Johnson Drive improvements. A variance was granted by the Board of Zoning Appeals to allow for the installation of a monument sign on the corner of the property in October 2014.

In 2016, the property underwent an expansion adding 531 square feet to the west side of the building in the place of a drive aisle around the building. An additional 265 square feet of outdoor patio area was also added adjacent to the north side of the addition. The project expanded the service areas of the building. The applicant had originally considered adding a second story to the building but did not pursue it. The proposed west side addition matched the architectural style of the existing building and was sided with matching materials. The roofline over the addition expanded the current line. A 10’x15’ overhead door faced the patio area which was enclosed with a wrought iron fence matching the existing fence.

**Plan Review**

The applicant is proposing to add approximately 4,300 square feet to the south side of the existing parking lot behind the building. This would accommodate approximately 18 additional parking stalls, reconfigure the dumpster, and replace the existing evergreen trees.

The Main Street District 1 zone does not require any off-street parking in the Downtown District (410.190.A). At the time of its creation, the City conducted a study of the existing traffic and parking conditions in the Downtown area to determine if the public on-street parking and off-street private parking was sufficient to support the mix of permitted uses. It was determined that it would be. In 2016 staff requested the applicant conduct a parking study to analyze the demand for parking generated by the subject property to confirm that this was still the case before adding onto the building. The study found that the amount of off-street parking provided



by The Bar was sufficient to contain most of their customers on site. Any spill over demand could be absorbed by the surrounding public parking on Johnson Drive which is less than half full. Most of the surrounding businesses are closed by 6:00 PM which coincides with The Bar's peak hours of operation. The City's on-call traffic engineer at GBA reviewed the study and agreed with the findings. The proposed expansion adds additional on-site parking thus reducing the demand for the on-street stalls from this use.

The additional parking field will add to the site's impervious surface area and generate additional storm water runoff. Stormwater runoff will be collected into an infiltration area at the southeast corner of the lot. The infiltration bed is designed to slow and capture water so that more of it can soak into the ground on or around the site rather than running off to the local stream system.

### **Design Guidelines Review**

City Code encourages reinvestment in existing buildings and structures, as well as compliance with the Design Guidelines. The Planning Commission has the authority to conduct Design Review of any proposed new construction. If appropriate, the applicant can be requested to make revisions to proposed plans, or additional stipulations can be added to any motion for approval. As Design Review is conducted, Staff encourages the Planning Commission to consider the intent of the Design Guidelines and evaluate whether this application meets the goals of the document.

### **Parking**

Section 3-2 of the Johnson Drive Corridor Design Guidelines addresses parking.

Parking quantity should not be the only issue considered in parking area design. Physical organization, distribution, pedestrian links, and screening are issues that must be addressed in parking lots and on-street parking.

- Parking lot islands and green space shall be incorporated within all parking lots to provide shade and visual relief from large expanses of pavement. Parking lots shall provide a minimum of 6% green space.
- Existing surface parking lots on side streets between Martway Street and Johnson Drive shall be effectively screened from the surrounding street network. Where a parking lot abuts a street, a minimum 9' wide buffer zone (including sidewalks) shall be required to effectively screen views to parked vehicles. Where a parking lot abuts an interior property line (not a street) a minimum of 4' shall be maintained as a green space. Screening shall be a minimum height of 3' and be of either softscape (vegetation) or hardscape (walls/fencing) or a combination of both.
- Parking lots in the Downtown District should not front on Johnson Drive, but be located behind the building with access from side streets.

**Staff Notes:** The applicant is proposing to extend the existing parking lot to the south without any other changes to the site access. Existing sidewalks and parking lot curbs were designed and replaced by the City as part of the Johnson Drive improvement project before the current owner purchased the property. Sufficient green space, buffer zones, and softscape screening are provided. Street trees and screening landscaping were installed at staff's request in 2014 when the building was renovated.

### **Screening & Landscaping**

Section 3-4 & 3-5 of the Johnson Drive Corridor Design Guidelines addresses screening and



landscaping.

Minimizing or eliminating the views to undesirable areas will improve the overall visual quality of the Johnson Drive Corridor. Appropriate areas to be screened include parking lots, delivery areas, loading docks, dumpsters, ground mounted mechanical equipment, utility service connections, ice and vending machines, freezers and coolers, and transformers.

- Screening can be accomplished by the incorporation of softscape (vegetation) or hardscape (walls).
- Plant materials are encouraged for screening uses.
- Shade along the Johnson Drive Corridor is a priority.
- Property owners are encouraged to provide landscaping on private property that is compatible with that on public property to ensure a consistent appearance along the corridor.

**Staff Notes:** The view of the south side of the building will be screened using Eastern Red Cedar trees to replace those which will be removed for the expansion of the parking lot. Yew shrubs will be installed to screen the parking stalls from Beverly Avenue.

**Code Review: Consideration of Final Site Plans (440.160)**

Final site plans shall be approved by the Planning Commission if it determines that:

1. The site is capable of accommodating the building(s), parking areas and drives with appropriate open space.

-The building, parking area, driveways, and open space have been designed to meet codes and guidelines.

2. The plan provides for safe and easy ingress, egress and internal traffic circulation.

-There is adequate space on the site to allow for circulation of customer traffic and service by trash haulers after the parking lot expansion is made with no impact to traffic on adjacent public streets.

3. The plan is consistent with good land planning and site engineering design principles.

-While the existing building is not in conformance with the Johnson Drive design guidelines for building placement, the proposed project does not make the site a greater nonconformity.

4. An appropriate degree of harmony will prevail between the architectural quality of the proposed building(s) and the surrounding neighborhood.

-The proposed project does not alter the building. Site changes are in compliance with the design guidelines for the downtown district.

5. The plan represents an overall development pattern that is consistent with the Comprehensive Plan and other adopted planning policies.

-While a freestanding building is not the intended development pattern for the downtown, the proposed on-site parking is appropriately designed.

6. Right-of-way for any abutting thoroughfare has been dedicated pursuant to the provisions of Chapter 455.



-Any required right-of-way changes for this site were addressed with the Johnson Drive improvement project. A plat has been prepared under Case #16-09. All of the applicable requirements of 455-Subdivisions have been satisfied.

**Staff Recommendation**

Staff recommends approval of the Final Site Plan for Case # 16-10 The Bar Parking Lot Expansion pending Planning Commission Design Review.



**STAFF REPORT**  
**Planning Commission Meeting March 27, 2017**

**AGENDA ITEM NO.:** 2

**PROJECT NUMBER / TITLE:** Case # 16-09

**REQUEST:** Preliminary & Final Plat of Downtown Mission, Lots 1-2

**LOCATION:** 6101 and 6201 Johnson Drive

**PROPERTY OWNER:**  
RH Johnson Company  
4520 Madison Ave, Ste 300  
Kansas City, MO

**APPLICANT:**  
Nick Ewing  
Sullivan Palmer Architects  
8621 Johnson Dr  
Merriam, KS

**STAFF CONTACT:** Danielle Sitzman

**ADVERTISEMENT:** February 21, 2017-The Legal Record

**PUBLIC HEARING:** March 27, 2017-Planning Commission





### **Property Information**

The subject property is developed with a restaurant and office building and is currently zoned "MS1" Main Street District 1.

#### Surrounding properties are zoned and used as follows:

West,North, East- "MS1" Main Street District 1 , Midsize free-standing retail, small row building retail/restaurant, Auto service, Office buildings, Public park,  
South-"MS2" Mission Community Center, Senior Independent Housing

#### Comprehensive Plan Future Land Use Recommendation for this area:

The subject property is identified as appropriate for "Mixed Use Medium-Density" development and Parks and Pathways.

#### All surrounding properties are currently developed:

Surrounding properties are developed with a mix of attached and freestanding buildings for commercial uses. The property directly to the west contains an unoccupied office building.

### **Project Background**

In 2014 the property changed ownership and underwent a renovation to convert it from a gas/service station to a bar and grill. A site plan review was conducted administratively by staff in August of 2014 as the proposed changes to the existing building were not significant in scope. Changes included the removal of the gas pump island canopy, conversion of one overhead door into storefront, and the addition of a small outdoor patio. All new materials used matched the existing ones. The applicant also resolved all staff comments regarding improvements to the site such as street trees, landscaping, equipment and trash screening. The surrounding sidewalk and streetscape had previously been reconstructed by the City as part of the Johnson Drive improvements. A variance was granted by the Board of Zoning Appeals to allow for the installation of a monument sign on the corner of the property in October 2014.

In 2016, the property underwent an expansion adding 531 square feet to the west side of the building in the place of a drive aisle around the building. An additional 265 square feet of outdoor patio area was also added adjacent to the north side of the addition. The project expanded the service areas of the building. The applicant had originally considered adding a second story to the building but did not pursue it. The proposed west side addition matched the architectural style of the existing building and was sided with matching materials. The roofline over the addition expanded the current line. An 10'x15' overhead door faced the patio area which was enclosed with a wrought iron fence matching the existing fence.

At this time the applicant is requesting approval of a two lot plat in order to divide the currently unplatted property into separate lots and transfer approximately 4,300 square feet of land from the office parcel to the bar parcel. The existing buildings will remain, however redevelopment of the property at 6201 Johnson Drive (office building) is anticipated.

The final plat will not include dedication of land for public purposes, therefore the City



Council is not required to review the preliminary plat. The decision of the Planning Commission to approve or deny the proposed plats will be final.

**Code Review: Consideration of Preliminary Plats (440.220)**

Preliminary plats shall be approved by the Planning Commission if it determines that:

1. The proposed preliminary plat conforms to the requirements of this Title, the applicable zoning district regulations and any other applicable provisions of this Code, subject only to acceptable rule exceptions.

-The proposed plats are in conformance. Existing buildings or site developments which may already be in nonconformance do not increase their non-conformity due to platting.

2. The subdivision or platting represents an overall development pattern that is consistent with the Master Plan and the Official Street Map.

-The plat represents a development pattern already established and supported by the Comprehensive Plan.

3. The plat contains a sound, well-conceived parcel and land subdivision layout which is consistent with good land planning and site engineering design principles.

-The plat supports good land planning and allows for future redevelopment in compliance with adopted standards.

4. The spacing and design of proposed curb cuts and intersection locations is consistent with good traffic engineering design and public safety considerations.

-The plat does not propose any changes to curb cuts or intersections.

5. All submission requirements have been satisfied.

-All of the requirements of 440.220-Submission of Preliminary Plats have been satisfied

**Code Review: Consideration of Final Plats (440.260)**

Final plats shall be approved by the Planning Commission if it determines that:

1. The final plat substantially conforms to the approved preliminary plat and rule exceptions granted thereto.

-A preliminary plat matching the final plat is under review with this application.

2. The plat conforms to all applicable requirements of this Code, subject only to approved rule exceptions.

-Code requirements are described below. The proposed plat is in conformance.

3. All submission requirements have been satisfied.

-All of the requirements of 440.250-Submission of Final Plats have been satisfied.



4. Approval of a final plat shall require the affirmative vote of a majority of the membership of the Planning Commission.

**Analysis:**

**Lots**

In the presented plat the applicant proposes to subdivide the subject property which is composed of two parcels into two lots. The subject property has never been platted before. The “MS1” District has no requirements for minimum lot sizes. The only yard requirements are for a front build-to line of zero. The existing buildings are 1-2 stories in height, below the maximum height of 3 stories or 45 feet.

- Lot 1: 122,669 sq ft or 2.8161 acres
- Lot 2: 25,352sq ft or .5820 acres

**Right-of-way**

No right-of-way is being dedicated at this time. The previous Johnson Drive street rehabilitation acquired land for public use at that time. Right-of-way needs will be reevaluated at the time of redevelopment of Lot 1.

**Easements**

No additional public easements are needed at this time.

**Staff Recommendation**

Staff recommends the Planning Commission approve the preliminary and final plat for Case # 16-09 the plat of land to be known as “Downtown Mission Lots 1-2”.